



CITY OF ARCATA

PLANNING COMMISSION SPECIAL MEETING

Council Chamber

June 27, 2023

736 F Street, Arcata

Tuesday, 5:30 p.m.

AGENDA

Special accommodations for the disabled who attend City meetings can be made in advance by contacting the City Clerk at 822-5953. Assistive listening devices are available.

Pursuant to Government Code section 54957.5, all writings or documents relating to any item on this agenda which have been provided to a majority of the Planning Commission, including those received less than 72 hours prior to the Planning Commission meeting, will be made available for public inspection in the agenda binder located on the counter in the lobby at Arcata City Hall, 736 F Street, during normal business hours, and on the City's website at www.cityofarcata.org.

PLEASE NOTE: Speakers wishing to distribute materials to the Planning Commission at the meeting are requested to provide 10 copies to the assigned project Planner.

How to Observe and Participate in the Meeting:

Observe:

Members of the public can attend the meeting in person or observe the meeting on Zoom (see below), on Access Humboldt Channel 10, online by visiting www.cityofarcata.org and clicking on the See Live Meetings, Agendas, and Archives button on the home page, or on the City's YouTube channel at <https://www.youtube.com/c/CityofArcataCA>

Public Comment Participation in Person and on Zoom:

Members of the public may attend the meeting in person and give public comment. They may also access the meeting via Zoom to provide public comment.

1. Join from a PC, Mac, iPad, iPhone or Android device: Please use this URL:
<https://us06web.zoom.us/j/96498107422>
2. If you want to comment during the public comment portion of any item, click on **raise your hand** on the right-hand side of your screen. When it is time for public comment on the item on which you wish to speak, the Clerk will unmute you. You will have 3 minutes to comment, subject to the Chair's discretion.

Or join by phone:

1. *67 1-669-900-6833
2. Enter Webinar ID: 964 9810 7422
3. If you are accessing the meeting via telephone and want to comment during the public comment portion of any item, press **star (*) 9** on your phone. This will raise your hand. When it is time for public comment on the item on which you wish to speak, the Clerk will

unmute your phone. You will hear a prompt that will indicate your phone is unmuted. You will have 3 minutes to comment, subject to the Chair’s discretion.

I. CALL TO ORDER.

A. LAND ACKNOWLEDGMENT

B. ROLL CALL

II. PUBLIC COMMENT.

This time is provided to address the Commission or submit written communications on matters that are on the Agenda. Unless otherwise required, there will not be an opportunity to speak on individual items. Each speaker may address the Commission on any or all of the Agenda items during their opportunity. The Commission encourages written communications, which have no limit on length.

III. BUSINESS ITEMS.

A. Approve Planning Commission Minutes - Special Meeting - June 13, 2023, 5:30 p.m.

B. Consider a Recommendation to the City Council on the General Plan Updates

The Gateway Area Plan (Plan) will be a new Element in the General Plan that addresses policy specifically for the approximate 138 acres in the plan area. The Gateway zoning ordinance, or Gateway Code, uses a Form-Based Code approach to growth and development in the Plan area. Form-Based Codes emphasize the design and massing of buildings, their interaction with the streetscape and deemphasize land uses. The draft Gateway Code implements the vision of the Gateway Area Plan. The Commission will consider the Gateway Code and make a recommendation to the City Council regarding its amendment and adoption.

RECOMMENDATION:

Staff recommends the Planning Commission review the Gateway Area Plan Form-Based Code zoning and provide direction to staff and a recommendation to the City Council. As time allows, the Commission should consider the General Plan topics held over from previous meetings in the “Bike Rack”. The Commission may also consider any other General Plan topic in preparation for the July 11, 2023, recommendation to Council on the General Plan, Gateway Area Plan, and Gateway Code.

IV. CORRESPONDENCE / COMMUNICATIONS.

V. ADJOURNMENT.



CITY OF ARCATA

PLANNING COMMISSION SPECIAL MEETING

Council Chamber
736 F Street, Arcata

June 13, 2023
Tuesday, 5:30 p.m.

MINUTES

I. CALL TO ORDER.

The meeting was called to order by Chair Scott Davies at 5:30 p.m.

A. Land Acknowledgment

B. Roll Call

PRESENT: Simmons, Yodowitz, Davies, Tangney

ABSENT: Lehman, Mayer

II. PUBLIC COMMENT.

12 members of the public spoke on items on the Agenda.

III. BUSINESS ITEMS.

A. Approve Planning Commission Minutes - Special Meeting - May 23, 2023, 5:30 p.m.

On a motion by Vice-Chair Tangney and second by Commissioner Simmons, the Minutes were unanimously approved.

B. Approve a Design Review Permit for Harder Remodel Addition at 40 E 7th Street; File No. 223-039-DR.

After receiving a staff report and public comment on the item, the CEQA Class 1 Exemption was adopted and then the Action was approved on motion by Vice-Chair Tangney and second by Commissioner Yodowitz, the Design Review Permit for Harder remodel addition at 40 E 7th St. was approved.

C. Consider a Recommendation to the City Council on the General Plan Updates

By consensus, the Commission decided to concentrate their efforts on the Gateway Code at each meeting through the July 11 meeting.

The Commission received a report then closed the item.

D. Consider Amending the Planning Commission Regular Meeting Schedule

On a motion by Commissioner Yodowitz and second by Commissioner Simmons, the Commission adopted Resolution No. PC-23-03 amending the Planning Commission regular meeting schedule beginning July 11, 2023.

IV. CORRESPONDENCE / COMMUNICATIONS.

6/13/2023 Planning Commission

Commissioner Simmons noted that design review seems unnecessary in some circumstances.

Community Development Director Loya stated that the March 14, 2023, Minutes would be corrected and presented at the July 11, 2023 meeting.

V. ADJOURNMENT.

The meeting was adjourned by order of the Chair at 8:11 p.m.

Minutes Acceptance: Minutes of Jun 13, 2023 5:30 PM (Business Items.)



STAFF REPORT

PLANNING COMMISSION MEETING

June 27, 2023

TO: Honorable Chair and Commissioners

FROM: David Loya, Director of Community Development

PREPARER: David Loya, Director of Community Development

DATE: June 21, 2023

TITLE: **Consider a Recommendation to the City Council on the General Plan Updates**

RECOMMENDATION:

Staff recommends the Planning Commission review the Gateway Area Plan Form-Based Code zoning and provide direction to staff and a recommendation to the City Council. As time allows, the Commission should consider the General Plan topics held over from previous meetings in the “Bike Rack”. The Commission may also consider any other General Plan topic in preparation for the July 11, 2023, recommendation to Council on the General Plan, Gateway Area Plan, and Gateway Code.

INTRODUCTION:

The Gateway Area Plan (Plan) will be a new Element in the General Plan that addresses policy specifically for the approximate 138 acres in the plan area. The Gateway zoning ordinance, or Gateway Code, uses a Form-Based Code approach to growth and development in the Plan area. Form-Based Codes emphasize the design and massing of buildings, their interaction with the streetscape and deemphasize land uses. The draft Gateway Code implements the vision of the Gateway Area Plan. The Commission will consider the Gateway Code and make a recommendation to the City Council regarding its amendment and adoption.

Gateway Code

The Gateway Code (Attachment A) provides the zoning standards and permitting requirements for development in the Gateway Area (see Figure 1, Gateway Area Plan at <https://www.cityofarcata.org/965/Arcata-Gateway-Area-Plan>). Importantly, the Gateway Code provides a ministerial permit process for projects that: provide moderate to high density housing, comply with the objective standards, and provide community benefits.

The Commission received a staff report on the Gateway Code on June 13. Commissioners provided suggested edits to the Gateway Code (Attachment B). This meeting will focus primarily on reviewing the Code and providing suggestions for edits.

Areas that have been of interest to the public include: inclusionary zoning, building height and density, step backs and setbacks, parking requirements and other mobility standards, solar shading, and revisiting the L/K couplet. Comments received on the Code are at

<https://www.cityofarcata.org/940/Engagement-Information> under the “Public Comments” tab for the June 10-16, 2023, and June 17-23, 2023, comments received.

The Commission may use the Framework to discuss changes to the Gateway Code (Attachment B).

General Plan

The Planning Commission has previously reviewed most Elements (see staff report from June 13). This report provides a synopsis of all the Elements that are being updated (Table 1 and Attachments C-H). The recommendations of Committees and the Commission were incorporated with few exceptions (Attachment I). The Commission should discuss which Elements they plan to make recommendations on July 11 and which they need more time to review based on the review history at this meeting (Table 2). Staff will compile each Commissioner’s position in the meeting to determine the Elements that will be part of the Commission’s July 11 recommendation. Elements that are not part of the July 11 recommendation will be carried over to July 25 and August 8 meeting. The Commission’s first Joint Study Session with the City Council to discuss the recommendation will be August 22.

Table 1. General Plan Element Review.

Chapter	Element	Attachment
1	Vision, Introduction	C
2	Land Use, Growth Management, Mobility & Circulation, Public Facilities & Infrastructure, and Gateway Area Plan	D
3	Housing (not updated here), Parks and Recreation	E
4	Open Space, Resource Conservation, Air Quality	F
5	Design, Historic Preservation	G
6	Public Safety, Noise, Health	H

Table 2. Summary of General Plan Element Review. This table accounts for the variation in review since May of 2022 in how General Plan Elements were reviewed. The three variables are: whether the PC March 27, 2023, Framework was used to make a recommendation; which Committees made recommendations on each Element; whether the Committee review occurred before or after the Planning Commission review; and which recommendations are included in the current working draft of each Element. Elements that show a body review but lack that body’s recommendation indicates that the body did not make a formal recommendation. The bodies are the Wetland and Creeks Committee (WCC), the Forest Management Committee (FMC), the Parks and Recreation Committee (PRC), the Transportation Safety Committee (TSC), the Historic Landmarks Committee (HLC), the Energy Committee (EC), and the Economic Development Committee (EDC). Dashes indicate that the data are not applicable to that Element (e.g., there was no Committee review).

General Plan Element	PC Framework Used?	Reviewing Committee	Recommend Timing	Recommendation Incorporated	Commissioner requests add'l review?
Growth Management	No	-	-	-	

General Plan Element	PC Framework Used?	Reviewing Committee	Recommend Timing	Recommendation Incorporated	Commissioner requests add'l review?
Open Space	No	WCC, FMC	After PC	WCC	
Resource Conservation	No	WCC, FMC	After PC	WCC and FMC	
Parks & Recreation	No	PRC	Before PC	PRC	
Land Use	Yes	-	-	PC	
Mobility and Alternative Transportation	Yes	TSC	After PC	PC and TSC*	
Public Facilities & Infrastructure	Yes	-	-	PC	
Public Safety	Yes	-	-	PC	
Historic Preservation	Yes	HLC	Before PC	HLC and PC	
Design	Yes	-	-	PC	
Health	Yes	-	-	PC	
Gateway	No	WCC, EC, EDC, FMC, HLC, PRC, and TSC	Before PC	WCC, EC, EDC, FMC, HLC, PRC, and TSC	
Air Quality	-	-	-	-	
Noise	-	-	-	-	

*All TSC policy recommendations are included in the except for the recommendation for the K/L street couplet, on which the Commission has already straw poll voted.

Bike Rack

As time permits, the Commission may decide to return to the bike rack items (Attachment B). The Commission should also use the Framework to add to the Discussion Guide/Bike Rack as necessary for any General Plan Elements that were reviewed prior to the Framework adoption in March of 2023. In particular, Commissioners provided comments on some of the early release Elements that did not receive in depth discussion. If the Commission wishes to return to those Elements, they should do so using the Framework.

Outstanding Recommendations – Almost all recommendations made by Committees and the Commission have been integrated into the draft General Plan documents. The changes have been integrated in line in the documents. The Gateway Area Plan was color coded to indicate the source of the recommended change. The revised drafts are located on the City’s website at <https://www.cityofarcata.org/974/General-Plan-Updates>. Changes that were not integrated into the Gateway Area Plan were tracked separately (Attachment I). Staff discussed which proposed changes would be added to the “Other Considerations” list with the recommending bodies at the time the

recommendations were made. The last Column in the Other Considerations table is the Planning Commission recommendation. Staff has populated the responses based on earlier decisions the Commission has made (e.g., retaining the K/L couplet and the L Street linear park as a policy in the plan). The Commission should use the Framework to confirm these decisions and to make recommendations for the open items by its July 11, 2023, meeting.

ATTACHMENTS:

- A. Gateway FBC 6.5.23 (PDF)
- B. 2023-06-27 Discussion Guide (DOCX)
- C. Ch 1 Community Vision (PDF)
- D. Ch 2 Community Development (PDF)
- E. Ch 3 Housing and Human Services (PDF)
- F. Ch 4 Environment (PDF)
- G. Ch 5 Design Historic (PDF)
- H. Ch 6 Health Safety (PDF)
- I. Other Considerations Gateway and GP Draft 6-22-23 (PDF)

Chapter 9.29 – Gateway Area Districts

Sections:

- 9.29.010 – Introduction
- 9.29.020 – Permits and Approvals
- 9.29.030 – Allowed Uses
- 9.29.040 – District Standards
- 9.29.050 – Supplemental to Districts
- 9.29.060 – Building Design Standards
- 9.29.070 – Streetscape
- 9.29.080 – Mobility
- 9.29.090 – Open Space
- 9.29.100 – Community Benefits

9.29.010 – Introduction

- A. Purpose.** This chapter establishes standards and requirements to implement the Gateway Area Plan.
- B. Gateway Districts.**
 - 1. The Gateway Area is divided into four form and design districts: Gateway Barrel District (G-B), Gateway Hub (G-H), Gateway Corridor (G-C), and Gateway Neighborhood (G-N). These four districts are collectively referred to in this chapter as the Gateway districts.
 - 2. The boundaries of the Gateway districts are shown in the City’s Zoning Map established by Sec. 9.12.020.
- C. Land Use Code Compliance.** Development in the Gateway districts must comply with all applicable provisions of this Land Use Code unless otherwise specified in this chapter. In case of conflict between this chapter and other provisions in this Land Use Code, this chapter governs.
- D. Relationship to Gateway Area Plan.** This chapter and the Gateway districts implement the Gateway Area Plan, an Element of the City’s General Plan. In case of conflict between this chapter and the Gateway Area Plan, the Gateway Area Plan governs.
- E. Applicability of Standards.**
 - 1. This chapter applies to all properties in the Gateway districts.
 - 2. Development must conform with the standards in this chapter unless deviation is

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

approved in accordance with Sections 9.29.020.C (Design Review) and 9.29.020.D (Gateway Use Permit).

- 3. This chapter is intended to allow for continued use and improvement of residential uses existing in the Gateway districts at the time of code adoption. Such uses are exempt from the requirements of Chapter 9.60 (Nonconforming Uses, Structures, and Parcels).

9.29.020 – Permits and Approvals

A. Purpose. This section establishes permit and approval requirements for proposed projects in the Gateway districts.

B. Gateway Ministerial Permit.

- 1. **Purpose.** The Gateway Ministerial Permit allows by-right approval of proposed projects consistent with this chapter. The Gateway Ministerial Permit is intended to facilitate the production of housing that provides public benefits consistent with the Gateway Area Plan.
- 2. **By-Right Approval Defined.** As used in this chapter, by-right approval means a City decision on a proposed development project where project conformance with objective standards is the sole basis to approve or deny the application. The review authority may not consider project conformance with subjective policies or requirements when acting on the application.
- 3. **Eligibility.** To be eligible for a Gateway Ministerial Permit, a proposed project must satisfy all of the following requirements:
 - a. The project must provide housing, either as a standalone residential or mixed-use project. For mixed-use projects, residential uses must either:
 - 1. Occupy at least two-thirds of the total floor area of the project; or
 - 2. Be built to a density of at least 25 units per acre.
 - b. The project must provide community benefits in compliance with Section 9.29.100 (Community Benefits).
 - c. The project must comply with all standards and requirements in this chapter. A project requesting a deviation from one or more standard in this chapter is not eligible for a Gateway Ministerial Permit.
 - d. The project must comply with all applicable local, state, and federal natural resource protection standards. See Section 9.29.020.B.4.c (Environmental Review).
- 4. **Application Process.**
 - a. **General.** A Gateway Ministerial Permit application shall be submitted and

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

reviewed in conformance with Chapter 9.70 (Permit Application Filing and Processing).

- b. **Review Authority.** Table 2-19 shows review authority for projects eligible for Gateway Ministerial Approval, based on project size.

Table 2-19: Gateway Ministerial Permit Requirements

Project Size	Review Authority	Public Notice	Administrative Hearing
New floor area less than 30,000 sq. ft and/or building height less than 37 ft.	Zoning Administrator	Notice of Administrative Decision	No
New floor area 30,000 to 40,000 sq. ft and/or building height 37 to 47 ft.	Zoning Administrator	Notice of Administrative Hearing	Yes
New floor area over 40,000 sq. ft and/or building height over 40 ft.	Planning Commission	Notice of Administrative Hearing	Yes

- c. **Environmental Review.**

1. City staff shall review the proposed project to determine compliance with all applicable local, state, and federal natural resource protection standards, including standards in:
 - (a) Chapter 9.53 (Historic Resource Preservation);
 - (b) Chapter 9.54 (Resource Conservation);
 - (c) Chapter 9.56 (Solar Siting and Solar Access): and
 - (d) Chapter 9.59 (Environmentally Sensitive Habitat Areas Protection and Preservation).
2. If the project does not comply with any applicable natural resource protection standard, it is not eligible for a Gateway Ministerial Permit.

- d. **Public Notice.**

1. Public notice shall be given for projects as shown in Table 2-19.
2. Notice of administrative decision, when required, shall contain the following information:
 - (a) The date of filing of the application and the name of the applicant.
 - (b) The City’s file number assigned to the application.
 - (c) A general description of the proposed project, including the project location.
 - (d) The date the Zoning Administrator will render a decision on the application, which shall be not less than 10 days from the date of

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notice.

- (e) The location and times in which the application may be reviewed by the public.
- (f) The procedure and time period in which the decision may be appealed.

- 3. Notice of administrative decision shall be distributed in the same manner as provided in Section 9.74.020.B.2.a and through electronic listserv notification.
- 4. Notice of administrative hearing, when required, shall be given consistent with Section 9.74.020 (Notice of Hearing).

e. Administrative Hearing.

- 1. When required by Table 2-19, an administrative hearing shall be held at the date, time, and place for which notice was given.
- 2. After receiving comment and considering the proposed project, the review authority must either approve the application, deny the application, or continue the hearing to a future date.
- 3. The hearing may be continued only if additional information is needed to determine project conformance with objective standards. A hearing may only be continued a maximum of three times after which the review authority must render a decision.

f. Decision. The review authority shall approve a Gateway Ministerial Permit if the project:

- 1. Satisfies all Gateway Ministerial Permit eligibility criteria in Section 9.29.020.B.3 (Eligibility);
- 2. Complies with all applicable objective standards in the Gateway Area Plan, General Plan, and Municipal Code. See Section 9.29.010 (Introduction) for instructions to resolve conflicting standards; and
- 3. Complies with all applicable local, state, and federal natural resource protection standards, including standards in
 - (a) Chapter 9.53 (Historic Resource Preservation);
 - (b) Chapter 9.54 (Resource Conservation);
 - (c) Chapter 9.56 (Solar Siting and Solar Access); and
- 4. Chapter 9.59 (Environmentally Sensitive Habitat Areas Protection and Preservation).

5. Appeals.

- a. A decision to approve or deny a Gateway Ministerial Permit may be appealed.

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- b. Zoning Administrator decisions may be appealed to the Planning Commission and Planning Commissions decisions may be appealed to the City Council.
- c. Appeals shall be filed and processed consistent with Section 9.76.030 (Filing and Processing of Appeals) except as follows:
 - 1. Any person may file an appeal.
 - 2. The subject of appeal is limited to the determination of project conformance with the approval criteria in Paragraph 9.29.020.B.4.f (Decision).
 - 3. The review authority may affirm or reverse the decision based solely on project conformance with the approval criteria in Paragraph 9.29.020.B.4.f (Decision). The review authority may not consider project conformance with subjective policies or requirements when acting on the appeal.

C. Design Review.

- 1. Design review is required in the Gateway districts if both of the following apply:
 - a. The proposed project does not meet the Gateway Ministerial Permit eligibility requirements in Section 9.29.020.B.3 (Eligibility); and
 - b. The project requires Design Review pursuant to Section 9.72.040 (Design Review).
- 2. Design review shall be conducted in accordance with Section 9.72.040 (Design Review).
- 3. To approve Design Review for a project that deviates from one or more standard in Section 9.29.060 (Building Design Standards), the review authority must find that either:
 - a. The standard is not applicable due to the nature of the project (e.g., modification to an existing structure); or
 - b. The project incorporates an alternative method to achieve the intent statement the proceeds the standard.

D. Gateway Use Permit.

- 1. A Gateway Use Permit is required in the Gateway districts for uses listed in Section 9.29.030.B (Gateway Use Permit Required) or if both of the following apply:
 - a. The proposed project does not meet the Gateway Ministerial Permit eligibility requirements in Section 9.29.020.B.3 (Eligibility).
 - b. The proposed project is listed in Table 2-20.

Table 2-20: Gateway Use Permit Requirements

Project Size	Review Authority
Existing Uses	
Expanding an existing commercial or industrial use	
2,500 square feet or less of new floor area	Zoning Administrator
More than 2,500 square feet of new floor area	Planning Commission
New Uses	
New residential uses less than 25 units per acre	
New commercial or industrial use	
2,500 square feet or less of new floor area	Zoning Administrator
More than 2,500 square feet of new floor area	Planning Commission

2. The review authority for Gateway Use Permits is shown in Table 2-20.
3. Procedures for a Gateway Use Permit are the same as in 9.72.080 (Use Permit and Minor Use Permit) except as specified in Paragraph (d) below.
4. The review authority may approve a Gateway Use Permit only after finding all of the following:
 - a. The design, location, size, and operating characteristics of the proposed activity will not impede or interfere with redevelopment of surrounding properties as envisioned in the Gateway Area Plan.
 - b. The proposed project is either minor in nature or incorporates features that contribute to the vibrancy of the Gateway Area. For the purpose of this finding, “contribute to the vibrancy of the Gateway Area” means the project will accommodate an estimated 100 new jobs or more, provide at least 25,000 square feet of resident-serving commercial uses, or include a hotel and other visitor-serving uses that promote local tourism.
 - c. The site is physically suitable for the type, density and intensity of use being proposed, including access, utilities, and the absence of physical constraints; and
 - d. Granting the permit will not be detrimental to the public interest, health, safety, convenience, or welfare, or materially injurious to persons, property, or improvements in the vicinity and zoning district in which the property is located.
5. A Gateway Use Permit required to expand a non-conforming use may be approved only if:
 - a. The expansion occurs on the same parcel of the existing non-conforming use;
 - b. The use is not heavy or moderate-impact manufacturing/processing; and

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- c. The review authority finds that sufficient measures are incorporated into the design to reduce off-site impacts associated with aesthetics, odor, and noise.

9.29.030 – Allowed Uses

- A. Permitted Uses.** Except as provided in Subsections B and C below, the following land uses are Permitted Uses:
1. Residential uses, including multi-family housing, rooming or boarding houses, residential care facilities, and existing single-family dwellings.
 2. Commercial uses, including retail, personal services, restaurants and bars, professional offices, medical services, and lodging.
 3. Recreation, education, and public assembly uses, including parks, playgrounds, schools, meeting facilities, studios, and theatres.
 4. Light industrial uses, including laboratories, research and development, and manufacturing/processing - low-impact.
 5. Other similar and compatible uses. See Section 9.29.030.D (Similar and Compatible Uses).
- B. Gateway Use Permit Required.** The following land uses require a Use Gateway Permit in the Gateway districts:
1. Retail with 20,000 square feet or more of floor area.
 2. Utility facilities.
 3. Moderate-impact industrial uses.
 4. Expansion of an existing vehicle services use.
 5. Other similar and compatible uses. See Section 9.29.030.D (Similar and Compatible Uses).
- C. Prohibited Uses.** The following land uses are prohibited in the Gateway districts:
1. New single-family dwellings.
 2. Heavy industrial uses, including high-impact manufacturing/processing.
 3. Construction yards.
 4. Service stations.
 5. Warehouses.
 6. Personal storage facility (mini-storage).
 7. Fuel dealers.
 8. Auto and vehicle sales and rental.

- 9. New vehicle services
- 10. Commercial agricultural production, excluding community gardens.
- 11. Other similar and compatible uses. See Section 9.29.030.D (Similar and Compatible Uses).

D. Similar and Compatible Uses. The Zoning Administrator may determine that a use is similar or compatible to an allowable use in the Gateway Districts upon finding that:

- 1. The characteristics of, and activities associated with the use are similar to one or more of the listed uses, and will not involve a greater intensity than the uses listed in the district;
- 2. The use will be consistent with the purposes of the district;
- 3. The use will be consistent with the General Plan and the Gateway Plan; and
- 4. The use will be compatible with the other uses allowed in the district.

9.29.040 – District Standards

A. Gateway Barrel District (G-B).

- 1. **Purpose.** The G-B district is a high-density walkable residential campus with internal circulation based primarily on bicycle and pedestrian modes of travel. Building and site design supports a pedestrian-friendly public realm compatible with the internal high-density residential character of the district.
- 2. **Building Placement.**
 - a. Table 2-21 and Figure 2-26 show building placement standards in the G-B district.

Table 2-21: G-B District Building Placement

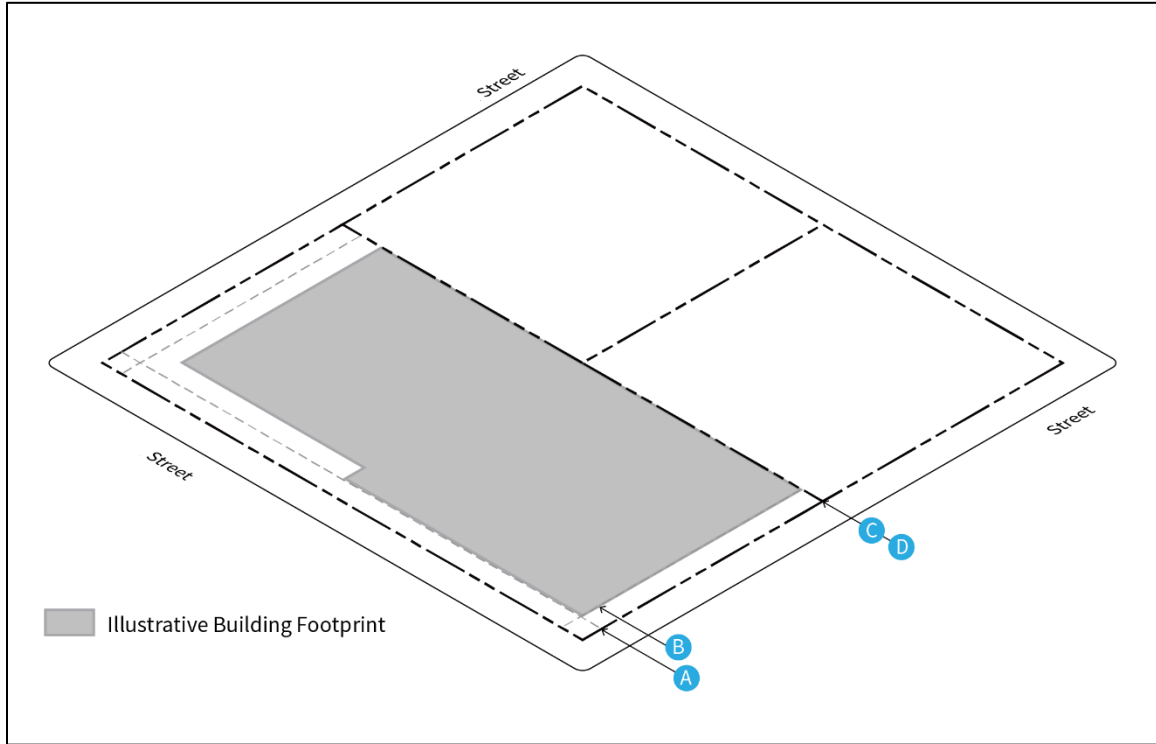
Setbacks		Building Frontage Type	
		Active	Non-Active
From property lines abutting a street			
A	Minimum	10 ft.	20 ft.
B	Maximum	20 ft. [1]	No maximum
From all other property lines			
C	Minimum	No setback requirement	
C	Maximum	No setback requirement	

Notes:

[1] A building may be setback up to 50 feet from the property line if the space between the building wall and sidewalk is occupied by a courtyard, plaza, or other form of publicly accessible open space.

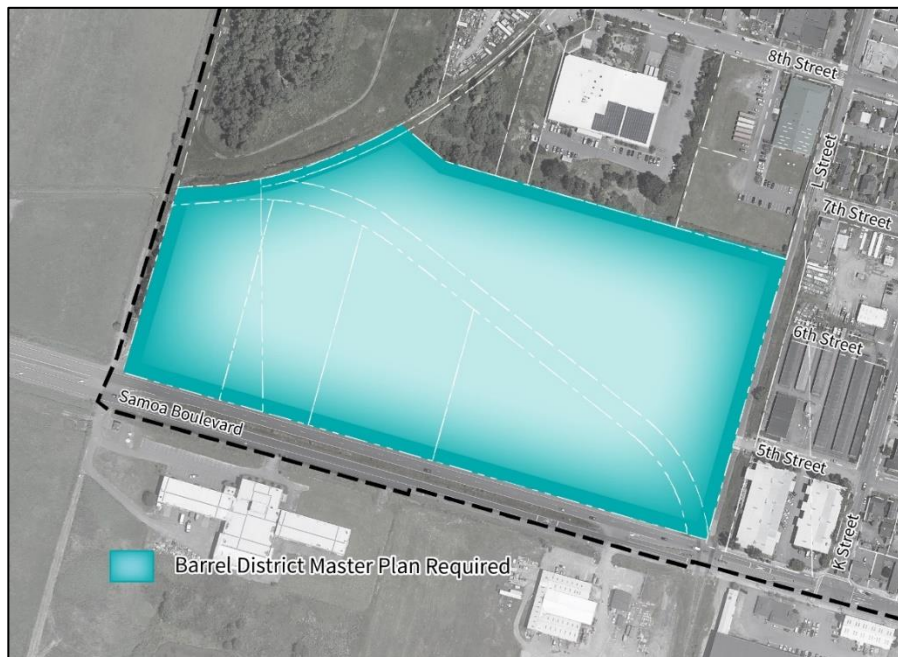
Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

Figure 2-26: G-B District Building Placement



- b. Within the area shown in Figure 2-27, building placement standards are as shown in Table 2-21 unless amended by the City-approved Barrel District Master Plan. See Paragraph 4 (Barrel District Master Plan) below.

Figure 2-27: Barrel District Master Plan Area



Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

- Building Massing.** Table 2-22 and Figure 2-28 show building massing standards in the G-B district. Building massing standards may be modified in the Barrel District Master Plan. The Barrel District Master Plan may allow building heights consistent with Table 2-22.

Table 2-22: G-B District Building Massing

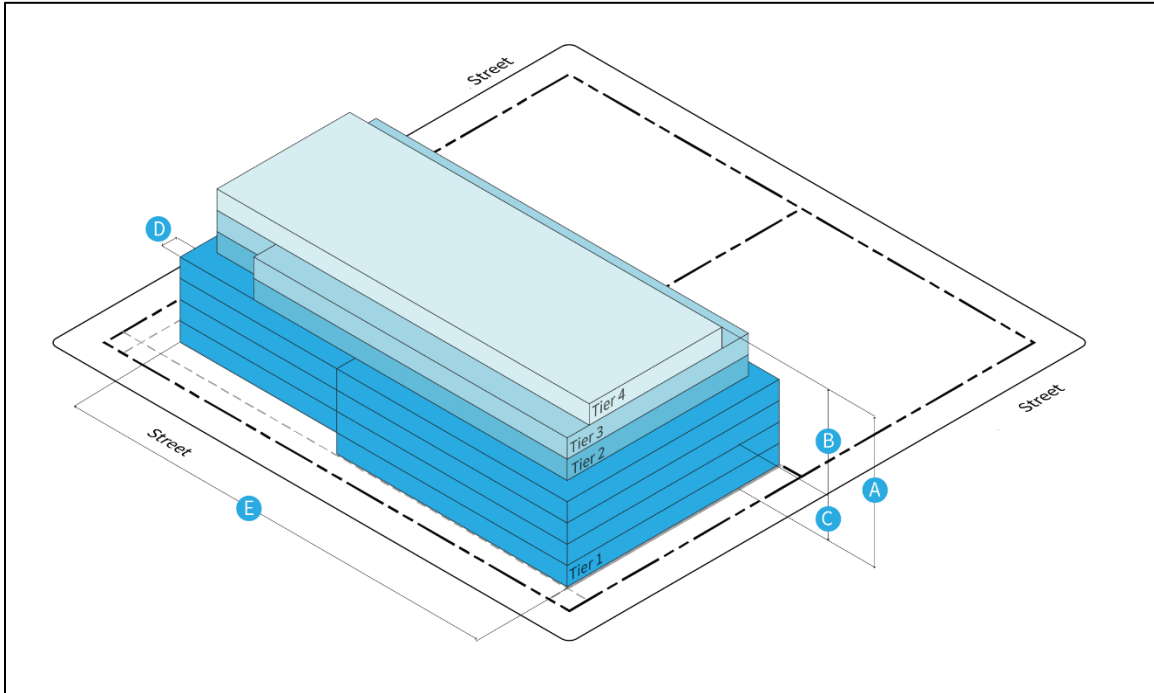
	Base Tier	Tier 1	Tier 2	Tier 3	Tier 4	
Height						
A	Feet, Max.	50 ft.	50 ft.	60 ft.	70 ft.	80 ft.
B	Stories, Max.	4	4	5	6	7
C	Stories, Min. [1]	2	2	3	4	5
Massing						
Height ratios (% of ground floor by story)						
	1-4	100%	100%	100%	100%	100%
	5-6	N/A	N/A	80%	80%	80%
	7	N/A	N/A	N/A	N/A	60%
Upper Story Step Backs (from ground-level street-facing building walls)						
	4 th story and below	None, except when required by 9.29.050.B (Enhanced Upper Story Step Backs)				
D	5 th to 7 th story	8 ft. min. for 75% or more of building street frontage [2]				
E	Max. Building Length [3]	300 ft. [4]				
	Building Modulation	See 9.26.060.B (Building Modulation)				

Notes:

- [1] Minimum height applies only to residential uses.
- [2] For buildings walls with less than 120 feet of street frontage, an 8 ft. step back is required for all but 30 feet of the frontage.
- [3] Measured parallel to the adjacent street.
- [4] See long building division requirement for buildings 150-300 feet in length.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

Figure 2-28: G-B District Building Massing



4. **Barrell District Master Plan.**

a. **General.**

- (1) A Master Plan is required for development with over 10,000 square feet of floor area in the area shown in Figure 2-27.
- (2) The Master Plan must accommodate a high-density walkable mixed-use residential campus with minimal vehicular infrastructure and overall site design that supports a pedestrian-friendly public realm.

b. **Master Plan Contents.** The Barrell District Master Plan must contain maps, graphics, and text that identifies the following:

- (1) Natural resource protection areas to remain as permanent open space.
- (2) Publicly accessible open space, including a new community square.
- (3) The location and design of new streets, greenways, and emergency vehicle access facilities.
- (4) The placement of buildings and surface parking.
- (5) Planned land uses, if known.
- (6) Building heights.
- (7) Requested modifications to building design standards in Section 9.29.060 (Building Design Standards), if any.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

c. **Circulation.**

- (1) The Master Plan must note deviations from the conceptual greenway configuration in Figure 2-56 and explain the need for these deviations.
- (2) The City may approve a Master Plan circulation system that deviates from Gateway Area Plan Figure 8 and Figure 9 upon finding that the deviation allows for superior circulation consistent with Gateway Area Plan goals.

d. **Open Space.**

- (1) The Master Plan must identify the location of a community square to serve as a neighborhood gathering space and visitor destination.
- (2) The Master Plan must note deviations from the community square standards in Section 9.29.090.B (Community Square) and explain how the deviation will provide for a superior design.

e. **Review and Approval Process.** The Master Plan will be reviewed and approved at noticed public hearings in the same manner as required for amendments to this chapter and the Gateway Area Plan if deviations from the standards are proposed.

B. Gateway Hub (G-H).

- 1. **Purpose.** The G-H district is a vibrant, high-intensity mixed-use area anchored by the Creamery building. New buildings and open spaces are located and designed to create a welcoming, vibrant, and pedestrian-friendly public realm.
- 2. **Building Placement.** Table 2-23 and Figure 2-29 show building placement standards in the G-H district.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

Table 2-23: G-H District Building Placement

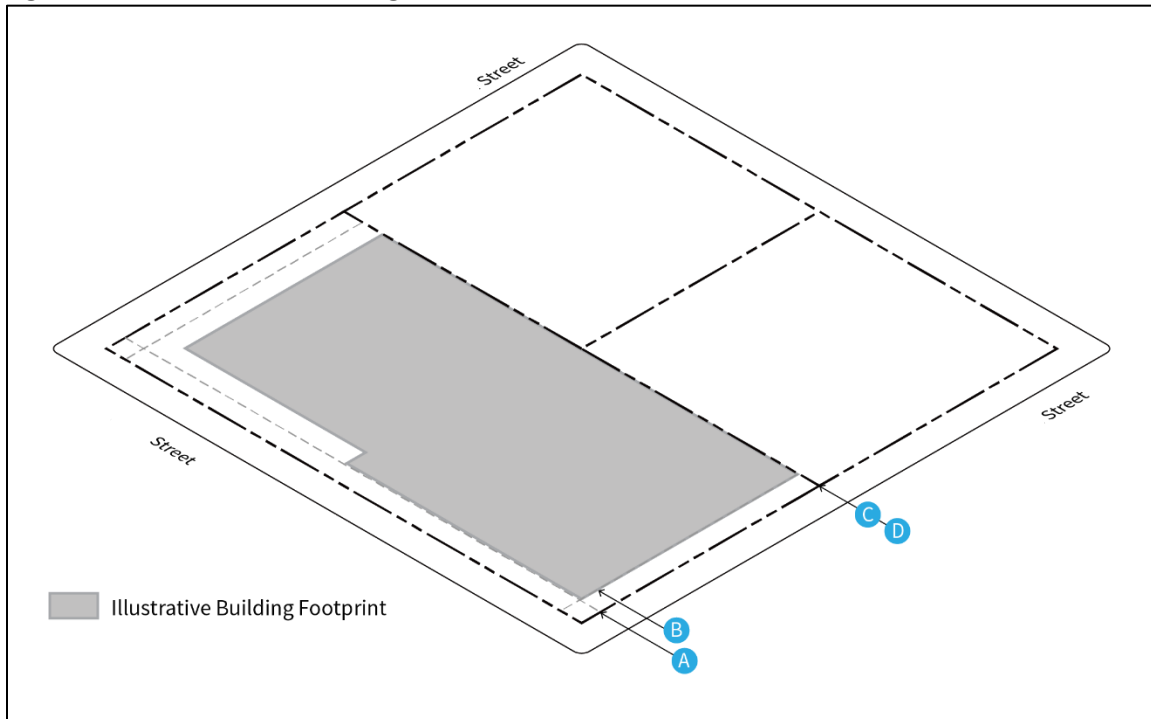
Setbacks		Building Frontage Type	
		Active [1]	Non-Active
From property lines abutting 8 th , 9 th and L Streets			
A	Minimum	15 ft.	N/A
B	Maximum	25 ft. [2]	N/A
From property lines abutting all other streets			
A	Minimum	10 ft.	20 ft.
B	Maximum	20 ft. [2]	No maximum
From all other property lines			
C	Minimum	No setback requirement	
C	Maximum	No setback requirement	

Notes:

[1] See Section 9.29.050.A (Active Frontage Types) for locations that require an active building frontage type.

[2] A building may be setback up to 50 feet from the property line if the space between the building wall and sidewalk is occupied by a courtyard, plaza, or other form of publicly accessible open space.

Figure 2-29: G-H District Building Placement



3. **Building Massing.** Table 2-24 and Figure 2-30 show building massing standards in the G-H district.

TABLE 2-24: G-H DISTRICT BUILDING MASSING

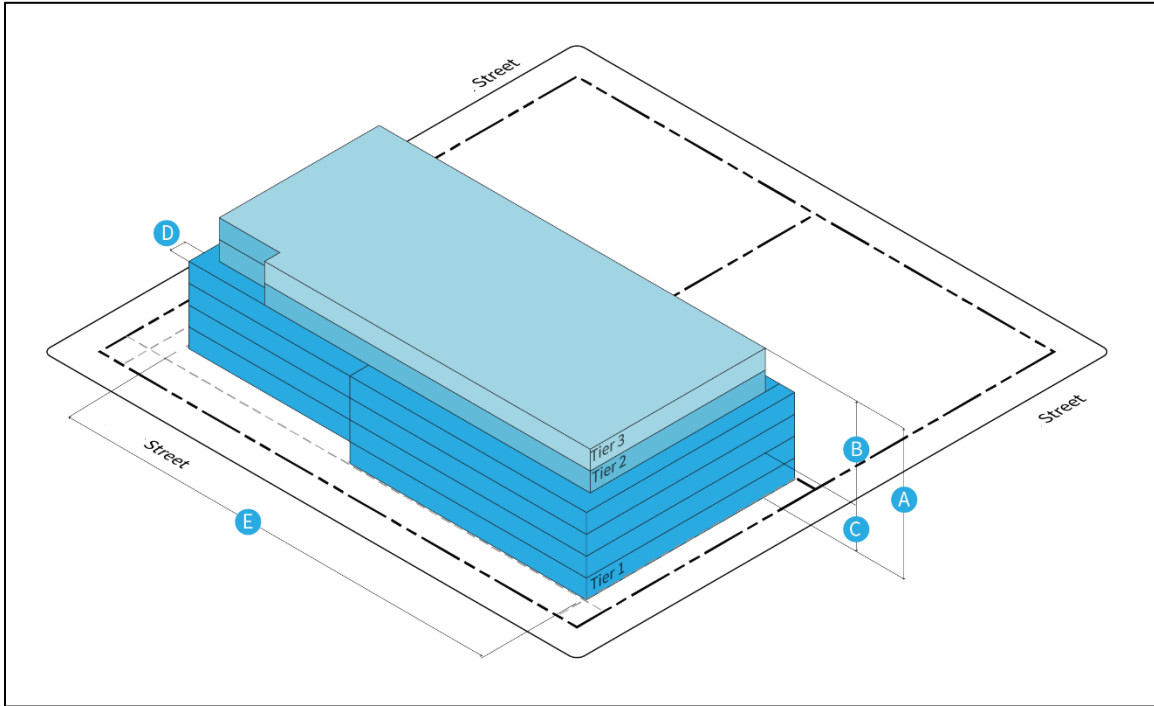
		Base Tier	Tier 1	Tier 2	Tier 3
Height					
A	Feet, Max.	50 ft.	50 ft.	60 ft.	70 ft.
B	Stories, Max.	4	4	5	6
C	Stories, Min. [1]	2	2	3	4
Massing					
	Height ratios (% of ground floor by story)				
	1-4	100%	100%	100%	100%
	5-6	N/A	N/A	80%	80%
	Upper Story Step Backs (from ground-level street-facing building walls)				
	4 th story and below	None, except when required by 9.29.050.B (Enhanced Upper Story Step Backs)			
D	5 th and 6 th story	8 ft. min. for 75% or more of building street frontage [2]			
E	Max. Building Length [3]	300 ft. [4]			
	Building Modulation	See 9.26.060.B (Building Modulation)			

Notes:

- [1] [1] Minimum height applies only to residential uses.
- [2] For buildings walls with less than 120 feet of street frontage, an 8 ft. step back is required for all but 30 feet of the frontage.
- [3] Measured parallel to the adjacent street.
- [4] See long building division requirement for buildings 150-300 feet in length.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

Figure 2-25: G-H District Building Massing



C. Gateway Corridor (G-C).

1. **Purpose.** The G-C district accommodates active, inviting, high-intensity, mixed-use development along major bicycle and vehicular gateways into the City. Pedestrian-oriented ground floor design aesthetic helps to slow passing vehicular traffic and encourage drivers to park and walk.
2. **Building Placement.** Table 2-25 and Figure 2-31 show building placement standards in the G-C district.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

Table 2-25: G-C District Building Placement

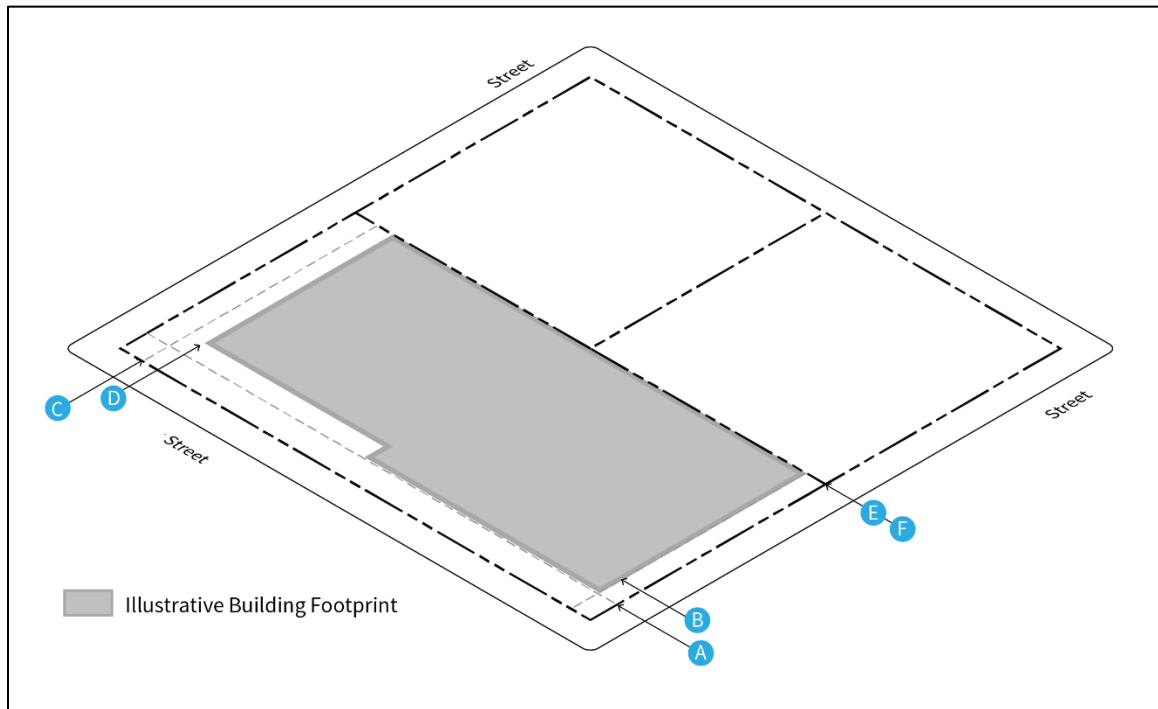
Setbacks	Building Frontage Type	
	Active [1]	Non-Active
From property lines abutting 8 th , 9 th , and L Street between 8 th Street and 9 th Street		
A Minimum	15 ft.	N/A
B Maximum	25 ft. [2]	N/A
From property lines abutting all other streets		
A Minimum	10 ft.	20 ft.
B Maximum	20 ft. [2]	No maximum
From all other property lines		
C Minimum	No setback requirement	
C Maximum	No setback requirement	

Notes:

[1] See Section 9.29.050.A (Active Frontage Types) for locations that require an active building frontage type.

[2] A building may be setback up to 50 feet from the property line if the space between the building wall and sidewalk is occupied by a courtyard, plaza, or other form of publicly accessible open space.

Figure 2-26: G-C District Building Placement



3. **Building Massing.** Table 2-26 and Figure 2-32 show building massing standards in the G-C district.

TABLE 2-26: G-C DISTRICT BUILDING MASSING

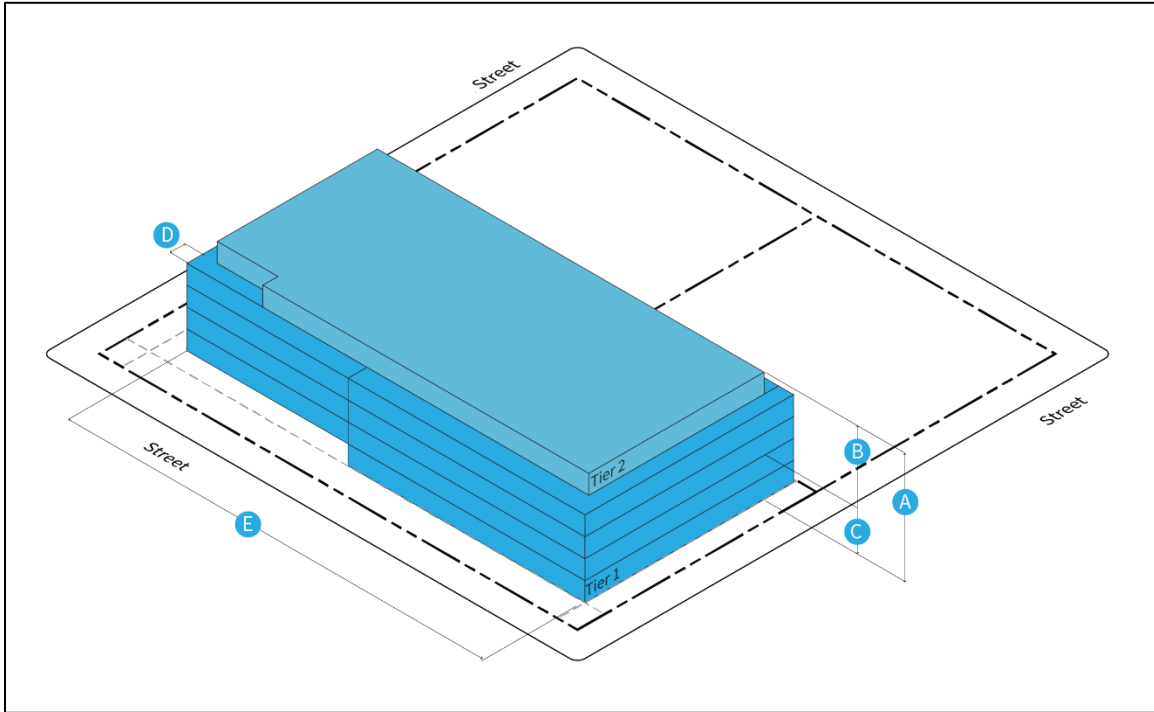
		Base Tier	Tier 1	Tier 2
Height				
A	Feet, Max.	50 ft.	50 ft.	60 ft.
B	Stories, Max.	4	4	5
C	Stories, Min. [1]	2	2	3
Massing				
	Height ratios (% of ground floor by story)			
	1-4	100%	100%	100%
	5	N/A	N/A	80%
	Upper Story Step Backs (from ground-level street-facing building walls)			
	4 th story and below	None, except when required by 9.29.050.B (Enhanced Upper Story Step Backs)		
D	5 th story	8 ft. min. for 75% or more of building street frontage [2]		
E	Max. Building Length [3]	300 ft. [4]		
	Building Modulation	See 9.26.060.B (Building Modulation)		

Notes:

- [1] Minimum height applies only to residential uses.
- [2] For buildings walls with less than 120 feet of street frontage, an 8 ft. step back is required for all but 30 feet of the frontage.
- [3] Measured parallel to the adjacent street.
- [4] See long building division requirement for buildings 150-300 feet in length.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

Figure 2-32: G-C District Building Massing



D. Gateway Neighborhood (G-N).

1. **Purpose.** The G-N district provides a transition from the high-intensity Gateway districts to lower intensity residential uses outside of the Gateway Area Plan boundary. Building and site design supports a pedestrian-friendly public realm compatible with the residential character of surrounding areas.
2. **Building Placement.** Table 2-27 and Figure 2-33 show building placement standards in the G-N district.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

Table 2-27: G-N District Building Placement

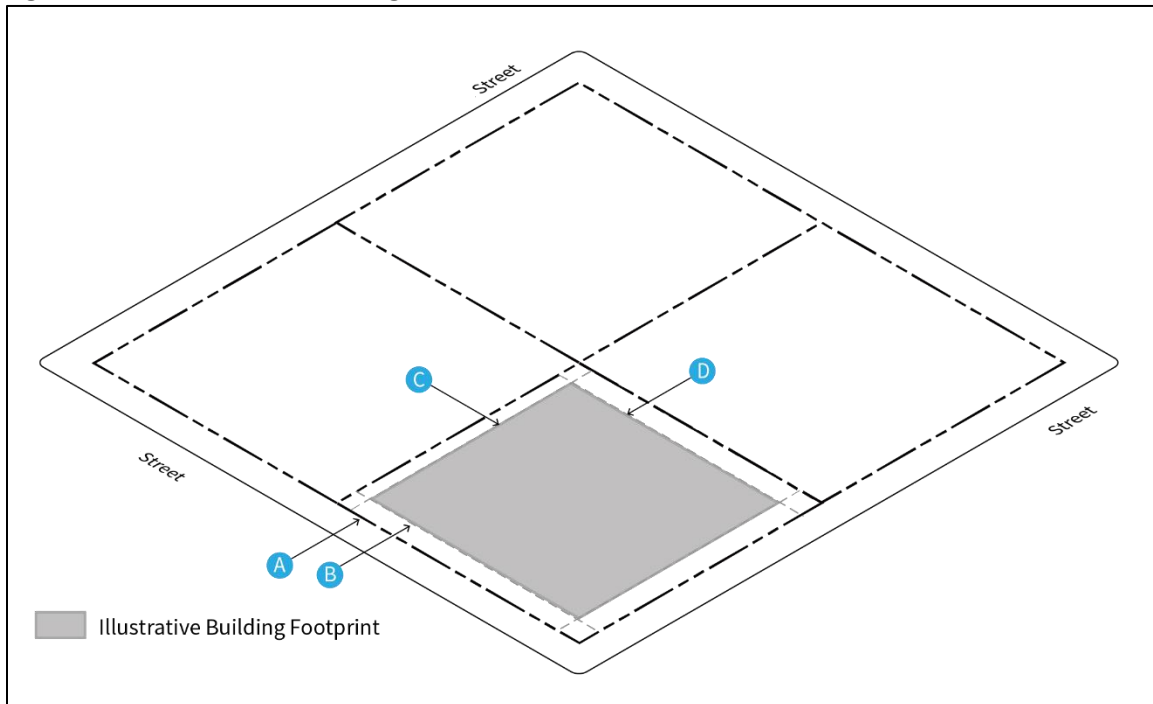
Setbacks		Building Frontage Type	
		Active [1]	Non-Active
From property lines abutting a street			
A	Minimum	10 ft.	20 ft.
B	Maximum	20 ft. [2]	No maximum
From interior side property lines			
C	Minimum	5 ft.	
	Maximum	No maximum	
From rear property lines			
D	Minimum	10 ft.	
	Maximum	No maximum	

Notes:

[1] See Section 9.29.050.A (Active Frontage Types) for locations that require an active building frontage type.

[2] A building may be setback up to 50 feet from the property line if the space between the building wall and sidewalk is occupied by a courtyard, plaza, or other form of publicly accessible open space.

Figure 2-33: G-N District Building Placement



- Building Massing.** Table 2-28 and Figure 2-34 show building massing standards in the G-H district.

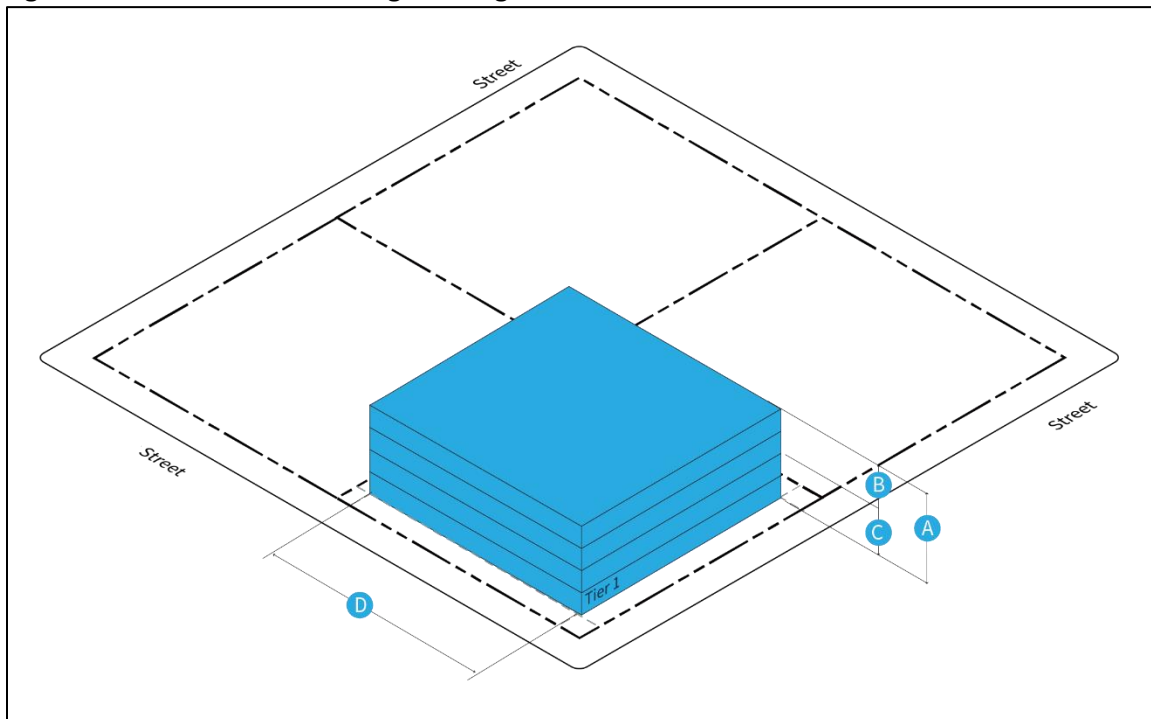
TABLE 2-28: G-N DISTRICT BUILDING MASSING

		Base Tier	Tier 1
Height			
A	Feet, Max.	50 ft.	50 ft.
B	Stories, Max.	4	4
C	Stories, Min. [1]	2	2
Massing			
Upper Story Step Backs (from ground-level street-facing building walls)			
	4 th story and below	None, except when required by 9.29.050.B (Enhanced Upper Story Step Backs)	
D	Max. Building Length [2]	300 ft. [3]	
	Building Modulation	See 9.26.060.B (Building Modulation)	

Notes:

- [1] Minimum height applies only to residential uses.
- [2] Measured parallel to the adjacent street.
- [3] See long building division requirement for buildings 150-300 feet in length.

Figure 2-34: G-N District Building Massing



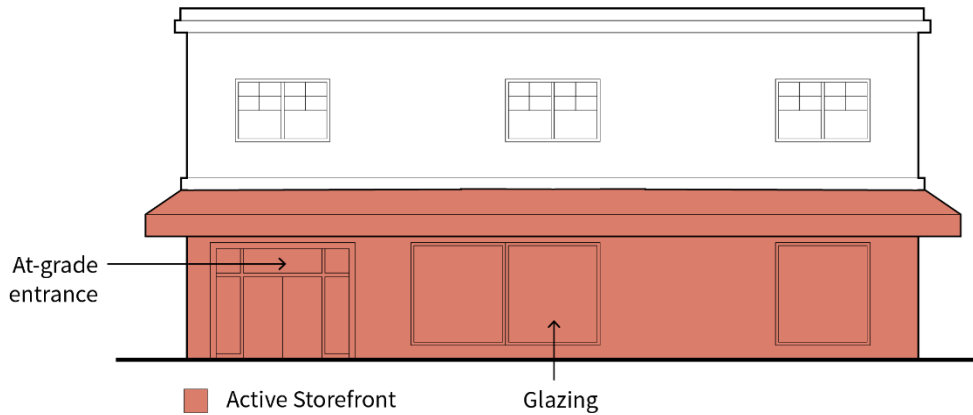
Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

9.29.050 – Supplemental to Districts

A. Active Building Frontage Types.

- 1. **Active Building Frontage Types Defined.** An active building frontage type is designed for non-residential use with glazing between the façade base and ground floor ceiling. An active frontage type provides at-grade access to the building interior directly from the adjacent sidewalk. Storefronts, galleries, and arcades are examples of active building frontage types. See Figure 2-35.

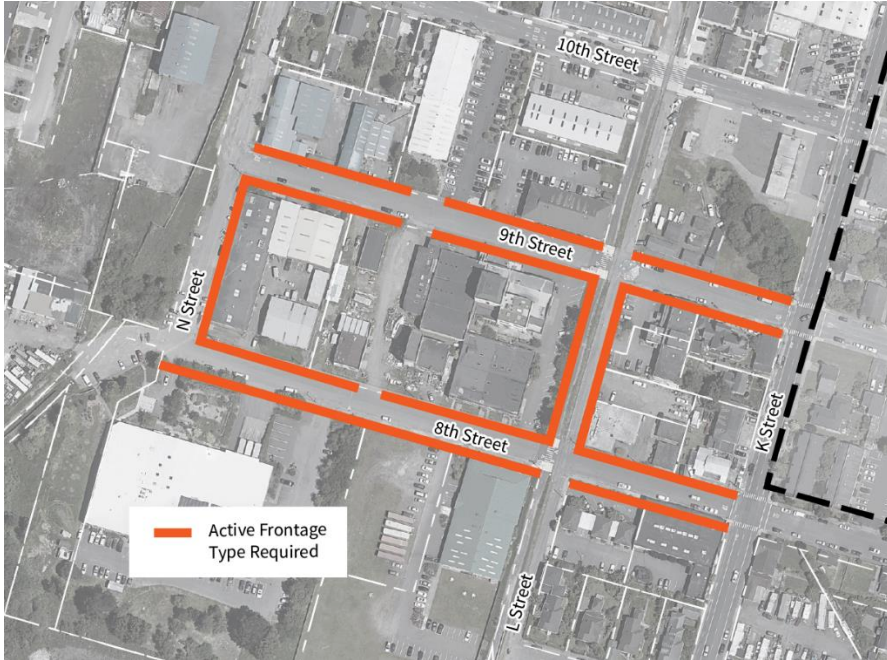
Figure 2-35: Active Frontage Types



- 2. **Required Active Building Frontage Type Locations.** Figure 2-36 shows location where an active building frontage type is required. In all other locations, active building frontage types are permitted, but not required.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

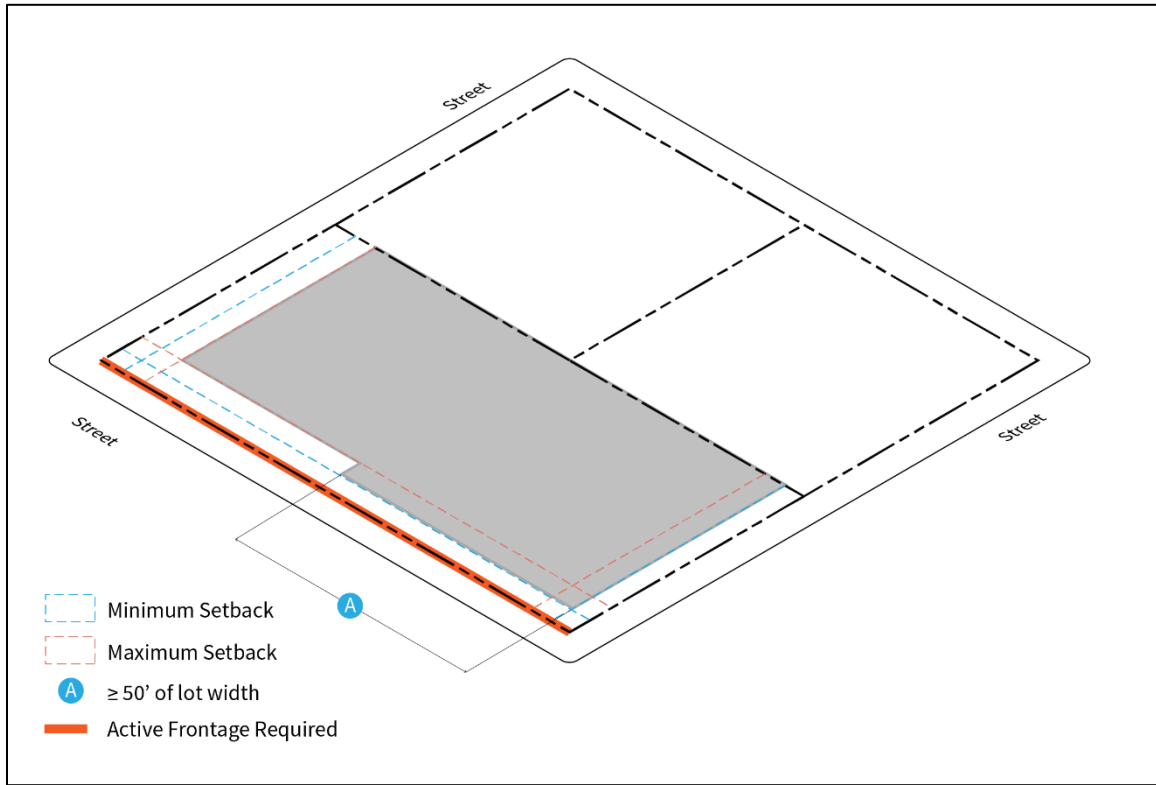
Figure 2-36: Active Building Frontage Type Required Location



- 3. **Active Building Frontage Type Standards.** The following standards apply to all active building frontage types.
 - a. **Maximum Setback.** A building wall with an active building frontage type may be setback no more than:
 - 1. 25 feet from the property line; or
 - 2. 50 feet from the property line if the space between the building wall and sidewalk is occupied by a courtyard, plaza, or other form of publicly accessible open space.
 - b. **Build to Zone.** A minimum of 50 percent of the lot width must contain a building located between the minimum and maximum setback for the district. See Figure 2-37.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

Figure 2-37: Active Building Frontage Placement



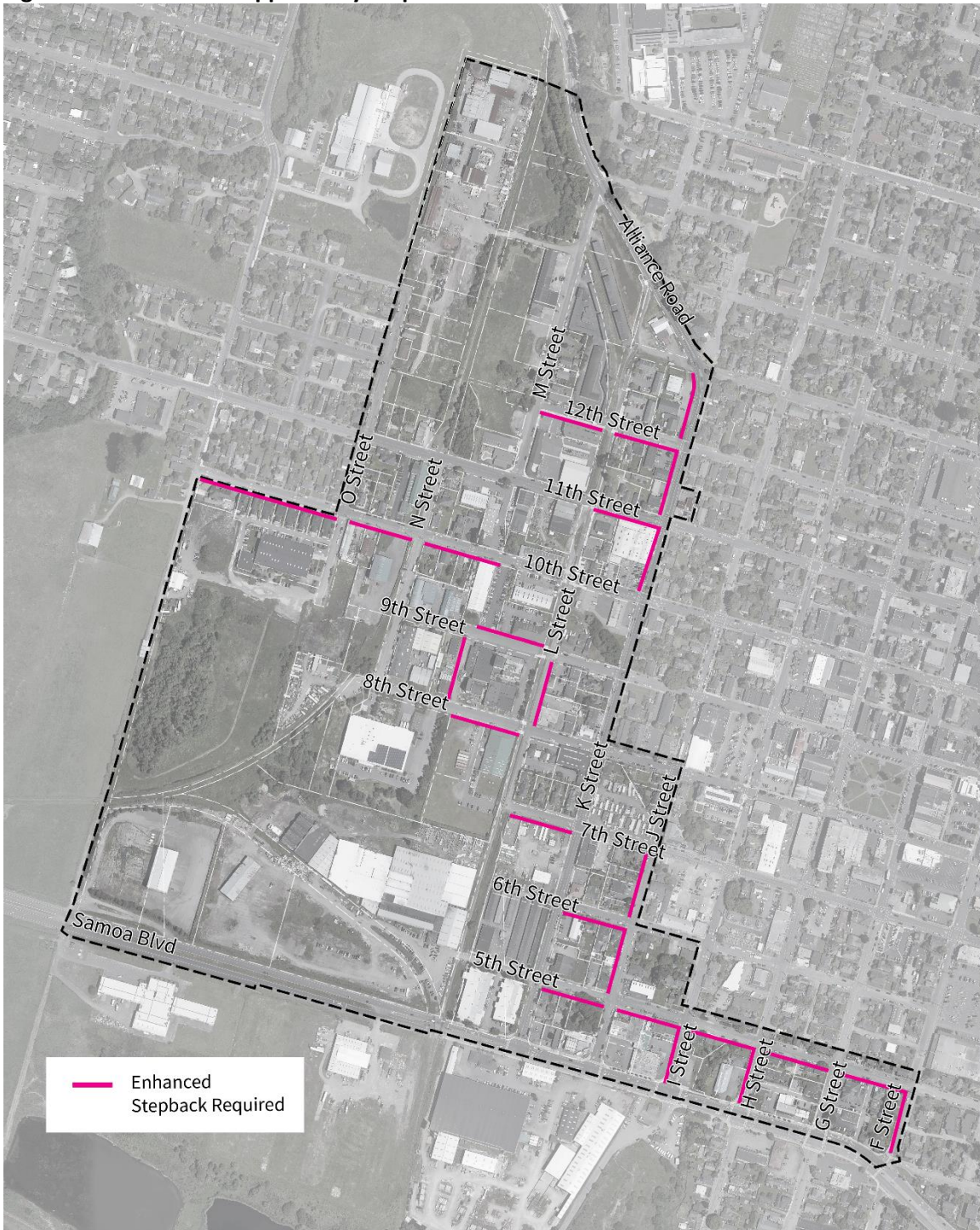
- c. **Transparency.** An active building frontage type must comply with the minimum transparency requirement in Section 9.29.060.G.2.C (Non-Residential Frontages).
- d. **Pedestrian Realm.** The pedestrian realm adjacent to an active building frontage type must be improved consistent with Section 9.29.070.A (Pedestrian Realm Dimensions).

B. Enhanced Upper Story Step Backs.

- 1. **Purpose.** This subsection establishes upper story step back requirements that exceed requirements in Section 9.29.040 (District Standards). These enhanced requirements are intended to reduce shadow impacts and provide context-sensitive massing adjacent to lower-intensity residential uses.
- 2. **Applicability.** Figure 2-38 shows street frontages subject to an enhanced upper story step back requirement.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

Figure 2-38: Enhanced Upper Story Step Back Location

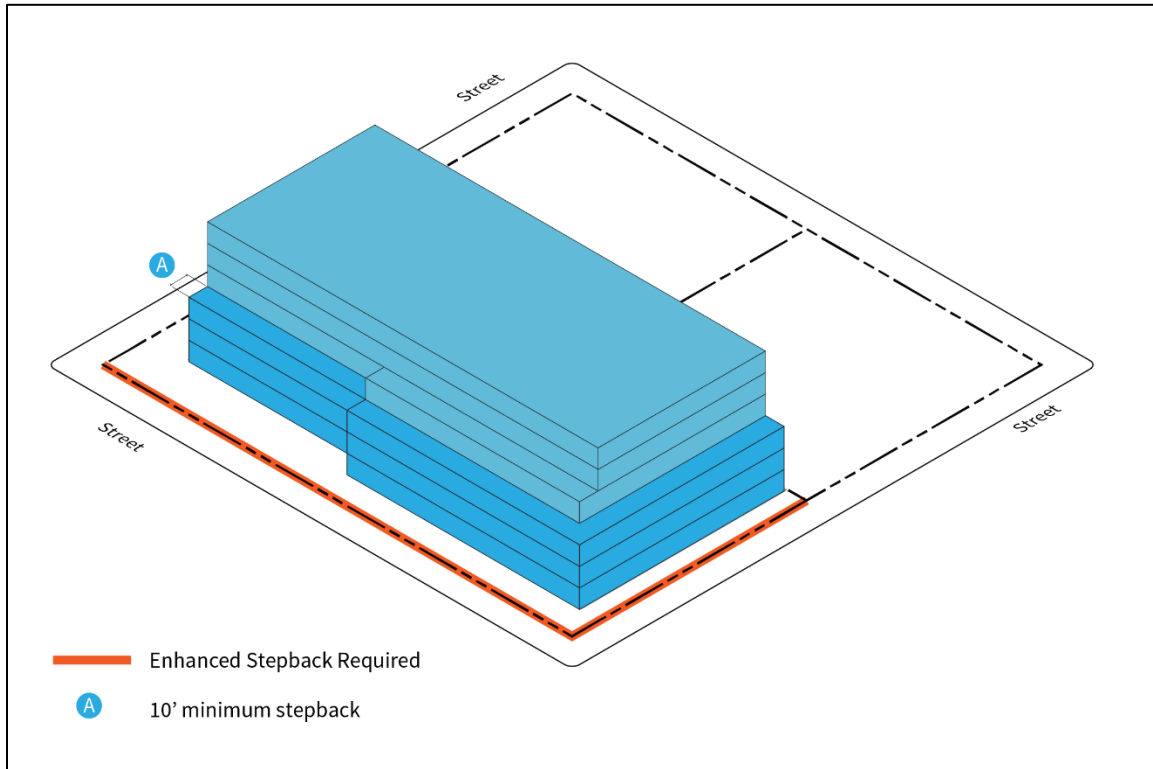


— Enhanced Stepback Required

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

3. **Standard.** For building frontages shown in Figure 2-38, the fourth story must be stepped back a minimum of 10 feet from the ground-level building wall for the full length of the building. See Figure 2-39.

Figure 2-39: Enhance Upper Story Step Backs



C. Historic Resources.

1. Design Review Requirement.

- a. Planning Commission Design Review is required for the exterior modification, demolition, or relocation of a historic resources listed in Table 9 of the Gateway Area Plan.
- b. Chapter 9.53 (Historic Resource Protection), Section 9.72.040 (Design Review), and the General Plan Historic Preservation Element do not require Design Review or any other type of discretionary approval for the exterior modification, demolition, or relocation of any other structure in the Gateway districts not listed in Gateway Area Plan Table 9.

- 2. **Creamery Property.** Structures in the area shown in Figure 2-40 may be demolished without Design Review or other type of discretionary approval. Demolition may occur as part of new development project or independent from new development on the property.

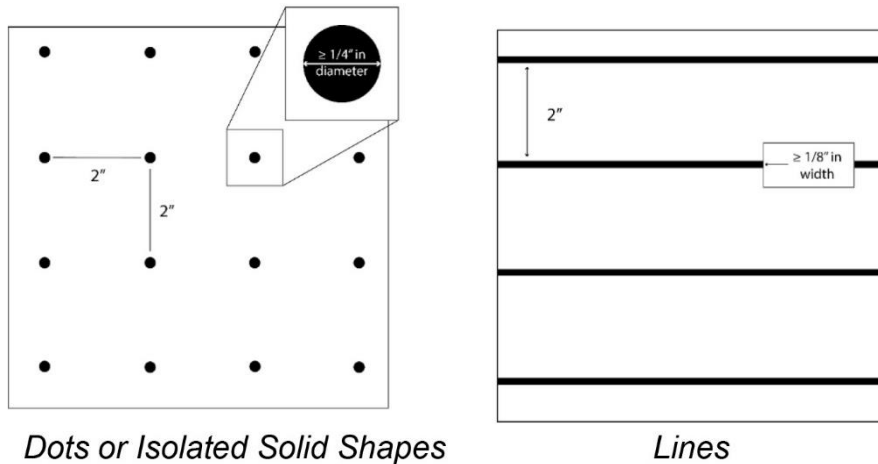
Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

Figure 2-40: Allowed Creamery Property Demolition



D. Bird Safe Buildings.

1. **Intent.** The intent of these standards is to reduce the risk of bird-to-building collisions.
2. **Applicability.** The bird-safe building standards in this subsection apply to new construction where glass or other rigid transparent material occupies 35 percent or more of the building façade.
3. **Standards.**
 - a. **Bird-Safe Glazing.** Any regulated continuous transparent material must meet at least one of the following conditions:
 - (1) **Threat Factor Rating.** A product with an American Bird Conservancy Threat Factor Rating of 30 or less.
 - (2) **Patterned Glazing Treatment.** Panes with patterns that are etched, fritted, stenciled, silk-screened, or otherwise permanently incorporated into the transparent material. If the pattern utilizes dots or other isolated solid shapes, each dot or shape must be at least a ¼ inch in diameter and be no more than 2 feet apart in any direction (see Figure 2-41). If the pattern utilizes lines, they must be at least ⅛ inch in width and spaced no more than 2 inches apart (see Figure 2-41, left).

Figure 2-41: Patterned Glazing Treatment Requirements**b. Interior Lighting.**

- (1) Nonresidential spaces shall have automatic light shutoff systems using timers, photo sensors, motion sensors, or a combination of sensors.

c. Site Design. No mirrors shall be placed in or near planted areas or water features, or in locations where they would reflect trees, plants, or water.**E. Landscaping.** Landscaping standards in Chapter 9.34 (Landscaping Standards) apply in the Gateway districts except as modified below.

1. All areas between a building and adjoining sidewalk shall be landscaped, except for:
 - a. Areas required for vehicular or pedestrian access to the property; and
 - b. Courtyards, outdoor seating areas, and other similar outdoor spaces for customers and/or the general public.
2. Landscaping shall consist of any combination of trees and shrubs, and may include grass or related natural features, such as rock, stone, or mulch. Non-plant materials may occupy no more than 25 percent of the landscaped area,
3. Minimum landscape area requirements in 9.34.020.A (New Projects) do not apply in the Gateway districts.

F. Projections Above Height Limit.

1. Building features may project above maximum height limit in accordance with 9.30.040.D.1 (Exceptions to Height Limits).
2. Rooftop solar energy facilities may project above the maximum building height by up to five feet.

- G. Inclusionary Zoning.** For projects with 30 dwelling units or more, the project provides a minimum of 3 percent of the units affordable to low income households or 5 percent of the units affordable to moderate income households as defined in Chapter 9.100 (Definitions).

9.29.060 – Building Design Standards

A. Purpose and Allowed Deviations.

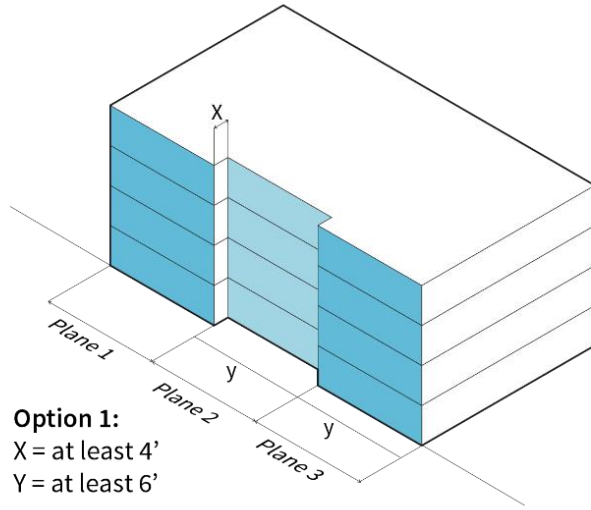
- 1. This section establishes building design standards for projects seeking by-right approval with a Gateway Ministerial Permit. A project is eligible for a Gateway Ministerial Permit only if it conforms to these standards.
- 2. A project that does not conform with these standards is allowed with Design Review. See Section 9.29.020.C (Design Review) for required findings.

B. Building Modulation.

- 1. **Intent.** The intent of the building modulation standard is to
 - a. Break down large building volumes to reduce the perceived mass and box-like appearance of buildings; and
 - b. Create visual interest on street-facing building facades.
- 2. **Standards.** A building frontage that is longer than 30 feet wide and faces a public street, right-of-way, or publicly accessible path, shall be modulated in one of the following ways:
 - a. Provide one horizontal change in plane for every 30 feet of frontage, rounded up to the nearest whole number. The change in plane must be at least 4 feet deep and 6 feet wide and must be open to the sky. See Figure 2-42.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

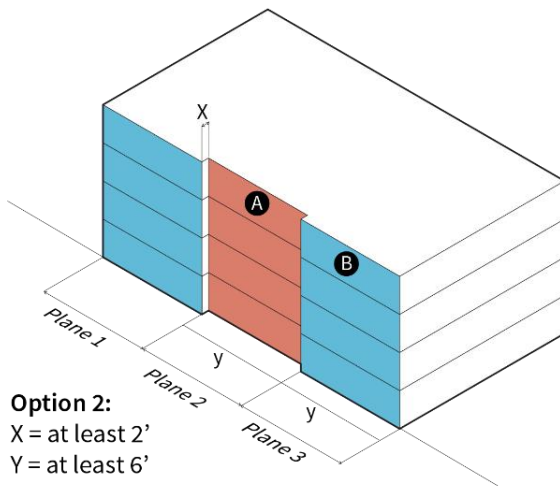
Figure 2-42: Minimum 4x6' Break



Option 1:
X = at least 4'
Y = at least 6'

- b. Provide a horizontal change in plane for every 30 feet of frontage, rounded up to the nearest whole number. The change in plane must be at least 2 feet deep and 6 feet wide and be combined with a change in material. See Figure 2-43.

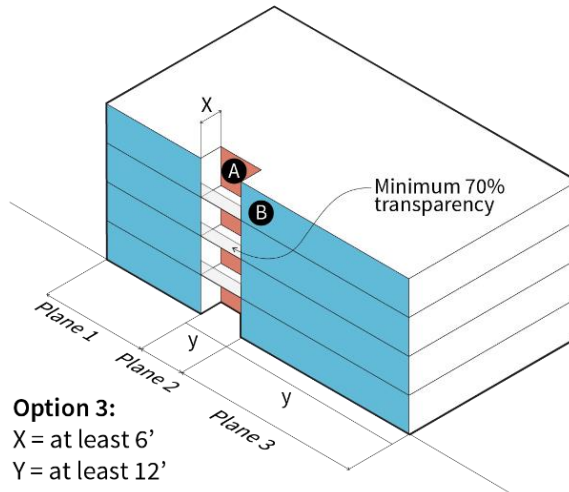
Figure 2-43: Minimum 2x6' Break with Material Change



Option 2:
X = at least 2'
Y = at least 6'
Ⓐ Material A
Ⓑ Material B

- c. Provide a horizontal change in plane at an interval of 50 feet or less. The change in plane must be at least 6 feet deep and 12 feet wide and be combined with a change in material. When implemented as building notches, notches may contain balconies as long as the railing is at least 70 percent see-through or transparent. See Figure 2-44.

Figure 2-44: Minimum 6x12' Break

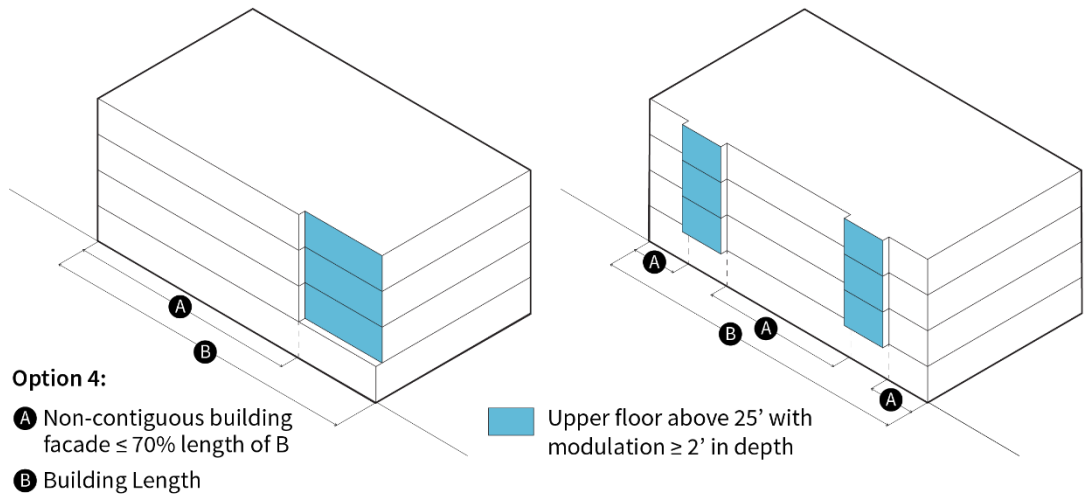


- Option 3:**
 X = at least 6'
 Y = at least 12'
 A Material A
 B Material B

- d. Provide upper floor modulation such that the building façade contains a continuous façade plane of no more than 70 percent of the façade length. The upper floor modulation must be a minimum of 2 feet in depth and may be a recess or a projection. See Figure 2-45.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

Figure 2-45: Upper Floor Modulation

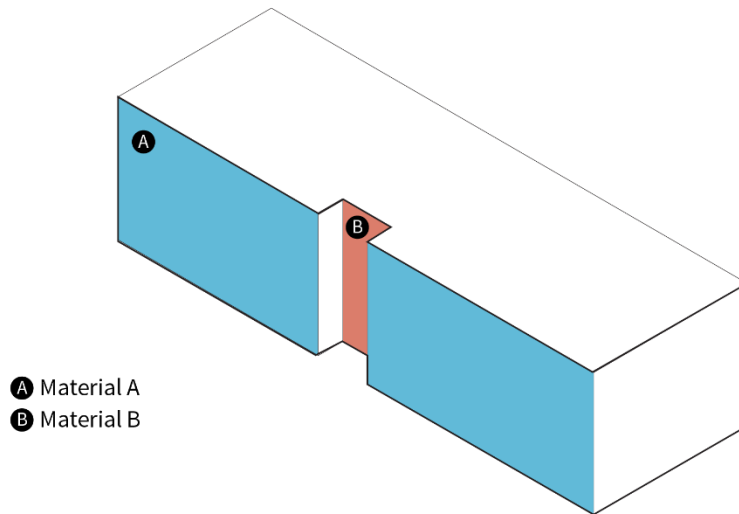


C. Long Building Division.

1. **Intent.** The intent of the long building division standard is to promote a human-scaled and pedestrian-friendly environment with long street-facing building facades that appear as two or more distinct volumes.
2. **Standard.** A building 150 to 300 feet in length, which faces a public street, right-of-way, or publicly accessible path, shall include at least one vertical facade break with a minimum area greater than 64 square feet, a minimum width of 8 feet, and a minimum depth of 4 feet. See Figure 2-46.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

Figure 2-46: Vertical Façade Breaks



D. Façade Articulation.

1. **Intent.** The intent of the façade articulation standards is to:
 - a. Create street-facing building facades that are varied and interesting with human-scale design details; and
 - b. Incorporate architectural elements that reduce the perceived mass and box-like appearance of buildings.
2. **Standards.** A project must incorporate at least two of the following façade articulation techniques on each building frontage that faces a public street, right-of-way, or publicly accessible path:
 - a. **Balconies.** At least 20 percent of the linear frontage of the building wall contains at least one above-ground balcony that is at least 4 feet in depth.
 - b. **Ground Level Porches and Patios.** At least one porch or patio every 50 feet of frontage. Minimum dimensions: 6 feet by 5 feet.
 - c. **Bay Windows.** At least 25 percent of all windows on the building wall consist of projecting windows. The furthest extent of each projecting window must project at least 1 foot from the building wall.
 - d. **Awnings.** At least 25 percent of all windows on the building wall feature awnings or other types of exterior treatments to shade windows.
 - e. **Visible Rooftop Landscaping.** Cascading rooftop landscaping for at least 50 percent of the building frontage.

- f. **Loggia.** An upper level covered exterior gallery or corridor (i.e., loggia) along the full length of the building wall.
- g. **Window Variation.** Variation in window size and pattern where the area and dimensions of windows are different for at least two of the stories.
- h. **Projecting Window Frames.** Projecting window frames where the depth of the frame must exceed the minimum dimension in Paragraph H (Windows) by at least 50 percent.
- i. **Contrasting Material and/or Color.** Variation in two of the following: exterior material, material size; texture and pattern; color.
- j. **Fine-Grain Building Materials.** Fine-grain building materials such as bricks and shingles that occupy least 50 percent of the wall area.
- k. **Awnings and Canopies.** Awnings and canopies that exceed minimum dimensions in Subsection F (Building Entries) by at least 50 percent.
- l. **Cornices and Decorative Horizontal Accent Lines.** Cornices and decorative horizontal accent lines that project at least 6 inches from the building wall for full length of building.
- m. **Vertical Accents.** Projecting or recessed vertical accents of at least every 30 feet for full length of the building.
- n. **Cornice or Parapet Cap.** A cornice or parapet cap that includes a change in material from the building wall and a minimum height of 8 inches and a minimum depth of 4 inches.
- o. **Rounded Corner Treatment.** A rounded corner treatment for the full height of the building at the intersection of two streets.

E. Roof Forms

- 1. **Intent.** The intent of the roof form standards is to:
 - a. Ensure that roof forms are varied and designed with architectural interest; and
 - b. Reduce the perceived mass of buildings as they meet the sky.
- 2. **Standards.**
 - a. **Roofline Articulation.** Projects must provide for roofline articulation by selecting one or more of the following techniques for each building frontage that faces a public street, right-of-way, or publicly accessible path:
 - (1) At least one change in roof pitch or form for every 30 feet of street-facing building frontage.
 - (2) A change in façade or roof height of at least 5 feet for a minimum of 25 percent of the building frontage.

- (3) At least one horizontal change in the street-facing building plane every 30 feet. Change in plane must be at least 4 feet deep, 6 feet wide, and open to the sky.
- (4) Green roof or roof landscaping along a minimum of 75 percent of the building frontage. Landscaping must be designed to be visible from the adjacent public sidewalk, street, pathway, or right-of-way.
- (5) A roof deck along a minimum of 75 percent of the building frontage. The roof deck railing must be within 5 feet of the street-facing parapet. At least one amenity structure for the use and enjoyment of the roof deck (e.g., pergola, wind barrier) permanently affixed to the roof deck must be visible from the adjacent public sidewalk, street, pathway, or right-of-way.
- (6) Varied roof types where at least two different roof types each occupy at least 25 percent of the building frontage. Roof types include gable, hipped, shed, and flat roof forms.
- (7) Overhanging eaves extending at least 2 feet beyond the building face for the full length of the building
- (8) Gables that break the horizontal eave at intervals of no more than 40 feet along the building façade.
- (9) Dormer windows, integrated into a sloped roof, occupying a minimum of 25 percent of the street-facing roof length as measured at the eave.
- (10) Decorative cornice and parapet treatments for the full length of the top-most roof line.

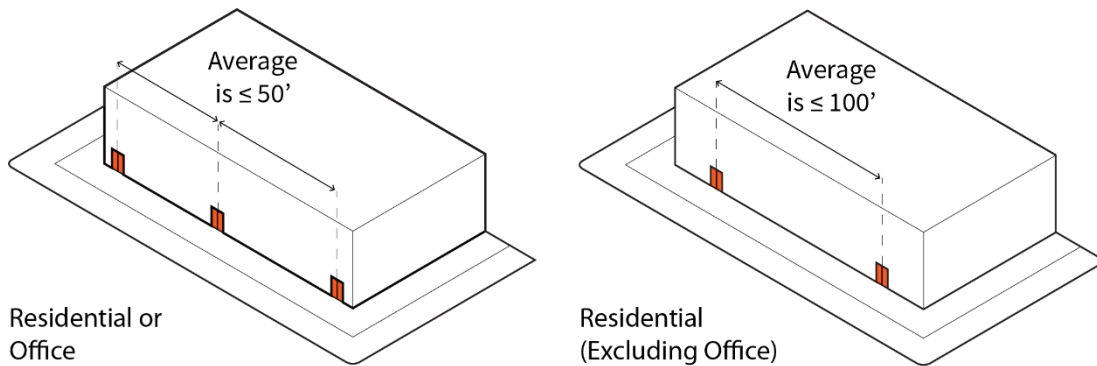
F. Building Entries.

1. **Intent.** The intent of the building entries standards is to:
 - a. Support cohesive neighborhoods and social interaction with outward facing buildings; and
 - b. Support a pedestrian-oriented public realm with an attractive and welcoming streetscape character.
2. **Standards.**
 - a. **Minimum Number of Entrances.** The ground floor of a building that faces a public sidewalk, street, pathway or right-of-way shall have entrances as required by Table 2-29. See Figure 2-47.

Table 2-29: Minimum Number of Entrances Required

Ground Floor Use	Average Distance between Entrances
Residential or Office	50 ft.
Non-residential (excluding office)	100 ft.

Figure 2-47: Minimum Number of Entrances

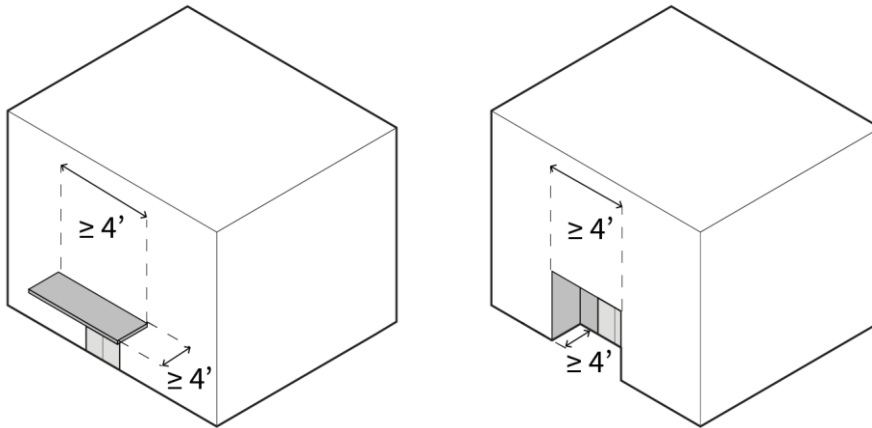


- b. **Corner Buildings.** A corner building must have an entrance facing both streets or have a single corner entrance accessible to both streets.
- c. **Functionality.** Entrances required by Paragraphs (a) and (b) above must remain functional and available for use by occupants.
- d. **Entrances to Individual Units.**
 - (1) For units adjacent to a public street that are accessed through ground level individual entrances (e.g., townhomes), the primary entrances must face the street.
 - (2) Entrances must be emphasized with one or more of the following:
 - i. An awning or canopy above the entry with a minimum outward projection of 3 feet and minimum width sufficient to clear the entrance on both sides.
 - ii. A recess in the building wall with a minimum width of 4 feet and depth of 2 feet. A recessed entry must feature design elements that call attention to the entrance through contrasting materials, crown molding, decorative trim, external lighting, differentiated paving in recessed area, or similarly obvious architectural feature.
 - iii. A covered porch, providing access to the entry, with a minimum dimension of 5 feet by 5 feet.

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- iv. A patio with minimum dimensions of 5 feet by 5 feet. A patio must include a row of shrubs, a fence, or a wall not to exceed 42 inches in height between the sidewalk and the patio to define the transition between public and private space.
- e. **Other Primary Entrances.** The following standards apply to all primary building entrances, excluding entrances to individual dwelling units.
 - (1) **Weather Protection.** Primary building entrances shall include weather protection with either:
 - i. A projecting awning, canopy, extended eave, or other similar feature above the entry, minimum 4 feet wide by 4 feet deep; or
 - ii. A recessed entry, minimum 4 feet wide by 4 feet deep. See Figure 2-48.

Figure 2-48: Weather Protection



- (2) **Visual Prominence.** Primary building entrances, excluding entrances to individual dwelling units, must be clearly visible from the street with visual prominence. Projects must select one or more of the following methods to satisfy this requirement:
 - i. A building wall modulation, either a recess or a projection, for the full building height above the entrance, minimum 4 feet in depth.
 - ii. A taller building roof element above an entry that projects above the adjacent roofline by at least 4 feet.
 - iii. A frontage court, minimum 25 feet wide by 25 feet deep, enclosed on at least three sides by building walls.
 - iv. A distinctive corner building treatment integrated into a corner entrance, such as rounded or angled facets or an embedded corner tower.

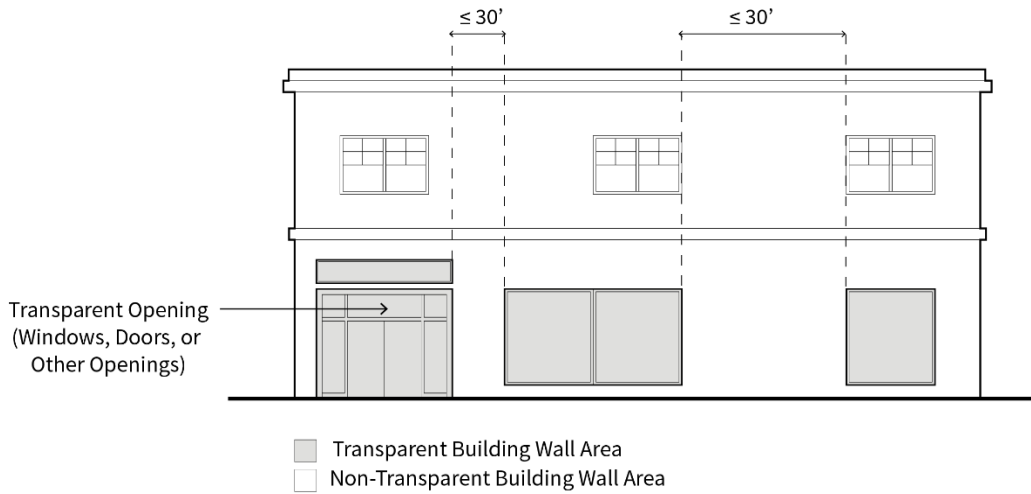
Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

- v. Entry materials or colors that contrast with surrounding façade treatment.
 - vi. Fenestration pattern that contrasts with surrounding window treatment.
 - vii. Projecting architectural elements surrounding the entrance, such as columns, porticos, and ornamental light fixtures.
 - viii. Artwork integrated into the entry design.
- f. **Visitable Entrances.** For at least 25 percent of the units, at least one entrance must be accessible via a route that does not have any stairs between it and the street lot line or an on-site parking space. The slope of the route may not exceed 1:8.

G. Ground-Floor Frontages.

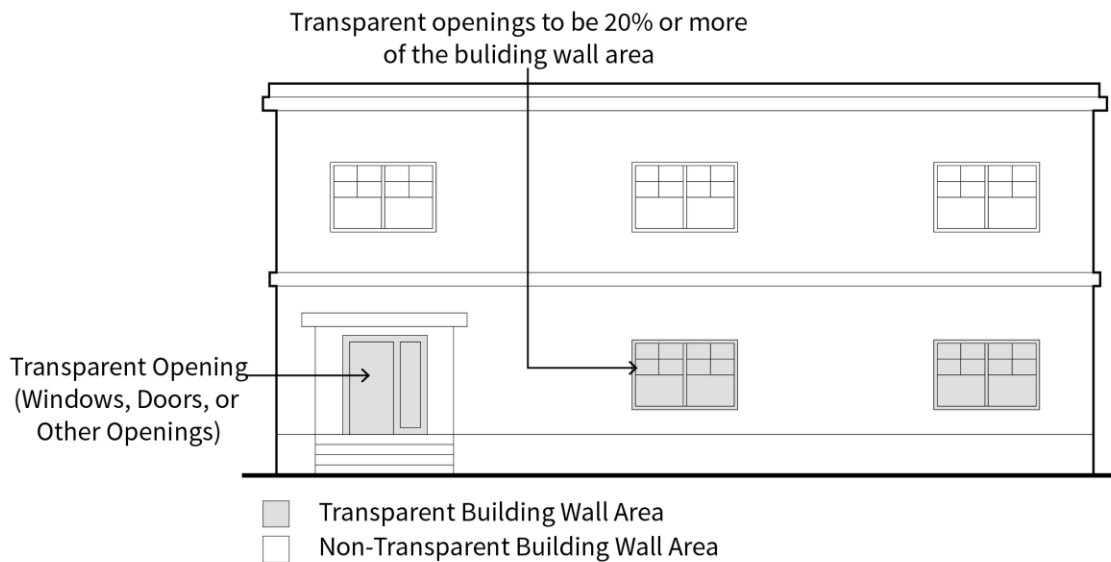
1. **Intent.** The intent of the ground-floor frontage standards is to:
 - a. Support an active and welcoming pedestrian environment;
 - b. Limit blank walls facing the street; and
 - c. Create an environment that will help attract and retain successful local businesses.
2. **Standards.**
 - a. **Openings Required - All Uses.** For all land uses, ground-level building walls facing and within 20 feet of a public sidewalk, street, pathway or right-of-way shall run in a continuous plane for no more than 30 feet without a window, door, or other similar building opening. See Figure 2-49.

Figure 2-49: Openings Required - All Uses



- b. **Ground-Floor Residential Openings.** A minimum of 20 percent of a ground-level residential building wall that faces and is within 20 feet of a public sidewalk, street, pathway, or right-of-way shall be comprised of entries, windows or glazing, and/or railings. Trim, including window shutters, is counted towards meeting this requirement. Garage doors are not included. See Figure 2-50.

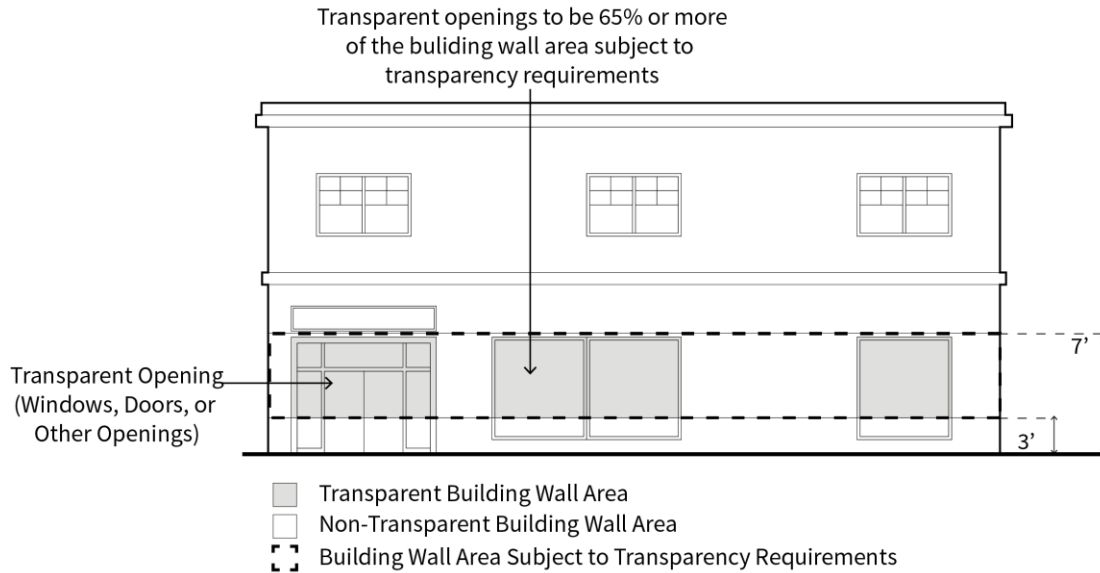
Figure 2-50: Ground-Floor Residential Openings



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- c. **Non-Residential Transparency.** A ground-level non-residential building wall that faces and is within 20 feet of a public sidewalk, street, pathway, or right-of-way must provide transparent windows or doors with views into the building for a minimum of 65 percent of the building frontage width located between 3 and 7 feet above the sidewalk. See Figure 2-51.

Figure 2-51: Non-Residential Transparency

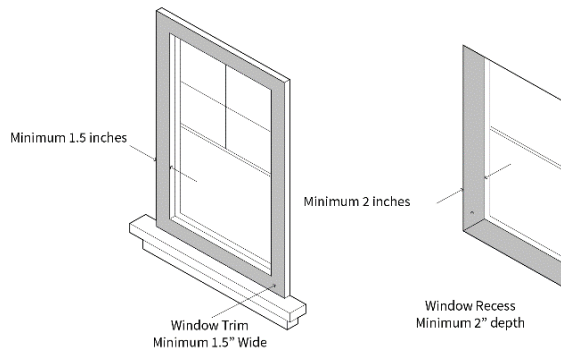


H. Windows.

- 1. **Intent.** The intent of the window standards is to:
 - a. Create visual interest and provide relief for flat walls;
 - b. Ensure long-term durability with quality materials; and
 - c. Prevent glare and ensure transparency of ground-floor openings.
- 2. **Standards.**
 - a. Windows for residential uses must have trim at least 1.5 inches in width or be recessed at least 2 inches from the plane of the surrounding exterior wall. See Figure 2-52.

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Figure 2-52: Window Trim or Depth



- b. Window designs must differentiate the various components of the building such as ground-floor retail spaces, stair towers, corners, or residential units. To satisfy this requirement different building components must feature variation in at least two of the following: fenestration size, proportions, pattern, and depth or projection.
- c. Window films, mirrored glass, and spandrel glass are prohibited along the ground-floor building frontage.
- d. Folding security gates (scissor gates) for storefronts, building entrances, and windows are permitted in the Gateway districts.
- e. Windows must comply with bird safe standards in Section 9.29.050.D (Bird Safe Buildings).

I. Garage Doors and Entries.

- 1. **Intent.** The intent of the garages doors and entries standards is to:
 - a. Minimize the visual dominance of garage entries and garage doors; and
 - b. Support a safe and inviting pedestrian environment.
- 2. **Standards.**
 - a. **Garage Doors Serving Individual Units.** Garage doors serving individual units may not face a public street. Such garage doors must be oriented towards an alley or a private street/driveway that is internal to the project.
 - b. **Shared Garages and Parking Structures.** The following standards apply to garages serving multiple dwelling units and/or non-residential uses.
 - (1) No more than 25 percent of the site frontage facing a street may be devoted to garage opening, unless the street frontage is less than 80 feet, in which case a 20-foot garage opening is allowed.

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- (2) Above grade structured parking levels facing a public right-of-way or publicly accessible open space/path, with the exception of vehicular alleys, must either be lined with commercial or habitable uses with a minimum depth of 20 feet or feature a façade with the appearance of habitable uses.
- (3) Partially sub-grade parking (“Podium parking”) shall not have an exposed façade that exceeds 5 feet in height above abutting grade at back of sidewalk.
- (4) Podium parking must include a landscaped planter between the street and the podium. The planter must be at least 4 feet wide with a planting height and vegetative cover sufficient to fully screen the podium edge and ventilation openings from view. At maturity, plantings must comprise a minimum of 75 percent of the total landscape planter.

J. Material Durability.

- 1. **Intent.** The intent of the material durability standard is to ensure that exterior building materials are high quality, durable, and convey a sense of permanence.
- 2. **Standards.**
 - a. **Material Lifespan.** Exterior finish materials shall have an expected lifespan of no less than 30 years as specified in the International Association of Certified Home Inspectors estimated life expectancy chart.
 - b. **Rainwater Protection.** Features to direct rainwater away from exterior walls shall include one or more of the following:
 - 1. Cornice, with drip at outer edge of corona (minimum 12-inch projection).
 - 2. Projecting eaves (minimum 12-inch projection).
 - 3. Scuppers, with or without downspouts (minimum 12-inch projection if no downspouts are used).
 - 4. Gutters, with downspouts or rain chains.
 - c. **Timber Protection.** Exterior timber shall be protected from decay by one or more of the following:
 - 1. Material properties (e.g., cedar).
 - 2. Staining and sealing.
 - 3. Painting.
 - d. **Metal Protection.** Exterior ferrous metals shall be protected from corrosion by one or more of the following:
 - 1. Metallurgical properties (e.g., galvanized, stainless, or weathering steel).
 - 2. Painting or other impermeable coating.

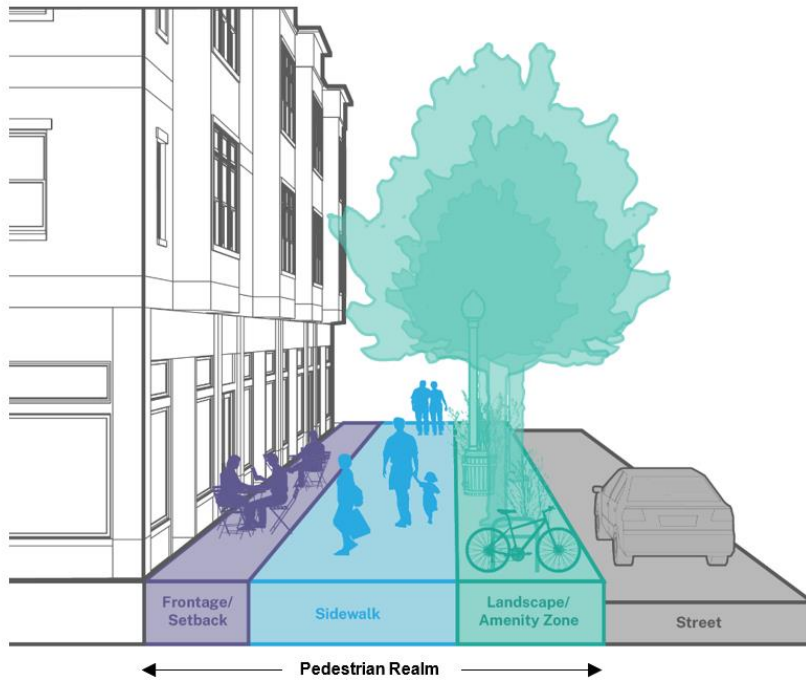
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9.29.070 – Streetscape

A. Pedestrian Realm Dimensions

- 1. This subsection establishes minimum dimensions for the pedestrian realm between the street curb and street-facing building wall. As shown in Figure 2-53, the pedestrian realm consists of a sidewalk, landscape/amenity zone, and building frontage zone.

Figure 2-53: Pedestrian Realm



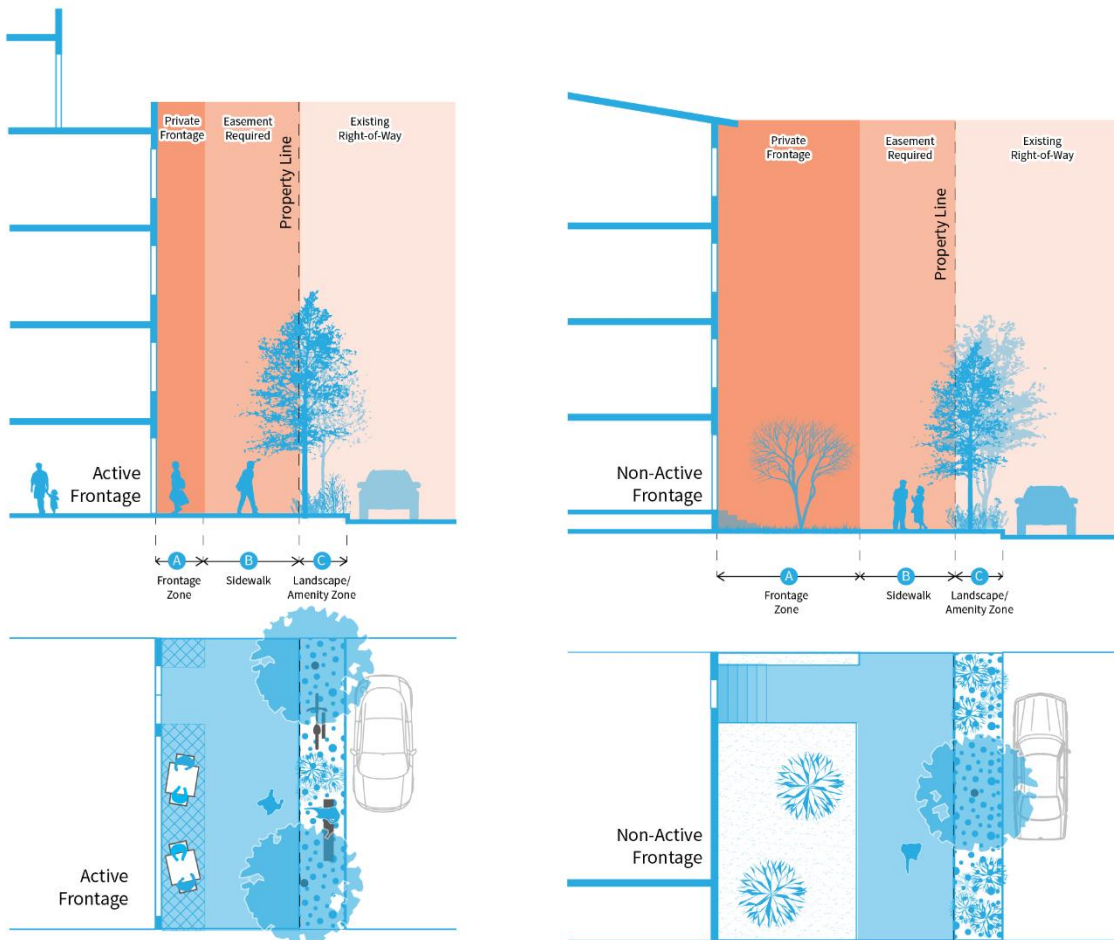
- 2. New buildings and other improvements shall be located on a site to allow for minimum pedestrian realm dimensions shown in Table 2-30 and illustrated in Figure 2-54. Minimum pedestrian realm dimensions are measured from the existing street curb or 20 feet from the existing or planned new street centerline, whichever is greater.

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Table 2-30: Pedestrian Realm Dimensions

Location	A Frontage Zone	B Sidewalk	C Landscape/ Amenity Zone
“Active Frontage Type Required” Locations shown in Figure 10			
Active Frontages	5 ft.	10 ft.	5 ft.
Non-Active Frontage	N/A	N/A	N/A
All Other Locations			
Active Frontages	5 ft.	8 ft.	5 ft.
Non-Active Frontage	15 ft	8 ft.	5 ft.

Figure 2-54: Pedestrian Realm Dimensions



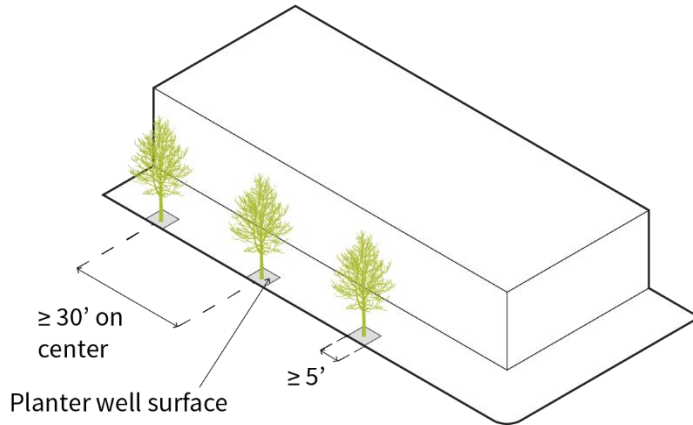
- Where the existing public sidewalk does not meet the minimum standard, a publicly accessible extension of the sidewalk into the property, with corresponding public access easement, shall be provided.

B. Pedestrian Realm Improvements.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

1. **When Required.** New pedestrian realm improvements are required when the development site borders or is traversed by an existing street or new local road connection and one or more of the following applies:
 - a. Development occurs on a vacant parcel.
 - b. A new primary structure is constructed.
 - c. The value of on-site improvements exceeds 33 percent of the value of the existing on-site improvements and an existing structure is not located within the pedestrian realm dimensions shown in Table 2-30.
2. **Frontage Zone.**
 - a. The building frontage zone adjacent to an active building frontage type may be occupied by sidewalk dining, outdoor displays, seating, and landscaping.
 - b. The building frontage zone adjacent to a non-active building frontage type may be occupied by stoops, porches, front yards, and landscaping.
3. **Sidewalks.** The developer shall install a sidewalk with the minimum pedestrian clear path shown in Table 2-30. The sidewalk shall be constructed consistent with the City's Standard Details.
4. **Street Trees.** Street trees shall be provided within the landscape/amenity zone as follows:
 - a. Spacing between trees: minimum 30 feet on center. See Figure 2-55.
 - b. Tree well size: minimum 5 feet each direction.
 - c. Planter well surfacing: tree grates, permeable pavers, decomposed granite, understory plants, or similar treatments as determined by City.
 - d. Street tree selection: tree species shall be selected from a City tree master list or in consultation with the City. Street tree species shall be consistent along each street.

Figure 2-55: Street Trees



5. Street Furnishings.

- a. Furnishings such as bicycle racks, seating, and trash receptacles shall be provided in the landscape/amenity zone consistent with City standards.
- b. Furnishing colors and materials shall be selected in consultation with the City to be durable, vandal resistant, and low maintenance.

6. Green Infrastructure. Green stormwater infrastructure to capture and treat runoff shall be incorporated into the landscape/amenity zone as required by the Municipal Regional Stormwater Permit.

7. Street and Pedestrian Lighting.

- a. Street and pedestrian lighting shall be provided in the landscape/amenity zone consistent with City standards.
- b. Light poles and fixture shall comply with City standard specifications and shall be selected to be durable, vandal resistant, and low maintenance.

9.29.080 – Mobility

A. Greenways.

- 1. **Conceptual Configuration.** Figure 2-56 shows the conceptual configuration of new greenways in the Gateway districts.

Note: The contents of this figure will be incorporated into the Gateway Plan and removed from the code.

Figure 2-56: Conceptual Greenway Configuration



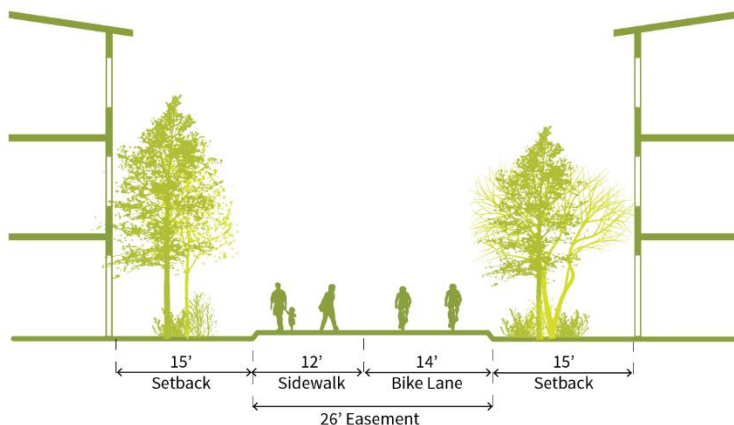
Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

2. **Final Configuration.** Final greenway configurations will be established in:
 - a. The Barrel District Master Plan; or
 - b. Development project applications for areas outside of the Barrel District.
3. **Greenways Required.** Greenways are required in the approximate locations shown in Figure 2-56. Development must provide greenways that create blocks consistent with Municipal Code Section 9.88.030.F.4 (Blocks). Block length and perimeter is measured along all publicly accessible thoroughfares, including streets and greenways.
4. **Standards.**
 - a. Greenways shall comply with standards shown in Table 2-31 and illustrated in Figure 2-57.
 - b. Greenways shall be designed to accommodate emergency vehicle access when required by the Arcata Fire District.

Table 2-31: Greenway Standards

	Minimum Dimension
Public Access Easement	26 ft.
Building-to-building	56 ft.
Pedestrian Realm	
Building Frontage Zone	15 ft.
Walkway	12 ft. (10' pavement plus 2' reinforced decomposed granite jog strip on one side)
Bike lane	14 ft. (Two 7' bike lanes)

Figure 2-57: Greenway Standards



- B. **Parking.** All off-street parking and loading requirements in Chapter 9.36 (Parking and Loading) apply in the Gateway districts except as follows:

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1. **Number of Spaces.** All land uses established after the Gateway Area Plan is adopted must comply with the minimum and maximum number of off-street parking space standards in Table 2-32.

Table 2-32: Required Number of Off-Street Parking Space

Land Use	Minimum	Maximum			
	All districts	G-B	G-H	G-C	G-N
Residential uses					
Units 1,000 sf and less	None	0.5 per unit	0.25 per unit	0.25 per unit	0.5 per unit
Units more than 1,000 sf	None	0.5 per unit	0.5 per unit	0.5 per unit	0.75 per unit
Commercial uses, including retail, restaurants, and personal services	None	1 per 500 sf	1 per 1,000 sf	1 per 1,000 sf	1 per 1,000 sf
Employment uses, including professional offices and R&D	1 per 1,500 sf	1 per 750 sf	1 per 1,000 sf	1 per 500 sf	1 per 500 sf
Hotels, motels, and inns	0.5 per guest room	1 per guest room	1 per guest room	1 per guest room	1 per guest room
All other land uses	None	1 per 1,000 sf	1 per 1,000 sf	1 per 1,000 sf	1 per 1,000 sf

C. Transportation Demand Management

1. **Non-Residential Transportation Demand Management Standards.** All new non-residential uses over 10,000 cumulative square feet in a project or on a parcel must prepare a TDM plan with the following programs and measures to encourage employees to use transit, ridersharing, walking, biking, and telework. TDM programs and measures may include:
 - a. Priority parking for carpools and vanpools.
 - b. Bicycle parking and storage as required by this chapter.
 - c. Bicycle commuter amenities including shower and changing facilities.
 - d. Maximum parking as required by this chapter.
 - e. Carshare parking as required by this chapter.
 - f. Parking cashout option where employees are given the option to receive a cash payment in lieu of a parking space.
 - g. Monetary incentives for alternative modes, such as subsidized transit passes, bike-share or carpools.
 - h. An on-site TDM coordinator to provide information on non-automobile travel options and coordinate TDM programs.

D. Unbundled Parking

1. **Unbundled Commercial Parking Spaces.** All commercial parking spaces must be unbundled from the cost of a leased commercial space, and the cost of the parking

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

space shall be included as a separate line item in the commercial space lease.

2. **Unbundled Residential Parking Spaces.** Payment for residential parking spaces must be unbundled from the cost of rent or purchase. The cost of the parking space must be included as a separate line item in the unit sale price or rental agreement.

E. Carpools.

1. Non-residential uses shall provide designated carpool/vanpool spaces as shown in Table 2-33.
2. These spaces shall be located closest to the main entrance of the project (exclusive of spaces designated for handicapped) and shall be included in the maximum allowable parking.

Table 2-33: Parking for Carpools and Vanpools

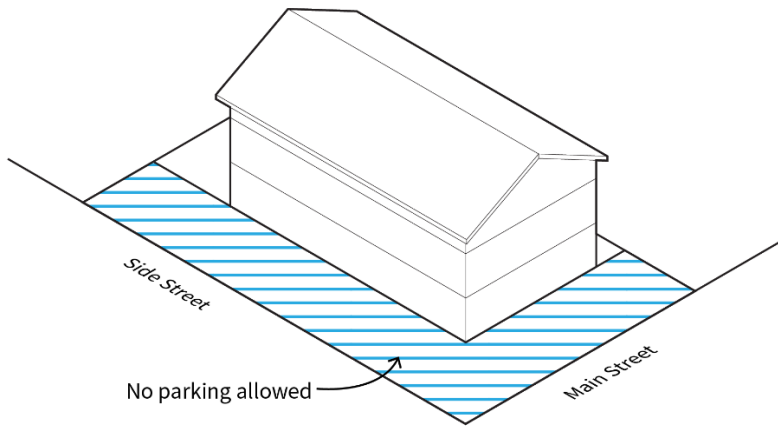
Floor Area of Employment Use	Number of Required Carpool/Vanpool Spaces
Less than 40,000 sq. ft.	0
40,000 sq. ft. or more	1

F. Parking Location and Design

1. **Alley Access.** For lots served by an alley, access to parking must be from the alley.
2. **Curb Cut Frequency.**
 - a. On a single lot, A maximum of two curb cuts for one-way traffic and one curb cut for two-way traffic are permitted per street frontage per 250 feet of lineal street frontage.
 - b. On a development site that occupies a complete block face, a new alley must be established to provide vehicle access. In such a case no other curb cuts are permitted.
3. **Curb Cut and Access Drive Dimensions.** The maximum width of a new access drive crossing a public sidewalk is 12 feet for a one-way access drive and 20 feet for a two-way access drive.
4. **Parking Placement.** Surface parking spaces may not be located in the area between the front and street side property line and a line extended horizontally from the exterior building walls to the edges of the lot. See Figure 2-58.

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Figure 2-58: Parking Placement



5. **Screening.** The perimeter of a surface parking lot facing a street shall be screened with a minimum 3-foot-high evergreen hedge, fence or wall. Fences must be at 75 percent opaque.

G. Bicycle Facilities

1. **Bicycle Parking Space.** Bicycle parking must be provided as shown in Table 2-34. Bicycle parking shall be designed and located consistent with Land Use Code Section 9.36.060.B.

Table 12-34: Bicycle Parking Spaces Required

Land Use	Number of Required Bicycle Parking Spaces
Neighborhood-serving commercial uses (e.g., restaurants, retail, personal services)	1 per 500 sq. ft. for first 5,000 sq. ft, then 1 per 1,000 square feet
Professional Office, R&D and other employment uses	1 per 500 sq. ft. for first 5,000 sq. ft, then 1 per 1,000 square feet
Other nonresidential uses	1 per 1,000 sq. ft. for first 5,000 sq. ft, then 1 per 2,000 square feet
Multifamily Residential	1 per unit

2. **Bicycle Parking.** The number of bicycle parking spaces required by Section 9.36.060 (Bicycle Parking) shall be calculated using the number of vehicle spaces required by Table 3-6 in Section 9.36.040.A (Parking Requirements by Land Use).

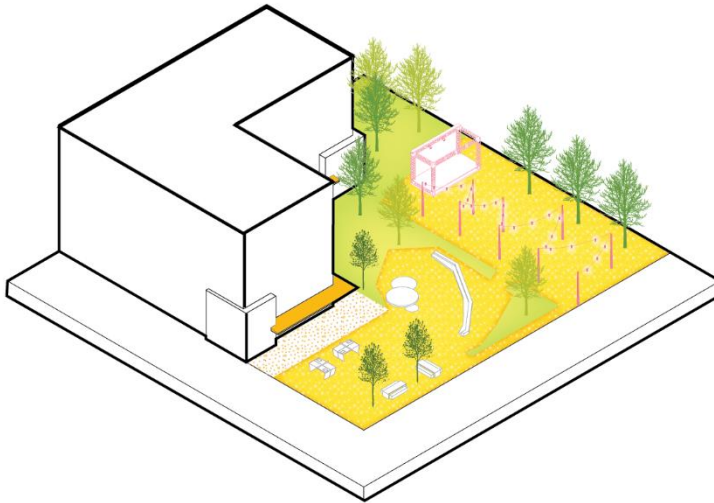
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9.29.090 – Open Space

A. Purpose. This section identifies requirements for open space and conservation in the Gateway districts. Standards apply to four types of open space as identified in the Gateway Area Plan: community square, linear park, privately owned publicly accessible open space, and passive open space.

B. Community Square.

1. **Definition.** A community square is a formal area with focused landscaping and hardscape for civic purposes and commercial activities, spatially defined by building frontages, and located at the intersection of important streets or pedestrian paths. See Figure 2-59.

Figure 2-59: Community Square**2. Barrel District Master Plan.**

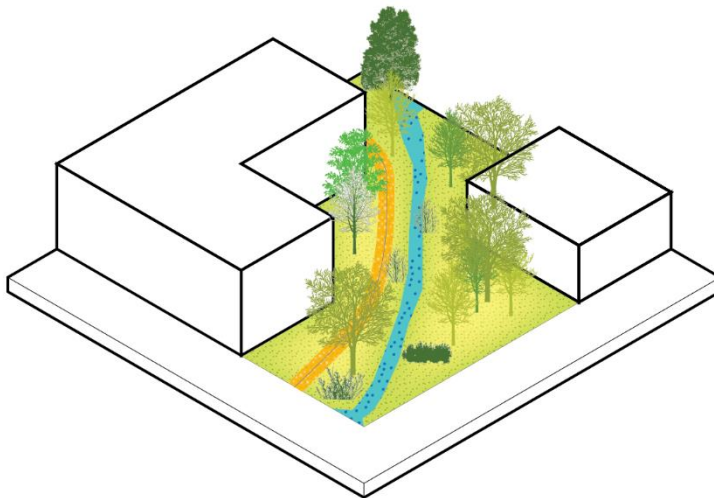
- a. The Barrel District Master Plan must include a community square within the Barrel District. The community square design in the approved final Master Plan must:
 1. Support civic and commercial activities such as farmers' markets, concerts and art fairs; and
 2. Serve as a flexible gathering space for all ages and abilities.
- b. The minimum size of the Barrel District community square is 0.50 acres.
- c. The community square design in the Master Plan must include the following:
 1. Street frontage on at least two sides.
 2. Retail, restaurants, and other active ground floor uses fronting the square.

3. If buildings front the square, active ground floor uses with such as outdoor seating.
4. Street trees along adjacent sidewalks.
5. Orientation to maximize solar access.
6. Traffic-calming features such as crosswalks, pedestrian-oriented intersections and landscaped buffers.
7. Furnishings such as benches, chairs, tables and drinking fountains.
8. Awnings and coverings, either attached to buildings or freestanding, to provide weather protection.
9. Features that encourage and accommodate public gathering, such as pavilions, kiosks, bandstands, and public art.
10. Lighting sufficient for nighttime use.

C. Linear Park

1. **Definition.** A linear park is a linear space for community gathering, strolling and access to nature that provides a green connector between destinations. See Figure 2-60. Linear parks may include Class I trails, Class IV Bikeways, recreational or fitness equipment, and other amenities available for public use.

Figure 2-60: Linear Park



2. Location.

- a. A linear park is required within the N Street right-of-way north of 11th Street.
- b. Linear parks may also be provided in other locations, such railroad rights-of-way, unused City-owned public rights-of-way, and parcels dedicated and floodways.

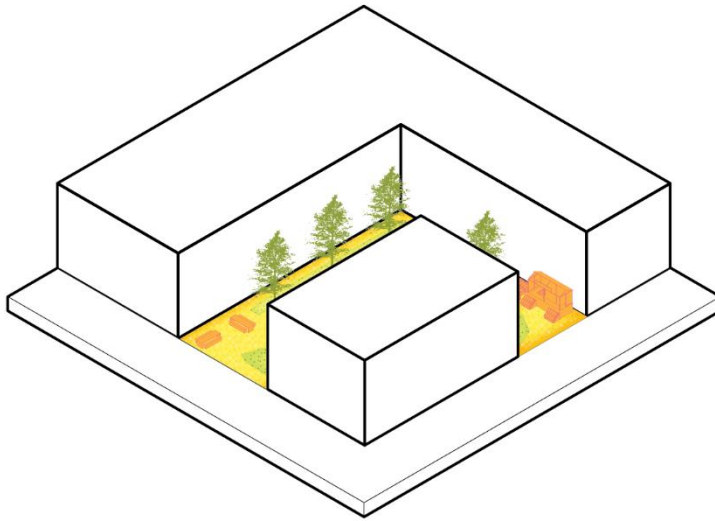
- 3. **Standards.** Linear parks shall conform with the following standards:
 - a. **Minimum Width:** The right-of-way width within which the park is located or 15 feet from the edge of riparian setback, whichever is greater.
 - b. **Natural Resource Protection.** The development and maintenance of a linear park shall comply with all applicable City riparian habitat and natural resource protection regulations.
 - c. **Lighting.** Bollards with integral lights or pedestrian scaled lights shall be placed along the linear park for visibility and security.
 - d. **Amenities.** Seating, bike racks, trash receptacles, and other pedestrian amenities shall be placed along the linear park.
 - e. **Special Uses and Accent Elements.** Special uses or accent elements should be placed along the length of the linear park such as public art, umbrellas and overhead structures, bike/scooter parking, recreation/activity elements, and group seating.
 - f. **Easements.** For linear parks on private property, public access easements are required to ensure permanent public access.
 - g. **Maintenance.** For linear parks on private property, all improvements shall be maintained by the property owner.
 - h. **Landscaping.** Linear park landscaping shall consist of unobstructed lawns, planting beds, trees and/or drought tolerant landscape as follows:
 - 1. Trees shall be arranged naturalistically and provide shade for trails that pass through.
 - 2. Hardscape shall be minimal and only in support of providing access with sidewalks and peripheral connections.
 - i. **Visibility.** Entrances shall be clearly marked and provided on both sides of the linear park.
 - j. **Street Crossings.** Where a linear park crosses over streets, Crosswalks, signage and other traffic-calming features shall be provided at those locations as required the City.

D. Privately Owned Publicly Accessible Open Space

- 1. **Definition.** Privately owned publicly accessible open spaces are courtyards, plazas, paseos, and other similar spaces open to the public and provided as part of new development on property that remains under private ownership. See Figure 2-61.

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Figure 2-61: Privately Owned Publicly Accessible Open Space



2. Amount of Open Space Required.

- a. Within the “private open space” area shown in Gateway Area Plan Figure 7, a project participating in the community benefits program must either:
 - (i) Provide publicly accessible open space in the amount shown in Table 2-35; or
 - (ii) Pay in-lieu fees to be used by the City to construct off-site public open space.

Table 2-35: Publicly Accessible Open Space Requirement

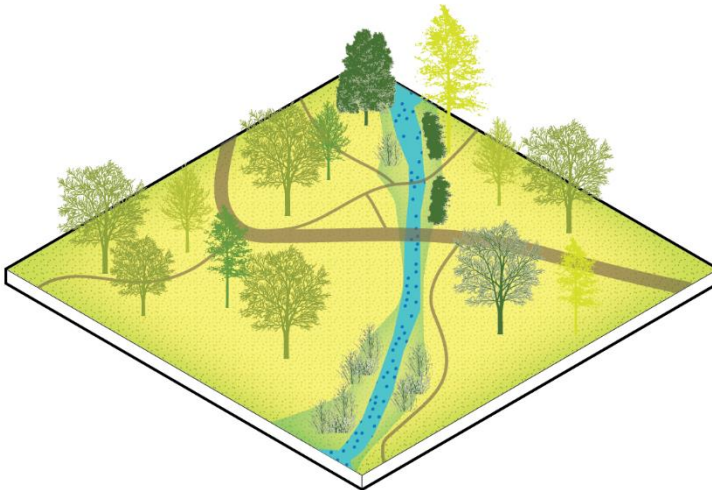
Site Area	Open Space Required (percent of site area)				
	Base – 4 stories	Tier 1 – 5 stories	Tier 2 – 6 stories	Tier 3 – 7 stories	Tier 4 – 8 stories
Less than 30,000 sq. ft.	None	None	10%	12.5%	15%
30,000 sq. ft. or more	None	7.5%	12.5%	15%	17.5%

- b. Outside of the “private open space” area shown in Gateway Area Plan Figure 7, providing publicly accessible open space is optional for a project participating in the community benefits program. To receive credit through the program, a project must provide publicly accessible open space in the amount shown in Table 2-35.
- 3. Minimum dimensions.** Open space shall have a minimum average dimension of 30 ft. in two opposing directions.

4. **Accessibility and Visibility.** Open space shall be directly accessible and visible from a public right-of-way, shall be at ground level and open to the sky, except as permitted in Paragraph 10 (Awnings and Other Coverings).
5. **Lighting.** Illumination levels in open spaces are required to maintain one horizontal foot candle across all walkable and seating areas in the open space, and along sidewalks adjacent to the open space. Lighting shall comply with Section 9.30.070 (Outdoor Lighting).
6. **Connection with the Sidewalk.** Where open spaces front onto a street, they shall connect to the sidewalk at grade level. Minor changes of elevation of no more than 2 feet are permitted within the first 15 feet back from the edge of sidewalk. Changes of elevation of no more than 4 feet are permitted, provided that the elevated area is located at least 15 feet from the sidewalk. Sunken plazas shall be no more than 18 inches below the street level. All connections must meet federal ADA guidelines, local accessibility codes, and building code.
7. **Placement of Elements Along Sidewalk Frontage.** At least 50 percent of the linear sidewalk frontage of the open space must be unobstructed by fixed elements, including walls or planters higher than 36 inches, fixed trash receptacles or elements that are permitted elsewhere in the open space. This zone of unobstructed open space shall extend back from the property line a minimum of 15 feet. Seating, including fixed seating, is permitted in this zone.
8. **Active Uses.** In order to activate and enliven open space areas, the following shall be required on sites with ground-floor non-residential uses.
 - a. No less than 50 percent of building frontages adjacent to the open space shall be composed of active uses, as measured in a linear direction along the perimeter.
 - b. Active uses, open spaces and entries shall be oriented to the open space.
 - c. Active uses are permitted to spill out into open space if they provide seating and shading.
9. **Open Space Furniture and Other Elements.** Open space furniture and other elements are permitted to occupy up to a maximum of 40 percent of the area of a plaza or open space. Allowable features include such items fixed or movable seating, plantings, lights, signage and trash receptacles.
10. **Awnings and other Coverings.** Permanent coverings associated with buildings, including awnings and bridges, and/or freestanding canopies, such as band shells, shall not cover more than 50 percent of the square footage of the open space, and shall have a minimum clearance of 8 feet. If overhanging a fire access lane, minimum clearance for coverings shall be established by the Building Code.
11. **Prohibited Elements:** The following shall not be permitted in or directly adjacent to open space.

- a. Building mechanical systems shall not be exhausted within or at the perimeter of open spaces. Mechanical intakes on adjacent building walls shall be installed at a minimum height of 15 feet above the open space grade.
 - b. Garage entrances, driveways, parking spaces and loading docks.
 - c. Trash or other solid waste storage facilities.
 - d. Service entrances, utility access, or other similar features.
12. **Residential Entries.** Entries to individual residential units are permitted in open spaces, if they are recessed by at least 5 feet from public or publicly-accessible private walkways and sidewalks.
 13. **Ground floor Windows.** Glazing on the ground floor, where provided for non-residential ground-floor uses, shall be transparent and non-reflective.
 14. **Fences, Walls and Hedges.** Fences, walls and hedges within open spaces are permitted with a maximum height of 36 inches.
- E. Passive Open Space,**
1. **Definition.** Undeveloped lands left in a natural state for conservation or outdoor recreation. See Figure 2-62.

Figure 2-62: Passive Open Space



2. **Location.** Existing protected wetlands and riparian areas throughout Gateway districts, both public and private.
3. **Standards.**
 - a. The development, use, and maintenance of a linear park shall comply with all applicable City, state, and federal natural resource protection regulations.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

- b. Passive open space shall protect and manage unique or significant natural features, such as rivers and streams, wetlands and marshes, environmentally sensitive areas, and wildlife habitats.
- c. Passive open space may allow for wildlife viewing, environmental interpretation and education, and nature photography.

9.29.100 – Community Benefits

- A. Purpose.** The community benefits program allows applicants to incorporate community benefits into proposed development projects in exchange for increased development potential and by-right approval. This program is intended to facilitate the production of new multifamily and mixed-use residential development with amenities that benefit the general public.
- B. Eligibility.**
 - 1. Multifamily and mixed-use residential projects in the Gateway districts may participate in the community benefits program. To be eligible for incentives, at least two-thirds of the total floor area of a mixed-use residential project must be occupied by a residential use.
 - 2. An eligible project must:
 - a. Meet the minimum density requirement shown in the development standards tables in Gateway Area Plan Table 7);
 - b. Satisfy Arcata’s inclusionary housing for the Gateway districts; and
 - c. Comply with all applicable development and design standards in this chapter.
- C. Tiers.**
 - 1. The community benefit program utilizes a tiered incentives system where projects that provide higher levels of community benefits are permitted greater intensity.
 - 2. Table 2-36 shows which incentive tiers are available in each of the Gateway districts.

TABLE 2-36: COMMUNITY BENEFIT PROGRAM TIER AVAILABILITY

District	Tier 1	Tier 2	Tier 3	Tier 4
Gateway Barrel (G-B)	Yes	Yes	Yes	Yes
Gateway Hub (G-H)	Yes	Yes	Yes	No
Gateway Corridor (G-C)	Yes	Yes	No	No
Gateway Neighborhood (G-N)	Yes	No	No	No

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

D. Incentives.

- 1. **Increased Height.** The maximum allowed building height of an eligible project providing community benefits is shown in the development standards tables in Section 9.29.040 (District Standards).
- 2. **By-Right Approval.** An eligible project providing community benefits shall be approved by-right with a Gateway Ministerial Permit. A Use Permit, Design Review, or other discretionary City approval is not required.

E. Benefits

- 1. **Available Benefits.**
 - a. To be eligible for incentives identified in Section 9.29.100.D (Incentives), a development project may select community benefits in the “Gateway Code Available Community Benefits” adopted by City Council resolution.
 - b. Point values assigned to each benefit provided are identified in the Available Community Benefits document.
- 2. **Points Required.** The minimum number of points required for each community benefit tier are identified in the Available Community Benefits document.
- 3. **Limitations.** The City may grant incentives only when the community benefits offered are not otherwise required by the Municipal Code or any other provision of local, state, or federal law.
- 4. **Timing.** Community benefits must be provided:
 - a. Prior to issuance of building permit for the payment of fees; and
 - b. Prior to final inspection for the construction of improvements.

Attachment: A. Gateway FBC 6.5.23 (4214 : GP #XVII FBC & GP wrap-up)

General Plan Update Discussion Guide
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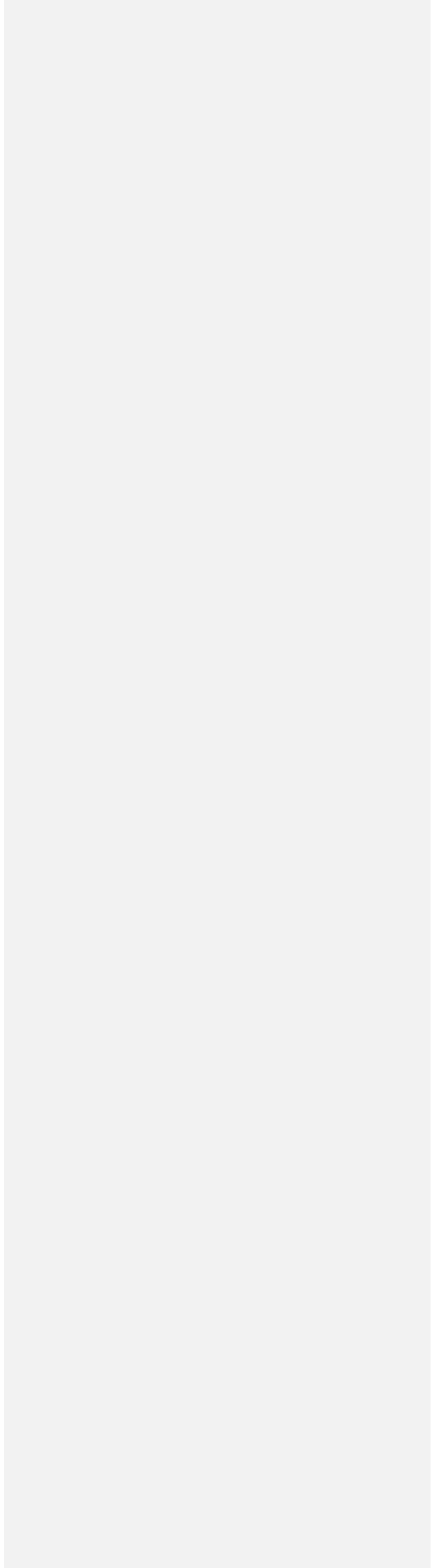
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Commissioner Compilation for June 27, 2023

Gateway Code

- 1) **Subsection 9.29.020.B.5.c.(1) – Appeals** Change “Any person may file an appeal” to “Any resident of Arcata may file an appeal.” [Staff: cannot limit to residents of Arcata.]
- 2) **Subsection 9.29.020.C.1 – Design Review** Change “Design review is required in the Gateway districts if both of the following apply:” to “Design review is required in the Gateway districts if either of the following apply:” (emphasis added). [Staff: this would exclude projects that we likely want to allow. If it were both, then projects that do not meet the objective standards, but are not required by 9.72.040 to have Design Review would not have a permitting pathway. As written, this allows the DR authority to approve a single-family addition, for example. These don’t meet the Gateway Code objective standards, and they are not required to do DR. Therefore, we would have to deny a minor remodel of a SF home with the proposed language.]
- 3) **Subsection 9.29.020.D.1 – Gateway Use Permit** Change “A Gateway Use Permit is required in the Gateway districts for uses listed in Section 9.29.030.B (Gateway Use Permit Required) or if both of the following apply:” to “A Gateway Use Permit is required in the Gateway districts for uses listed in Section 9.29.030.B (Gateway Use Permit Required) or if either of the following apply:” (emphasis added) [Staff: This would expand the allowable uses to almost anything – even disallowed uses (or at least it conflicts with the current disallowed uses as applied). We have more control over what new uses are allowed through the similar and compatible interpretation. This proposed change would put it in the hands of the applicant to decide. We would have a hard time denying a project if we left it to the eligibility requirement for an application. Also, the table becomes moot if D.1.a stands on its own.]
- 4) **Subsection 9.29.020.D.3 – Gateway Use Permit**
This subsection reads as follows: “Procedures for a Gateway Use Permit are the same as in 9.72.080 (Use Permit and Minor Use Permit) except as specified in Paragraph (d) below.”
My comment/question: What is the “Paragraph (d)” mentioned above? Is this a typo, and it is meant to referred to section 9.29.020.D.4(d)? [Staff: good catch. Yes. That should be “4.”]
- 5) **9.29.050.E. Landscaping**
Proposal: Define “landscaping” to require native trees, shrubs, plants, etc. wherever landscaping is required or permitted.
- 6) **9.29.050.G. Inclusionary Zoning**
Consider adding an in-lieu fee option to providing on-site inclusionary housing units. Also, consider allowing construction of off-site inclusionary units with the Gateway Plan Area, with appropriate approval to ensure equivalence with on-site units.
Per the Western Center of Law & Poverty, most California jurisdictions range from 10-20% inclusionary housing requirement. What can we do to maximize the likelihood of hitting this range?
- 7) **9.29.060 Building Design Standards**
 - 9.29.060.D. – **Facade Articulation**: Consider requiring 4 or more (or some number more than 2) of the design treatments to break up building massing.
 - 9.29.060.G.2.a. **Ground Floor Frontages: Standards. Openings Required - All Uses**: Consider requiring openings every 20-25 feet.
 - 9.29.060.H.2.c. **Windows**: Proposal: prohibit mirrored and highly reflective glass at all levels of a building, not just the ground floor.

- 8) **9.29.080.B.1. and Table 2-32. Mobility. Parking. Number of Spaces.** Proposal: Eliminate parking minimums. Let the market decide. See, e.g., *How Parking Destroys Cities*, Michael Manville, *The Atlantic Monthly*, May 18, 2021. **[Staff: many comments have been made to this effect. The TSC recommends removing parking minimums citywide – see Mobility Element]**
- 9) **9.29.050.G. Inclusionary Zoning** Consider adding an in-lieu fee option to providing on-site inclusionary housing units. Also, consider allowing construction of off-site inclusionary units with the Gateway Plan Area, with appropriate approval to ensure equivalence with on-site units. Per the Western Center of Law & Poverty, most California jurisdictions range from 10-20% inclusionary housing requirement. What can we do to maximize the likelihood of hitting this range? **[Staff: The IZ requirements should be lower than the state density bonus thresholds. If not, every project becomes a density bonus project, and our design standards and community benefits are impacted. See <https://www.meyersnave.com/wp-content/uploads/California-Density-Bonus-Law-2021.pdf> and the staff report for March 27, 2023, for a summary of density bonus law and the Gateway Plan. Commission could raise the IZ requirements to 4% Very-low income and 9% Low or Moderate income to be just below the density bonus threshold. As for in lieu fees, this would run counter to the desire to have mixed-income neighborhoods and buildings. Requiring on-site affordable housing is one of the most effective ways to combat gentrification.]**
- 10) **9.29.100 – Community Benefits**
- Question: Where is Table 7 – Development Standards? ? **[Staff: in the Gateway Area Plan. We could pull these into the Code if this is confusing.]**
- General comments re residential density:
- Proposal: Eliminate any maximum residential density. Allow unlimited residential density subject, of course, to other codes. Consider a minimum floor area for residential units, but keep it low to maximize the number of units and reduce costs per unit. **[Staff: there is no max density currently proposed]**
 - Finally, because a form-based code would be new to Arcata, we have no history here to accurately assess its potential impacts on our community; we can only make predictions, based on information from staff, consultants, our own research, etc. Given the uncertainty of the effects of a form-based code on our community, I suggest adding a provision along the following lines:
 - “Two years after the effective date of this Chapter 9.29, or six months after the issuance of a certificate of occupancy for the first project approved pursuant to this Chapter, whichever comes last, and then every two years thereafter, the Planning Commission shall undertake a review of this Chapter and determine whether to recommend that the City Council amend, modify or delete, in whole or in part, any of its provisions.” **[Staff: While unnecessary because the Council and the Commission can undertake a review and initiate a code amendment at any time, there is no reason this could not be added.]**
- 11) *I propose we only allow ministerial permit hearings to be continued once. (page 4)*
- a. *The hearing may be continued only if additional information is needed to determine project conformance with objective standards. A hearing may only be continued a maximum of three one times after which the review authority must render a decision. (page 4)*
 - b. *Currently hearings can be continued 3 times for a total of 4 meetings on what should be a ministerial, check the box process. If the developer is missing key information at the first meeting I would grant them 1 meeting to come back with that information. But if they are proposing something very different than the objective standards in the form based codes that requires 4 meetings of discussion, they should really go through the discretionary review process.*
- 12) *I propose we reduce street facing setback minimums by at least 10ft. (Tables 2-21, 2-23, 2-25, 2-27)*

- a. We have a setback minimum + a “pedestrian realm” minimum creating a very large area that can’t be built on. This will unnecessarily deter development and housing production. The pedestrian realm requirements adequately provide a setback on their own.
- 13) I propose we remove the interior and rear property line setbacks requirements for the G-N district. (Table 2-27)
- a. These setbacks assume that the neighboring areas will perpetually be exclusionary single-family neighborhoods. We shouldn’t unnecessarily burden new housing construction.
- 14) I propose remove all the remaining parking minimums. We should not be mandating parking in the Gateway area. (Table 2-32)
- 15) I propose we change minimum building heights to 3 stories. (Tables 2-22, 2-24, 2-26, 2-28)
- a. Encourages density.
- 16) I propose we apply minimum heights to both residential and commercial uses. This will encourage more mixed use development/ a more consistent building form. (Tables 2-22, 2-24, 2-26, 2-28)
- a. Adding new residential units on top of existing commercial buildings is often impossible. This policy would hopefully encourage new commercial buildings to add residential units at the time of construction which would save us trouble later on.
- 17) I propose we increase the minimum residential density above 25 units per acre (page 2)
- 18) I propose we require a minimum lot coverage and a minimum residential density rather than letting developers achieve eligibility through either means (page 2). This could be accomplished with the following text change.
- a. The project must provide housing, either as a standalone residential or mixed-use project. For mixed-use projects, residential uses must either:
- 19) I propose we eliminate enhanced upper story setback requirements. (page 23)
- a. We heard from David at last meeting that these requirements are particularly costly to developers and could make projects infeasible.
- 20) I propose we prohibit garage doors from facing public streets in all situations. Garage doors, if any, should open onto alleyways. (page 40)
- a. Garage doors on public streets decrease pedestrian and bicycle safety and comfortability. We should be prioritizing Gateway streets for non-vehicle forms of transportation. If garages are necessary, their entrances can be on an alley that will not impede non-motor transportation.
- 21) I propose we do not tie bicycle parking requirements to vehicle parking requirements. Particularly given the fact that I have also proposed that we reduce or eliminate all vehicle parking requirements (page 50)
- 22) I propose that the employment and residential use bike parking requirements listed in Table 2-34 (mis-labeled “12-34”) be clarified as requiring long-term, weather-protected, and secure parking spaces, and that a smaller number of short-term bike parking spaces also be required for these uses. (page 50)
- a. Long-term, secure, and covered bike parking is absolutely essential for encouraging bicycle use.
- 23) I propose we allow a community square that does not have car access. (page 51)
- a. The plan currently calls for the community square to have street frontages on at least 2 sides. I don’t think we have to mandate that the community square be accessed by vehicles.
- 24) I propose we rename the sub-districts within the Gateway Area in consultation with the Wiyot Tribe (see Section 9.29.010.B).
- a. We’ve heard this request from multiple members of the public. This would be a good reminder of the Indigenous people of Arcata.

- 25) I propose we change the wording of the non-residential transportation demand management to note that the list is not exhaustive, and also that the TDM plan requirement cannot be met solely with measures that are already required elsewhere in the code. (page 48)
- a. Having a set list unnecessarily limits this great policy.
- 26) I propose we add a maximum street tree spacing
- a. Section 9.29.070.B.4 implies that street trees are required, but provides only a minimum spacing (not a maximum). With no maximum spacing, a developer could conceivably meet the requirement with a single tree. (page 44)
- 27) I propose we increase the inclusionary zoning requirements and also have them begin at 20 rather than 30 units. I also think we may want to consider a second, higher tier for particularly large developments.
- a. Inclusionary zoning and deed restricted affordable units are an important component of the gateway plan
- 28) I propose we suggest to City Council that they pass a resolution (similar to what they are doing with the Community Benefits Program) that allows them to change these numbers more easily than changing the gateway code itself.
- a. This will allow the city to turn this dial depending on its impacts on development and affordability.
- 29) **Recommended change:** Gateway District boundaries should NOT straddle Coastal Zone boundary. This recommendation was previously made with regard to the text of the GAP, BEFORE the current GAP review protocols.
- 9.29.010- Introduction B. Gateway Districts. 1 and 2:
The Gateway Area is divided into ~~five~~ **four** form and design districts: Gateway Barrel District (G-B), **Gateway Barrel COASTAL District (G-BC)**, Gateway Hub (G-H), Gateway Corridor (G-C), and Gateway Neighborhood (G-N). These four districts are collectively referred to in this chapter as the Gateway districts.
2. The boundaries of the Gateway districts are shown in the City's Zoning Map established by Sec. 9.12.020. **[Revise boundaries so no boundary straddles Coastal Zone boundary] [Staff: this recommendation is incongruent with the LCP draft to date. The LCP will be an overlay zone. The base zone will be the existing citywide zoning. Staff recommends the Gateway follow the same zoning established elsewhere in the Coastal Zone.]**
- 30) **Recommended change:** The language suggested reflects staff's insistence that the FBC would NOT prevent existing property owners from continuing viability of non-residential property uses in the Gateway, or maintaining and reasonably improving their properties to support continuing non-residential use, even though the primary purpose of the GAP is to promote residential development, and support conversion of under-utilized Industrial properties to residential use:.
- 9.29.010- Introduction B. Gateway Districts. 3:
3. This chapter is intended to allow for continued use and improvement of **residential** uses existing in the Gateway districts at the time of code adoption. **Such Residential** uses are exempt from the requirements of Chapter 9.60 (Nonconforming Uses, Structures, and Parcels. **Non-residential uses existing at the time of code adoption will be subject to Chapter 9.60 relating to Nonconforming Uses, Structures, and Parcels. [Staff: this would conflict with other parts of the code re: Gateway Use Permit uses and would make existing legal uses non-conforming and unable to expand or rebuild. Staff cautions against this approach. The current provisions were based on community feedback. Many businesses in the Gateway are concerned that they will lose their ability to grow with the community if they are considered non-conforming.]**

31) **Recommendation:** *By-right approval – The Planning Commission did agree in principle that a tiered review approach to ensure that the public has some role, and that the project conforms to objective standards should apply, but we have NOT agreed on size or other tiering break-points. The Planning Commission has still NOT addressed the specifics of Ministerial Permit “by-right” approval with regard to the Gateway Plan, and a broader discussion should take place before the Planning Commission recommends this text for adoption. This discussion should extend to a Planning Commission study session with the City Council. I strongly suggest:*

Table 2-19: Gateway Ministerial Permit Requirements

Project Size	Review Authority	Public Notice	Administrative Hearing
New floor area less than 10,000 sq ft and/or building height less than 37 ft	Zoning Administrator	Notice of application and Notice of Administrative Decision	No, with appeal possible to Planning Commission
New floor area 10,000 sq ft to 30,000 sq ft, and/or building less than 37 ft	Planning Commission	Notice of application for Ministerial Approval and Notice of Administrative Hearing	Yes
New floor area 30,000 sq ft to 40,000 sq ft and/or building height 38 ft to 47 ft	Planning Commission	Notice of application for Ministerial Approval and Notice of Administrative Hearing	Yes
New floor area over 40,000 sq ft and/or building height over 47 ft	Planning Commission	Notice of application for Ministerial Approval and Notice of Administrative Hearing	Yes

32) **Recommendation: d. Public Notice.** *Add the following language, in red, to accommodate public notice of an application for Ministerial Approval, which staff’s version does not provide or accommodate. This would provide members of the public with sufficient notice so that they, too, can review the details of an application prior to both Planning Commission and Zoning Administrator’s determination of whether the application appears to meet objective standards. The draft FBC provides details of the Administrative Hearing involving only the Administrator, but the City would still need to determine appropriate format for the Planning Commission’s consideration. While approval could still be Ministerial, the public will have better and earlier access to the information on which a determination will be based:*

1. Public notice shall be given for projects as shown in Table 2.19 .
2. Notice of application for Ministerial Approval, when required, shall contain the following information:
 - (a) The date of filing of the application and the name of the applicant
 - (b) The City’s file number assigned to the application.

- (c) A general description of the proposed project, including the project location
- (d) The date the application's compliance with objective standards required for Ministerial approval may first be considered for Planning Commission agenda public administrative hearing, as required and shown in Table 2.19

3. Notice of administrative decision, when required... [Staff: the Gateway Code also provides process for noticing and review procedures for Zoning Administrator and the Planning Commission. See 9.29.020.B.4.e and Table 2-19. The red text was incorporated in the draft code]

33) Recommendation: 9.29.040 – District Standards, A. Gateway Barrel District (G-B). *As previously recommended, the Barrel District boundaries should NOT straddle the Coastal Zone. Even if the Gateway code standards are largely similar for areas of the Draft's Barrel District are largely similar within and outside the Coastal Zone, the Planning Commission should discuss and address separate standards for coastal zone areas and those outside the Coastal Zone. The risk of adopting the proposed standards in a district that straddles the Coastal Zone is that the City may have trouble imposing those standards anywhere in that portion of the Gateway Area. The requirement for a Master Plan in a major portion of the Coastal area helps, but still doesn't answer this concern. [Staff: see comment above.]*

34) Recommendations: Barrel District Master Plan requirements

These requirements should refer to the standards related to revised district boundaries to separate out the Coastal Zone of the Gateway Area:

- *Given its size and importance, the Master Plan should not be subject to Ministerial approval. It's unlikely that CA Coastal Zone standards would permit Ministerial approval in any case!*
 - *Ideally, a full site development and master design plan should be required, but if the developer cannot provide it as part of an early "Master Plan" proposal, then AT LEAST the following (below)*
 - *Several text areas require a community square. It makes more sense, and provides greater design flexibility, to indicate a contiguous area, without locking the design to a square configuration.*
 - *Recommended changes marked in Red, below:*
- b. Master Plan Contents. The Barrel District Master Plan **must** contain **a detailed site plan including maps and, graphics, as well as and** text that identifies the following:
- (1) Natural resource protection areas to remain as permanent open space **with appropriate management plan.**
 - (2) Publicly accessible open space, including a new community **square gathering contiguous area of at least XX? acres.**
 - (3) The location and design of new streets, greenways, and emergency vehicle access facilities.
 - (4) The placement of buildings and surface parking.
 - (5) Planned land uses, ~~if known.~~
 - (6) Building heights **and bulk.**
 - (7) Requested modifications to building design standards in Section 9.29.060 (Building Design Standards), if any. **[Staff: These are good suggestions. This would be on consent if not for the statement that the barrel district should be separated into coastal and non-coastal areas. As for ministerial approval, if the proposal does not have exceptions from the code, staff recommends this master plan, which is non-regulatory, be a ministerial approval at the Planning Commission level. If they seek exceptions, it would require a Gateway Use Permit.]**

35) Recommendations: B. Gateway Hub

These comments relate to Table 2-23 and 2-24, and Figure 2-29, and the accompanying text:

- The Gateway Hub proposed standards still have not taken into account strong public recommendations to consider L Street as part of a linear park, or to retain L Street as a pedestrian and cycle oriented corridor, rather than as part of an L/K Street one-way vehicular couplet. The Planning Commission and City Council SHOULD incorporate design standards appropriate to an L-Street linear park and pedestrian / bicycle corridor alternative.
- Re Table 2-24: The step-back requirement should begin at the 3rd Story, not the 5th story
- Maximum building length should be no longer than 150 feet for Ministerial approval. (It may still be possible to build longer buildings with appropriate visual modulation or articulation but not for Ministerial approval, especially to avoid creating future inflexible “white elephant” mega-structures.)

36) Recommendations for C. Gateway Corridor:

These recommendations address Table 2-25 and Figure 2-26 , and the accompanying text.

Recommended changes are marked in **Red**.

- Language encouraging driver distraction is NOT a recipe for pedestrian safety! Moreover, addressing language to encourage drivers to park and walk assumes there are sufficient places for them to park!

Purpose. The G-C district accommodates active, inviting, high-intensity, mixed-use development along major **pedestrian, bicycle** and vehicular gateways into the City. Pedestrian-oriented ground floor design aesthetic **enhances pedestrian, cyclist, stationary, and slow-pace experience. helps to slow passing vehicular traffic and encourage drivers to park and walk.**

- Consider a 15-foot or greater setback requirement for “Non-active” building frontages on for larger and higher buildings especially where a Zero-setback risks blocking sunlight to lower stories of adjacent buildings, or outdoor land uses. (The solar access review requirements are not sufficient for this purpose, as they are geared toward power and energy considerations, rather than more simply preserving neighbors’ access to light and sunshine.) A Zero-setback could still be allowed if applicants can demonstrate that their developments would not unduly block neighbors’ light, but Ministerial approval would not be available for Zero-setback very large or very high buildings.

37) Recommendations for D. Gateway Neighborhood:

These recommendations address Table 2-27 and Figure 2-33, and 2-34 , and the accompanying text.

- Minimum setback requirements in this district should be similar to those in the rest of Arcata, especially for buildings intended for non-residential uses, and for bulky buildings which could shade-out residences. (Exceptions could be made for multi-parcel developments being developed together, where shading of new uses is anticipated as part of the development, however such multi-parcel developments may not necessarily qualify for Ministerial approval.)
- Building massing: Stepback requirements should start at 3 stories. No single structure in the Neighborhood district should be longer than 150 feet, even with visual modulation or articulation. (Larger, multi-parcel, or institutional developments may link separate structures through covered walkways, etc.) Again, this will prevent the creation of future “white elephants.” Exceptions may be made, but not for Ministerial approval.

38) Recommendations for 9.29.050 – Supplemental to Districts considerations:

- In Figures 2-36, 2-37, and 2039: Active Building Frontage Type Required Location (and related text), eliminate the “active building frontage” requirements along L Street and on corners that include L Street. Ground-floor at-grade access, large windows, and narrow setbacks could still be possible, but would not be required, in keeping with the possibility of developing an L-Street linear park.
- The Figure 2-38 Enhanced Upper Story Step Back Location map goes some way toward protecting neighbors’ sunlight, but will ultimately be confusing. ALL bulky buildings should be subject to setback requirements at the 3rd story and above. And, there is little difference between an 8-foot setback and the more effective 10-foot “enhanced” setback from a public perspective.

D. Bird-safe buildings: THANK YOU for this section! See Red for recommended changes:

:

2. **Applicability.** The bird-safe building standards in this subsection apply to new construction where glass or other rigid transparent or highly reflective material occupies 25 ~~35~~ percent or more of the building façade.
3. Standards. a. **Bird-Safe Glazing or highly reflective surfaces.** Any regulated continuous transparent or highly reflective material must meet at least one of the following conditions:
- a. Bird-Safe Glazing.....(2) Patterned Glazing Treatment. Panes with patterns that are etched, fritted, stenciled, silk-screened, or otherwise permanently incorporated into the transparent or highly reflective material

39) Recommendations on E. Landscaping:

The Gateway Code’s landscaping standards should incorporate understanding that “landscaping” is not limited to vegetation, and that high quality landscape design may attractively combine vegetation, aquatic features, and “hardscaping” to enhance human experience and protect or enhance an area’s ecological functions. Thus, the landscape standards in the Gateway Code’s language should address vegetation and hardscape aspects of “landscaping” design together.

“2. Landscaping shall ~~combine consist of any combination of~~ trees, ~~and~~ shrubs, ~~and may include~~ grass or related natural features, such as rock, stone, or mulch. ~~Concrete or paving ground surfaces~~ ~~Non-plant materials~~ may occupy no more than 25 percent of the landscaped area ...”

Ideas for Discussion

1) 9.29.050 – Supplemental to Districts

Figure 2-36: Active Building Frontage Type Required Location

Question: Why are active building frontages only in the area of 8th, 9th, K and N Streets? Should active building frontages be required elsewhere?

9.29.080.A.3. Mobility. Greenways Required: As drafted this code section states in relevant part:

“Greenways are required in the approximate locations shown . . . “(emphasis added). My question: Is the word “approximate” too vague or ambiguous to be legally enforceable? [Staff: they would be designated further in the plan that is called for.]

2) 9.29.050.B. Enhanced Upper Story Step Backs

What analysis was done to derive these step backs? What would impacts be if they were reduced?

Table 2-19: Gateway Ministerial Permit Requirements I support Commissioner Mayer’s suggestions (1) that projects less than 10,000 sf or 37 feet high be subject to ZA approval in the first instance, and those greater than 10,000 sf or 37 feet high be subject to Planning Commission approval in the first instance; and (2) that a notice of application be required for all projects.

- 3) **Recommendations:** 9.29.040 – District Standards A. Gateway Barrel District (G-B). 2. Building Placement: See **Table 2-21 and Figure 2-26** and related narrative standards
- * Please indicate later location in the FBC text for the definition of “Active” building frontages (9.29.050 – Supplemental to Districts A.)
 - * There is NO requirement for any setback whatsoever from either side or rear parcel boundaries. However, a zero-setback standard should best be linked to OTHER design standards as well. Zero-setbacks may reasonably allow for row houses or townhouses up to +/- 4 stories, but 5-7 story apartment buildings SHOULD be set back from side and rear property boundaries, if only to provide reasonable protection from fire and seismic hazards, as well as for light to reach structure interiors. Building massing standards may address this, but that should also be reflected in setback standards and diagrams.
 - * It’s great that the standards allow for courtyards and plazas, but a maximum 50’ setback seems arbitrary, especially if that setback encompasses publicly accessible non-parking/ non-driveway space that provides access to residential or commercial space. If the FBC must indicate some maximum for such setbacks, 100’ would provide both greater flexibility, and the potential for such setbacks to accommodate pleasant and usable courtyards or plazas. If developers DO want zero-setbacks to the sides and rear of structures higher than 4-stories (+/- 47’), those would still be possible, just not approvable with Ministerial permitting.
- 4) **Recommendations/ Questions related to Table 2-22, Figure 2-28,** and corresponding text:
- Previous Planning Commission discussions indicated that the “tiering” for purposes of Community benefits required should begin at 4 stories (+/- as indicated, 50 feet height).
 - Previous Planning Commission discussion preferences indicated that massing/step-back requirements should begin at 3 stories (i.e., anything above 2 stories), not at 5 stories (60 ft height) as indicated in the chart. Those discussions took place before current decision procedures were adopted.
 - Planning Commission and public preferences for maximum building length, especially where any Ministerial approval is possible, have been considerably LESS than 300’ for a single structure! Arcata now has only very few buildings of that length, and those are either Industrial or on the Cal Poly campus. Even design “modulation” at 150’ would not compensate for allowing such mega-structures with Ministerial approval. Segmenting major developments into smaller units will avoid Ministerially approving structures that could become unadaptable “white elephants” in the future, especially under private ownership. **[Staff: not aware that the Commission made decisions on these matters, but these are good points of discussion.]**
- 5) **Question for consideration: Gateway Use Permit.** Table 2-20 indicates Gateway Use Permit Requirements, including for NEW commercial or industrial uses. The Use Permit thresholds indicate only floor area. But what if the uses do not take place in structure interiors, but are OUTDOOR uses? I can imagine this situation with regard to outdoor industrial operations, material or vehicle storage or transfer, or for outdoor event, entertainment, or performance spaces. How would the Gateway Code address such uses that do NOT involve large or permanent structures with related floor area? 5.c might address this in terms of off-site impacts, or an Administrator’s decision that such a use is “similar or compatible” with an allowable use, but this is not clear.
- 6) **Comments on 9.29.060 – Building Design Standards:**
- C. Long Building Division – 2. Standard – I strongly recommend that Ministerial approval should NOT be available to any single structure longer than 150 feet in any dimension. So, the building modulation standards should apply, but a “Long Building Division” standard similar to this should apply for any building longer than 100 feet.

- Roofline articulation – *All of those choices are good to qualify for Ministerial approval, except the roof deck option, which may benefit building users, but a roof deck in itself provides no visual benefit to the public.*
 - Entrances --
“Functionality. Entrances required by Paragraphs (a) and (b) above must remain functional for entry as well as exit and available for use by occupants” . *An “entrance” that only functions as an exit should not qualify as an entrance for this purpose.*
 - Entrances –
“d. Entrances to Individual Units.
(1) For units adjacent to a public street that are accessed through ground level individual entrances (e.g., townhomes), the primary entrances must face the street **or publicly accessible courtyard or plaza.**” *This change may enhance livability, walkability, and safety, especially for young children and elderly residents and passers-by, increasing the usability of publicly-accessible private spaces for all.*
 - Entrances –
“iv. A patio with minimum dimensions of 5 feet by 5 feet. A patio ~~must~~ **may** include a row of shrubs, a fence, or a wall not to exceed 42 inches in height between the sidewalk and the patio to define the transition between public and private space.”
 - Garage doors and entries –
“Shared Garages and Parking Structures. The following standards apply to garages serving multiple dwelling units and/or non-residential uses. (1) No more than 25 percent of the site frontage facing a street may be devoted to garage opening, unless the street frontage is less than 80 feet, in which case a 20-foot garage opening is allowed. **Garage and parking structure entrances with curb cuts are not permitted on K Street or L Street.**” *Other appropriate places for this requirement would be in the Circulation section of the GAP and in this Code’s 9.29.070 – Streetscape. Protecting pedestrian and cyclist safety as well as protecting cycle or motor traffic should prevail where garage entrances/ exits / curb cuts can instead be located on east-west streets or on alleys.*
- 7) **Comments on Streetscape 9.29.070:**
- **Figure 2.53 Pedestrian Realm** *may somewhat misrepresent the area between a building and street areas where motor vehicles are allowed, by showing the frontage/ setback area as paved, when it could (perhaps should) actually be landscaped (e.g., with materials that still allow pedestrians to walk on them), whether it is “Active” or “Non-Active” frontage, while providing access for mobility-impaired people.*
 - **Frontage zone:** *The distinction in terms of what the frontage zone may contain appears to be over-restrictive for “non-active” frontages, as long as those uses do not obstruct movement of people using wheelchairs or strollers. There is little reason why a “non-active” frontage may not also include dining, seating, or outdoor displays. In fact, especially during the recent Pandemic, some highly successful outdoor adaptations have been on “non-active” frontages.*
- 8) **Comments on 9.29.080 Mobility:**
- On **“Greenways”** and **Figure 2-56** -- *The Draft indicates that the Greenways map will be removed from the Gateway Code, and instead incorporated into the GAP. However, this Figure still makes NO provision for an L Street Linear Park, or even a significantly enhanced or shaded L Street bikeway.*

- On **Greenways**: This map, its conceptual basis, and its associated standards must be seriously reconsidered in light of an L Street Linear Park alternative and in close consultation with the Arcata Fire Department.
- On **Parking** -- Offstreet parking standards (Table 2-32): *Significant discussion should address the basis of a policy decision to require NO parking whatsoever for residential or most commercial uses anywhere in the Gateway Area, except for hotels and some offices, while imposing extremely low maximum parking allowances for residential and commercial uses. If the Code's intention is to promote viable residential and commercial development, wishful thinking about the impact of those low maximums may not support desired private investment; no parking minimum requirements at all for residential development may actually scuttle the GAP's mixed-use aspirations, as well as aspirations supporting a variety of housing tenancy and ownership types.*
- **Transportation Demand Management – Non-residential standards** : 10,000 sq ft seems very low for a Demand Management Plan requirement to kick in. That would be the equivalent of four small shops or offices. Such a low-threshold standard would also be difficult to enforce, especially if the non-residential users are commercial renters rather than their landlords, and where the number of employees varies enormously between business users occupying the same extent of building space.
- **F. Parking Location and Design**: *Add after "Alley Access" section: "Site designs for commercial or residential projects that qualify for Ministerial approval may not have primary access for motor vehicles to parking from K Street, L Street, or N Street if access from an east-west street or from an alley is possible. (Emergency access may be from K, L, or N Street).*
- **2. Bicycle Parking**: *It makes NO SENSE to link the number of bicycle parking spaces to the number of required motor vehicle parking spaces, if the GAP intends to encourage cycling and discourage driving!*

9)

Bike Rack

The following document the Bike Rack items that the Commission will resolve as time allows consistent with the Meeting Framework adopted March 14, and amended thereafter. Items shown in grey were discussed at a prior hearing but no decision was made. Items without highlighting have not been discussed.

Land Use Element

Ideas for Discussion

- 10) **LU-1k: Support and revitalize other existing neighborhood and commercial activity areas.** Although not a "neighborhood" center, it would also help to have some explicit mention of Uniontown, especially in light of AB 2011. Uniontown might be a prime target for mixed use redevelopment (and a reasonable one), if not under its current owners, then under some future ownership by 2045.
- 11) **LU-2: Residential Land use** That's real estate-talk. Change that to "residents." The policy refers to "in higher density developments". Clarify: Does that refer to RM and RH only? What about in those mixed use developments we're expecting, and in :PD Planned Developments?

- 12) **LU-2c: Planned Development – residential: Add:** Planned Development may also incorporate non-residential uses *where they will not reduce safety or livability for residents, and must include adequate walkways, and set conditions for commercial operations.* (Avoid a scenario where commercial use is added to a residential :PD and brings dangerous vehicle traffic or constant loud noise into a previously kid-friendly, quiet area.)
- a) The Implementation Measures list calls for the City to review sites in the :PD combining zone, and possibly releasing some of them from :PD requirements. However, new state housing laws already limit City discretion for projects that include affordable housing, and exempt some of those projects from CEQA review. The City should generally retain the discretionary review that the :PD combining zone provides, especially for already developed sites, to ensure that intensified development there does not threaten safety or existing environmental assets and recreational spaces.
- 13) **LU-3a Commercial use classifications** “Large scale retail uses shall require a use permit due to evaluate...” *Can we add a threshold size or scale?*
- a) “Potential impact on existing and projected traffic conditions” – Add: pedestrian and residents’ safety
- b) **Table LU34 COMMERCIAL LAND USE CLASSIFICATIONS:** (Questions mainly)
- c) What is the rationale for adding Travel trailer [RV] parks to principally permitted uses in the C-G zone?
- d) Will eliminating animal sales and services make existing pet stores and veterinary services in C-G non-conforming? Or are these rolled into some larger category?
- e) Add to the “Gas sales” category electric vehicle charging stations? What about zip car rentals, etc?
- f) Restaurants, Bars, Taverns and pubs, nighclubs: Will bars still require a Use Permit? If we’re now allowing on-site cannabis consumption, should these be added to the list?
- g) Commercial Recreation / Entertainment: How come “outdoor recreation uses and services” are NOT allowed in either the C-C or C-M zones? Should they be?
- h) Educational, Cultural & Religious Uses: Since no “Religious Uses” are actually listed, and since the City has limited authority to regulate them anyway, should we take “Religious Uses” off the category title? (AND ... Does the City have discussion / condition procedures set up for when an Arcata church decides XYZ is actually a religious use, and demands services to support it?)
- i) Urban Agriculture: Not allowed in the C-C zone. So, NO herb or vegetable gardens on a temporarily vacant lot downtown? What about as an accessory use? (No commercial herb gardens in backyards and roofs? Or is that allowed under some other rule?)
- j) Commercial – General This is mainly Valley West. With a max residential density up to 50 “units” per acre in addition to commercial uses on the same site (???), with density bonuses likely to allow up to 90 dwellings per acre, what do we envision in Valley West for this allowable density, especially in light of AB 2011?
- 14) **LU-3e Commercial – Central** : Residential use is allowed as the primary use on vacant sites. Presumably, NO maximum density & no parking? Given current vacancy rates, may existing commercial buildings be converted to residential use anywhere in C-C? [Staff Response - I think that is the next step. This could be an implementation measure]
- 15) **LU-4b Little Lake** : The City has sat on cleaning up its Little Lake site for 20 years. There’s some new activity there now. (I’d heard “staging and material storage” for the WWTP upgrades?) The draft policy is: “... The site shall be planned as a mixed-use development including passive recreational

uses and a dog park. Development shall be consistent with the adopted Long Range Property Management Plan.” That plan indicates the site should be used for “economic development,” which presumably means jobs. But the property management plan doesn’t go further than this. *I hope* our Sea Level Rise discussions on Tuesday will help us envision what types of structures could be safely allowed on that site – IF ANY – and strongly recommend against allowing permanent structures, or ANY “mixed use” that includes housing.

- i) Throughout the Plan, let’s replace the term “passive recreational uses” with something that actually relates to land use / infrastructure, like “recreation facilities for walking, running, sitting, nature observation, and social interaction.” It’s more words, but better connotation in our sports-dominated society. [Staff response – no mixed use or residential use is planned on this site]

16) Table LU-6: AGRICULTURE AND NATURAL RESOURCE LAND USE CLASSIFICATIONS

- a) Coastal-dependent recreation in the A-E zone: What would this be? Duck hunting blinds? Kayak docks & rentals?
- b) Keeping confined animals isn’t allowed in the A-R zone. No backyard hen coop? No backyard goat pen? It’s odd that hens are allowed in residential zones but not in an ag zone. It might help to re-state the list of allowable uses to reflect scale of confined animal keeping (I think the LUC does this.)
- c) “Silvicultural operations” and “Aquacultural operations” are not allowed in either agricultural zone. It might make sense to allow tree nurseries and fish ponds, for example, with a Use Permit to protect groundwater and prevent noxious odors.
- d) Farm worker housing policy is clear for diked/ reclaimed former tidelands (**LU-6d2**) but not for other ag lands. Farm worker housing should count as “residential units” and “dwelling units” with standards identical to other housing or ADUs.

17) 2.3 IMPLEMENTATION MEASURES TABLE – This is a bare-bones list, focusing on the near-term, with little except the “ongoing” items and Economic Development Strategic Plan 5-year updates that carries us beyond the first couple of years. It would be great to develop a much more substantial list of implementation actions to achieve the goals of the many policies in the Land Use element.

- a) It would help to include specific implementation measures for Policy LU-1b “Promotion of infill development and designated Infill Opportunity Zones,” if only to identify a time frame for action.
- b) LU-4 Pedestrian-friendly activity centers: These measures are more policies themselves, than specific implementation actions, and will be only parts of the types of form-based standards that would be needed to implement them. Once we have experience with a form based code in the Gateway, would it make sense to include an implementation measure to consider developing appropriate standards for the other activity centers?
- c) LU-5 Business park plans: The city should seriously revisit the “business park” master plan idea for Little Lake, even though the City is committed to putting those 12 acres to some economic use. Developing a site plan for Little Lake: Yes. But let’s reconsider calling it a “business park.”
- d) LU-6 Planned Development Overlay: An inventory of :PD sites will be useful. (See comments above.) But beware of using this review to eliminate City development guidance and discretion as a gift to developers.

18) LU-7 Commercial Visitor Serving Overlay – Is there a rationale for retaining the Visitor Serving zoning designation? It seems the proposed Land Use classification system has already assumed that the

Commercial General classification is appropriate for Valley West, especially as so many of the motels there no longer serve “visitors.”

- 19) **Land Use Designations** We did address the two rezoning proposals that appear to have received the most public attention to date. However, we have not addressed any of the other specific rezoning proposals at any point in our process, other than through the Gateway Area Plan discussions.

[staff] indicated that at the end of our May 9 meeting there will be an opportunity to address the rest of the rezoning proposals, so it would be important to indicate my concerns beforehand. So I’m writing them out briefly:

- a) Exhibit 1: I-L to C-M south of Giuntoli to Boyd Rd:
- b) Since C-M could include residential uses, and there remain several Industrial/ Industrial-Commercial types of uses. Because many of those parcels are quite large, would a Planned Development permit still be required? If affordable housing is included, would there then be no housing density maximum? How would allowable intensities be regulated in C-M in this area? (Or, would we need to wait for a draft amended LUC to figure that out?) What objective design code standards would / could apply for a C-M one in this location? (It would be in the Giuntoli / Valley West “infill opportunity zone”? Or outside of it? HOW would the C-M zone protect possible future residents from lingering contamination from former industrial uses? (E.g.: the easternmost parcels are now the school bus lot and shop; many anecdotes about former practices of history of oil, lubricant, & other chem dumping, etc on the site.) Are any of the current uses actually Industrial, rather than things that could transfer to Commercial in the transition time? (And would they then be non-conforming?)
- c) Exhibit 2: I-G to I-L east of West End Road:
- d) WHY rezone? This appears to be the land behind the wetland back of what’s now the Cannabis Innovation Zone? Last we heard, was this the land proposed for the “eco-burial” site? The Property Report on the cit GIS already lists this as BOTH I-G and I-L. Since the site is already so heavily disturbed, with access ONLY through the adjacent Industrial land, is the purpose of eliminating possible heavier industry there as a buffer to the NR-TP land to the east?? Why not just leave zoning as it is? (Or, is it too ambiguous?)
- e) Exhibit 3: R-VL to R-M lots west of Alliance & south of Spear:
- f) I propose we retain the current zoning in this area, at least until developer(s) or owners actually request rezoning. The area is adjacent to A-E land (agriculture, even when used intensively by Sun Valley’s bulb operation), and within the Coastal Zone. Up-zoning this area now will be, essentially, an invitation to land speculators.
- g) Is the purpose of up-zoning to R-M be to encourage developers to combine parcels to build larger projects? Is the City’s concern that the combination of subdivision and ADUs on existing small landlocked lots make for a continued fragmented residential development?
- h) Realistically, developing any of those small parcels at R-M density would probably require assembling a multiple-parcel project site. That would then be big enough to require a PD anyway, which could effectively increase actual housing density, without also opening the area up to the density bonus requirements above and beyond the face value R-M maximum. The current R-L zoning already would allow ADUs, and SB9 subdivision, effectively increasing the amount of housing the area could provide.
- i) Several parcels at the south end have broad water pipe easements. (Didn’t we approve a PD there a few years ago? It appears that’s never actually been built.)
- j) Many of those interior parcels are “land locked” though they have shared driveways. The northern parcels were only recently rezoned from A-R to R-VL; at least, the City’s 2008 zoning map (online) still shows them as A-R in the Coastal Zone. And the area just to the interior (west,

and south of the parcels proposed for rezoning) is still A-E (coastal), part of Arcata's greenbelt. Rezoning this land to R-M now would effectively dump our longstanding policy of gradual transition from lower density / lower height development to much higher density adjacent to A-E land in western Greenbelt.

- k) Exhibit 6: C-G to C-M for all parcels facing G Street from 11th to 18th ("Northtown"):
- l) This would encourage redevelopment with commercial uses on ground and apartments above, with NO maximum use intensity, and no parking requirements. If this rezoning occurs, we should have "objective standards" in place at the time of the rezone for such intensified development in this area. (A Northtown FBC would be at least as necessary here as one is necessary to the Gateway. That would get us things like appropriate utility connections, wider sidewalks, appropriate upper story design, and limited curb cuts onto our major N-S street. Would any SB 2011 provisions override our C-M?
- m) Exhibit 7: R-H to C-C for two corner parcels on the east side of J Street, at 6th and 8th:
- n) Seems to make sense, since this would bring those whole blocks into C-C. Housing would still be possible with C-C. What makes less sense is **why those blocks aren't included in the Gateway.**
- o) Exhibit 8: R-M to R-H on parts of 4 blocks, 5th & 6th Streets between F & I:
- p) I'm also wondering why this area wasn't included in the Gateway. Presumably at R-H Question: **Will the alley between G & H, and 5th & 6th be retained as public right-of-way, even if a developer attempts to redevelop that whole block?**
- q) Exhibit 9: Uniontown C-G to C-M; and enclave south of Uniontown R-M to R-H:
- r) Is the reason for rezoning Uniontown to allow for redevelopment of the center with housing as well as stores? Rezoning the enclave south of Uniontown to R-H could allow density up to 90 dwellings per acre, with density bonuses; so over 1000 more people. Could work – if they don't all bring their cars ... This rezoning would be an investment windfall for current owners there.
- s) Exhibit 10: Several parcels at the south end have broad water pipe easements. (Didn't we approve a PD there a few years ago? It appears that's never actually been built.)
- t) Exhibit 10: C-G to C-M for multiple parcels south of Samoa, between E & I Streets:
- u) Retain the current zoning at least pending policies that will be in the Coastal Element / Local Coastal Program update. C-M zoning could allow new housing to be built in the mixed-use zone. While this is right on Samoa, it goes against the principle of NOT adding new housing south of Samoa, where SLR, liquefaction, etc could put new residents in harm's way (likely renters).
- v) Exhibit 11: R-M to R-H for the Bayside Road townhouse and apartments:
- w) Retain current R-M zoning. Have the owners of these two apartment properties actually asked for this rezoning? These are high quality, relatively new rental housing, at a scale appropriate for this part of the neighborhood. Additional density here (up to 90 units per acre, de facto) likely means knocking down existing high quality and reasonably affordable housing. Rezoning these areas now, before developing the multiple use potential of the Sunnybrae shopping center areas, is really just a gift directly to the landlord(s).
- x) Exhibit 12: R-VL to R-L on Buttermilk frontage:
- y) The owners will love it, especially with SB9 subdivision possibility. Have any of them asked for this rezoning?
- z) Exhibit 13: PF to R-VL, pumping station (?) & land?:
- aa) Recommend either keeping the PF designation for now, or changing zoning to **R-L** instead of R-VL on this ½ acre site. Potential access from Anderson Lane instead of Old Arcata Road? If the public facility is no longer needed (a pumping station?) does it make sense to replace it with another public facility? If it will be privately developed, doesn't it make sense to allow slightly higher density right on the road, at least R-L instead of R-VL, even though the neighbors have R-VL?

- bb) Exhibit 14: The Gateway Area
 cc) Presumably will be subject to Gateway zoning – to be addressed with the GAP. Avoid designating any zone that straddles the Coastal Zone boundary (e.g., the Barrel District).

Public Safety Element

Consent Added After the Scheduled Meeting Date

1. The **“Guiding Principles and Goals”** section, which now follows the background / overview material, should precede it, appearing immediately after the 2 introduction paragraphs, and before the first “Overview” section. (This should be where the “Guiding Principles and Goals” should appear in EACH element. This is not just an “editing” suggestion, since the Guiding Principles and Goals should actually guide the entire element, its policies, and implementation measures.
 - **Add after “D”:** “Address increasing risks of flooding associated with sea level rise and rising groundwater levels in terms of both safety of people and property, and in terms of long term land use policy. (The Coastal Element also addresses these.)”
 - **Add after “J”:** “Foster community safety by developing hazard mitigation, emergency response, and long-term resilience programs through open, participatory, and responsive planning and decision processes, and support for community safety communication, education and training, organization, and working groups.”
 - **Add after “J”:** “Cooperate and coordinate with regional bodies, neighboring communities, and major institutions, as well as state and federal agencies to address emergency response, hazard mitigation, post-disaster plans, and planning to increase Arcata’s and our region’s resilience.”
- 1) **PS-1a City Emergency Response Plan:** ADD at the end: “The City will periodically revise Arcata’s Emergency Response Plan with open communication and community participation in response to community concerns.” (The Emergency Response Plan is the most opaque of the City’s plans. Let’s change that!) Also: Do we want to mention health emergencies, or is that a County responsibility?
- 2) **PS-1b Evacuation routes / transportation facilities :** ADD at the end – “The City shall coordinate with regional jurisdictions, transportation and health care providers, and Cal Poly Humboldt to develop plans for evacuation, transportation, or remaining in place during emergencies.”
- 3) **PS-1d Siting and design of critical facilities:** *Should we consider electric power as “critical”?* ADD at the end: “The City should consider opportunities to relocate critical facilities to less dangerous locations, and do so where relocation is feasible.”
- 4) **PS-1e Development & design standards for emergency response:** ADD at the end: “The City shall work with Fire and emergency response organizations to acquire and operate equipment that is sized appropriately for varied access and response contexts.” (Remember, it may be the Fire District not the City of Arcata itself that makes those equipment choices and purchases.)
- 5) After or before **PS-1f Citizen training ... ADD a Policy:** “The City shall assist neighborhood and community-based groups who request help aiming to support education, cooperation, and mutual aid before, during, and after emergencies, apart from and in addition to the CERT, or the County’s Office of Emergency Services.” (Such community-based efforts have been extremely effective, and may provide alternative support that boosts safety in situations where formal organizations don’t work well, especially those linked with or dependent on police or fire district responses.)
- 6) **PS-1h Severe Weather Hazards:** **MOVE** this policy to just after **PS-1e Development & design standards**
- 7) **Table PS-1 GEOLOGIC HAZARD LAND USE MATRIX:** This Table requires explanation! Even if it refers to another document, something in the Plan should explain categories, standards, abbreviations, etc! The Draft eliminated even the little explanation in the deleted text box. The Table will mean little to the general public without further explanation.

- 8) **PS-2e Shoreline hazards (tsunami inundation, tidal flooding):** Are emergency shelter locations considered “critical”? Our current zoning allows emergency shelters on South G Street, within the shoreline hazard area. Should we designate alternative / additional locations? **ADD:** “The City shall seek locations for emergency shelters and services in locations outside the shoreline hazard area.”
- 9) **PS-3c Hillside development standards: #3. Vegetation removal: Add to complete:** “Vegetation removal in the natural area of each lot shall be subject to review and approval by the City. The City may require hillside development approval to include a vegetation management program to reduce fire risks, including monitoring and enforcement provisions.”
- 10) **PS-3g in “OTHER GEOLOGIC HAZARDS AND AIRSPACE PROTECTION”:** Airspace Protection should be a separate policy category, NOT lumped in with “other geologic hazards”! **ADD** a policy to **restrict Drone operation**, especially around the low-fly approaches to the airport, around power lines, and in wildlife areas (the Marsh, though I think there’s already a drone prohibition there). This is probably beyond the General Plan’s scope, but those mylar balloons should be prohibited too, since they can knock out power lines, clog waterways, and endanger wildlife. **[Staff: supports adding this. The mylar balloon piece may be interesting to weave in...]**
- 11) **ADD after PS-4h: Drainage Master Plan** – “The City shall update its Drainage Master Plan periodically, at least once every [10? 5?] years, or whenever significant new hydrologic data appear to make building or development based on the existing Plan’s assumptions obsolete.”
- 12) **POLICY PS-5 FIRE HAZARDS Objective:** “Minimize risk of personal injury and property damage resulting from structural (urban) and wildland fires. Manage City forests to sustain ecosystems and their services in ways that also reduce risks of injury to people and damage to property.” (Refer also to the Forest Management Plan, which will be updated, and which should be consistent with and subsidiary to the General Plan.)

Policy Pitch Added After the Scheduled Meeting Date

1. **Fire Hazards Overview** (p. 6-4): The first paragraph of the overview addresses the USFS “broad brush” fire hazards classification. This is NOT one that is particularly meaningful in Arcata, since it fails to differentiate parts of the city. While a good warning, this broad brush use of federal and even state classifications in the first two paragraphs of the overview would scare any potential new resident, developer, or insurer clean out of town! Start with an Arcata-oriented description, referring to a more finely-differentiated fire hazard map, which should be developed separately from the city-wide multi-hazard map. This isn’t just an editing matter; differentiating among risk levels at a finer grain than the 70% of the city in the Wildland Urban Interface is a significant policy matter with important implications for development location and intensity, and investment-motivating fire protection and prevention policies.
2. **Hazardous Materials Overview** (pp. 6-4 & 6-5): **ADD** to p. 6-4 list: “4. Cleaning up, remediating, and restoring areas contaminated by toxic chemicals, in accord with state and federal programs and standards.” Mention ongoing assessments and cleanup status of known contaminated sites. (Since those assessments and cleanups can take 20 years, Plan readers in 2030 might still be concerned about the same places! The Little Lakes assessment has been going on since at least 2004!) This might also be the place to note that Arcata is a **Nuclear Free Zone**.
3. **Airspace Protection Overview** (p. 6-5): Address drone operation in Arcata airspace! Mention PG&E’s frequent helicopter inspections of their transmission lines, which now include extremely low flights over residential areas. **[Staff: It isn’t clear that this is policy]**
4. **Climate Change Adaptation and Resiliency Overview** (p. 6-5 & 6-6): This language is so general, most of it could be moved to the “Introduction” at the start of the Element! But it’s a good statement, and its position here fulfills the state requirement.

5. **PS-5b Review of development for fire safety: ADD at end of policy:** "... and design features, building height and bulk. The City shall not permit construction of any building or development that the Arcata Fire Protection District's plan review indicates cannot be adequately protected from fire risk by the District, or through mutual aid agreements with other fire districts in the region." **[Staff: This is too broad.]**
6. **PS-6c Use of potentially harmful materials on public lands and rights-of-way: ADD** at the end: "The City shall also prevent utility companies from applying toxic substances along their transmission lines or other facilities within City limits." *(PG&E isn't a public agency, and a City prohibition might not stand up in court. But a very clear City policy in the General Plan will help make sure PG&E won't spray herbicides in Arcata, regardless what they do elsewhere.)* **[Staff: we would like to support this, but as written, it is unlikely enforceable]**
7. **PS-7a Development/building and site design standards for crime prevention: ADD** at the end: "Video surveillance that unduly invades privacy shall not be an acceptable part of any Arcata design standard or City practice." **[Staff: need to better define unduly invading privacy and confirm that there is no case law around this issue]**
8. **6.3 IMPLEMENTATION MEASURES PS-3 Evacuation Planning: ADD:** "Develop protocols for providing resources and assistance to community members within the City through a variety of means when remaining in place appears to be more prudent than evacuation, and in circumstances where the Emergency Operations Center cannot provide adequate help."
9. **6.3 IMPLEMENTATION MEASURES PS-5 and PS-6** "Evaluate renewing a cooperative agreement with CALFIRE" and "Wildland Urban Interface Risk Reduction Program": *Add the Fire Management Committee to the list of responsible parties to consult in this evaluation.*

Other Matters

1. **POLICY PS 8 HUMAN HEALTH HAZARDS:** *Is this section cut because there will be a "Healthy Community" element?*
2. **PS-4c: Limitations on development within Flood Zone.** This section describes requirements for building within Flood Zone A. Why are we allowing any new building within Flood Zone A?

Editorial Comments

For anything you might abbreviate later, **write it out in full the first time you mention it in each Element**, and ideally make a list of ALL abbreviations to be included as a Plan Appendix. Examples in the Safety Element draft: CERT (mentioned on p. 6-2, but not written out in full until policy PS-1f on p. 6-8); HPM (mentioned at end of top paragraph on p. 6-2).

Avoid text boxes that don't reach the full page width – transferring them to some online media platforms doesn't work well. (A small box on a full-page PDF doesn't read well on a phone!)

The Redwood Coast Tsunami Working Group does a huge job. But although it's been around for several years now, as a pretty ad hoc group, its records and reports aren't easily available to the public (who will be reading this plan; see p. 6-2). The last sentence on p. 6-2 refers to a map of hazard areas in Figure PS-a, located in a map pocket at the end of this Element. NB: In a digital version of this plan, there's NO map pocket. So including a digital map at a reasonable, readable scale is important.

Flooding hazards overview (p. 6-3): Add a statement regarding flooding associated with sea level rise, and possible rises in groundwater in coastal areas. (Even if that's covered in the Coastal Element, the Safety element should address it too.)

Arcata's Drainage Master Plan Goals: P. 6-3: Mention Plan date (or last update)

Design Element

Discussion Items

1. **D-1b: Emphasize Arcata Plaza area as a community focal point. “Buildings facing the Arcata Plaza shall be multi-story.”** Are we suggesting this as a future policy? Do we want to? Some of the buildings on the plaza are not now multi-story. **[Note this is addressed above with a specific proposal for revision]**
2. **D-1e Promote energy efficiency and solar access.** Site and building design shall emphasize energy efficiency, ~~and~~ solar orientation and minimize shading of adjacent structures to the extent feasible, balancing development needs with solar access.
 - a. We should consider direct sunlight in terms beyond just energy efficiency, especially as direct sunlight is the basis of many buildings' and yards' existing design and use. The City's design policies should NOT support new buildings' suddenly cutting off direct sunlight to neighbors!
3. **D-3j: Streamside riparian areas.** Policy comment: It should not be possible to just dig a ditch and then have it become a riparian area that must “be retained in a natural state.” It would be easy to impede development if this were true.
4. **D-7e: Upgrade of non-conforming landscape.** This states, “**When improvements are made to structures on sites where landscaping is non-conforming, landscaping should be required to be upgraded if feasible.**” Policy comment: Requiring the “non-conforming” (Who decides what is non-conforming?) landscaping be upgraded is a barrier to making improvements to structures. Do we want to impose that requirement?
5. **D-1h City edges** - Restricting development in surrounding open space lands to very low height, bulk, and density (minimum parcel size from twenty to sixty acres; **?? maximum structure height, and 50 feet maximum structure length or width visible from Arcata**)
 - a. As with Agricultural zoning, we should discuss and consider what IS (and what shouldn't be) allowed in agricultural and resource zones, and since much of that bordering area is beyond Arcata's city limits, where zoning is actually controlled by the county, or DESIGN policies should address structures, lighting, etc, regardless of parcel size. To what extent does prohibiting massive buildings or many buildings require such large parcel sizes, when “agricultural processing” or massive greenhouses may still build out Arcata's “greenbelt”?
6. A question or two: why is former Policy D-2i (Design of signs) deleted? Don't we want the General Plan to contain some policy guidance on signs? **[Staff: These are too detailed for a general plan policy. This detail is already included in the zoning ordinance.]**
7. Another question: In Policy D-4c (Grading and hillside subdivisions), why are criteria 1 – 7 deleted? **[Staff: These are too detailed for a general plan policy. This detail is already included in the zoning ordinance.]**
8. Question: In Policy D-5b (single-family residential design on existing lots in hillside areas), why are criteria 1 – 6 deleted? **[Staff: These are too detailed for a general plan policy. This detail is already included in the zoning ordinance.]**

Editorial Comments

1. **D-1h, #3:** Editorial change: 101 is a U.S. highway, not a state highway.
2. **D-3c:** Editorial change: 101 is a U.S. highway, not a state highway. This change should be made throughout the document.

Prior Decisions

Vision

"Arcata Today" (edit) – *"Arcata today: Arcata is home, a work in progress, with its natural beauty and resourceful citizens, and exemplary quality of life."*

Add a statement after "We live resourcefully" and before "We move forward," or somewhere **before** "We're drawn to the Plaza": *"We are resilient. We face hazards by adapting to a changing environment while limiting our harm to the ecosystem and its functions, and to global climate."*

We share the benefits of California Polytechnic University Humboldt – ADD to this statement: *"We work together to ensure that the University supports Arcata's vision for our future, just as the City is an accommodating welcoming host to the University."*

Add a statement (perhaps at the end): *"We work with neighbors. We work with neighboring jurisdictions and regional partners to tackle problems we can only solve together, and we expect and encourage our neighbors to work with us to fulfill our community's vision."*

I support the "Revised Vision Statement" (Appendix D). I don't support the "Reorganized Vision Statement" (Appendix D2). The former is well written (see comments below) and concise. The latter has some empty language and is not nearly as effective. **Change neighborhood language and accept.** from the 3/27/23 meeting

"We're drawn to the Plaza" – Cut the added statement at the end. It would be fine to ADD a statement supporting community public spaces throughout the City, but not tacked on to this statement that recognizes the Plaza as Arcata's **unique center.** This is from Appendix D2 from the 3/27/23 meeting
Under both Equity and Connection and Community: Religion is not called out as a characteristic. Do we want to call it out?

Change Health and Safety to include "well being"

Land Use

1. I propose adding the Sunset Neighborhood to the implementation measure I introduced at a previous land use element meeting and setting a timeline on that implementation measure of 2 years.
 - a. The sunset neighborhood has many of the same qualities that make it suitable for allowing increased density and mixed uses as the Bayview, Arcata Heights, and Northtown neighborhoods. This is also a direct suggestion from members of the public. The planning commission should consider the merits of up-zoning and allowing mixed use in that neighborhood in the future as well.
 - b. This implementation measure should be drafted as follows:
 - i. **City staff shall bring before the planning commission a proposed rezone to consider allowing mixed uses and more housing in current R-L neighborhoods within walking distance of the plaza and Cal Poly Humboldt**
 1. The Bayview, Northtown, Arcata Heights, and Sunset neighborhoods currently only permit low density housing despite being within

walking distance of downtown and/or Cal Poly Humboldt. Staff shall develop a plan to upzone these neighborhoods in line with the Strategic Infill Redevelopment Program and bring the plan back to the planning commission for consideration within 2 years.

- a. Responsible Party: Community Development
- b. Time Frame: Year 2

2. I propose changing the definition of **Residential High Density [R-H]** as follows:
 - a. **Residential High Density [R-H]** High density residential uses are designated in central Arcata and other areas to allow increases in higher density above present levels multi-family housing located in proximity to commercial and employment uses, public services, schools, and parks. Local-serving commercial uses such as corner grocery stores, coffee shops, etc. shall be permitted in [R-H] zones.
 - b. We discussed this change at the last meeting regarding the land use element. Other Planning Commissioners and one member of the public agreed that allowing smaller, locally serving commercial uses in these zones created more walkable neighborhoods. It's nice to have a corner store that you can walk to rather than needing to use other modes of transportation to access another part of the city.
3. I propose an additional policy as follows:
 - a. **LU-1x Reduce Parking Maximums in the most walkable areas of the city. Reduce maximum allowable parking requirements within Infill Opportunity Zones to promote walkable communities.**
 - b. Reducing maximum parking allowed within the areas planned to be the most walkable within our city will ensure that these areas are not dominated by cars and parking lots, making the more inviting for non-vehicle forms of transportation.
4. I propose changing the pie chart on page 2-2 of the Draft Land Use Element (Packet pg. 15) to split the residential slice into R-VL, R-L, R-M, and R-H zoning. This will give the reader a more accurate representation of how we dedicate land within the city to the various kinds of housing.
5. I propose changing the policy **LU-1e** as follows:
 - a. **LU-1e Development of a diversity of housing types.** The land use plan map shall provide sufficient quantities of land in the various residential use categories to allow for development of a variety of types of new housing units and residential environments. The purpose shall be to ~~maintain~~ **achieve** an appropriate balance between single-family housing on individual lots and multi-unit housing types.
 - b. As currently written, this policy assumes that we currently have an appropriate balance between land zoned for single family housing and multi-family housing. This word change erases that assumption while leaving in the desire to achieve an appropriate balance.
6. I propose changing the definition of **Commercial – General [C-G]** as follows:
 - a. **Commercial - General [C-G]** This designation provides the full range of retail, entertainment, and service commercial uses **primarily** in, Valley West. C-G development must provide convenient access for patrons arriving by bicycle, public

transit, motor vehicle, or on foot. ~~Businesses in the C-G area will be expected to provide sufficient on-site parking.~~ C-G areas are intended to have convenient access from residential areas in order to provide for day-to-day shopping and service needs. Residential densities allow up to 50 units per acre.

- b. As currently written, this section equates automotive travel with convenience. It also dooms Valley West to continue its current car-oriented development. I recommend that we remove parking minimums in C-G and allow developers to determine the correct amount of parking they feel is necessary for their projects to be commercially viable. This will allow the community to hopefully develop as more walkable and less car dependent over time as nearby residential densities increase.

7. I recommend we change policy **LU-4h** as follows:

- a. ~~LU-4h Petroleum extraction and processing~~ **Energy development, production, and use.** The City of Arcata recognizes the national need for the responsible exploration, recovery, development, distribution, and processing of the country's energy resources. ~~However,~~ The City also recognizes the potential adverse impacts such activities may have. Petroleum extraction is of particular concern on the North Coast. On- or offshore petroleum extraction will result in negative impacts on the climate and our coastal scenic resources, as well as our sensitive land and marine environments. ~~petroleum extraction and processing can will have on the climate as well as sensitive land and marine resources and on the scenic quality of coastal resources.~~ Therefore, the City finds that, in order to minimize adverse impacts to such resources, on-and off-shore petroleum product exploration, recovery, and processing should be confined to those geographic areas which now accommodate these uses and activities. Consistent with this policy, the City shall prohibit on-shore petroleum exploration, production, and processing within its boundaries, and shall oppose the use of off-shore areas south and west of Arcata and in Humboldt County in general for such uses.

- b. I don't see any reason for Arcata to "recognize the national need for" oil and gas exploration and development. We're in a climate crisis after all.

8. Although the need for housing is discussed, the issue of homelessness is not mentioned directly. I feel strongly that we should do so. Safe and secure housing is a human right. We live in the most prosperous country in the world; we should not have people without homes. We can address that in the following sections.

- a. **Guiding Principle C: Allow Encourage** ~~for~~ a range of housing choices that includes affordable ~~dwelling~~ **housing** for all community residents, including currently unhoused people, that accommodates families as well as individuals and groups, and varies in size and type to reflect the diverse character of the community and to provide equitable access to opportunities and resources in all of Arcata's neighborhoods.

9. **LU-6b: Compatibility between agricultural and adjacent non-agricultural uses.**

This section begins with "Agricultural practices can include spraying of herbicides," Do we want to legitimize the use of herbicides in Arcata by explicitly including this statement in our General Plan? I suggest striking "spraying of herbicides" and starting the

section with, “Agricultural practices can include application of fertilizer, operation of farm equipment. ...”

10. Principles and Goals

- a. Add a statement regarding “acknowledging and acting on strategies brought forward by members of Arcata’s Black, Indigenous, and People of Color communities.” (In the draft there’s quite limited one tacked on to Principle A. Please also avoid acronyms.)
- b. Locate and allow location of land uses to minimize risks and exposure to environmental hazards, including seismic hazards and flooding. (Even though this should ALSO be included as a SAFETY and an ENVIRONMENTAL JUSTICE principle, it’s first and foremost a LAND USE principle and so should be up-front in this element.)
- c. Seek and encourage open and cooperative consultation and cooperation with managers of state land not subject to Arcata’s land use authority. (This refers to both the University and to the State’s retained jurisdiction in coastal areas, as well as a few others.)

11. **LU-1i: Maintain Arcata’s Historic Plaza Area as a major community center:** The draft policy and with “Residential units shall be included, where feasible, in all new commercial development within the Plaza Area.” This repeats gist of LU-1f (Inclusion of residential uses). And an editing suggestion: Instead of “Residential units,” “housing” is more meaningful.

12. **LU-2: Residential Land use “Objective”** – add: “Allow for a mix of housing types and densities to ensure residents at all ages, income levels, and abilities ...”

13. **LU-4 Industrial Land Use Objective:** Hasn’t the City been supporting industries for 150+ years? (Is 50 years a typo?)[Staff response – yes. We’ll fix]

14. **LU-6a Agricultural and Natural Resource classifications – Agriculture Exclusive [A-E]:**

- a. Consider adding to the end of the A-E classification text “*Agricultural and aquacultural product processing facilities for products originating outside of Arcata, and which are essentially industrial and require large-scale industrial buildings [add a threshold size?] are not appropriate for the A-E zone.*” (Presumably, existing ag-industries already approved may remain.)

13) I propose changing policy **LU-1c** as follows:

- a) **Prioritization of transit and active transportation.** Reduce or eliminate minimum parking requirements ~~citywide. in areas where transit and active transportation is planned to support the transportation needs of the community, including neighborhoods where biking infrastructure, trails, complete streets, and transit is or is planned to be accessible.~~
- i) I recognize that this policy was already changed based on my comments at a previous meeting. However, I worry that this current language accepts that there are areas of the city that we are choosing to leave un-walkable. My proposal makes this policy inclusive of the entire city.

14) **LU-2b: Diversity and choice in residential environments and LU-2c: Planned Development - residential.** These two sections represent another opportunity to incorporate wording to indicate that the **City encourages housing for all, including currently unhoused people.** I can suggest wording if we choose to do so.

15) **LU-6c: Protection of agricultural lands and uses within the City.** The second paragraph starts with “Private and public non-vehicular recreational activities such as hiking, riding, fishing, hunting, and other recreational activities....” I suggest we specify that the riding is non-motorized by adding that to the wording: “Private and public non-vehicular

recreational activities such as hiking, **non-motorized compatible** riding, fishing, hunting, and other recreational activities....” **[Staff: work on this]**

- 16) **LU-1d: Streamlined Review and Standards in Infill Opportunity Zones:** We still haven’t discussed what this will be, either in the Gateway, much less city-wide. *When will we discuss and resolve this? Let’s not assume the model we adopt for the Gateway will hold City-wide. But it would be VERY CONFUSING if it doesn’t!* I’m very uncomfortable including this as a policy unless we have those discussions BEFORE making a recommendation to the City Council. *I recommend changing this policy to CONSIDER adopting streamlined review and standards for Infill Opportunity “Zones”.* Unless we have already addressed this in depth, include developing and adopting those changes as in implementation action.
- 17) **LU-1j: Encourage Valley West’s growth as a major community center for north Arcata:** Eliminate the sentence “High density residential use in the Valley West Infill Opportunity Area will be streamline”. It seems this is already part of the Infill Opportunity policy elsewhere, and we still haven’t figured out what that “streamlining” will be. [develop objective standards to guide development review and approval...] staff needs to come back with these first.
- 18) **LU-3a Commercial-Central[C-C] :** ‘The Commercial-Central Zone will continue to have no upper density limit’. *ADD: however, conditions of permit approval must avoid dangerous effects on public safety.*
- 19) **Table LU-4 INDUSTRIAL / PUBLIC FACILITY LAND USE CLASSIFICATIONS**
- a) “EDUCATIONAL, CULTURAL & RELIGIOUS USES” are not listed for either of the Industrial zones. Would this prohibit vocational education facilities on an industrial site? (I support prohibiting K-12, daycare, or preschool facilities in Industrial zones.)
- b) What is the point in including RELIGIOUS USES in this category? Can we eliminate mention of “Religious Uses” in this part of the Land Use element? (It doesn’t show up there in our current LUC.) Remembering 1st Amendment rights, the City has little regulatory authority over “Religious Uses” beyond enforcing its own property rights on city-owned sites, and safety rules.
- c) **Urban Agriculture:** I suggest **allowing some “urban agriculture” on I-G and I-L sites**, perhaps with a Use Permit to set appropriate conditions. Why is urban agriculture NOT allowed on Industrial sites (I-G or I-L), especially considering what IS allowed on them, and considering that industrial factories (with large-scale industrial structures) have been allowed on Ag Exclusive land? Which leads to ...
- 20) **LU-1q State mandated housing production** -- The new proposed LU-1q works well. I like that this policy represents a City commitment to advocating for reasonable state approaches to housing production in a small city like Arcata. However, **I suggest ending the policy statement with “... meet both state objectives and City need for housing,” omitting the end of the draft sentence.** The plan says that elsewhere.
- 21) **New Policy LU-6f Restoration of former tidelands.** I’d like to propose an additional land use policy for inclusion in the Ag section of the land use element.
- a) **LU-6f Restoration of former tidelands.** The City of Arcata recognizes the need to restore former tidelands to salt marsh in order to adapt to rising sea levels and promote biodiversity and a safe environment. The City shall encourage and support the restoration of former tidelands, currently zoned Agricultural Residential [A-R] or Agricultural Exclusive [A-E].
- 1) **Table LU-4 INDUSTRIAL / PUBLIC FACILITY LAND USE CLASSIFICATIONS**
- a) Residential uses are allowed in I-L zones, though limited and requiring Use Permits. When we amend the Land Use Code and its Use Permit standards for residential uses of I-L sites, let’s think clearly and protectively about what IS allowed there, and who is vulnerable to those hazards (even in I-L permitted uses).

Ideas for Discussion

- 20) LU-1k: Support and revitalize other existing neighborhood and commercial activity areas. This section promotes travel by walking, biking, and transit. One of its intentions to reduce vehicle miles traveled. Yet, it also encourages "improvement of parking." Do we want to also encourage the conversion of parking lots to other uses, such as housing, walkways, playgrounds, etc.? develop language that focuses on other uses
- 21) Housing density limits are not expressed quantitatively in this element. Nonetheless, I suggest we consider adding language to address the possibility of housing bonuses affecting the overall density of development.
 - a) LU-2a: Residential Land Use Classifications. This section discusses different residential density zones. Given that the density bonus can be large and the rules covering the bonus are evolving rapidly, we can add language here so that we end up with a reasonable densities we can live with.
 - b) LU-3a: Commercial-Central (C-C): The last sentence reads, "The Commercial-Central Zone will continue to have no upper density limit." Do we really want to say that? Would a 12-story building be allowed in this district?

Circulation

- 1) Consider changing the name of the Element to "Circulation and Mobility Element." Beyond simply adding the statement at the end of the introduction, full explanation that circulation and mobility do NOT necessarily relate only to vehicular transportation, with pedestrian and bike facilities tacked on, is important.
- 2) Consider the following:

2.7 INTRODUCTION

Circulation and mobility consider how people and goods move through and around the City. The circulation element addresses how a comprehensive, integrated transportation network can be planned to achieve maximize individual mobility in a manner consistent with community character and environmental protection. The City is committed to providing a complete, connected, multimodal transportation and mobility network. California law requires that transportation and land use policies be closely correlated. The Arcata General Plan accomplishes this in two ways. First, travel demand has been forecasted based on the amount and distribution of growth anticipated allowed by the land use plan. Second, the policies of the transportation, land use and air quality policies are linked elements have been interwoven to provide a balance between land uses and the transportation facilities that serve them. The overall theme of this element is achieving a balanced transportation and mobility system that is safe, accessible, comfortable, accommodating, and welcoming to all users. Transportation and mobility planning and policies in Arcata will put the safety of people first, both outside of vehicles and in them. CONSIDER ADDING explanation here recognizing that mobility goes beyond just the transportation conditions focusing on vehicular roadways.

- 3) Safety first: Include clear wording in the Introduction, Guiding Principles and Goals, and throughout the Element's Policies to indicate that protecting and improving safety must come first in all policies and transportation / circulation / mobility planning decisions. Such wording is in line with a "Vision Zero" approach to transportation planning that strives to eliminate traffic-related death and injury as the highest priority in transportation planning, above and beyond speed, convenience, and

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financial cost. (Detailed suggestions for new wording are included in the "Track Changes" version of the MSWord draft Circulation Element.)

4) **Guiding Principles and Goals:** This entire section should be moved from the draft's current location to an up-front location immediately after the Element's **Introduction**. The **Guiding Principles and Goals** must be the basis for the rest of the descriptions, analysis, planning policies, and implementation actions that follow. In the draft, the **Guiding Principles and Goals** are hidden right before specific policies but AFTER the "Proposed Circulation Network," halfway through the Element. They should certainly precede, not follow, the "Proposed Circulation Network" section.

5) **Transportation and Mobility Equity, & Safety First:** Add explicit principle and goal, as well as explicit mention in relevant policies, of the City's intention to develop transportation and mobility policies and improvements to achieve mobility and transportation equity. Add to the "Principles and Goals" section "The City recognizes that safe mobility is a right of all people in Arcata. The City will adopt policies and pursue plans that further transportation and mobility equity." Such changes should appear in the "Guiding Principles and Goals" section, and in other relevant policies and specific plans for improvement, as indicated in my detailed suggestions.

Guiding Principles and Goals
The City of Arcata shall:

- A. Provide a connected multimodal transportation and mobility system which allows that contributes directly to the safety, health, economic vitality, and quality of life of all people in Arcata. ~~residents, and efficient travel.~~
- B. Recognize that safe mobility is a right of all people in Arcata. The City will adopt policies and pursue plans that further transportation and mobility equity.
- A. Put safety first in all transportation and mobility planning, policies, and projects.
- B-C. Create a transportation system which provides that incentivizes a choice of travel modes and is safe, accessible, comfortable, accommodating, and welcoming to all users.
- C-D. Provide for increased use of active and shared transportation modes as alternatives to the single-occupant vehicle, including walking, rolling, bicycling, public transit, carpooling/vanpooling, and ridesharing.
- D-E. Manage the street and highway system to promote more efficient use of existing capacities rather than increase the number of travel lanes.
- E-F. Create a multimodal transportation system which that will improve the livability of residential neighborhoods, including use of methods to calm or slow traffic and reduce through-traffic on local neighborhood streets. ADD statement on varied ability mobility here
- F-G. Educate residents, employees, and students about the importance of using alternative forms of transportation and mobility instead of the single-occupant automobile.
- G-H. Promote land use patterns that encourage walking, rolling, bicycling, and public transit use.
- H. Establish a set of curb-fee-based parking prices that are high enough to maintain an adequate supply of available spaces drive more active and shared transportation

6) **Policy Pitch Section**

1) **Accessibility and mobility for people with varied abilities and disabilities:** Add explicit inclusive policies and language throughout the Element to address needs of people of diverse abilities and disabilities. (I have suggested detailed language in the "Track Changes" version of the Draft submitted to staff.) In the policy list, start with Policy T-1, BALANCED TRANSPORTATION SYSTEM WITH CHOICE OF MODES: After T-1e (or somewhere before): Insert this or similar policy –

a) **T-1f Improve accessibility and mobility:** The City shall undertake a comprehensive program to assess and improve accessibility and mobility for people of varied physical abilities and disabilities." (In the Implementation measures list, this should happen in the next 1-2 years.)

2) I propose we change policy T-6 in the following ways:

a) **Objective.** Manage parking to reduce the incentive for single occupancy vehicle use. Provide an adequate supply of parking in perimeter lots downtown. Minimize the impacts of Cal Poly Humboldt State University parking into adjacent neighborhoods. Ensure that new development provides an adequate but not excessive supply of parking.

T-6a **Downtown parking.** The following shall apply to parking within the Downtown area:

~~1. Assess and plan for future parking needs. Municipal parking lots shall be provided in the perimeter of downtown to create an adequate parking supply to serve existing businesses, future development, and to replace on-street parking removed for pedestrian, bicycle, and landscaping improvements. One municipal lot is planned to complete the City's parking system, but assess the need for additional parking lots may be provided if additional demand or opportunities arise. The City shall explore implementing a smart parking meter system in the Downtown area to manage parking demand while generating revenue to support public transit and/or active transportation.~~

b) The concept of "adequate supply" of parking seems to reflect the assumption that a particular land use or number of people automatically translates into a certain amount of driving and parking. The evidence doesn't bear out this assumption. Rather, we know now that the parking supply helps dictate the amount of driving. Reflecting this, and in alignment with the city's other mobility goals, it seems logical to establish an objective to manage parking to achieve mode shift, for example by charging for parking. For similar reasons, the idea of adding more parking lots to downtown based on "demand" seems outdated. Instead, managing parking through a smart meter system reflects modern best practices in parking management, and would reduce the subsidy for driving and create a new revenue source to help fund other city projects such as protected bike lanes and public transit. See the work of Donald Shoup for much more on this topic.

3) With regard to the **Gateway Area, within the Circulation Element:**

The draft's "**Proposed Circulation Network**" section addresses street and circulation changes City staff now propose in the draft Gateway Area Plan. Current language is "Additionally, implementation of mobility improvements within the Gateway Area Plan, including the "K" and "L" Streets couplets, and the 8th and 9th Street couplets extension, will alleviate traffic congestion within the Gateway and will ensure all transportation modes remain comfortable, convenient, safe, and attractive ..." However, significant disagreement among Arcatans, is far from resolved, especially about the proposed K/L Street one-way couplet. I recommend the following:

The City should fully investigate and publicly assess detailed alternatives to provide access to the west side of the Gateway Area. These will include a K Street / L Street one way couplet, and detailed alternatives that would retain and improve the L Street corridor as a Class 1 bikeway, pedestrian way, and linear park, and retain K Street as a 2-way Arterial, with safety and traffic flow improved by possible means including new city-operated vehicle and pedestrian signals, left turn lanes, on-street parking adjustments, and vehicle access to new development west of K and L Streets mainly via East West streets; or other options including completing portions of M or N Street.

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4) I propose we stop using Level of Service as a management consideration for city streets. And prioritize traffic calming and safety on all city streets regardless of classification. We can accomplish this through the following policy change:

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a) ~~Deprioritize LOS shall not be a management consideration for city streets. Decades of research and experience show that projects which attempt to relieve congestion and improve LOS simply attract more traffic and are ultimately unsuccessful. Furthermore, congestion is~~

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often desirable from a safety standpoint, as it results in slower traffic speeds. To that end, use LOS to reduce speeds and encourage mode shift. Appendix A of this Element describes existing and projected traffic volumes and LOS for key City intersections. Although several unsignalized locations are projected to operate at LOS C or better, locations which experience higher volumes such as US 101/Sunset Avenue interchange, Alliance Road at Foster Avenue, Alliance Road at "M" Street/15th Street, and locations on 14th Street at "C" and "D" Street corridors are projected to operate at LOS D, E or F. Improvements anticipated by this plan (see Figure T-4) are expected to improve the LOS to acceptable levels for all intersections while balancing the priorities of active transportation goals. See appendix A for the complete analysis.

5) Traffic signals in policy T-4b Vehicular Circulation:

With anticipated population to 28,000, and much denser development patterns in much of the city, it will soon be time to reconsider the City's longstanding determination not to develop a traffic signal (traffic light) system of its own, above and beyond the CalTrans lights on Samoa Blvd and a few other high traffic locations. A Circulation Element policy would open that possibility:

3. Improvements at intersections. Improvements at intersections shall be designed to allow the safe, comfortable, convenient and accessible use of streets and walkways for all roadway users.

a) Minor improvements at intersections. Minor projects to improve traffic safety include redistributing lane allocations and coordination of traffic signals. Improvement projects shall be designed to accommodate the needs of pedestrians and bicyclists.

The City shall consider developing City-operated traffic signals and signalized pedestrian crossings to accommodate new or denser land uses, traffic patterns, and safety concerns, especially Downtown, in the Gateway Area, and in the Giantuli / Valley West area.

b) ~~6. Minimize the installation of new traffic signals.~~ New traffic signals shall be provided ~~only in instances~~ where there is no feasible alternative to relieve a ~~demonstrated~~ safety problem at an intersection (~~based on documented accidents~~). Alternatives which shall be studied prior to signals include roundabouts or installation and monitoring of all-way stop signs.

~~6. Minor improvements at intersections. Minor projects to improve traffic safety~~

6) Transportation Advisory Committee: Policy T-4 4c 1 in the draft now hides the role of the Transportation Safety Committee within a single subsection of the sub-policy related to "Slowing Traffic." I suggest a broader role for a re-named, re-framed committee, possibly a "Transportation Advisory Committee." The Transportation Safety Committee's narrowly defined role is too easy for City staff and officials to overlook, and even now does not reflect the diverse community concerns that come before them. I suggest:

T-1g ADD POLICY: TRANSPORTATION ADVISORY COMMITTEE: The City shall consider renaming its Transportation Safety Committee, and broaden its role to advise City staff, Planning Commission, and City Council on matters related to transportation safety, and diverse community concerns related to transportation, circulation, and mobility safety and accessibility.

7) I propose the following changes to the section "Functional Classifications of the Street System":

- a) All streets within Arcata city limits, with the exception of access-controlled segments of Highways 101 and 299 and certain rural roads, are lined with homes and businesses and will be managed primarily to provide safe access and high-quality public space, regardless of functional classification. Slow speeds and traffic calming will be prioritized on all city streets. [delete the rest of the classifications]

The Federal Highway Administration's functional classification system is not a useful tool for guiding the design of city streets. This system is based on a suburban style of development that assumes dead-end local cul-de-sacs with houses on them feed into ever larger streets (collectors and

then arterials) whose job is to get the residents of those houses to other places. Even in this context, the scheme fails, because most commercial destinations are concentrated on collectors and arterials, creating the deadly “stroad” effect of streets that are designed primarily to move cars at high speeds but also have lots of destinations and multimodal use for which they are not designed. In a gridded streets system, such as the one that prevails in much of Arcata, functional classification makes even less sense. Our city streets all serve multiple purposes - as places for walking, biking, rolling, driving, and riding from one place to another, but also for accessing our destinations and even for social gathering. Pretending that access is just for local streets while others (arterials and collectors) are primarily for moving people quickly around in the city, while ignoring that all of our streets are in fact lined with destinations that people need to access, is unhelpful and leads to dangerous designs. It is not a coincidence that traffic collisions in Arcata are concentrated on the designated arterial streets, which are designed for speed and capacity rather than for access and safety. We should abandon this inappropriate way of thinking about our street system.

b) If this is unacceptable, then as an alternative, I would propose adding this language as a header to this section. All streets within Arcata city limits, with the exception of access-controlled segments of Highways 101 and 299 and certain rural roads, are lined with homes and businesses and will be managed primarily to provide safe access and high-quality public space, regardless of functional classification. Slow speeds and traffic calming will be prioritized on all city streets. The language would serve as a reminder that even arterials should prioritize access and safety over convenience. This language would also be in more compliance with the “Complete Streets” policy included elsewhere in the plan.

8) I propose the following changes to the section titled “Operational analysis and intersection level of service (LOS) Summary:

a) LOS shall not be a management consideration for city streets. Decades of research and experience show that projects which attempt to relieve congestion and improve LOS simply attract more traffic and are ultimately unsuccessful. Furthermore, congestion is often desirable from a safety standpoint, as it results in slower traffic speeds. Appendix A of this Element describes existing and projected traffic volumes and LOS for key City intersections. Although several unsignalized locations are projected to operate at LOS C or better, locations which experience higher volumes such as US 101/Sunset Avenue interchange, Alliance Road at Foster Avenue, Alliance Road at “M” Street/15th Street, and locations on 14th Street at “G” and “H” Street couplets are projected to operate at LOS D, E or F. Improvements anticipated by this plan (see Figure T-k) are expected to improve the LOS to acceptable levels for all intersections while balancing the priorities of active transportation goals. See appendix A for the complete analysis.

b) The effect of induced demand is well documented in transportation planning, and is even referenced in Arcata’s own planning documents. Managing for LOS means adding vehicular capacity (whether that means adding lanes or making smaller “functional improvements”), but the principle of induced demand dictates that any resulting reductions in congestion will be temporary - the street will fill back up with more cars soon. Managing for LOS is just pretending that induced demand isn’t real, when we know it is. In other words, managing for LOS just doesn’t work.

Instead of managing for LOS we should be managing to reduce vehicle miles traveled (VMT) in order to reduce environmental impacts. The State recognized this in 2013 with the passage of SB 734 which required all environmental studies for proposed projects in the state to switch from LOS to Vehicle Miles Traveled (VMT) as the critical measure of a project’s impact. Previously, the state, its local municipalities, and its regional governments had been basing an

assessment of a project's environmental consequences based solely on whether the project would create congestion. By focusing on VMT instead of LOS, CEQA now puts the planning onus on the reduction of car trips.

Furthermore, even if we could reduce congestion with engineering projects, it is not clear if that would really be desirable. Congestion, by definition, slows down traffic, and slower speeds result in greater safety for all road users. It's time for Arcata to stop prioritizing the annoyance of minor delays for drivers over the lives of community members and the environment. If fully rejecting LOS is out of the questions, other cities, like Seattle, have [reformed their LOS](#) to set specific target rates of transportation modes (e.g., walking, biking, transit, and driving) rather than solely focusing on driving.

9) I propose we update the section discussing the 2017-2022 (Transit Development Plan) TDP to state that the 2017 TDP is out of date, and a new one is about to be adopted. [A 2023 Transit Development Plan will be adopted soon. The City shall make an effort to follow the recommendations in the 2023 TDP.](#)

10) I propose the following changes to the section titled "Existing Bicycle and Pedestrian Facilities":

Arcata's bicycle transportation system consists of Class I off-street shared use paths, Class II bike lanes, Class III bike routes, and bicycle boulevards on public streets. Class I facilities are multi-use paths that provide a completely separated right-of-way for the exclusive use of bicycles and pedestrians with cross flows of motorized traffic minimized. Class II bike lanes provide a striped and signed lane for one-way bicycle travel on a street or highway within the paved area of a roadway. Class III bike routes are specially designated corridors in which the travel lanes are shared by motor vehicles and bicycles and are usually marked with on-street pavement stencils. [Research has shown that Class III bike routes do not provide adequate safety or comfort for bicyclists unless significant additional design features are included.](#) Bicycle boulevards are a type of Class III facility on low-volume roadways which prioritize the use of bicycles with traffic controls, signage, roadway markings, and traffic calming measures, including bicyclists having the right-of-way. [Class IV bike lanes are protected from traffic by a vertical barrier. Arcata does not currently have any Class IV bike lanes, but research has shown that most people will not bike on busy streets without them.](#)

Arcata currently provides a [comprehensive](#) bikeway network connecting most major areas of the City on primary arterial streets, [but many of the current facilities do not provide adequate protection for the comfort and safety of bicyclists.](#) The primary Class I shared use path along the L Street rail alignment provides a north-south connection from the southern City limits and to the Humboldt Bay Trail south to Eureka, connecting to Alliance Road north of the Gateway area, and connects to Foster Avenue at Sunset Avenue. Additional Class I facilities provide brief connections between existing roadways and on-street bicycle facilities. Most Class II bike lanes are located on north-south streets, while Class III bike routes and bicycle boulevards provide east-west connection on key streets. The western portion of the City (west of Alliance Road) is least served by bike lanes, providing an opportunity to expand the bike lane system to encompass more residential areas. Figure T-h presents the existing bicycle and trail facilities.

See the discussion of Class IV bike lanes above for the reasoning for these changes.

11) I propose the following changes to the section titled "Proposed Circulation Network":

Arterial, collector, and local roads will provide access to new and established residential, commercial, and industrial areas, connecting those areas with the existing local and regional transportation system. Buildout of the General Plan land uses to year 2045 will increase multimodal, access and parking demands and will result in areas already under stress to exceed acceptable limits for safety and delay. As presented in Appendix A Table T-3, forecasted traffic operations at several intersections are projected to degrade to LOS D, E, or F.

In order to accommodate the existing and planned land uses within the City, a robust network of multimodal safety capacity improvements will be needed. Based on buildout of the General Plan land uses and forecasted traffic operations, several improvements are planned for most of the intersections projected to operate deficiently, mainly installation of roundabouts. At the US 101/Sunset Avenue interchange, the City is currently undergoing the Project Approval and Environmental Document (PA&ED) phase of the interchange improvement, which proposes to install two roundabouts at the interchange including pedestrian and bicycle facilities. Additionally, implementation of the mobility improvements within the Gateway Area Plan, including the "K" and "L" Streets couplets, and the 8th and 9th Street couplets extension, will alleviate traffic congestion within the Gateway and will ensure all transportation modes remain comfortable, convenient, safe, and attractive to residents, workers, students, and visitors.

a) See the discussion above about LOS and congestion management for an explanation of these changes. Table T-5 and Figure T-k should be modified accordingly to remove projects motivated solely by congestion concerns.

[make this language consistent with the deprioritize]

Add Class IV facilities to Table T-6.

12) I propose that we make the following changes to Guiding Principle D:
 Manage the street and highway system to promote more efficient use of existing capacities facilities rather than increase the number of travel lanes or make other capacity enhancements.

See the discussion above regarding LOS and congestion management for an explanation.

[make this language consistent with the deprioritize]

13) I propose the following changes to Policy T-1d:
 Critical transportation facilities for emergency vehicle access and emergency evacuation shall be maintained and improved as a priority need. However, when determining needed improvements, ease and speed of emergency vehicle access shall at all times be weighed against safe design for all street users. Critical transportation facilities include the major routes into and out of the City such as Highways 101, 299, and 255, their interchanges with City streets and primary intra-city street connections including Samoa Boulevard, 11th Street, "G" and "H" Streets, Sunset Avenue, L.K. Wood Boulevard, Alliance Road, Janes Road, and Giuntoli Lane. Due to the potential for structural failure of these facilities in a seismic emergency, alternative routes and procedures for their use shall be identified.

Emergency access is very important, but road design should not simply maximize emergency access or minimize response times in the absence of other considerations. Statistics indicate that more people in the US die from car crashes than from fires, crime, etc., so maximizing lives saved

means that safe road design proposals can't be automatically vetoed only because of emergency access concerns.

14) Policy T-3: Ensure this policy is consistent with the adopted Regional Transportation Plan, which calls for doubling transit trips by 2025, again by 2030, and again by 2040.

15) I propose the following changes to Section T-3a:

The City shall ~~maintain~~ improve the existing A&MRTS routes (as shown in Figure T-de), frequency, and level of service as funding permits until increased demand, additional development, and transit planning studies identify the need for either route modification, an expanded route system, or increased service on existing routes. ~~The transit planning studies should evaluate the cost-effectiveness and feasibility of increased routes and service based on projected capital and operating costs, fare box recovery, and state and federal subsidies (see Policy T-3c for planning criteria).~~

The city can't meet its goal to increase transit ridership just by maintaining existing service levels. We have to improve the service as funding permits.

16) I propose the following changes to Policy T-3c:

Public transportation is both a civil right and a critical climate solution, and should be designed to provide service competitive with automobile travel in terms of access, convenience and comfort. Potential improvements to the transit system should be assessed according to the best available evidence of both need and existing and induced demand, an enterprise activity and its services must be designed to be as efficient and productive as possible. As a transit operator, the City must balance demand with resources for a sustainable system. The City shall consider adding transit routes or modifying existing transit routes and level of service based on the transit planning efforts described in Policy T-3a. Criteria to evaluate and identify thresholds for changes to the A&MRTS system shall be developed. General guidelines for planning future routes and service include:

- 1. Accessibility of route to residents and employees. Calculate the number of people living or working within walking distance of the route (typically 1,000 feet). Assuming 1% to 3% of that population would use transit (based on existing transit mode share by census block), determine if the route will serve an adequate population for cost-effective service.
- 2. Review the housing density within the proposed route corridor. Minimum densities of at least seven dwelling units per acre are necessary to support local transit service. Ideally, the average housing density within a transit corridor or transit served nodes should range between eighteen to twenty dwelling units per acre, depending on the proximity to stops.
- 3. Evaluate the efficiency and directness of future routes. Compare bus travel time with automobile travel time to avoid a disproportionality which favors automobile use. Determine if the route requires inefficient loops which take riders out of their way and discourages transit use. Design routes to be as direct as possible with turnarounds at endpoints.
- 4. Evaluate the diversity of the destinations served. Efficient routes serve a diversity of land uses including residential, employment, schools, and shopping. Evaluate the number of activity centers connected by the route and the transfer opportunities provided.

While I acknowledge that there are legal and practical limitations to the city's ability to provide public transportation, I believe it is counterproductive to view it as a "business" rather than as a

basic right. We do not ask roads to pay for themselves (and they don't), and we shouldn't ask public transportation to do so either.

Additionally, while there is nothing inherently wrong with the enumerated planning guidelines, I believe it is preferable to allow the guidance to evolve as evidence and best practice evolve, rather than immortalizing them in the General Plan.

17) I propose the following change to Policy T-3b:

Short- and long-range transit plans shall be coordinated with the regional transit service provided by the Redwood Transit System. The City supports regional transit plans which improve service and timed transfers, and reduce headways for intercity travel. In the interest of enhanced coordination and efficiency for local and regional service, the City shall immediately begin planning to merge A&MRTS with the Humboldt Transit Authority. (make this language more broad to include evaluation and potential merger)

- a) All other major transit services in Humboldt County are managed by HTA. Fully integrating A&MRTS into the HTA system will allow easier coordination and greater efficiency. I am aware that this has been discussed for years, but I can think of no good reason not to do it.

18) I propose the addition of a Policy T-3h:

T-3h. The City shall study investigate the possibility of gaining its traditional fixed-route bus system with an on-demand microtransit system which could serve lower density areas and feed into the fixed route system to increase transit mode share.

- a) Significant technological advances and planning innovations have occurred in public transit since the last General Plan was adopted. It is increasingly accepted in transit planning that microtransit can be a good option for areas without high enough density to support traditional fixed-route buses. The city should explore this possibility for improving the transit system.

19) I propose the following change to Policy T-4 Objectives:

Plan an internal street system the circulation network consistent with Figure T-k and Figure T-i and Arcata's small-town, non-metropolitan character to create Complete Streets solutions that are appropriate to individual contexts; that best serve the needs of all people using streets and that support the land-use, climate, safety, and environmental quality targets and policies of the City and which: 1) efficiently utilizes existing facilities and reduces need for investment in new or expanded street and highway facilities or capacities; 2) improves connectivity of streets to provide for direct routes between origins and destinations; 3) has a high quality of regular maintenance and repair; and 4) maintains a level of service which minimizes delays, but allows for higher levels of congestion during the short peak periods on weekdays.

- a) See above discussion of LOS.

[make consistent with deprioritization]

20) I propose the following modifications to the section titled "No additional vehicular travel lanes":

Street projects shall not be designed to improve vehicular traffic flow shall emphasize intersection improvements and facility maintenance. If congestion occurs, it shall be welcomed or managed using alternative methods such as diversion of trips to other travel modes or intersection improvements. Construction of additional arterial street vehicle travel lanes shall not be considered only when no other feasible congestion management methods are available and if unless it supports the land-use, climate, safety, and environmental quality targets and policies of the City.

a) See above discussion of LOS and congestion. These edits reflect the fact that adding lanes is not the only way to increase capacity, and that the principle of induced applies to any increase in capacity.

[make consistent with deprioritization]

21) I propose the following changes to Policy T-4c:

~~The City shall employ the following range of measures to reduce speeds and "calm" traffic throughout the city in the various commercial areas, near schools, public recreation areas and in residential neighborhoods to improve safety and comfort for those walking, rolling, biking, and taking transit.~~

a) Traffic calming is critical for safety, and there is no reason to limit this safety work to only certain areas of the city. This is related to the thinking about functional classification, which has resulted in dangerous arterial street designs.

22) I propose the deletion of Section T-4c.4:

~~4. All neighborhood streets shall remain open to through vehicle travel unless there is a demonstrated safety problem that cannot be adequately addressed through the measures identified above.~~

a) The "Slow Streets" movement has shown how effective it can be to close local streets to through traffic, for improving safety and invigorating neighborhoods. There's no reason to take this option off the table in Arcata.

[implementation measure - City shall consider implementing a slow streets program...]

23) Table T-7: Add stop signs back into the list of traffic calming measures; Add lowered speed limits as allowed by law.

24) I propose the following change to Policy T-5a.2:

Maintain existing bicycle routes and provide additional routes where feasible connecting the various neighborhoods with Cal Poly Humboldt State University. Class ~~III~~ bike lanes shall be provided on routes with the highest bicycle demand, or where there is sufficient right of way.

a) See above discussion about Class IV bike lanes.

25) I propose the following changes to Policy T-6:

~~**Objective.** Manage parking to reduce the incentive for single occupancy vehicle use. Provide an adequate supply of parking in perimeter lots downtown. Minimize the impacts of Cal Poly Humboldt State University parking into adjacent neighborhoods. Ensure that new development provides an adequate but not excessive supply of parking.~~

T-6a **Downtown parking.** The following shall apply to parking within the Downtown area:

- ~~1. Assess and plan for future parking needs. Municipal parking lots shall be provided in the perimeter of downtown to create an adequate parking supply to serve existing businesses, future development, and to replace on-street parking removed for pedestrian, bicycle, and landscaping improvements. One municipal lot is planned to complete the City's parking system, but Assess the need for additional parking lots may be provided if additional demand or opportunities arise. The City shall explore implementing a smart parking meter system in the Downtown area to manage parking demand while generating revenue to support public transit and/or active transportation.~~

a) The concept of “adequate supply” of parking seems to reflect the assumption that a particular land use or number of people automatically translates into a certain amount of driving and parking. The evidence doesn’t bear out this assumption. Rather, we know now that the parking supply helps dictate the amount of driving. Reflecting this, and in alignment with the city’s other transportation goals, it seems logical to establish an objective to manage parking to achieve mode shift, for example by charging for parking.

For similar reasons, the idea of adding more parking lots to downtown based on “demand” seems outdated. Instead, managing parking through a smart meter system reflects modern best practices in parking management, and would reduce the subsidy for driving and create a new revenue source to help fund other city projects. See the work of Donald Shoup for much more on this topic. Verify this has been changed.

26) I propose the following changes to Policy T-8a:

Developers shall be required to construct transportation improvements along their property frontages. Where appropriate, a traffic impact study shall be required which identifies on-site and off-site impacts and mitigation measures.

The developer shall be required to provide all necessary access and circulation facilities within the property and such facilities shall be designed to meet City standards. The following improvements may be required, based on the individual context and the needs of all people using streets and the right-of-way; and that support the land-use, climate, safety, and environmental quality targets and Complete Streets policies of the City:

1. If development is located on an existing street:
 - a. dedication of right of way;
 - ~~b.—widening of street along property frontage to provide for a travel lane;~~
 - c. bicycle lane and parking lane;
 - d. reconstruction of curb, gutter and sidewalk;
 - e. transit facilities and landscaping within the right of way.
2. If development is located in a new growth area not served by streets:
 - a. dedication of right of way to construct a street to connect the project site to a public street, which accommodates all modes of transportation, particularly those walking, rolling, biking, and using transit;
 - b. construction of the street and connecting intersection(s) to City standards;
 - c. after the dedication is accepted, the City will maintain the street.
3. In all instances, the developer shall be responsible for mitigating any off-site ~~traffic mobility~~ impacts of the proposed development in a manner consistent with the policies of this plan. Measures may include ~~a reduction in the size or density of the development;~~ installation of additional pedestrian, bicycle and transit amenities to encourage alternative travel modes; or implementation of Transportation Demand Management measures.

See above discussion of LOS and congestion management.

27) I propose the following change to Policy T-8c:

The City may adopt a citywide traffic impact fee to fund transportation improvements to mitigate the ~~traffic mobility~~ impacts of new development. The traffic impact fee may

substitute in whole or in part for the off-site mitigation requirements described in Policy T-8a, but would be in addition to the developer's responsibility for on-site and frontage improvements. The traffic impact fee may be used to fund roadway extensions, intersection improvements, safety improvements, transit facility improvements, and pedestrian and bicycle facilities or amenities.

- a) This change is to reflect the discussion of LOS above, to ensure that the focus is on multimodal mobility, not traditional "traffic impacts," i.e., congestion.

28) I propose the following change to Policy T-8d:

A&MRTS should continue to fund capital and operating expenses through fare box revenue, Cal Poly Humboldt State University subsidies, and state and federal subsidies. The City will explore the possibility of new development contributing a one-time fee towards A&MRTS capital expenses through the citywide traffic mitigation fee ordinance and funding transit through parking meter revenues where feasible for some language like this.

- b) See above discussion of metered parking.

Public Facilities and Infrastructure

1. **PF-2a Capacity and management of City wastewater collection system** – Add language at the end of the policy: "... The City shall consider adopting building and land use code policies that provide incentives for design, operation, and technology for buildings and sites to minimize wastewater as well as stormwater loads." *(We already have policies for minimizing stormwater flows to sewers in the MP4 program. This would add policies to reduce wastewater discharges to sanitary sewers, and thus loadings to the WWTP.)*
2. **PF-2d Composting and beneficial reuse of biosolids ...:** Add language at end of policy: "... This requires the City to protect the quality of its sludge by implementing an industrial and high-volume discharger wastewater pre-treatment program. (See Policy PF-2g Source Control Program, below.)" **[staff: this is unnecessary]**
3. **PF-2e Treatment of wastewater from other communities** – Add language at end of policy to read "... The City shall not enter into any new agreements for processing wastewater from other communities, nor shall the City accept additional loadings from any connection from other communities through the Fieldbrook Glendale system." *(This may seem like overkill, since the contracts probably already mention this, and LAFCo should also have insisted on it. But it's important NOT to take it for granted! There have been several recent cases in which water supply extensions in the county have been proposed to do similar things, such as getting water to the proposed Casino hotel in Trinidad by extending water lines from McKinleyville)*
4. **Policy PF-3 STORMWATER MANAGEMENT** – Cut "... and acquire easements and properties for effective drainage management" from the goal list. This is a policy means to achieve the goals, not a goal in itself. The Policy is already stated in **PF-3e.**
5. **PF-5e maintenance of City streets and rights of way**—Add language at end of policy: "...The City shall comply with Americans with Disabilities Act requirements as a minimum, and seek to develop City rights-of-way beyond those requirements to safely accommodate mobility by people of all abilities and disabilities."
6. **POLICY PF-6 INTEGRATED WASTE MANAGEMENT** – Add language at end of "Objective": "... Coordinate with regional bodies to develop effective regional solid waste management systems."
7. **2.12 IMPLEMENTATION MEASURES** – Add as an implementation measure to review and update the WWTP operations and facilities plan periodically to take into account changes associated with sea

level rise and climate change. This is either an ongoing or periodic action, which would implement PF-2b Arcata wastewater treatment system.

8. PF-5a: Facilities for community service and private organizations. Suggest adding the bold phrase in this first sentence: "Community service organizations, as well as non-profit and private organizations serve an important and vital role in the health of our community and offer shelter, assistance, training and other human services."

9. PF-6a: Source Reduction. Suggest replacing #6 with the following: "Moving away from using paper copies for as much City business as possible by working with electronic mail, forms, and agendas, and re-using of scrap paper if possible, if copying is necessary."

1. Specific suggestions for the "Guiding Principles and Goals" appear here, even though they are now later in the Draft Element [Staff: unclear what the add is]:NOTE: added language is the gold language.

Guiding Principles and Goals.

- A. Provide an adequate, safe, and affordable water supply and delivery system for day-to-day and emergency needs.
- B. Maintain and improve wastewater management systems that will protect water quality in an affordable manner by updating wastewater technology and reducing wastewater and stormwater loads that the City must treat. Maintain the Arcata Marsh and Wildlife Sanctuary as an exemplary model of how natural systems can be effectively and efficiently used to treat and reclaim wastewater.
- C. Utilize natural systems and processes for managing stormwater with preference for approaches that reduce stormwater flows to City facilities while also preventing undesirable flooding.
- D. A. Promote lifelong learning by supporting educational facilities and programs at all levels.
- E. A. Recognize that public facilities are the primary gathering places for social, cultural, political, educational and entertainment events/celebrations, and that these facilities are important components of the community's identity.
- D. Advance Meet state-mandated waste diversion goals set forth in state mandates and the City's Zero Waste Action Plan. Arcata will strive to become a leader in developing small city waste reduction programs. Publicly advocate reducing solid waste as the first priority for waste management; and promote recycling-based manufacturing through: City purchase of recycled products, education, and community support. Support new technology and education programs that reduce solid waste by an additional 10% every five years and maintain a long-term strategy for achieving "Zero Waste".
- E. Promote lifelong learning by supporting educational facilities and programs at all levels. The City government will encourage educational institutions to cooperate with the City to achieve City goals in our shared space.
- F. Recognize that public facilities are the primary gathering places for social, cultural, political, educational and entertainment events and celebrations, and that these facilities are important components of our the community's identity. Protect public civil and Constitutional rights in Arcata's public sphere, and especially on City property and public rights of way, and in any facility that receives City support.

2. PF-2b Arcata wastewater treatment system – Add language at the end of the policy: "... Goals, priorities, planning assumptions, and the best available science on which they are based, shall be reviewed publicly through City committees and the Planning Commission." "... Goals, priorities, planning assumptions, and the best available science on which they are based, shall be reviewed publicly through City committees and the Planning Commission at the discretion of the City Council."

[Staff: This would be supported by staff if the specificity of committees and commission was removed. All of these policy choices are vetted publicly, and the Council will likely want the ability, not the requirement, to refer such decisions to committees or the Commission as it chooses.]

3. POLICY PF-4 EDUCATIONAL FACILITIES. Objective. This section currently reads: "Identify student enrollment increases, based on the projected future population of the City, and coordinate with

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local school (public and private) districts, Cal Poly Humboldt State University, and other education providers to maintain and improve educational facilities and services, **while preserving established community/student ratios.** In light of the expected growth of Cal Poly Humboldt, can we realistically expect to **“preserve established community/student ratios.”** I suggest we can’t. Do we want to change this wording? Proposed change: Delete it **preserve established community/student ratios.**

4. PF-2c Change and add **[Staff: change “California” to “state” and “compliance with the state water quality control board”]**

PF-2c Protecting, improving, and restoring water quality: Protecting surface and ground water quality, preventing water pollution, restoring water quality in waterways and wetlands within the City and in receiving waters of California and the United States shall guide design, construction, and operation of the City’s water management infrastructure. The City shall use necessary resources to comply **Compliance with California Regional Water Quality Control Board wastewater treatment and discharge standards.** The City shall regularly test its wastewater discharges and make necessary adjustments in treatment processes levels, to ensure that effluent it meets California Regional Water Quality Control Board standards, and of. The City shall also keep its National Pollution Discharge Elimination System Permit (NPDES) permit, current and in compliance with U.S. Environmental Protection Agency standards.

5. **PF-2f Maintain the Joint City/ Cal Poly Humboldt Wastewater Utilization Program –**

PF-2f **Maintain the Joint City/ Cal Poly Humboldt ~~State University~~ Wastewater Utilization Program. Recognize that Cal Poly Humboldt ~~State University~~ faculty and students were instrumental in the design, testing, and development of the Arcata Marsh and Wildlife Sanctuary. The City and the University jointly participate in a wastewater utilization program, which provides ongoing research projects for students and faculty studying wastewater, stormwater, and water quality issues. The City and University maintain an five-year agreement to operate the program, with the City providing the funding and the University providing the student research and faculty advisors. The City shall renew the program with the University when the current agreement ends, and the City should collaborate with Cal Poly faculty in seeking funds for future research. ~~as long as there are funds available to compensate the University.~~**

Change shall to should in last sentence.

(Considering the additional burden that Cal Poly’s expansion will impose on the City’s treatment system, directly and indirectly, paying for research into the system’s future operation improvements should be a JOINT funding effort, especially since the environmental and civil engineering focus at the new Cal Poly should enable the University/City collaborative to seek grants to fund the research. In fact, the University should probably provide money to the city for this, rather than the other way around!)

6. **PF-1c Water Conservation – Add language at the end of the policy: ...**

7.

8. **“In response to extreme water shortages, the City may consider imposing sharply graduated excessive use rates and/or excessive use fines, additional forms of water rationing, warnings, and physical flow restrictions to water users who fail to respond to less severe sanctions.**

[[add Rachel’s first and last sentence.

Building and site development permits that require connections to the City’s water system shall incorporate water conservation design features and best management practices.” *(Presumably, objective standards for those design features and BMPs will be defined somewhere in the building or land use code, or by state standard.)*

Other Matters

3. **PF-5d: Telecommunications facilities.** This section states that, "These facilities shall be screened from view and associated equipment rooms and switching devices shall be designed and landscaped to blend with their surroundings." The new facility at 11th and M Streets does not meet these conditions. Can that be remedied?

4. **General:** *There are several situations where we should consider possible zoning changes reflecting recent status of uses that might be considered for Public Facility zoning, or Public Facility zoning that should be considered for other potential uses. It would be important to solicit input from the management and owners of those facilities. These include such sites as the Mad River Community Hospital (current site Master Plan to be updated); UIHC/Potawot (some of the site covered by conservation easement); Open Door Community Health Clinic (serves some public needs, including emergency facilities); HealthSport (privately owned/ managed facility after initial public partnership); and possibly others. It would be useful to provide some pathway for charter schools NOT operating on school district property to have some pathway to develop permanent facilities that would be zoned as Public Facilities, rather than jury rig the current underlying zoning to accommodate them.*

Develop some language around other public facilities and add it to the PF Element, then reference the LU Element

5. The "Guiding Principles and Goals" (now p. 2-78 of Draft) should be moved to the BEGINNING of the element, before the overview of current facilities. They are intended to be the basis of the analysis of planning needs, and of the policies and implementation actions that follow.

6. An introduction paragraph should be added at 2.10, indicating which facilities & infrastructure the element addresses, and which it explicitly does NOT address, including park and open space material that appears in the open space and conservation Element(s), healthcare facility material (if we decide NOT to add it here), etc.

7. Move the sections of the Element about schools and other public facilities NOT related to water or sanitation either to the beginning of the element (before the parts about water and sanitation) or to the end. But don't strand them between the wastewater and the garbage! That re-ordering should happen in both the "Overview" part and the "Policy" part of the Element.

8. **Make the changes in brown below**

POLICY PF-4 EDUCATIONAL FACILITIES

Objective. Value Arcata's educational institutions and facilities as keys to achieving the high educational standards that will lead to prosperity and community wellbeing. Enlist schools and the University in support of Arcata's future prosperity, and our diverse and tolerant cultures. Seek the cooperation of Arcata's educational institutions to achieve City goals. Identify student enrollment increases, based on the projected future population of the City, and coordinate with local school (public and private) districts, Cal Poly Humboldt State University, and other education providers to maintain and improve educational facilities and services. Coordinate with Cal Poly Humboldt to project demand for City services and facilities based on anticipated increases in enrollment and employment, while preserving established community/student ratios.



ARCATA HIGH SCHOOL

PF-4a Coordination with Arcata, Pacific Union, and Jacoby Creek school districts, the Northern Humboldt Unified High School District, and with Charter School operators. The City shall provide demographic information to assist the School Districts and charter schools in projecting future student enrollments. The City shall encourage the school districts and charter schools to expand existing schools rather than designating new sites for this purpose.

9. PF-4a Clarify how the Plan will treat Charter Schools, which are public schools, that do not operate in buildings and facilities zoned for "Public Facility" use. There are (or have recently been) a few actual private schools, also operating in facilities not zoned for "Public Facility" use. **public and charter schools are allowed in all residential zones, in the creamery district, in the gateway area, and in Public Facilities zoned parcels]**
10. PF-6a Source reduction – Almost all of the "examples of effective source reduction and reuse activities that shall be promoted" are private personal actions, largely unrelated to City actions or policies. They may (hopefully) seem routine by 2045. They don't seem appropriate to include in this Plan. The exception which should remain is #9, which is a City policy/action, and should remain: "Incentives such as on-call garbage collection and differential solid waste fees shall be used to encourage source reduction." [proposal is to delete this section.]
11. PF-1a: Suggest striking the phrase, "...though the City is well within its water allotment." It is not necessary and is out of place.
- PF-1a Water supply. Surface and subsurface water quantities that supply the City are dependent on rainfall and adequate upstream storage. The City shall continually monitor the water quantity and quality in its system and adhere to the Humboldt Bay Municipal Water District's rationing system to ensure that adequate supplies reach all users, though the City is well within its water allotment.
12. Page 2-80: HBMWD's Rationing System: The percentage in #3 is incorrect. It currently reads, "...when Ruth Lake reaches 30% capacity..." It should read, "...when Ruth Lake reaches 70% capacity..." [Delete the box related to PF-1c but refer to the HBMWD policy in the text.]
9. PF-2b: Suggest inserting the bold phrases into the last sentence and rewording slightly to read: The City acknowledges that it must plan for the possibility of a 1 meter sea level rise by 2050 and shall ensure ongoing treatment system planning, investments, and mitigations are consistent with this possible sea level rise, while balancing the City's existing investments with habitat restoration and sea level adaptation priorities. [[Discuss with LCP]]

[Staff: This is not the adopted policy of the City and we would not suggest we make it so. The NOAA guidance eliminated the H++ scenario for the purposes of SLR planning. The H++ predicts up to a meter by 2050. This language needs to be finessed if adopted to reflect the policy work that has led up to this point]

10. Page 2-76: The Zero Waste Action Plan calls for, "... a goal of achieving 90% landfill diversion by 2027." Is this realistic? Do we want to continue to claim this? [ES can verify and update the date]
11. Page 2-77: Are we conforming with AB1383? This seems like a huge effort. How will the City do this? What is the timeline? [have ES provide information on this]

Editorial Suggestions

The "Overview" sections for each set of facilities refer to many management plans. Note that all of these must presumably be (or be amended to become) consistent with the General Plan.

Editing: Indicate in the element WHEN (the YEAR) each of these plans was adopted or most recently updated. Readers should know the plan is adopted in 202(4?) so that most recent version is as of now.

As elements are amended in the future, those dates can also be updated.

Format: Avoid text boxes that use less than the full width of a page in a digital version. The "side-by-side" formatting makes the document difficult to move between digital formats.

Consider adding a section about health care facilities. Even though that may also appear in the “Healthy City” element (if such a thing will really exist!) it’s important to at least mention Mad River Community Hospital, United Indian Health Center/ Potawot, and Open Door Clinic, since their presence and development all also have significant land use and service implications.

Revise basic info about schools in Arcata according to the editing suggestions I’ve provided in the MSWord “track changes” version I’m submitting attached to the same email as this set of suggestions. My suggestions are detailed and extensive.

The Overview / background about changes in state solid waste diversion policies should be shortened (as suggested in the “track changes” version I’ve submitted), remembering that the Plan looks forward and must still make sense to someone reading it in 2045. The background of increasingly stringent state regulation should focus on goals that Arcata must reach, and on the idea that Arcata needs to anticipate more stringent state regulation, and that Arcata can become a small town leader in progressive waste management.

Public Safety

- 1. I propose adding a new policy PS-8g
 - a. **Traditional ecological knowledge:** The City of Arcata acknowledges the value of Indigenous sciences and knowledge and the need for Indigenous perspectives in responding to the climate change crisis. The City shall work to support Indigenous-led climate adaptation approaches and shall work collaboratively with tribes and tribal governments for mitigation, adaptation, and resilience to climate change. This policy applies to all previous policies in this section.
 - b. Traditional ecological knowledge holds important information regarding adapting to climate change and developing a more sustainable and safe community.
 - 2. I Propose adding a new policy PS-5f
 - a. **PS-5f: Smaller Fire Trucks** The City and Arcata Fire District shall jointly investigate the feasibility of purchasing smaller fire trucks that are more maneuverable and perform better on pedestrian friendly streets.
 - b. Large fire trucks often require wide streets that are unsafe. Cities across the country are exploring purchasing smaller fire trucks like those used in the rest of the world in order to allow safe fire access while preserving safe streets. See article for more details: <https://usa.streetsblog.org/2018/12/10/safety-officials-to-cities-stop-buying-such-huge-trucks/>
- [Staff: This could probably be an implementation measure instead of a policy. If adopted on consent, we will move it to the implementation measures]**

Policy Pitch

- 1. I propose adding a new policy PS-7g
 - a. **PS-7g: Reducing Armed interactions with the Police** The City of Arcata recognizes that unnecessary interactions with armed police officers have the potential to end tragically. The City

and the Arcata Police Department shall jointly explore opportunities to reduce interactions between members of the public and armed police officers.

- b. This goes along with the antiracism section above. Obviously, there is a need for an armed police force. But many interactions with the police do not require an armed officer, for example routine traffic stops. I think it would be fruitful if the City and the Police Department jointly explored opportunities to reduce these kinds of unnecessary interactions. This article covers why these kinds of reforms are necessary and also discusses some of the efforts other cities are making. <https://www.theguardian.com/us-news/2022/sep/21/us-police-violence-traffic-stops> [Staff: see above PS-7f recommendation]

2. I propose adding a new policy PS-7f

- a. PS-7f: Anti-racism The Arcata Police Department shall institute policies and trainings in order to combat and prevent both systemic as well as overt racism within the Department.
- b. Felt like it was missing from this section

[Staff: instead of PS-7f and -7g, propose: PS-7f. Principled Policing The Arcata Police Department shall conform to State and Federal law, California Peace Officer Standards and Training (POST) standards, Department policies, and 21st Century Policing best practices to combat racial profiling and bias in policing and to promote de-escalation and principled policing.]

(Adopted and Added to conflicts table: The Arcata Police Department shall institute policies and trainings in order to combat and prevent both systemic as well as overt racism within the Department.)

13. PS-6g: Hazardous materials education program. Suggest including the following sentences shown in bold: The City shall work with the Humboldt County Health Department and the California Department of Toxic Substances to develop **and promote** educational materials explaining hazardous materials' impact on people, plants, and animals, and provide information on alternatives to hazardous materials. **The City shall also keep a compendium of Material Safety Data Sheets (MSDSs) for all potentially hazardous materials that are used by all departments in the City. All this information—including the MSDSs—shall be made available to the public.** [Staff: this is unnecessary as it is required by law. But we can add it if the commission wishes to.] **"city promotes public education about storage, use, and release of hazardous materials."** **"implementation measure to identify how folks can learn about the City's MSDS forms" refer to illness prevention program.**

14. Implementation Measures. PS-7: Add the following: **The City will maintain and have available all MSDSs for hazardous material utilized by the City.** [Staff: this is unnecessary as it is required by law. But we can add it if the commission wishes to.]see above

15. PS-8d: Sea Level Rise. Suggest inserting the following sentence and phrase shown in bold: **"Using guidance from the State and other climate scientists, the City will plan for a sea level rise of 1 meter in the year 2050. Using this assumption, the City will incorporate consideration...."** (Also, the word "local" in the last sentence has a typo.) [Staff: we recommend against committing to a specific elevation or set of guidance sources. The science is evolving, and the Council should commit to adaptation based on adaptive pathways, given latest science and social impact over time.]

Historic Preservation

Consent

- 1) Make the following change to H-6a,
 - a) **H-6a General Incentives for Historic Structures and Sites.** Those properties listed as Historic Landmark or as Potentially Historic structures or sites shall be eligible for the following incentives and assistance:
 - 1. Exemption from the requirements to provide any additional off-street parking, except for additions 200 or more square feet in size.
 - b) We need to reduce reliance on cars, so elimination of the off-street parking requirement makes sense.
- 2) I propose we change Policy H-4 in the following way:
 - a) **Objective.** The Central Arcata, Arcata Heights, and Bayview neighborhoods, are designated Neighborhood Conservation Areas. Ensure that new construction, modifications or alterations of historic structures, and significant changes to other structures are harmonious with established and planned neighborhood elements within ~~the existing character of~~ the Central Arcata, Arcata Heights, and Bayview neighborhoods.
 - b) *Pitch: We already made a similar change in the Land Use Element. This language is less vague and doesn't carry the same negative connotations.*
- 3) I propose we change Policy H-4 in the following way:
 - a) H-4b **Design review.** All structures located within an NCA shall be subject to design review and approval. Prior to approval of any exterior change requiring a building permit, the Design Review Authority shall make a finding that the alteration or addition is compatible with established and planned neighborhood elements ~~and does not destroy the historical or architectural elements character of the property, or the surrounding neighborhood conservation area~~.
 - b) *Pitch: We already made a similar change in the Land Use Element. This language is less vague and doesn't carry the same negative connotations. [Staff: The historical elements on listed or potential resources will be protected through CEQA]*
- 4) **H-1 DESIGNATED HISTORIC LANDMARKS Objective:** Designate and preserve significant buildings, structures, and sites that are representative of the city's social and physical development ...
 - a) **ADD at end** "Support property owners' efforts to preserve, restore, and adapt use of significant structures and sites for continuing significance."
- 5)

Policy Pitch

- 1) Make the following change:
 - a) **H-2d Design review approval.** Design Review and approval shall be required from the appropriate review authority for all exterior alterations to ~~noteworthy structures~~ potentially historic structures, when or if alterations require a building permit, including changes in types of materials and additions. The review authority may request a recommendation from the Historic Landmarks Committee prior to its decision on the project. ~~In the event that the City reasonably believes that a structure may be potentially historic but said structure is not on the potentially historic buildings list, the National~~

Register or listed as a local, state or national landmark, the City shall initiate the process of listing the structure on the potentially historic buildings list. If the City decides to initiate such listing process, the permit shall not issue pending completion of that process.

- b) Not sure what a “noteworthy” structure is – it is not defined - so I suggest changing the term to make it consistent. More significantly, the City may or may not be able to keep current its listing of potentially historic buildings. In order to prevent a building that is historic but not on the list from falling through the cracks, I propose adding language that requires some level of review to determine whether an unlisted property is or is not a potentially historic resource.

2) Implementation measures

- a) Make all implementation measures “ongoing”.
- b) Should we say each implementation measure should be “ongoing,” rather than adopt a specific time frame? I don’t know how realistic the stated time frames are and don’t see the harm in keeping the time frames open-ended. [Staff: we generally outline timeframes to accomplish the measures to ensure that the objectives are being attained. We can spread the timeframes out if the Commission does not believe they are actionable, but we recommend setting timeframes to monitor progress implementing the policy.]

3) H-1b Local Historic Landmarks designations: *The Policy should indicate WHO is responsible for determining Local Historic Landmark designation, even if the Overview material also explains this. Also, “... One or more of the following criteria shall be required for a structure or site to be eligible for listing,*

- a) [*including* that the structure, or historically significant features of the site, must be at least 50 years old. *(This is important, since without it proponents could seek to list as “historic” locations commemorating potentially momentous but very recent events, with no additional features. There may be other ways to protect such sites, but they shouldn’t be subject to historic preservation General Plan policies or Land Use Code designation.)*[Staff: the designation of historic resources actually can be made on structures that are less than 50 years old. This is embedded in the Secretary of Interior’s standards, as well as CEQA case law. Staff will not recommend the Council adopt this change.]

4) H-1d Design criteria for alterations of and additions to local Historic Landmarks:

- a) *Specify the Review Authority as of 2023! And “1. Any exterior modifications or alterations, including changes in materials” Elsewhere in this Element, similar policies specify that this is for modifications that require a building permit. In this case, the threshold is much lower, but is not specified. Presumably, that could include just slightly changing a paint color. (Also, in H-3c for the Plaza.) Let’s specify thresholds or provide examples to avoid trivial and expensive review requirements. [Staff: the first change is unnecessary and will be dated at the zoning ordinance is updated from time to time. The second change does not have a specific recommendation for modifying the language, but staff notes that this level of detail should be left to the zoning ordinance. The current ordinance specifies this detail.]*

5) H-7a Cultural Resources Project Review: As part of the environmental and project review process, all proposed discretionary projects under the California Environmental Quality Act shall be subject to cultural resources sensitivity review by the local area Tribal Historic Preservation Officers (THPOs)

Under these conditions, ONLY discretionary projects subject to CEQA will be referred to the THPOs for review. With this language, under proposed ministerial approval processes for large new infill

housing projects, those projects aren't discretionary, so aren't subject to CEQA review, and so wouldn't be referred to the THPO for Tribes' review. Even if the eventual permit requires work to stop if arch material is unearthed, damage could already be done. If we're proposing some means to determine whether a project meets "objective standards" that would allow for ministerial project approval, we should make sure that those "objective standards specified for streamlined approval of new housing or mixed use projects should include a THPO response to the City's request for THPOs to consider project plans."

[Staff: the City is working directly with the Wiyot Tribe to refine this language. Staff recommends the Commission defer to their judgement as to how their cultural resources are managed and identified through our planning processes.]

Other Issues

- 1) H2-c and H2-d: Who provides "Design Review" and who is the "Review Authority?"
- 2) H1-d and H1-e: This policy states that the changes to historic buildings cannot occur without approval from a "Review Authority."
 - no exterior modifications
 - no additions,
 - no new construction on the site,
 - a) Who is this Authority? Further, the need to employ a "cultural resources consultant" to evaluate possible changes is another possible obstacle to modifications. These restrictive regulations and the need to navigate the approval process could discourage changes that may be desirable, such as energy conservation measures, addition of an ADU, or construction of new units through infill, among others. Do we want to be this restrictive? **[Staff: The review authority is identified in our zoning ordinance. It depends on the type of project it is, but most historic resources are reviewed by either the PC or the CC. You can review these at https://www.coderepublishing.com/CA/Arcata/61/LUR/ArcataUC0970/ArcataUC0972.html#0972_040. Without amending CEQA, which holds special process for historic resources, we are obligated to review projects on historic resources with a higher degree of scrutiny. ADU's on historic properties are explicitly excluded form the permissive state law due to this.]**

Editorial Comments

Policy H-1a, 4th line – Delete "and counting"

Policy H-1d and throughout the document in several places – (1) the phrases "review authority" and "design review authority" must be defined (maybe they are, somewhere else in the General Plan), (2) need to be consistently lower or upper case; and (3) referred to consistently as the "review authority," "Review Authority," "design review authority" or Design Review Authority.

If the phrase "review authority" is different than "design review authority," that needs to be clarified and defined.

Policy H-1f – change "HSU" to Cal Poly Humboldt

Policy H-2a – needs reformatting

Policy H-2b - change "HSU" to Cal Poly Humboldt; change "noteworthy" to "potentially historic"

Policy H-2d – should be renumbered as H-2c or moved to appropriate location

Policy H-2c - should be renumbered as H-2d or moved to appropriate location

Policy H-3b – needs reformatting

H2-c and H2-d are reversed.

The [AP style guide recommends](#) capitalizing the word Indigenous

Design

1. D-1g: Provide for bicycles, pedestrians, and transit in design. Suggest changing one word as follows: “Design shall ~~incorporate~~ encourage provisions for bicycle and pedestrian circulation,....”
2. D-2b: Streetscape design. Suggest changing this to read: Future changes to public street rights-of-way in the downtown shall include amenities such as street furniture, access and safety for pedestrians and bicycles, while maintaining vehicle access including the possibility of “car free” streets. And add a #10 to the list: Consider the possibility of “car free” streets.
3. D-2h: Site design, including parking areas. Suggest the third sentence be modified as follows: Parking areas shall be the minimum necessary and be separated from the street and sidewalk by a landscape buffer.
4. D-6a, #8: This begins with, “Parking areas are encouraged to be provided....”. We should not be encouraging parking. Suggest the following, “If parking is necessary, the minimal necessary parking should be located....”
5. 5.1 INTRODUCTION: Reverse the order of the first 2 paragraphs. It’s Arcata’s plan; state requirements come second.
6. Guiding Principles and Goals - I: Incorporate “green building” and “universal design” concepts and features into new and renovated structures.
 - a. Since this is for new and renovated structures, Arcata should aspire to advance accessibility as well as sustainability over the next 2 decades to 2045, not play catch-up to current minimum practices.
7. POLICY D-1 OVERALL COMMUNITY DESIGN 3. This shall be accomplished by Providing articulation in building mass, surfaces, rooflines, wall planes, and facades, and including distinctive architectural features and ornamentation.
 - a. Architectural distinction comes from many types of features beyond “ornamentation.”
8. D-3g Wooded hillsides. Views of wooded hillsides forming the City’s eastern edge from vantage points along public streets west of the State Route 101 should not be blocked by development to the extent practicable, balancing development rights in these areas. Any impairment or partial obstruction of these ocean views from new development shall be the minimum necessary for allowable development. The City shall encourage Cal Poly Humboldt to avoid blocking views in its new development.
9. D-3j Streamside riparian areas. Creeks or drainage channels and any associated riparian vegetation shall be retained in a natural state and incorporated into site design as a visual asset to development which that adjoin them. Design codes should encourage “daylighting” streams on City and private property, and restoration of riparian ecology and function.
10. D-4a Design of roadways, and subdivision and redeveloped site improvements. New subdivisions and infill or redeveloped site design shall provide orderly arrangement of
 1. Proposed street alignments including sidewalks and bikeways shall conform to the relevant Transportation and Road Safety Plans, wherever possible. Unless it is demonstrated to be infeasible, all new lots shall have frontage on a public street or improved alley, or publicly accessible courtyard or open space.
39. Appropriate landscaping and illumination shall be applied to enhance safety and provide attractive screening and distinctions between spaces; illumination of streets shall be

unobtrusive and the lowest intensity compatible with safety, complying with night sky standards wherever possible.

- a. Where more lighting for safety appears to conflict with less lighting for night skies, design standards should provide clear standards and rationales for priority of lighting choices.
11. D-4b **Lot patterns.** Lot boundaries should be regular in shape and lots should either have direct access to a public street or to an access easement which connects to a public street. Clustering of lots with common open space areas and/or common parking lots is encouraged. Condominium or other common multiple-unit tenancy designs may be approved with alternative vehicular access requirements, within fire safety considerations.
 - a. Designing livable developments not dominated by roadways and parking, and with safe interior spaces free from motor vehicles is a crucial aspect of creating pleasant living spaces, especially for children and people with mobility or sensory impairments.
12. D-6b **Design of Mixed Use Development...** Require main building entries to be visually prominent and oriented to a public street or pathway, or publicly accessible courtyard.
13. D-7d **Site design criteria.** Landscaping shall be an integral part of site development, connecting site design elements, enhancing the site identity, and creating a pleasing appearance. Landscape designs shall conform to the following criteria: should incorporate existing natural vegetation where appropriate; provide for erosion control, help to manage stormwater onsite for absorption and percolation, and for privacy, and beauty.
14. I propose the following change to policy D-1e
 - a. D-1e **Promote energy efficiency and solar access.** Site and building design shall emphasize energy efficiency, solar orientation and consider minimize shading of adjacent structures to the extent feasible, balancing development needs with solar access.
 - i. The current language values existing structures' shading concerns over the potential for new, energy efficient development. Should we minimize shading caused by a new, energy efficient building, presumably by reducing the number of units, in order to protect the solar access of an existing structure?
15. I propose the following change to policy D-2b
 - a. Provide or improve bike lanes, with an emphasis on protected bike lanes, where appropriate.
 - i. *We've discussed the safety benefits of protected bike lanes before.*
16. I propose the following change to D-3h
 - a. D-3h **Farmlands and open countryside.** Views of farmlands and open countryside — in the Arcata Bottom, along the State Route 101 south of Samoa Boulevard, north of Giuntoli Lane, and along State Route 255 west of the City, should be protected to the extent practicable, balancing development rights in these areas. Any impairment or partial obstruction of these views from new development shall be the minimum necessary for allowable development.. New development should be sited and designed to minimize any impairment of such views.
 - i. *All of the other view policies have similar balancing language*
17. I propose the following change to D-6a
 - a. The siting and design of buildings shall promote energy-efficiency and consider solar access, balancing development needs with solar access, and shall minimize impacts on other nearby uses.
 - i. *Same change as D-1e. Don't discourage new energy efficient structures because they will shade existing structures.*
18. I propose the following change to policy D-2b
 - a. D-2b **Streetscape design.** Future changes to public street rights-of-way in the downtown shall include amenities such as street furniture, access and safety for pedestrians and bicycles, while maintaining vehicle access.

- i. *We may some day in the future want to have a plaza/downtown with reduced vehicle access. This deletion makes that possible.*

1. **Policy D-2b** I propose changing Policy D-2b to maintain sub-policy 10, regarding undergrounding of utilities, instead of eliminating that policy, as the draft does. I would either keep the former language of **“Require undergrounding of utilities and elimination of poles and overhead wires,”** or change that language to **“Encourage undergrounding of utilities and elimination of poles and overhead wires.”**

- a. My rationale: Undergrounding of utilities provides for a more aesthetic (and perhaps safer) streetscape. I recognize that undergrounding costs substantially more than overhead utilities, so I would be OK with changing the wording to “encourage” rather than “require.” **[Note: this is discussed in a proposal below as well]**

2. **Policy D-2e** I propose changing Policy D-2e to maintain the deleted first sentence, but change the wording slightly, so the first sentence would read: **“The height, scale and mass (volume) of new buildings and additions to existing buildings shall be compatible with take into account other buildings in the immediate vicinity.”**

- a. My rationale: I assume the former language was stricken because the word “compatible” is subjective and could lead to varying interpretations of what and what is not permissible. However, to “take into account” a new structure’s impact on its neighbors only means that the impact of such features as height, scale and mass be recognized. This does not require project-by-project review of height, scale and mass, as I believe the existing zoning code does, and any potential new form-based code for this district would consider the impacts of new structures on existing ones, through regulation of such things as setbacks, height restrictions, rear yard requirements, etc.

As a point of comparison, Policy D-5a lays out quite specific design criteria for multi-family residences.

[Staff: this policy has led to disagreements about buildings that meet the code standards for height and setbacks & etc, but are larger than buildings around them. Development should be consistent with objective standards. If we wish to transition between smaller buildings and larger buildings, a standard could be established that no building could be more than say two stories larger than some percentage of the buildings immediately adjacent to them. This of course would significantly impact the development potential of our vacant sites, but it would be an objective standard. Any subjective standard will inevitably lead to disagreements about how to interpret when a four story building is proposed next to a one story building.]

3. **Policy D-1b** I propose changing the following highlighted language:

- a. **Emphasize Arcata Plaza area as the central—the main—the main community focal point (Not applicable in Coastal Zone).** Buildings fronting on streets around facing the Arcata Plaza shall be multi-story. Architectural and other design elements shall emphasize the importance of the Arcata Plaza as **the City’s center as the community’s main focal point** for commerce, entertainment, and special events. Designs shall promote pedestrian access and continuity of retail space at the **street level back of sidewalk**. Parking should be accommodated off-site to the extent practicable.

b. We should talk about this! The Plaza SHOULD remain Arcata’s central community focal point! This is one of Arcata’s most distinctive features, and our plan should endorse that. Also, calling for all and only multi-story buildings fronting on the Plaza may contradict some aspects of the Plaza historic district, even if the number of stories isn’t one of the Plaza’s listed features.

4. D-5b Single-family residential design Add statements concerning accessory dwellings, but especially this one, after the statement re hillside areas: In light of policies that encourage addition of accessory dwelling units on “single family” zoned lots, ensure strive for that accessory dwelling unit design that: complements surrounding development; and retains privacy; access to direct sunlight; and a sense of appropriate scale. [Staff: the term “ensure” should not be used in this context. Instead, “strive for” or something less rigid could be used.]

5. Policy D-2b I propose the following changes in red text to this policy:

D-2b Streetscape design. Future changes



to public street rights-of-way in the downtown shall focus on improving include amenities such as non-obstructive street furniture, access and safety for pedestrians, ~~and bicycles,~~ and reasonable and safe while maintaining vehicle access. The following design features should be considered in future improvement projects:

1. Increase the width of sidewalks.
2. Demarcate pedestrian crosswalks with pavement marking or special paving materials or colors.
3. Provide or improve bike lanes, where appropriate.
4. Incorporate street trees in appropriate locations.
5. Use special paving materials or patterns for sidewalks at key locations or intersections.
6. Provide landscape screening between parking lots and the street, with preference for parking associated with new buildings behind, or away from street frontage.
7. Provide shielded and directional street and parking lot lighting that is adequate for safety but that is not overly bright.
8. Establish a uniform lighting fixture and post (or pole) design for streetlights;
9. Establish a uniform design for various items of “street furniture,” such as benches, trash receptacles, water fountains, etc.;
10. Strongly encourage undergrounding of utilities to eliminate poles and overhead wires. undergrounding of utilities and elimination of poles and overhead wires.

a. We should reconsider several design features in Policy D-2b Streetscape design, as shown on the next page. Street furniture can enliven a streetscape, but it’s crucial that over-furnishing doesn’t negate the value of widened sidewalks by blocking easy passage of more than one person walking abreast, wheelchairs, etc. We can be explicit here about preferential location for parking behind or away from street frontage (6). Reconsider uniform lighting design or street furniture for downtown. Varied functions and locations call for different lighting designs, not uniform ones, and street furniture should accommodate its locations’ unique characteristics. Uniform requirements will also lock the City into purchases from a limited number of vendors, and potentially “freeze” designs after they are no longer the most appropriate choices. Finally with 2045 in mind, we SHOULD strongly encourage utility “undergrounding,” and sooner rather than later!

6. **Policy D-2e Design criteria for new structures and additions:** Each building shall have an entry from the sidewalk to the street-level floor. Building elevations shall be articulated: long, continuous, unbroken wall and roof planes should be avoided. The visual organization and proportions of building elevations — including the size, spacing and shape of window and door openings — should be harmonious consistent with neighboring buildings. Architectural detailing and ornamentation, such as cornices, eaves, recessed or covered entryways, and awnings, are encouraged. Design review applications shall include depiction of buildings on adjoining lots, either in elevation drawings and or photographs.

a. Requiring consistency can stifle desirable innovations. For projects that require design review, whether we call for consistency or harmony, understanding the CONTEXT of the proposals requires both elevation drawings or simulations AND photographs of neighboring structures and sites. Digital documents make that more straightforward than old and expensive paper blueprints or color prints.

7. **Policy D-3f** I propose the yellow highlighted changes to policy D-3f: **Bay and ocean views.** Views of Arcata Bay and the Pacific Ocean from vantage points along public streets in hillside areas of Arcata shall not be blocked by development be preserved to the extent practicable, balancing development rights in these areas. Zoning and land use policy decisions, which expand or limit property rights, shall analyze the potential for newly allowable development to obstruct views of the Bay or ocean. Any impairment or partial obstruction of these ocean views from new development shall be the minimum necessary to for allowable reasonable development.

a. Since changes to our zoning actually expand or limit those property rights, this is a pretty meaningless statement. Protecting views must be taken into account in making zoning and land use policies in the first place! CEQA caselaw has recently limited the usefulness of CEQA in protecting viewsheds; Arcata must make our own policies beyond using CEQA as a case-by-case crutch.

Health

1. Introduction, Bottom of Page 2-1: There are four hospitals in Humboldt County, not two. The statement should include Redwood Memorial Hospital in Fortuna and Jerold Phelps Community Hospital in Garberville, as well as St. Joseph Hospital in Eureka.

2. Guiding Principle and Goals – amend A and E as follows:

a. A. Ensure all residents can enjoy physical, social, and mental health and wellbeing, regardless of their race, age, gender, sexual orientation, social position, economic position, culture, background, or any other factor of identity. ...

E. Support healthy eating and active living programs and facilities to enable them.

3. H-1a **Determinants of health.** Work with the community and stakeholders in medical, mental, and dental healthcare fields to identify determinants in the Arcata community that impact health and wellbeing, paying specific attention to factors affecting specific groups or demographics disproportionately. Facilitate access to financial and professional resources to document and utilize necessary epidemiological and health care data.

4. Add a new policy H-1e

a. H-1e Retain existing healthcare institutions and resources located in Arcata. City policies should strongly support retention of existing healthcare institutions, facilities, and services

located in Arcata, including Mad River Hospital, Open Door Clinic, United Indian Healthcare, and other more specialized medical practices.

- i. As small community hospitals and non-profit healthcare clinics close or limit services under financial and regulatory pressure, Arcatans' access to healthcare institutions in our city may be threatened. While the City and residents of Arcata have no control and very little influence over institutions so fundamental for our health and well-being, the City can ensure that it devotes sufficient effort and resources to take full advantage of state, federal, and privately offered opportunities to keep local healthcare providers here in Arcata, and even attract innovative new ones.

5. Add new policy H-1f

- a. H-1f **Land use regulation to support health care.** Amend land use regulations to support provision and accessibility of healthcare services, including locations of healthcare providers in multiple-use and commercial land use classifications.

- i. Land use regulations can ensure that bona fide medical services can be located in commercial or mixed-use zones. Transportation, parking, noise, and other considerations should be considered in project approval, with special conditions appropriate to the specific medical service. (This may include targeted bus stop locations, parking conditions, etc.)

6. H-2b **Opportunities for healthy eating.** Support City and community programs that promote nutritional health, to improve opportunities for ~~Ensure~~ residents of all races, neighborhoods, ages, genders, incomes, and abilities ~~have opportunities for~~ food security and healthy eating.

7. H-3a **1.** Integrating physical activity into students' daily educational experience through both programming and the design of school sites and structures, and linkages with City facilities.

8. H-3b **Healthy workplaces.** The City shall seek and support resources to increase opportunities for healthy behaviors on workplace sites.

9. H-4b **Transparent and responsive government.** Increase government transparency and build trust among Arcatans of all ages, races, identities, and income levels. Demonstrate the City's credibility, reliability, openness, and community orientation through establishing a coordinated set of proactive channels for community-wide engagement between City departments, stakeholders, and the public. Respond promptly, reliably, and openly to criticism of City operation and administration from members of the public. Continue this engagement on an ongoing basis regardless of specific plans or projects underway. Evaluate implementation of plans, projects, and programs to ensure that established policies are being carried out. Monitor how implementation is serving different groups within the community, and respond to community critiques. ~~In comparison to each other.~~

10. Add new policy: H-4h **Senior engagement.** Support capacity and initiatives by Arcata's senior citizens and residents to advocate for changes in Arcata's infrastructure, programs, and city operations to help Seniors remain healthy and active.

11. Add new policy: H-4i **Engage with Arcatans of multiple abilities.** Support capacity among Arcatans of differing abilities and disabilities to represent their interests and concerns to the City with regard to City infrastructure, programs, and operations that can improve possibilities for people of different abilities to pursue healthy, active, mobile lives in Arcata.

12. Amend the implementation measures as follows:

- a. H-1 **Access to services 4.** The City shall work with existing healthcare institutions and medical practices in Arcata to retain services and improve access to them.

- b. **Health equity** Routinely engage people knowledgeable about needs of people of varied abilities to ensure that City facilities, programs, and practices serve people of varied abilities and disabilities. Strive for Universal Design in all City facilities.

- c. H-2 Opportunities for healthy eating 4. Work with existing commercial businesses to improve access to healthy food.
 - i. [Let's not underestimate the willingness of businesses to see providing healthier food options as a smart business opportunity! The creativity of Arcata's business community is an asset, and even chain grocery stores and restaurants respond to local governments' as well as consumers' urging and market demand.]
 - d. H-6 Community Engagement Protocol - Respond promptly and openly to critiques of City operations and decisions, including those made through administrative channels, and especially to those engaging with City committees, the Planning Commission, and City Council.
13. C-X Safe, convenient, connected, and multi-modal transportation 2. Ensure public health and equity considerations are included as part of transportation network improvement decisions, including accessibility and mobility considerations for people of diverse abilities and disabilities.
14. C-X Targeted improvements to existing transportation network
- 2. When designing new or improving existing streets, implement complete streets policy to incorporate pedestrian and bicycle safety improvements. Ensure that facilities and design promote and support mobility by people whose mobility is impaired, who move slowly, or who need mobility assistance. Use cost-effective street improvements ...
 - 4. Consider developing City-operated traffic lights with appropriate signals for people with impaired vision, hearing, or mobility.

- 1. Transportation, Page 2-17: Suggest changing the last sentence to read, "Recognize that active transportation is inaccessible can be difficult for many Arcata residents, specifically persons with disabilities, families with children, and lower-income Arcatans." [Staff: suggest the language should stay as it is. There are times of day and locations when/where bus service is inaccessible. This language points to the systemic cause. The phrase "can be difficult" is ambiguous as to cause. The former gives clear responsibility and line of action to fix the systemic problem.]
- 2. I propose we add the following sentence to policy H-2e

H-2e Alcohol misuse. Reduce the misuse of alcohol and the prevalence of alcohol-related accidents and injuries, paying specific attention to groups that may be at higher risk for alcohol misuse. Alcohol misuse is linked to injuries, violence, unsafe sexual behavior, adverse birth outcomes, blood pressure, heart disease, stroke, liver disease, cancer, and decreased emotional wellbeing. The City shall consider the impact of land use policies and programs on alcohol misuse. The City shall including promoting etc.... increased public transit access, availability, and frequency near bars and other establishments that serve alcohol.

 - i. Lack of access to alternative transportation is a cause of drunk driving. The City can work with A&MRTS and HTA to increase public transit availability near bars and other businesses that supply alcohol at appropriate times.
- 3. Amend Implementation Measure H-7
 - a. H-7 Health data As part of the City's Annual General Plan Progress Report submitted to the State Office of Planning and Research, include applicable information from departments, residents, and other stakeholders about how improvements to support health, or recent developments changes in programs, policies and services that threaten it, have affected residents. Give specific attention to discrepancies faced by specific neighborhoods and groups.

[Staff: The purpose of this measure is to concentrate attention on underserved neighborhoods that have disparate investment due to higher concentrations of lower socioeconomic households and people of color. Broadening the scope to all neighborhoods detracts from the emphasis on racial equity. Staff recommends not striking specific. Also, it is unclear what "developments" means in this context.]

Editorial Comments

1. Introduction, Page 2-3, second paragraph under "Assets to health and wellbeing in Arcata" – change "stoke" to "stroke"
2. Policy H-4a, Page 2-8 – Capitalize "Equity Arcata"
3. The policy statement should come FIRST, not last in a policy paragraph.
 - H-2 d **Commercial tobacco.** The City shall prioritize the need of non-smokers to breath smoke-free air in public places. ~~Policies should discourage. Reduce the~~ use of commercial tobacco. Commercial tobacco use is linked to asthma, cancer, COPD, diabetes, tooth loss, heart disease, stroke, and birth outcomes. ~~The City shall prioritize the need of non-smokers to breath smoke-free air in public places.~~

Chapter 1 – Community Vision

Introduction

Vision Statement

Arcata Community Vision

Arcata is a community that prioritizes ...

Conservation

We are committed to living well, and within Arcata's resource base. We will prevent pollution, protect the environment, and conserve water, wastewater, energy, and land so they may be enjoyed by the seventh generation. We promote compact growth to maintain a permanent greenbelt. And we pride ourselves on our continuing efforts to preserve the unique, natural beauty within and around the City.

Prosperity

We are committed to ensuring that everyone has access to quality life-long learning, education, and opportunities for economic success. We support and encourage economic activity across industry sectors from retail activity, restaurants and entertainment to entrepreneurs, artisans and small businesses.

Health and Safety and Community Well Being

We ensure that Arcata is a safe and health place to live, work, play, and learn. Our economy, education, health services, built environment, and social and community context all support physical, mental, and social well-being. We see opportunities for artistic expression as a key component of a healthy community.

Equity

We ensure a welcoming, safe, and equitable community where everyone has the opportunity to attain their full potential and no one is disadvantaged in achieving this potential because of race, religion, social position, economic position, culture, background, or any other socially defined circumstance. We recognize that the City of Arcata ~~lies~~ is located on the ancestral ~~lands~~ territory of the Wiyot peoples ~~Tribal Lands~~ and work in partnership with local tribes to dismantle the legacy narratives of settler colonialism. We offer reconciliation and respect to their elders, past and present.

Connection and Community

Seniors and students, families and singles, people from every race, religion, ethnicity, gender identity, and socioeconomic strata all share a sense of connection. Everyone feels welcome to actively participate and involve themselves in shaping our collective community. This sense of community is present in all civic and government activities, neighborhood and interest groups, community events, cultural events, and social activities for the entire region. Our connectivity is regional; we work with partners to tackle problems we can solve only together.

Diverse Character

Arcata is made up of a variety of distinctive, interconnected neighborhoods with diverse architectural heritage and well-designed, high quality gathering spaces for neighbors and visitors. New development complements the character of the neighborhood in which it is located. Arcata's focus is on creating human-centered spaces that are well-designed and enjoyable to exist in, with a focus on future development that will encourage both livability and "thrivability". We work with Cal Poly Humboldt with mutual respect and consideration to support this vision.

INTRODUCTION

As The Arcata General Plan: 2020 did from the year 2000 to 2022, the Arcata General Plan 2045 will help shape how the city of Arcata will look, function, provide services, and manage resources for the next 20 years (2022 to 2045).– The plan is the City's “constitution” for physical development and change within the existing and future city boundaries. The plan is a legal mandate that governs both private and public actions. The general plan is atop the hierarchy of local government laws regulating land use. Other laws and policies, such as specific plans, subdivision regulations, and the zoning ordinance are subordinate to, and must be consistent with, the general plan. This means that each decision made by an elected or appointed body is vetted for consistency with the General Plan. Comprehensive in scope, the plan conveys the fundamental values of the community that public decision-makers will use to guide the City's evolution, from its physical development to the ever-changing network of services provided to its citizens.



AUTHORITY AND PURPOSE OF THE GENERAL PLAN

California State law requires cities and counties to prepare and adopt a general plan. The Government Code Sections requiring general plans are listed and summarized in the box on the following page.

PLANNING COMMISSION & CITY COUNCIL REVIEW & ADOPTION

~~The City of Arcata Planning Commission conducted public hearings and reviewed the Draft General Plan prepared at the direction of the City's General Plan and Specialized Task Forces.~~

Government Code § 65300. Each planning agency shall prepare and the legislative body of each county and city shall adopt a comprehensive, long-term general plan.

Government Code § 65300.5. The Legislature intends that the general plan and elements and parts thereof comprise an integrated, internally consistent and compatible statement of policies for the adopting agency.

Government Code § 65300.7. Requires planning agencies and legislative bodies to implement this article in ways that accommodate local conditions and circumstances, while meeting minimum requirements.

Government Code § 65302. The general plan shall consist of a statement of development policies and shall include a diagram or diagrams and text setting forth objectives, principles, standards, and plan proposals. The plan shall include the following elements:

Land Use	Circulation	Open Space	Safety
Housing	Conservation	Noise	

Government Code § 65303. The general plan may include any other elements or address any other subjects which, in the judgment of the legislative body, relate to the physical development of the county or city. The City of Arcata General Plan includes the following other elements: Growth Management, Air Quality, Public Facilities & Infrastructure, Design, Parks & Recreation, Historical Preservation, Health, Local Coastal Element, and the Gateway Area Plan.

California Coastal Act. Public Resources Code § 3000 et. seq. The General Plan must comport with legislative policies relating to the California Coastal Act (see box on next page).

~~They forwarded a Planning Commission Draft to the City Council. After conducting their own public hearings, the Council adopted this General Plan on October 4, 2000.~~

LOCAL COASTAL LAND USE PLAN

A large portion of Arcata lies within the California Coastal Zone. The Coastal Act of 1976 requires the City to have a Local Coastal Program certified by the State Coastal Commission. The Local Coastal Program consists of two parts. The first part is the Coastal Land Use Plan, the Local Coastal Element, comprised of goals and regulatory policies. The second part is a set of implementing ordinances to carry out the policies of the Coastal Land Use Plan, the Coastal Zoning Ordinance. The City's Local Coastal Program provides additional specificity in addition to Citywide General Plan policies to address requirements of the Coastal Act. The Arcata General Plan: 2020 is a combined document meeting both the State General Plan requirements and serving as the Coastal Land Use Plan portion of the City's Local Coastal Program [LCP]. Policies which are a part of the LCP are designated with a wave symbol.

Chapter three of the Coastal Act, "Coastal Resource Planning and Management Policies," provides the standards for the review and certification of the Local Coastal Program. The Local

Attachment: C. Ch 1 Community Vision (4214 : GP #XVII FBC & GP wrap-up)



~~Coastal Element of the General Plan satisfies these Coastal Act Requirements. Coastal Land Use Plan. These standards are briefly summarized in the following box. A matrix (Table I-1 Coastal Policy Matrix) located at the end of the Introduction lists Coastal Land Use Plan policies, together with the specific Coastal Act requirements to which each policy is addressed. Various plan maps and diagrams throughout the plan show the boundary of the Coastal Zone.~~

California Coastal Act

The Coastal Act's policies guide coastal zone conservation and development decisions to protect California's coastal resources and provide for their wise use. These state policies call for:

- Providing for maximum public access to and recreational use of the coast, consistent with private rights and environmental protection.
- Protecting marine and land resources- including wetlands, rare and endangered habitat areas, environmentally sensitive areas, tidepools, and stream channels.
- Maintaining productive coastal agricultural lands.
- Directing new housing and other development to urbanized areas with adequate services rather than allowing a scattered, sprawling, wasteful pattern of subdivision.
- Protecting the scenic beauty of coastal landscape.
- Locating any needed coastal energy and industrial facilities where they will have the least adverse impact.
- Designating coastal dependent development, where appropriate.

ORGANIZATION OF THE PLAN

The Arcata General Plan ~~2045~~2020 consists of ~~sevensix~~ chapters, which arrange ~~fourteenth~~~~thirteen~~ elements into related groups, as shown in Figure I-a. In addition to the seven required elements identified in Government Code § 65302, this General Plan includes ~~sevensix~~ optional elements:

Growth Management (Chapter 2)	Design (Chapter 5)
Air Quality (Chapter 3)	Parks and Recreation (Chapter 3)
Public Facilities and Infrastructure (Chapter 2)	Historical Preservation (Chapter 5)
<u>Health (Chapter 6)</u>	<u>Gateway Area Plan (Chapter 2)</u>
<u>Local Coastal Element (Chapter 4)</u>	

The Housing Element is revised on a periodic cycle pursuant to state law.

~~The required Housing Element and the optional Parks and Recreation Element were not revised as part of this update. The Housing Element (adopted January 6, 1993) and Parks and Recreation Element (adopted November 16, 1994) are in separate documents.~~

Each element begins with an introduction, which includes an overview, guiding principles, and goals. The introduction is followed by a detailed policy section and concludes with a section

identifying implementation measures which require future actions. All elements ~~have equal weight under the law and~~ are designed to be consistent with each other.

PLANNING COMMISSION & CITY COUNCIL REVIEW & ADOPTION

The updated General Plan 2045 was adopted by City Council on XXX, 2023 following substantial public involvement, review of several City committees, and Planning Commission recommendation. This involvement is reviewed in further detail below.

~~The previous comprehensive update of the General Plan was completed over twenty years ago. Since that time, several of the elements were individually updated and several implementing plans have been adopted by the City. These implementing plans include:~~

- ~~Community Forest Management Plan~~
- ~~Creeks Management Plan~~
- ~~Drainage Master Plan~~
- ~~Source Reduction and Recycling Element~~

Attachment: C. Ch 1 Community Vision (4214 : GP #XVII FBC & GP wrap-up)

PUBLIC PARTICIPATION IN CREATING THE 2045 GENERAL PLAN

Preparation of the 2045 General Plan was a community effort, guided by public engagement throughout Phases 1 and 2 of the City’s Strategic Infill Redevelopment Program (SIRP) development. The updated General Plan elements found in this 2045 document are a key product of the SIRP. Phase 1 of the SIRP started in 2017 and produced both the City’s 6th Cycle Housing Element (adopted December 2019) and its Infill Market Study (adopted February 2021). Phase 2 began and has focused largely on updating existing General Plan Elements and developing those new to the 2045 update—namely, the Gateway Area Plan and the Health Element.

Public engagement in preparation of the 2045 General Plan took various forms, giving Arcatans diverse opportunities for participation. City staff conducted virtual visioning sessions, led walking tours of the Gateway Area, hosted an on-line visioning survey and an in-person “Open House” event, and tabled at various community gatherings. Individual engagements are identified in further detail below.

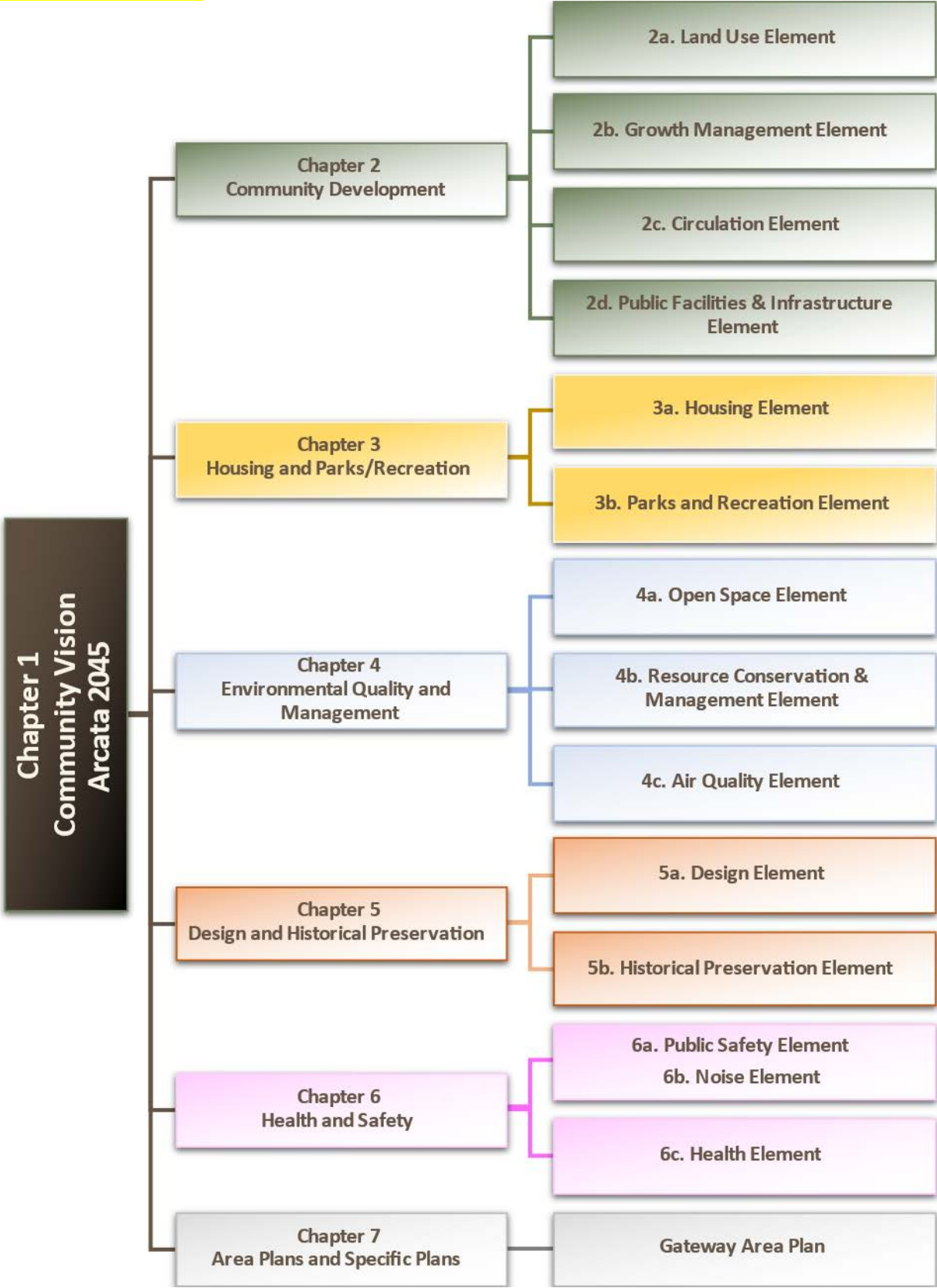
This General Plan 2045 also includes extensive input from decision makers. The Arcata Planning Commission as well as the City of Arcata’s nine City Committees worked through chapters of the General Plan and Gateway Area Plan at their regularly scheduled meetings. The Planning Commission and the City Council also held study sessions on various aspects of the planning effort, such as sea level rise and the Gateway Area Form-Based Code. Each of these hearings provided opportunities for public comment.

Taking the community’s lead from public engagement conducted, Community Development staff consulted with Planwest Partners to produce the revised elements that represent Arcata’s General Plan 2045. The Health Element was drafted in partnership with ChangeLab Solutions, a Bay Area-based nonprofit that specializes in health equity.

A final factor of engagement to note is that the City of Arcata contracted with Equity Partners, local consultants, to convene a diverse group of community members and solicit their feedback for General Plan development. The City’s Community Development department sought this partnership for two primary reasons: 1) community calls for increased attention to equity (specifically racial equity) and 2) difficulty gathering comprehensive input from all facets of the Arcata community to inform the General Plan update.

The City recognizes that centering marginalized voices and making decisions to improve systems and services for the most systematically disadvantaged residents improves quality of life for everyone in a community. Equity Partners aided the City in striving for these objectives by facilitating a series of meetings that prioritized the voices of Black, brown, and disabled residents. Equity Partners then compiled the visions for Arcata heard throughout these meetings in a report. Staff utilized the recommendations therein to create new policies and modify draft policies.

This Figure to be Updated



Attachment: C. Ch 1 Community Vision (4214 : GP #XVII FBC & GP wrap-up)

~~Preparation of this General Plan was a community effort, guided initially by General Plan Task Forces. Task Force sponsored neighborhood forums, and a community-wide “visioning” and goal-setting workshop were held to collect community input. Community representatives, especially the 34 members of the General Plan Task Force and three specialized Task Forces (Environmental Policy, Health and Safety, and Design and Historical Preservation) volunteered countless hours throughout the phases of the plan’s preparation. The final adopted plan followed numerous public hearings by the Planning Commission and City Council, where additional community input was received and incorporated.~~

~~The City of Arcata made General Plan preparation an open process with many opportunities for review and comment. Opportunities for public participation were incorporated into all phases of the General Plan’s progress. Public participation was initiated by appointment of citizens to the General Plan and Specialized Task Forces. This was followed by five neighborhood forums, held in November and December of 1996, to gather citizen input. A community-wide workshop, held in April, 1997, and attended by more than 130 community members, helped to define goals and overall direction. The Task Forces held more than 100 regular meetings, all open to the public.~~

~~The four Task Forces convened regularly to complete a Preliminary Draft of the General Plan, which was submitted to the Planning Commission and the City Council, in April 1998. The Commission and the Council held a General Plan study session in April 1998 to review Task Force recommendations, after which the Preliminary Draft Plan was the subject of public review and discussion at two neighborhood workshops in May and June. The Task Forces reviewed input from these workshops and completed their revisions to be incorporated into the public hearing draft of the General Plan. The Public Hearing Draft, along with an Environmental Impact Report, was completed in September 1998.~~

~~The Planning Commission (pictured, from left to right: Jack Hitt, Aldaron Laird, Carlel Dellabalma, John Graves, Elizabeth Conner, Robert Flint, and Ann King Smith) conducted hearings for the General Plan starting in early 1999.~~

REGIONAL AND LOCAL SETTING

Regional Setting. The City of Arcata is located on the Northern California coast, 275 miles northwest of San Francisco, in the heart of the redwood region. It is in the west-central portion of Humboldt County, six miles north of the City of Eureka, the County seat. Arcata is situated at the north end of Arcata Bay, which is part of Humboldt Bay, the second largest marine embayment in California. The City is located on U.S. Highway 101, which connects to Eureka and the San Francisco Bay Area to the south, and to Crescent City and the Oregon Coast to the north. The City is at the western terminus of State Highway 299, which connects Arcata and the north coast to Redding and the Upper Sacramento Valley to the east.

Local Setting. The City is situated on a coastal terrace, the lower portions of Fickle Ridge and the easterly portions of the Arcata Bottom, between Arcata Bay and the Mad River.

Arcata's Neighborhoods. Arcata is a city comprised of several distinct neighborhoods and recognized community areas. While the Plaza is the commercial, cultural, social, and civic center of activity, residential neighborhoods and employment centers offer unique living, working, shopping, learning, recreation, and community opportunities and facilities. ~~Figure 1b shows the general locations and names of the City's varied neighborhoods.~~

Arcata's Residents. 2021 census estimates put Arcata's population at 19,114. Its residents reflect the spirit of innovation the university brings to the community and a deep reverence for the environment and natural resources the North Coast region is graced with. Arcatans describe their collective identity as:

- Progressive
- Niche
- Collaborative
- Funky
- Inclusive
- Outside-the-box
- Creative
- Outdoorsy
- Community-oriented
- Emerging

HISTORY

The City of Arcata acknowledges that the lands we are located on are the unceded ancestral lands of the Wiyot tribe. The land that Arcata rests on is known in the Wiyot language as Goudi'ni, meaning "over in the woods" or "among the redwoods." Past actions by local, State and Federal governments removed the Wiyot and other Indigenous peoples from the land and threatened to destroy their cultural practices. The City of Arcata acknowledges the Wiyot community, their elders both past and present, as well as future generations. This Acknowledgment seeks to aid in dismantling the legacy narratives of settler colonialism.

For time immemorial, the Wiyot have inhabited the Humboldt Bay region, including in the area now known as Arcata. An Algonquian-speaking people, the Wiyot lived along the lower Mad River, other local streams, and along Humboldt Bay. Their way of life was shaped by the remarkable surroundings of forested hills, bountiful streams and rivers, and the Pacific and Bay shores, which generously provided for both their survival and cultural needs.

Following Humboldt Bay's settlement by European-Americans in the spring of 1850, Arcata served as a supply center for the interior mining districts of the Trinity and Klamath River regions. During the first decade of contact with European-Americans, the population of the Wiyot Tribe fell from approximately 1,500-2,000 to 200 due to genocide and disease.

The importance of gold in the region was soon eclipsed by lumbering. It was timber resources—particularly the vast, virgin forests of giant redwoods which covered the ridges and valleys along California's North Coast—that sustained the development of Arcata through the 19th century and into the mid-twentieth century. For more information on this time period in Arcata, see the Historical Preservation Element.

The establishment of the Humboldt State Normal School in 1913 made Arcata a university town, drawing young people from other parts of the state, country, and world to study on its forested campus. At first, the school was established as a teacher's college. Over the century since, it grew to an accredited university with extensive research facilities—joining the California State College system in 1960 and becoming the third polytechnic university in California in 2022. California State Polytechnic University, Humboldt (Cal Poly Humboldt) brings over 8,000 new student community members to Arcata and annually contributes 4,900 jobs and \$459 million in industry activity to the local economy.

As logging declined in Humboldt County throughout the latter 20th century, Arcata established itself as an innovative leader in the national environmental movement—largely due to the tireless advocacy of its student population. The city became home to the Northcoast Environmental Center (NEC) and the Arcata Recycling Center (one of the nation's first recycling institutions). Most notoriously, throughout the 1970-1980s the Arcata City Council led the development of the Arcata Marsh & Wildlife Sanctuary—the community's innovative, award-winning wastewater treatment plant. The Marsh hosts over 300 bird species and acts as a community hub with 5 miles of walking and biking paths.

Arcata today is known for its vibrant arts and cultural life, unique local business, and tight-knit community. Recognizing inequities past and present, the City strives to consistently improve services and quality of life for all residents. Arcata's history—full of advancement and achievement—situates the community well to solve the challenges it will face in its next chapter.

FUTURE TRENDS AND ASSUMPTIONS

The last twenty years have brought significant ~~change~~changes to Arcata and further ~~transformation is~~changes seem certain for the future. The Arcata General Plan: ~~2045~~2020 includes goals, policies, and implementation measures that anticipate and help shape those ~~shifts~~changes, to ensure that they reflect the ~~community's~~shared ~~sense of~~values of the community. Future trends may reflect changes in social, economic, cultural, and environmental factors ~~— that come from~~ both from within Arcatainside and outside its boundaries—, but they will be moderated and shaped locally by the guidelines expressed in this plan.

One of the largest changes to life in Arcata over the next twenty years is likely to be the expansion in Cal Poly Humboldt's student, faculty, and staff community as the university continues the transition to a polytechnic. This shift will require additional planning for housing and infrastructure. It will also bring more technical expertise, scientific and innovative capacity, and young community members to Arcata.

General Plan Assumptions

1. ~~Modest~~ growth from ~~18,801~~~~16,400~~ in ~~2020~~~~1997~~, to a ~~2045~~~~2020~~ population of about ~~2720~~,000.
2. ~~Cal Poly Humboldt~~~~No new large-scale employers.~~
- 3-2. ~~Continue to be a City where~~ HSU, health providers, education facilities, small-scale manufacturing, agricultural, and retail trade ~~continue to be~~ are the primary employers.
3. ~~No new large-scale employers in City Limits.~~
4. ~~Cal Poly Humboldt~~HSU will operate year-round instruction and enrollment will increase from ~~6,000~~~~7,500~~ in ~~2020~~~~1997~~ to ~~12,000~~~~8,500~~ full-time equivalent students in ~~2045~~~~2020~~.
5. ~~Infill development will be prioritized and expansions of~~ Infrastructure will be limited.
6. The Coastal Zone, resource constraints, and greenbelt will limit outward expansion.

OPPORTUNITIES AND CHALLENGES

The following opportunities and challenges were identified through public engagement and community visioning for the General Plan 2045. These themes are also visible throughout annual City Council and department goals in the decade leading up to the Plan update as well as planning documents of partner entities (such as Cal Poly Humboldt, the Wiyot Tribe, the County of Humboldt, and scientific government agencies). These represent predominant forces of change the City of Arcata is planning for. As they unfold, these factors will affect the City's priorities in implementing this Plan over the next 20 years.

Opportunities identified:

- With increased active transportation infrastructure, Arcata can offer a car-free lifestyle to able-bodied residents. Regional trail projects running through and extending out from the city can allow for sustainable transportation to and from neighboring communities.
- The strong network of nonprofits pushing positive progress in Arcata combined with resident and City commitments to increasing equity may lead to more socially and environmentally just outcomes for vulnerable Arcatans.
- Wider community recognition of Indigenous rights and the need to connect more with local tribes—looking to these partners for guidance and collaboration—can help build a more secure, sustainable regional future.
- Technological developments will bring new industries and businesses to Arcata.
- The university's transition to a polytechnic can grow capacity in Arcata's Science, Technology, Engineering, and Mathematics (STEM) sectors—filling identified workforce gaps in areas such as healthcare.

Predicted challenges:

- Sea level rise will increasingly impact the community, specifically the south end of Arcata near Humboldt Bay.

- Weather cycles will continue to increase in intensity due to climate change, leading to increased emergency events and potential influxes of refugees from neighboring communities.
- Arcata has limited land available for development. The capacity of public infrastructure will need to be expanded and fortified as Arcata's population grows.
- The percentage of community members experiencing houselessness may increase as housing costs continue to rise faster than salaries.

As the above assumptions and predictions reveal in a glance, conversations about Arcata's future during the development of the General Plan 2045 centered around climate change, houselessness, equity, the evolving university-off-campus community relationship, community health, Humboldt County's evolving economy, public safety, and population growth.

~~The most significant ongoing trends concern Arcata's working, living, learning and natural environments. For example, most of the large-scale timber industry employers are now gone. Today's Arcatans are more likely to work for Humboldt State University, a small employer, or to be self-employed.~~

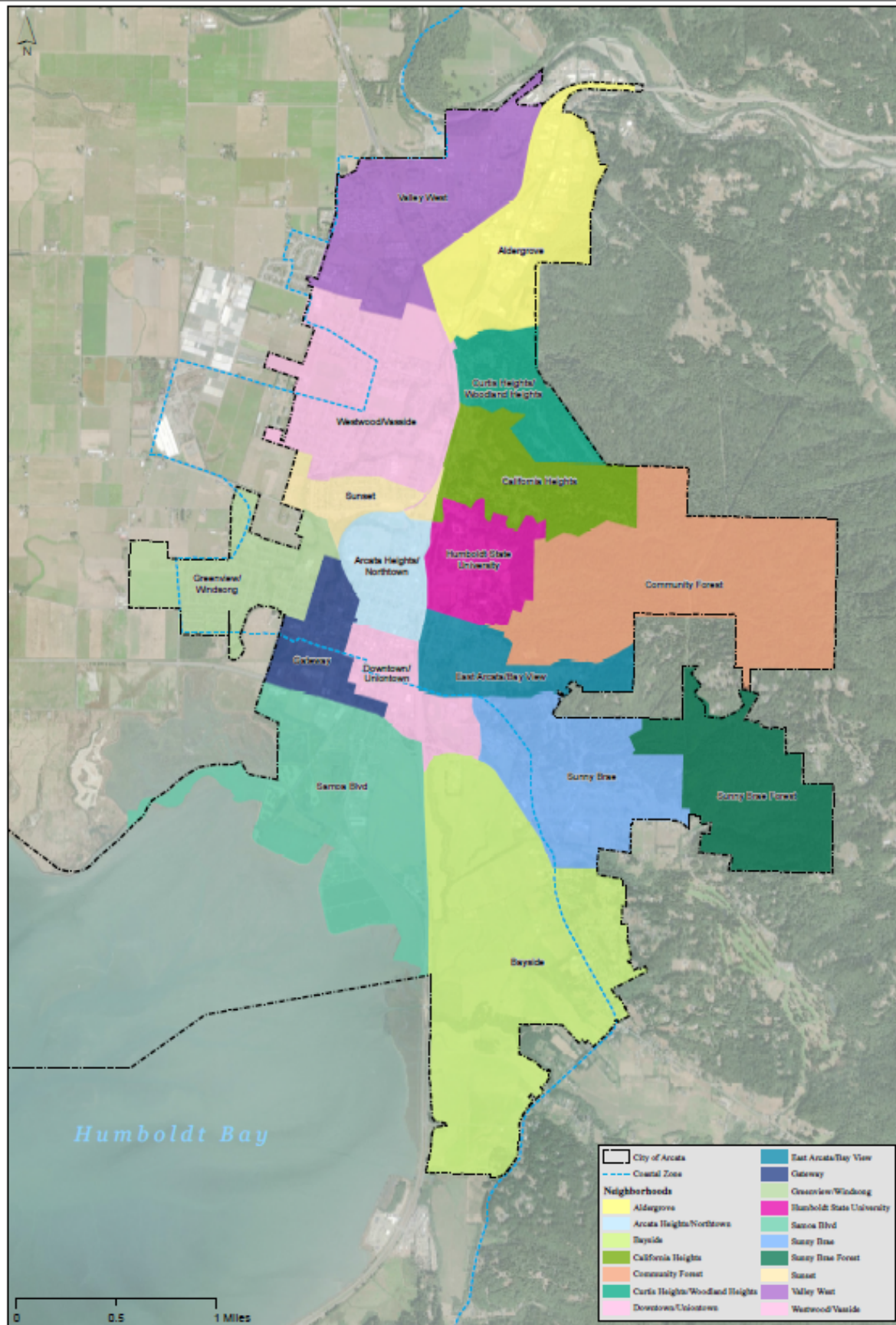


Figure X. City of Arcata Neighborhoods

Created by Planner's Partners
Map Date: 07/10/2009



In terms of physical development patterns, ~~infill redevelopment it is expected that there will be increased use~~ of vacant and underdeveloped parcels within City limits ~~is expected~~ as opposed to outward expansion ~~and sprawl~~. The Growth Management Element establishes an Urban Services Boundary that limits the extension of City services and outward growth of the City.

Transportation and other community infrastructure systems are expected to increase in efficiency, due to changes in travel choices and improved management of facilities. The General Plan emphasizes non-motorized vehicle connections throughout the city and more efficient transit service to reduce reliance on single-occupancy vehicles. Water, wastewater, energy conservation, and source reduction measures will minimize the need for increases in infrastructure capacity and promote more efficient operation of existing systems.

Protection and enhancement of the natural environment and preservation of open space will continue to be high priorities. Creek, wetland, bayland and tideland, agriculture, forest, soil and mineral resource issues, and the broader issue of natural biological diversity and ecosystem function, are specifically addressed in the General Plan. The protection of open space within the City and an agricultural and forest greenbelt around the City are integral to the future form of the City.

Reduced air, water, and noise pollution, along with increased protection from natural and man-made hazards, are expected in the future. Emergency preparedness will increase through neighborhood-based efforts such as the ~~Community Neighborhood~~ Emergency ~~Response Services~~ Teams (~~CERTTNEST~~), and improved coordination with emergency response agencies. California's North Coast is seismically active and care must be taken to avoid unnecessary ~~harm related human exposure~~ to hazardous and unstable areas. Improved identification of these areas, and construction methods to increase stability of new and retrofitted structures, will help minimize risks and reduce potential damage.

The architectural diversity and historic structures that form Arcata's special ambience and character are important community assets to be preserved ~~into for~~ the future. The General Plan includes Design and ~~Historic Historical~~ Preservation Elements that provide guidelines for compatible design and resource protection. The Design Element is intended to ensure ~~community~~ diversity and quality in the built environment, ~~encouraging with small-scale~~ structures that are harmonious with their neighborhood context and natural settings. The ~~Historic Historical~~ Preservation Element designates significant landmarks and sites that are representative of ~~the City's~~ various periods in social and physical development ~~in Arcata~~.



IMPLEMENTATION MEASURES

At the end of each element there is a list of measures that will implement General Plan policies. These directed actions are what transform the objectives of the policy they correspond to into real outcomes in Arcata. The implementation measures have an alpha-numeric reference corresponding to their related element. For example implementation measures related to the Land Use Element are labeled LU-1, LU-2, and so on. For each measure, a responsible party is identified and a time frame is given. Time frames are expressed as Year 1, Year 2, and so on, to indicate the year after General Plan adoption they are scheduled to be carried out. Some of the implementation measures are identified as ongoing. Many of the measures will be incorporated into the City’s Land Use Code which will replace the Land Use and Development Guide (LUDG). These code amendments are all identified as Implementation Measure LU-1, as shown in the example below.

IMPLEMENTATION MEASURE

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
LU-1	<u>Land Use Code Zoning</u> Amendments Revise the City’s Land Use Code (formerly the Land Use and Development Guide, or LUDG) and other zoning codes so zoning districts and land use regulations to be consistent with the General Plan land use designations, densities, and policies.	Community Development Department/ Planning Commission	Year 1



Attachment: C. Ch 1 Community Vision (4214 : GP #XVII FBC & GP wrap-up)

DEFINITION OF TERMS

The most frequently used General Plan terms are defined below. A more complete definition of terms may be found in the glossary.

- Goal** A general, overall, and ultimate purpose, aim, or end toward which the City will direct effort.
- Principle** An assumption, fundamental rule, or doctrine that will guide general plan policies, proposals, standards, and implementation measures. The State Government Code (§65302) requires that general plans spell out the objectives, *principles*, standards, and proposals of the general plan. “Adjacent land uses should be compatible with one another” is an example of a principle.
- Policy** A group of related actions or means that will be employed to achieve objectives.
- Objective** A specific measurable statement of desired future condition toward which a policy is directed. Where possible, objectives are time-specific. The State Government Code (§65302) requires that general plans spell out the *objectives*, principles, standards, and proposals of the general plan. “The addition of 100 units of affordable housing by 2005” is an example of an objective.
- Standard** A rule or measure establishing a level of quality or quantity that must be complied with or satisfied. The State Government Code (§65302) requires that general plans spell out the objectives, principles, *standards*, and proposals of the general plan. Examples of standards might include: five acres of parkland per 1,000 population; the traffic Level of Service (LOS) C; or a residential density standard of two units per acre.
- Implementation Measure** Actions, procedures, programs, or techniques that carry out policies.
- Program** An organized activity carried out in response to adopted policy to achieve an objective. Programs establish the “who,” “how,” and “when” for carrying out the “what” and “where” of goals and objectives, subject to budgetary allocations of resources.

||

TABLE I -1 COASTAL PLAN POLICY MATRIX

Note: the Coastal Act Requirements and General Plan Policies have been paraphrased for this table; please see text of the plan for complete language.

COASTAL ACT REQUIREMENTS		ARCATA LOCAL COASTAL LAND USE PLAN POLICIES			
Topic	Section	Requirements	Element	Policy	Policy Name
New Urban Development	30244	where development would adversely impact archeological or paleontological resources, reasonable mitigation measures shall be required	Historical Preservation	H-7a	Archeological surface reconnaissance studies
	30250a	proximity of new urban development to existing development	Historical Preservation	H-7b	Mitigation of potential archeological impacts
			Historical Preservation	H-7c	Monitoring of construction
			Historical Preservation	H-7d	Discovery of archeological resources
	30250b	location of new hazardous industrial development	Growth Management	GM-4a	Urban Services Boundary
			Public Facilities	PF-5b	City administrative and operation facilities
	30250c	visitor-serving facilities outside existing developed areas	Land Use	LU-2a	Residential land use classifications
	30251	scenic and visual qualities of coastal areas shall be considered and protected as a resource of public importance	Land Use	LU-4a	Industrial uses
	30252	maintenance and enhancement of public areas through the amount and location of new development	Land Use	LU-3a	Commercial Land Use classifications
	30255	priority of coastal dependant development over other development	Open Space	OS-3b	Development limitations and management for productive resource areas
Land Use			LU-1a	Land Use Plan diagram	
			Land Use Resource Conservation	LU-1g RC-4c	Coastal Dependent Land Uses Habitat value protection

TABLE I -1 COASTAL PLAN POLICY MATRIX (continued)

Topic	Section	Requirements	Element	Policy	Policy Name
Public Access & Recreation	30210	access to recreational opportunities; conspicuously posting access entrance	Open Space	OS-4b	Coastal access policy
			Resource Conservation	RC-4b	Access to Arcata Bay
	30211	development not to interfere with the public's right of access to the sea	Resource Conservation	RC-4a	Protection of Arcata Bay open waters and mudflats
			Resource Conservation	RC-4f	Management of bayfront and marsh areas for coastal access, recreation and tourism
	30212	new development projects; provision for access from the nearest public roadway; exceptions	Resource Conservation	RC-4a	Protection of Arcata Bay open waters and mudflats
			Resource Conservation	RC-4f	Management of bayfront and marsh areas for coastal access, recreation and tourism
	30212.5	public works facilities distribution through out the area to mitigate against impacts	Land Use	LU-5a	Public Facility uses
			Resource Conservation	RC-4b	Access to Arcata Bay
	30213	lower cost visitor and recreational facilities protected, encouraged, and provided; encouragement and provision, overnight room rentals	Resource Conservation	RC-4c	Coastal-dependant and public trust tidelands uses
			Resource Conservation	RC-4f	Management of bayfront and marsh areas for coastal access, recreation and tourism
	30214	implementation of public access policies for time, place, and manner of public access	Resource Conservation	RC-4b	Access to Arcata Bay
			Resource Conservation	RC-4c	Coastal-dependant and public trust tidelands uses
			Land Use	LU-3a	Commercial use classifications
			Resource Conservation	RC-4b	Access to Arcata Bay

Attachment: C. Ch 1 Community Vision (4214 : GP #XVII FBC & GP wrap-up)

TABLE I -1 COASTAL PLAN POLICY MATRIX (continued)

Topic	Section	Requirements	Element	Policy	Policy Name	
Public Access & Recreation (continued)	30220	protection of certain water-oriented activities that are not suited for inland water	Resource Conservation	RC-4c	Coastal-dependant and public trust tidelands uses	
	30221	oceanfront land suitable for recreational use and development shall be protected	Resource Conservation	RC-4c	Coastal-dependant and public trust tidelands uses	
	30222	visitor-serving recreational facilities have a priority over private residential, general industrial or general commercial, but not over agriculture or coastal-dependant industry	Open Space	OS-2b	Development limitations and management for maintenance of biotic resources and diversity, including aquatic resources and sensitive habitats	
	30222.5	oceanfront land suitable for aquaculture use and development shall be protected	Resource Conservation	RC-4e	Aquaculture and agricultural use of coastal wetlands and tidelands	
	30223	upland areas necessary to support coastal recreational uses shall be reserved	Land Use	LU-5a	Public Facility Uses	
	30224		encourage recreational boating use by developing boating facilities	Resource Conservation	RC-2c	Permitted uses and activities in streamside protection areas
				Resource Conservation	RC-4c	Coastal-dependant and public trust tidelands uses
				Resource Conservation	RC-4f	Management of bayfront and marsh areas for coastal access, recreation and tourism

TABLE I -1 COASTAL PLAN POLICY MATRIX (continued)

Topic	Section	Requirements	Element	Policy	Policy Name
Water & Marine Resources	30230	marine resources shall be maintained, enhanced, and where feasible, restored; special protection shall be given to areas and species of special biological or economic significance	Resource Conservation	RC-1c	Habitat value protection
			Resource Conservation	RC-2c	Permitted uses and activities in streamside protection areas
			Resource Conservation	RC-2g	Maintenance of streams as natural drainage systems
			Resource Conservation	RC-4a	Protection of open waters and mudflat areas of Arcata Bay
	30231	coastal waters maintenance to protect human health; maintain optimum marine organism populations; minimize adverse waste water, runoff, surface water effects.	Open Space	OS-1a	Designation of native biotic resources & ecosystems
			Community Facilities	CF-2b	Arcata Marsh Wastewater treatment system
			Community Facilities	CF-2a	Capacity and management of city wastewater collection system
			Community Facilities	CF-3a	Utilization city streams and watercourses as natural drainage systems
			Community Facilities	CF-3b	Control of stormwater runoff, flooding and erosion
			Resource Conservation	RC-2c	Permitted uses in streamside protection areas
			Resource Conservation	RC-2g	Maintenance of streams as natural drainage systems
			Resource Conservation	RC-4c	Coastal-dependant & public trust uses of tidelands

TABLE I -1 COASTAL PLAN POLICY MATRIX (continued)

Topic	Section	Requirements	Element	Policy	Policy Name
Water & Marine Resources (continued)	30233	diking, filling or dredging permitted where there is no feasible less environmentally damaging alternative; mitigation	Resource Conservation	RC-3b	Filling of wetlands
	30234	commercial fishing and recreational boating facilities protected and upgrade in accordance with demand	Resource Conservation	RC-4d	Diking, dredging, filling, and shoreline structures
	30234.5	economic, commercial, and recreational importance of fishing shall be recognized and protected	Resource Conservation	RC-2c	Permitted uses and activities in streamside protection areas
Industrial Development	30235	revetments, breakwaters, etc. permitted when designed to eliminate or mitigate adverse impacts			
	30240	environmentally sensitive habitat areas shall be protected against any significant disruption; adjacent developments shall be sited and designed to prevent impacts which would significantly degrade the area	Resource Conservation	RC-1c	Habitat value protection
			Land Use	LU-6a	Agricultural and Natural Resource classifications
			Open Space	OS-2b	Development limitations and management for maintenance of biotic resources and diversity, including aquatic resources and sensitive habitats
	30243	productivity of soils and timberlands protected; conversions	Resource Conservation	RC-2c	Permitted uses and activities in streamside protection areas

TABLE I -1 COASTAL PLAN POLICY MATRIX (continued)

Topic	Section	Requirements	Element	Policy	Policy Name
Industrial Development	30260	location or expansion of coastal-dependant industrial facility	Land Use	LU-4a	Industrial uses
			Land Use	LU-4d	New business park
	30261	use of tanker facilities; natural gas terminals	Land Use	LU-4g	Coastal related resource extraction and processing
	30262	oil and gas development	Land Use	LU-4g	Coastal related resource extraction and processing
	30263	refineries or petrochemical facilities	Land Use	LU-4g	Coastal related resource extraction and processing
	30264	thermal electric generating plants	Land Use	LU-4g	Coastal related resource extraction and processing
	30265	offshore oil transport and refining	Land Use	LU-4g	Coastal related resource extraction and processing
	30265.5	coordination of offshore oil transport and refining activities	Land Use	LU-4g	Coastal related resource extraction and processing
	30232	oil and hazardous substance spills	Public Safety	PS-6d	Siting of facilities handling hazardous waste
	Hazards	30236	alteration of rivers and streams permitted for water supply, flood control and improvement of marine habitat	Resource Conservation	RC-2c
Public Safety				PS-2a	Development within fault zone/ fault rupture areas
30253		safety, stability, pollution, energy conservation, visitors	Public Safety	PS-4c	Limitations on development within floodplains
			Open Space	OS-5a	Open space easements for safety hazard areas

TABLE I -1 COASTAL PLAN POLICY MATRIX (continued)

Topic	Section	Requirements	Element	Policy	Policy Name
Agriculture	30241	maximize the amount of prime agricultural land maintained in agriculture production to assure the protection of the agricultural economy; conflicts minimized	Resource Conservation	RC-2c	Permitted uses and activities in streamside protection areas
			Resource Conservation	RC-4d	Diking, dredging, filling and shoreline structures
			Resource Conservation	RC-5a	Protection of agricultural lands and uses in the City
	30241.5	agricultural lands viability determination	Land Use	LU-1e	Protection of natural resources and agriculture lands
			Land Use	LU-6a	Agriculture & Natural Resource classifications
			Land Use	LU-6c	Protection of Agricultural lands and uses
	30242	lands suitable for agricultural conversions	Land Use	LU-6c	
			Resource Conservation	RC-4b	Access to Arcata Bay
			Resource Conservation	RC-4c	Coastal-dependant and public trust tidelands uses
			Resource Conservation	RC-4f	Management of bayfront and marsh areas for coastal access, recreation and tourism
30254	new or expanded public works facilities shall be designed and limited to accommodate needs generated by development or uses permitted consistent with the provisions of this division	Growth Management	GM-4b	Services outside City boundary	
		Growth Management	GM-4c	Requirements and procedures for modifying Urban Services Boundary	
		Community Facilities	CF-1b	Capacity and management of city water system	

TABLE I -1 COASTAL PLAN POLICY MATRIX (continued)

Topic	Section	Requirements	Element	Policy	Policy Name
	30254.5	sewage treatment plants and conditions	Community Facilities	CF-2b	Arcata Marsh Wastewater treatment system
			Community Facilities	CF-2a	Capacity and management of city wastewater system

Chapter 2 – Community Development

Land Use Element

Growth Management Element

Mobility and Circulation Element

Public Facilities and Infrastructure Element

Gateway Area Plan

LAND USE ELEMENT

REV. 5.9.23 updated

2.1 INTRODUCTION

Overview of Existing Land Uses and Resources. Arcata's land use patterns have been shaped by many factors and reflect more than one-hundred and ~~forty-sixty~~ years of community development. The early settlement of the town, initially called Uniontown, grew around a central plaza. Arcata's Plaza area, including a hotel, storehouse, and other commercial buildings, was originally constructed in the late 1800s and early 1900s and continues to be a major the center of the community. The timber industry also left an indelible land use imprint on the community. Many of the lumber mills which operated in the late 1800s to mid-1900s, are now closed and the abandoned mill sites are undergoing a slow transformation to other uses. California State Polytechnic University Humboldt (Cal Poly Humboldt) Humboldt State University occupies approximately 160 acres east of State Route 101, within walking distance of the downtown area. The northernmost campus in the California State University system, it is the community's major employer and regional education center ~~for as many as 8,500 full-time equivalent students.~~

The City of Arcata acknowledges that the lands the City is located on are the unceded ancestral lands of the Wiyot tribe. The land that Arcata rests on is known in the Wiyot language as Goudi'ni, meaning "over in the woods" or "among the redwoods." Past actions by local, State and Federal governments removed the Wiyot and other Indigenous peoples from the land and threatened to destroy their cultural practices. The City of Arcata acknowledges the Wiyot community, their elders both past and present, as well as future generations. This Acknowledgment seeks to aid in dismantling the legacy narratives of settler colonialism.

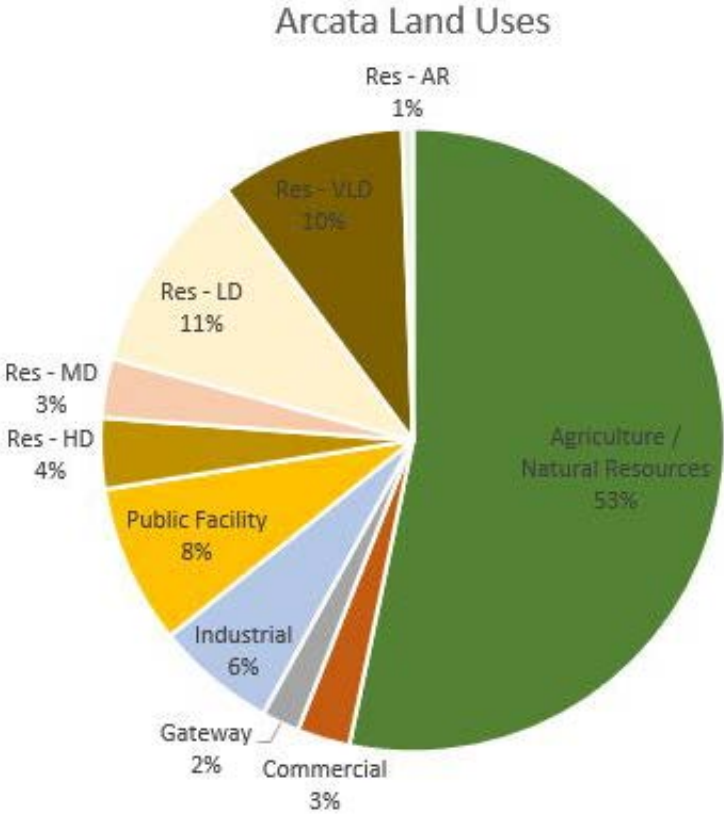
Long before Humboldt Bay was "discovered" by European-Americans, the bay and tidelands were used ~~intensively~~ by the Wiyot for food collection and ~~as a site for~~ villages. After early



European settlement, ~~Arcata's bayfront served there was~~ a community dock and shipping port on Arcata Bay. The Arcata marsh now functions as a world-renown wastewater treatment facility and wildlife refuge. ~~Many Some~~ residential areas ~~of Arcata~~ such as Sunset, Sunny Brae, ~~Preston Ridge~~, and Bayside were developed while unincorporated, and then subsequently annexed to the City, creating the opportunity for diverse community nodes. Their lot sizes, subdivision designs, and

street configurations reflect County, rather than City, development standards. These and other factors have influenced the existing land use characteristics of the community and will play an important role in shaping land use patterns for the future.

Arcata’s residential, commercial, industrial, agricultural/natural resource, and community facility uses are similar to those found in other communities. It is Arcata’s mixture of history, the diversity of uses ~~within those land use categories,~~ and the value placed on our history and diversity, that make the city unique. The Land Use Element contains goals, policies, and implementation measures for each land use category. These are intended to guide future land use decisions, preserve important historical elements ~~of the past,~~ and maintain the present diversity of use and character.



By recognizing and valuing the diversity of existing neighborhoods and prioritizing existing place-based strengths, the City can support the development of more connected and vibrant communities—and ultimately grow a more equitable, prosperous, and resilient region by targeting the places with the most potential to have a transformative impact on community and city prosperity.

In order to achieve these goals, the City of Arcata’s Land Use Element identifies local “activity centers”, meaning places within the City where a diversity of economic, physical, social, and civic assets cluster at a walkable neighborhood scale. Over the General Plan planning period, the City will target investment in economic development, infrastructure, and placemaking resources in ways that strengthen these centers, support their growth and development in more equitable ways, improve connectivity within and between them, and ensure that existing residents and businesses benefit from this new investment.

These identified existing activity centers are:

The Downtown/Uniontown Activity Area surrounding the Arcata Plaza and the core of Arcata’s historic downtown is located north of State Route 255, west of State Route 101 and Cal Poly Humboldt, and ~~west~~ east of K Street/Alliance Road. ~~While~~ the Plaza area includes a mix of shopping, dining, and office uses, as well as residential, cultural, and civic activity. The Arcata Plaza is a unique historic public plaza and is home to many local events, including the Farmer’s

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Market and Kinetic Sculpture Race, and has historically been considered is the commercial, cultural, social, and civic center of the community. The downtown area is identified as one of the City's Priority Infill Opportunity Zones (See Figure X). y, there are several other distinct neighborhood activity centers that provide employment and services for local residents. These neighborhood centers are: y.

The Valley West-West Activity Area Neighborhood Center, in the northern part of the City between State Routes 101 and 299. This mixed commercial/residential area includes access to Carlson Park, a regional draw with access to the Mad River, as well as a shopping center and transit hub, visitor-serving commercial areas along Valley West Boulevard, a mix of businesses along Giuntoli Lane, and surrounding nearby high-density residential and light and heavy industrial areas. A portion of Valley West, including the shopping center and a portion of parcels along Giuntoli Lane, are identified as one of the City's Priority Infill Opportunity Zones (See Figure X).

The Westwood/Sunset Neighborhood Center, in the western part of the City. It includes a shopping center and commercial businesses along Alliance Road, Sunset Arcata Elementary School, and surrounding residential areas. The Northtown Activity Area, located between the central-commercial area and Cal Poly Humboldt. This area functions as the de-facto corridor between Arcata and the University, and includes a mix of commercial and medium-density residential developments. The "Arcata Heights" Neighborhood Conservation Area is located to the west, comprising primarily low-density residential uses as well as Arcata High School and the Stewart School building.

The Sunny Brae Neighborhood Center, in the southwestern part of the City. This area includes Sunny Brae shopping center and adjacent professional offices, the Sunny Brae School, and surrounding residential areas.

The Bayside Neighborhood Center, in the southern part of the City. It includes several commercial businesses, the Jacoby Creek School, the Bayside Post Office and Grange Community Center, and surrounding low-density residential area.

The West End/ Aldergrove Employment Center, in the northeastern part of the City to the east of the Valley West activity area. This area has a mix of general and limited industrial uses, including the Aldergrove Industrial Park, the Foodworks Culinary Center, and surrounding commercial, residential, and public open space areas.



The Samoa Boulevard and South G Street Employment Center, in the southern part of the City. This area includes general and limited industrial, general commercial businesses on both sides of Samoa Boulevard, and surrounding high-and medium-density residential areas., vacant

~~land west of "K" Street, and a proposed business park site south of Samoa Boulevard.~~

Other smaller neighborhood-scale activity areas include:

- The Westwood/Sunset Neighborhood Center, in the western part of the City includes a shopping center and commercial businesses along Alliance Road, Arcata Elementary School, and surrounding low-density residential areas.
- The Sunny Brae Neighborhood Center, in the southwestern part of the City. This area includes Sunny Brae shopping center and adjacent professional offices, the Sunny Brae School, and surrounding residential areas.
- The Bayside Neighborhood Center, in the southern part of the City. It includes several commercial businesses, the Jacoby Creek School, the Bayside Post Office and Grange Community Center, and surrounding low-density residential area.

Infill Opportunity Zones.

In the Housing Element update in 2019, the City identified four neighborhoods whose current mix of uses present opportunities for infill development. These areas are in close proximity to transit and existing activity areas, and the proposed mix of land uses are intended to allow residents to live closer to their places of work and thereby encourage alternative modes of transportation. These areas collectively are anticipated to produce approximately half of the City's infill growth during the *General Plan: 2045* planning period. The areas described below reflect current configuration as enacted at a policy level through the adoption of the City's 6th Cycle Housing Element; future planning may make minor modifications, to add or subtract acreage to areas described below.

Downtown Opportunity Zone

The Downtown Opportunity Zone includes the City's center of commerce, encompassing roughly fifty-four acres of the Downtown and surrounding near Downtown areas. Land use designations include Central Commercial, Commercial General, Residential Medium Density, Residential High Density, and Public Facilities. This area is generally built out, but there are some parcels that have significant potential for redevelopment as well as several smaller parcels that could either be redeveloped if the zoning allowed more flexibility.

Valley West Opportunity Zone

The Valley West Infill Opportunity Zone includes roughly thirty-four acres in the north of the City situated along Giuntoli Lane and between Valley East and Valley West Boulevards. Zoning includes a mix of Commercial General and Industrial Limited. The Valley West commercial areas have significant redevelopment potential for both housing and economic development opportunities.

The Gateway Area Infill Opportunity Zone

The Arcata Gateway Opportunity Zone in the southwestern part of the City. This area includes one hundred and thirty-eight acres of land including a mix of commercial, industrial, and residential uses including the Creamery Building, which houses a variety of businesses and cultural organizations, including the Arcata Playhouse, Arcata's Local Arts Agency. The Gateway Area Plan establishes a broad range of high- and medium-density residential and mixed-use redevelopment with an emphasis on housing creation in this area due to proximity to Downtown and other community services and destinations.

Craftsman's Mall/St. Louis Opportunity Zone

The Craftsman's Mall/St. Louis Road Opportunity Zone includes roughly forty-one acres near the geographic center of the urbanized portions of the City and Cal Poly Humboldt. The average parcel size is 0.7 ac or 29,145.7 sq ft, and the Craftsman's Mall property, which has the highest immediate potential for redevelopment is owned by Cal Poly Humboldt. This area is near Cal Poly's campus and is an ideal location for high density housing. It is surrounded by lower density residential zoning districts. The area also includes existing industrial uses.

The planned distribution of land uses throughout the City ~~and Sphere of Influence~~ are shown on the land use diagram, which is included as Figure LU-a ~~in the map pocket at the end of the element~~. In addition, the Land Use Element addresses the City's Planning Area. The Planning Area extends north to the Mad River, west to the Pacific Ocean, south into Arcata Bay, and east to include the upper watersheds of Arcata's creeks, as shown on Figure LU-b and described in the Growth Management Element.

FIGURE LU-a LAND USE DIAGRAM (11 X 17) (Note: The figure below is a placeholder that shows proposed updates and will be replaced when approved)

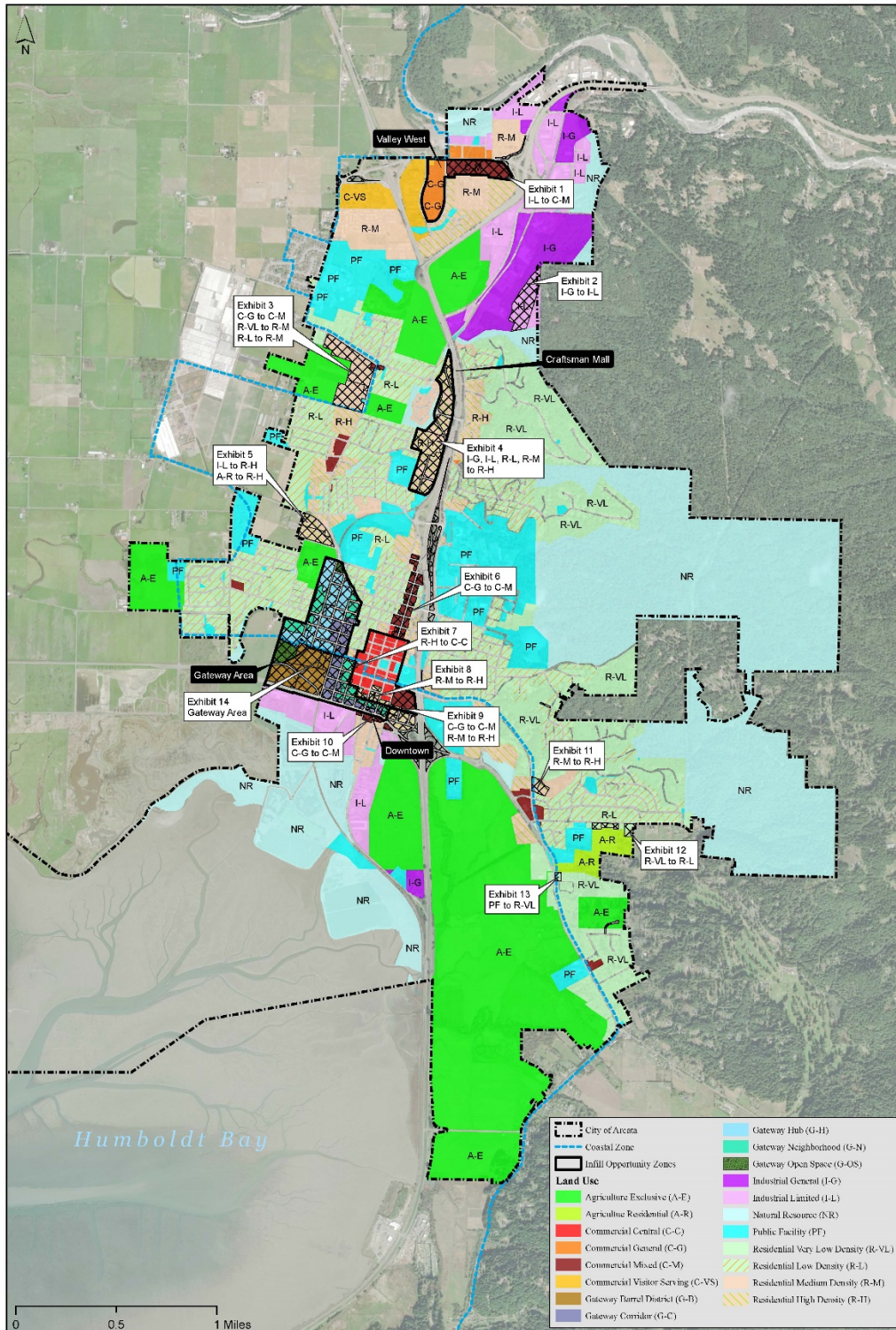


Figure X. City of Arcata General Plan Land Use Updates

Created by PlanWest Partners
Map Date: 6/6/2022



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure LU-b CITY OF ARCATA PLANNING AREA

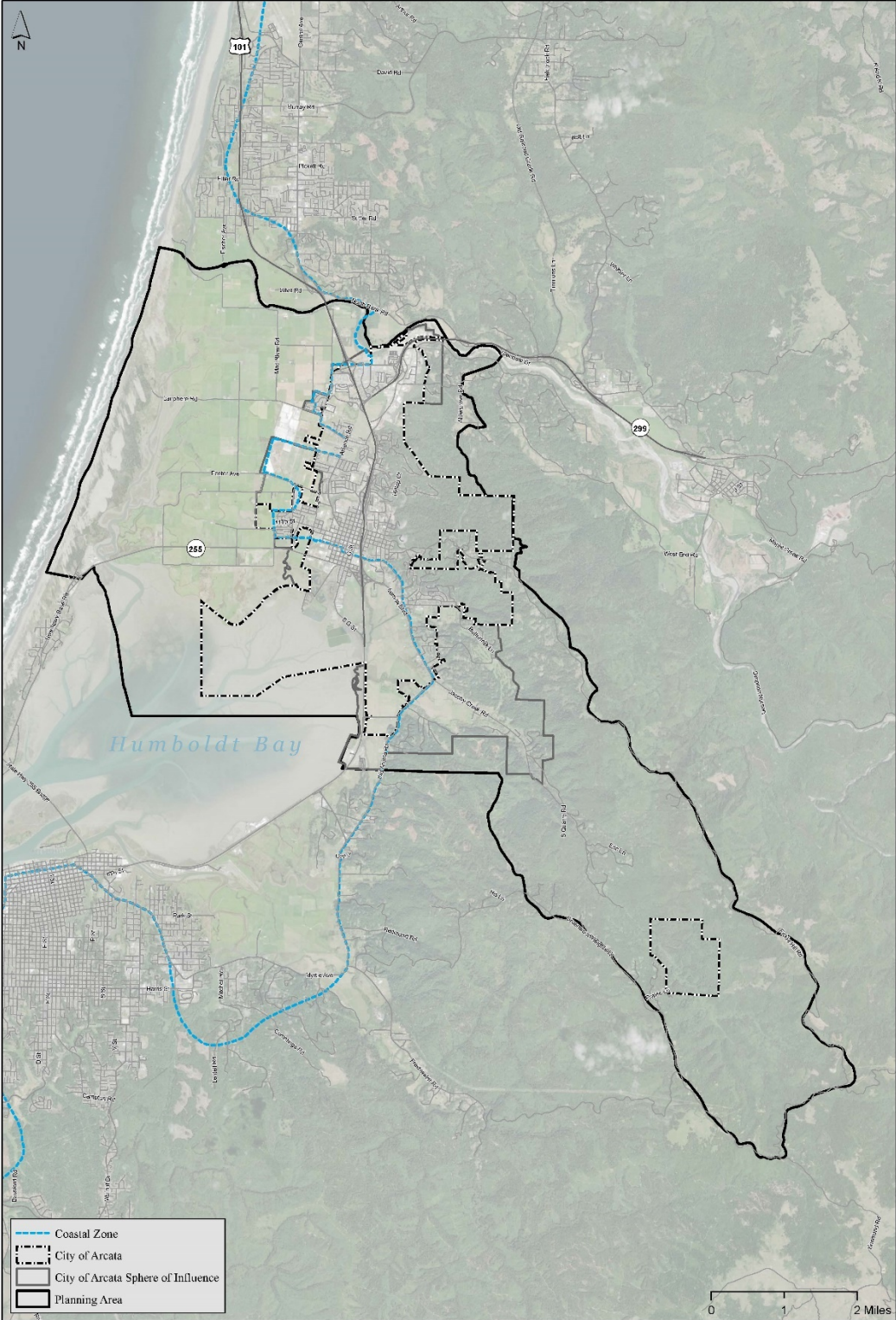


Figure X. City of Arcata Planning Area

Created by PlanWest Partners
Map Date: 5/16/2022



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Guiding Principles and Goals.

- A. ~~A.~~—Create a welcoming, safe and racially equitable community through the promotion of racial equity, diversity, and accessibility in all City neighborhoods and land use. Achieve this by encouraging housing development and business development in and around existing activity centers, and promoting access to transit and healthy food, among other strategies.
- B. Acknowledge and act on land use strategies brought forward by Arcata’s Black, Indigenous, and People of Color communities.
- B. Establish and maintain a greenbelt around the City that consists of agricultural, forest, and natural resource lands, focusing future developing in infill areas, as opposed to annexation of these lands. Preserve, as productive natural resources areas, the open agricultural lands in the Arcata Bottom, the forests on the eastern hillsides, and aquaculture in Arcata Bay. Protect other natural resource lands along the bayfront and watercourses for their ecological value as natural resource lands and community open space.
- CB. ~~Encourage~~ Allow for a range of housing choices that includes affordable housing dwellings for all community residents, including currently unhoused people, that accommodates families as well as individuals and groups, and varies in size and type to reflect the diverse character of the community and to provide equitable access to opportunities and resources in all of Arcata's neighborhoods.
- DE. Encourage retail, service, and professional businesses to locate and stay in the Commercial ~~Activity-Central~~ Areas by increasing the amount of housing there.
- ED. Promote commercial uses in the Downtown, Gateway Area, Westwood, Valley West, Sunny Brae, Bayside, and Greenview neighborhood centers to meet day-to-day retail and service needs of nearby residents.
- FE. ~~Concentrate-Designate~~ industrial uses in existing employment centers and encourage labor intensive and technology-driven industrial and business uses in these areas rather than resource intensive uses.
- GF. Maintain community facilities such as schools, community centers, parks and recreation areas, and other civic uses and ensure they are equitably distributed and located in areas that are accessible to all segments of the community.
- HG. Encourage infill development of vacant, brownfield, and underutilized land designated for development as a way of meeting housing and employment needs without major extensions of infrastructure and services. Encourage high-density residential infill development and minimal parking ratios in Infill Opportunity Zones throughout the City. Ensure displacement and affordability issues related to the City’s infill strategy are addressed.
- I. Permit vertical and/or horizontal mixed-use development close to clusters of activity and at major transportation crossroads where they can take advantage of higher levels of access.
- JH. Retain agricultural and natural resource lands within the City.
- KI. ~~Encourage~~ Promote mixed use commercial/residential areas throughout the City through ~~by~~ encouraging residential units on upper floors in commercial areas and other available

strategies.

- L. Locate and allow location of land uses to minimize risks and exposure to environmental hazards, including seismic hazards and flooding.
- M. Seek and encourage open and cooperative consultation and cooperation with managers of state land not subject to Arcata's land use authority including Cal Poly Humboldt, lands within the California Coastal Commission's retained jurisdiction, and others.



2.2 POLICIES

The Land Use Element contains the following policies:

- LU-1 Overall Land Use Development Pattern: ~~Land Use Plan Map~~
- LU-2 Residential Land Use
- LU-3 Commercial Land Use
- LU-4 Industrial Land Use
- LU-5 Public Facilities
- LU-6 Agricultural and Natural Resource Lands

POLICY LU-1 OVERALL DEVELOPMENT LAND USE PATTERN: ~~LAND USE PLAN MAP~~

Objective. Establish a pattern of development that concentrates activity and amenities in a pattern of centers, clusters and mixes of uses to support the City's focus on infill development and active transportation. Provide an overall land use arrangement that concentrates city-wide uses and functions in these areas with an emphasis on areas within walking distance of the central Plaza Area, Cal Poly Humboldt, linked with a series of and existing neighborhood and employment centers which provide a mix of commercial services, residential uses, and community facilities.

~~LU-1a **Land use plan diagram.** The land use plan diagram (Figure LU-a) for lands within the City and Sphere of Influence and the planning area land use map (Figure LU-b) show planned land uses for the City and surrounding areas. The land use categories, and the amount of City and Sphere of Influence land allocated for each category, are included in Table LU-1.~~

~~LU-1b **Coastal land use plan.** The western portion of the Arcata Bottom, lands south of 7th and 8th Streets west of State Route 101, and lands south of Bayside and Old Arcata Roads east of State Route 101 are within the Coastal Zone, created by the California Coastal Act. The land use designations within the Coastal Zone are included in the Local Coastal Element which is part of the City's Local Coastal Program (LCP).~~

~~LU-1c **Maintain Arcata's Plaza Area as the a major community's center in central Arcata.** The Plaza has historically been, and should continue to be, the a commercial, civic, social, and cultural center of the community, and the main a location for community wide special events. The area surrounding the Plaza (Commercial-Central area), including the adjacent commercial and mixed-use areas, shall be planned to accommodate the greatest concentrations of retail establishments, entertainment services, restaurants, and business and professional offices within the city. High density residential use in the Commercial-Central area is encouraged as an Infill Opportunity Zone. Residential units shall be included, where feasible, in all new commercial development within the Plaza Area. n~~

TABLE LU-1 LAND USE PLAN CATEGORIES AND ACREAGES

Arcata General Plan: 2045 Land Use Designations		Acres	%
A-E	Agriculture—Exclusive	1,040.9	19.4%
A-R	Agriculture—Residential	29.0	0.5%
C-C	Commercial—Central	25.8	0.5%
C-G	Commercial—General	25.0	0.5%
C-M	Commercial—Mixed	52.9	1.0%
C-VS	Commercial—Visitor Serving	42.2	0.8%
G-B	Gateway Barrel District	28.9	0.5%
G-C	Gateway Corridor	17.3	0.3%
G-H	Gateway Hub	32.0	0.6%
G-N	Gateway Neighborhood	19.2	0.4%
G-OS	Gateway Open Space	6.9	0.1%
I-G	Industrial General	130.7	2.4%
I-L	Industrial Limited	200.9	3.7%
NR	Natural Resources	1,804.8	33.7%
NZ	Not Zoned	20.4	0.4%
PF	Public Facilities	441.5	8.2%
R-H	Residential—High Density	205.5	3.8%
R-L	Residential—Low Density	552.6	10.3%
R-M	Residential—Medium Density	166.9	3.1%
R-VL	Residential—Very Low Density	514.5	9.6%
<i>Total</i>		5,357.8	100%

Note: The figures above do not include road rights-of-way, interchanges and, or the portion of Arcata Bay within the City limits, nor do they include the 565-acre Jacoby Creek Forest, a City-owned non-contiguous incorporated area. These combined areas cover an additional 2,251 acres.

~~LU-1d1e — Mixed-use neighborhood centers. The existing neighborhood centers of Westwood, Valley West, Sunny Brae, Uniontown, and Greenview are designated as multiple-use sites where additional retail establishments, personal and business services, and other neighborhood-oriented commercial services are encouraged to be developed. The City encourages residential uses in mixed-use centers. Substantial additions to these existing centers shall should include residential units on upper floors where feasible or in separate buildings. Upgrading these centers shall include additional landscaping, improvement of parking lot designs, and provision of transit access, such as bus turnouts. Appropriate lands near these centers are designated for higher density residential uses in order to to encourage walking and bicycling to neighborhood services.~~

~~LU-1e1af~~ **Protection of natural resources and agricultural lands.** The designation of open space throughout the City, and the agricultural greenbelt area around the City, are

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

integral to Arcata's future form. Agricultural [A-E] and Natural Resource [NR] designated lands make up over half of the community land base. Their productive, open space, and natural resource values are important to the community and shall be preserved and conversion to other non-compatible uses shall be prohibited.

LU-1f1bg Promotion of infill development and Designated Infill Opportunity Zones. The City encourages appropriate redevelopment of certain parcels of land ~~which that~~ are either underutilized, brownfields, or vacant, but surrounded by existing urban development. These sites represent development opportunities using existing infrastructure, and shall have priority for development over vacant sites that are located outside the urban services boundary (designated in the Growth Management Element) ~~which that~~ require investment in extension of infrastructure and services. The City's Infill Opportunity Zones are intended to encourage and streamline infill development in these areas, based on available land and proximity to transit, services, jobs, and activity centers. Infill development may include new residential units ~~on upper floors of in~~ new or existing commercial structures, development of second units on residential lots, and new or expansion of existing residential and commercial structures consistent with the provisions of the applicable land use plan designations. Infill Opportunity Zones are shown on the land use plan diagram (Figure LU-a). ~~The Planned Development procedure shall be encouraged for coordinated development on larger infill sites.~~

LU-1c Prioritization of transit and active transportation. ~~Reduce and/or eliminate minimum parking requirements citywide. transit and active transportation is planned to support the transportation needs of the community, including neighborhoods where biking infrastructure, trails, complete streets, and transit is or is planned to be accessible~~

LU-1d Reduce Parking Maximums in the most walkable areas of the city. Reduce maximum allowable parking requirements within Infill Opportunity Zones to promote walkable communities.

LU-1geh Streamlined Review and Standards in Infill Opportunity Zones. ~~As described in the City's 6th Cycle Housing Element, Priority~~ Infill Opportunity Zones are intended to identify areas where the City plans to accommodate high density residential uses through a ministerial review process in order to facilitate housing production. In addition to creating a ministerial pathway for residential projects, applicable standards (parking ratios, height limitations, site coverage, etc.) shall be modified to allow consistent with development at greater densities with a larger reliance on multi-modal transit.

LU-1f Development of a diversity of housing types. The land use plan map shall provide sufficient quantities of land in the various residential use categories to allow for development of a variety of types of new housing units and residential environments. The purpose shall be to achieve an appropriate balance between single-family housing on individual lots and multi-unit housing types.

LU-1g Inclusion of residential uses. Commercial developments should include residential units where feasible. Where residential units are incorporated, some ground floor commercial should generally be maintained. Any residential development in new structures shall comply with the development standards of the applicable land use category.

LU-1h Variety of Industrial Uses. Provide sufficient land for a broad range of industrial uses to retain and expand existing uses and attract new industries. Continue to support industrial uses that are intended to retain and generate living-wage jobs.

LU-1i Activity areas diagram. The activity areas diagram (Figure LU-c) for lands within the City and Sphere of Influence and the planning area land use map (Figure LU-a) show existing activity areas for the City and surrounding areas. Continue to invest in improving housing, job, and active transportation access to these centers. Target investment in economic development, infrastructure, and placemaking resources in ways that strengthen these centers, support their growth and development in more equitable ways, improve connectivity within and between them, and ensure that existing residents and businesses benefit from this new investment.

LU-1j Maintain Arcata's Historic Plaza Area as a major community center in central Arcata. The Plaza has historically been, and should continue to be, a commercial, civic, social, and cultural center of the community, and a location for community-wide special events. The area surrounding the Plaza (Commercial-Central area), including the adjacent commercial and mixed-use areas, shall be planned to accommodate retail establishments, entertainment services, restaurants, and business and professional offices. High density residential use in the Commercial-Central area is encouraged as an Infill Opportunity Zone. Housing shall be included, where feasible, in all new commercial development within the Plaza Area.

LU-1k Encourage Valley West's growth as a major community center for north Arcata. Ensure City resources are allocated to improving accessibility, safety, and attractiveness of this ~~visitor-serving~~, mixed use area, which functions as both a tourism destination and a vibrant neighborhood commercial center for many neighboring residents. The area surrounding the Valley West Shopping Center, including the adjacent commercial, office, and residential areas, shall be planned to accommodate a large concentration of high-density residential development as well as retail establishments, entertainment services, restaurants, and business and professional offices. Continue to promote Carlson Park as a regional draw to access the Mad River. High density residential use in the Valley West Infill Opportunity AreaZone will be streamlined and residential units shall be included, where feasible, in all new commercial development within the Valley West Area.

LU-1l Support and revitalize other existing neighborhood and commercial activity areas. The existing neighborhood centers of Westwood, Sunny Brae, Bayside, and Greenview

are designated as multiple-use sites where additional retail establishments, personal and business services, and other neighborhood-oriented commercial services are encouraged to be developed. The City encourages residential uses in mixed use centers. Reinvestment to upgrade and/or expand existing commercial activity areas shall be encouraged. Upgrades should include improvements to existing centers including ~~of parking and~~ landscaping areas; provision of facilities to accommodate bicycles, pedestrians, and transit and to reduce vehicle trips; and residential units on upper floors or in new structures. Appropriate lands near these centers are designated for higher density residential uses to encourage walking and bicycling to commercial services.

LU-1m Support and revitalize the West End/ Aldergrove Employment Center as an industrial and business hub for north Arcata. This area has a mix of general and limited industrial uses, including the Aldergrove Industrial Park, the Foodworks Culinary Center, and surrounding commercial, residential, and public open space areas. Light and general-industrial uses shall be encouraged to relocate and expand in this area.

LU-1n Support and revitalize the Samoa Boulevard and South G Street Employment Center as an industrial hub for south Arcata. This area includes general and limited industrial, general commercial businesses on both sides of Samoa Boulevard, and surrounding high-and medium-density residential areas. Light and general-industrial uses shall be encouraged to relocate and expand within the Adaptation Zone boundary of this area as described in the City’s Local Coastal Program.

LU-1o Land use plan diagram. Ensure the distribution of land use designations guides permitted uses to align with the intended outcomes of pattern of development identified. The land use plan diagram (Figure LU-b) for lands within the City and the planning area land use map (Figure LU-c) show planned land uses for the City and surrounding areas. The land use categories, and the amount of City land allocated for each category, are included in Table LU-1.

LU-1p Gateway Area land uses. Gateway Area land use designations allow for a broad range of mixed-use redevelopment with an emphasis on housing creation. Gateway Area land use designations and policies are described in the Gateway Area Plan.

LU-1q Coastal land-use plan. The western portion of the Arcata Bottom, lands south of 7th and 8th Streets west of State Route 101, and lands south of Bayside and Old Arcata Roads east of State Route 101 are within the Coastal Zone, created by the California Coastal Act. The land use designations within the Coastal Zone are included in the Local Coastal Element, which is part of the City’s Local Coastal Program (LCP).

TABLE LU-1 LAND-USE PLAN CATEGORIES AND ACREAGES

<u>Arcata General Plan: 2045 Land Use Designations</u>	<u>Acres</u>	<u>%</u>
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A-E	Agriculture - Exclusive	1,040.9	19.4%
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C-G	Commercial - General	25.0	0.5%
C-M	Commercial - Mixed	52.9	1.0%
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G-H	Gateway Hub	32.0	0.6%
G-N	Gateway Neighborhood	19.2	0.4%
G-OS	Gateway Open Space	6.9	0.1%
I-G	Industrial General	130.7	2.4%
I-L	Industrial Limited	200.9	3.7%
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NZ	Not Zoned	20.4	0.4%
PF	Public Facilities	441.5	8.2%
R-H	Residential - High Density	205.5	3.8%
R-L	Residential - Low Density	552.6	10.3%
R-M	Residential - Medium Density	166.9	3.1%
R-VL	Residential - Very Low Density	514.5	9.6%
<i>Total</i>		5,357.8	100%

Note: The figures above do not include road rights-of-way, interchanges, or the portion of Arcata Bay within the City limits, nor do they include the Jacoby Creek Forest, a City-owned non-contiguous incorporated area.

~~LU 1g **Coastal dependent land uses.** Coastal dependent developments shall have priority over other development or uses on or near the Arcata Bay shoreline. Coastal dependent developments shall not be sited in a wetland. Where appropriate, coastal dependent developments should be accommodated within reasonable proximity to the coastal dependent uses they support.~~

LU-1r **State mandated housing production.** The City recognizes that the state's housing goals have resulted in laws that increase density above City established base density, removed discretion in housing development, required streamlining in approval processes, established by-right development for certain housing types, and has reduced local control over many land use decisions related to housing production. The City shall monitor and comment on state actions to advocate for reasonable solutions to housing production that meet both state objectives and City need for housing.

[add a pie chart of residential zoning]

POLICY LU-2 RESIDENTIAL LAND USE

Objective— Allow for a mix of housing types and densities to ensure residents at all ages, income levels, and abilities have safe, healthy, and affordable homes that meet their physical, social, and economic needs through housing production, preservation, and conversion that is compatible with established and planned neighborhood character design elements. ~~meet the physical, social, and economic needs of residents, with new and converted housing designed to be compatible with the established neighborhood character.~~

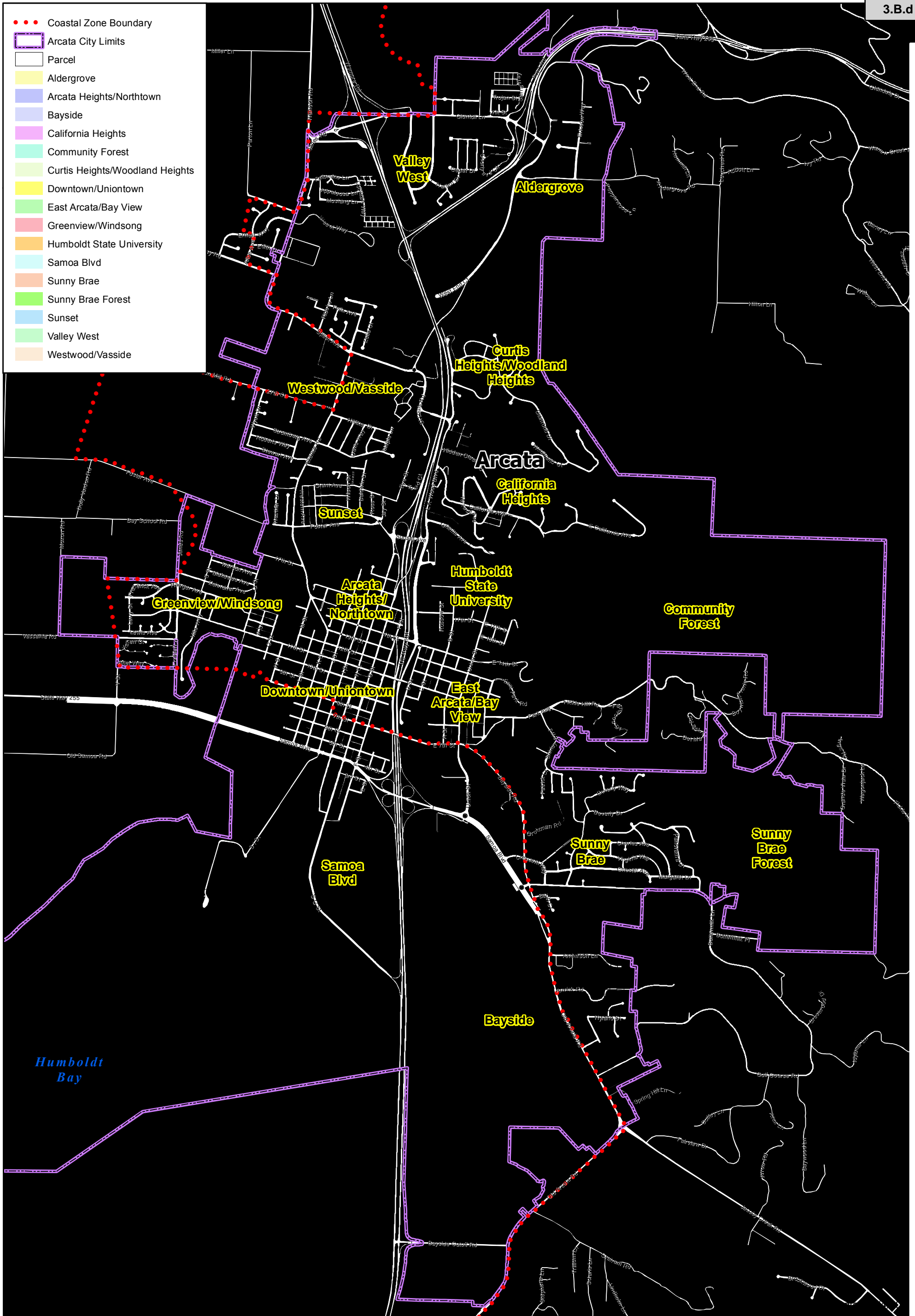


LU-2a **Residential Land Use Classifications**— The following land use designations are applicable to residential lands. Table LU-2 defines permitted uses, densities, lot sizes, and other development standards for each classification.

Residential Very Low Density [R-VL]— This designation allows the lowest density residential development in areas where physical constraints, protection of natural features, or preservation of semi-rural character are important considerations. The R-VL designation is applicable primarily for lands with steep slopes and where the open space character of Arcata's hillsides and perimeter lands are to be preserved. Individual homesites are allowed in hillside areas, as long as precautions are taken to prevent the excessive removal of vegetation and strict grading controls are enforced to prevent erosion. Development and grading on areas characterized by slopes over 25% should be avoided. Review of development proposals in all hillside areas shall ensure that seismic and geologic hazards are avoided or mitigated. More detailed hillside development standards are included in Policy PS-3c.

The R-VL designation allows creation of lots as small as 20,000 square feet, in the less-steep areas of Arcata's hillsides. Newly created lots in this zone must contain a buildable area of sufficient size and flatness to allow development without significant environmental damage or landform alteration. The development regulations for R-VL are intended to balance protection of the sensitive hillside environment with the need for quality housing sites. This designation is also intended to protect the existing rural environment in certain outlying areas of Arcata and to provide a transition between more urban uses and agricultural operations.

Residential – Low Density [R-L]. The low density/low-density residential designation primarily provides for single-family homes, duplexes, and accessory dwelling units on individual lots. This designation is found throughout the community, including the



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Figure LU-c

Arcata Neighborhoods



City of Arcata



0 1,000 2,000 Feet

older, historical neighborhoods surrounding the Plaza Area, Sunny Brae, Sunset, ~~Preston Ridge Area~~, and Greenview ~~Terrace~~. Terrace. Under current state land use and planning law, single-family zones are authorized to construct multiple dwelling units per lot in single family residential zones and may allow up to four residential units on most R-L lots.

Residential – Medium Density [R-M] ~~–~~ Medium density residential unit types typically include duplexes, townhouses, co-housing, low density apartments, and modular housing located in mobile home parks ~~–~~.

Residential High Density [R-H] ~~–~~ ~~The Colony Inn and Humboldt Green~~ ~~–~~ multi-family units are representative of R-H density. ~~R-H~~ High density residential uses are designated in central Arcata and other areas to allow ~~increases in~~ higher density ~~above present levels~~ multi-family housing located in proximity to commercial and employment uses, public services, schools, and parks. Local-serving commercial uses such as corner grocery stores and coffee shops shall be permitted in [R-H] zones.

- LU-2b **Diversity and choice in residential environments.** ~~The land use plan map shall provide sufficient quantities of land in the various residential use categories to allow for development of a variety of types of new housing units and residential environments. The purpose shall be to maintain an appropriate balance between single family housing on individual lots and multi unit housing types.~~ The City shall encourage residential developments ~~which~~ that collectively provide a variety of choices for housing the community, including currently unhoused people. ~~consumers~~ Units shall be diverse in terms of types of units, location, unit sizes, costs, design, amount of privacy, and neighborhood environment. Inclusionary measures shall be provided for affordable housing, and the City shall continue to collaborate with the Wiyot Tribe to further City and tribal housing goals. To encourage this, the City will implement inclusionary zoning in higher density developments and provide incentives to developers to include low and moderate income housing units in their proposals.



TABLE LU-2 RESIDENTIAL LAND USE CLASSIFICATIONS

ALLOWABLE USES	R-VL	R-L	R-M	R-H
RESIDENTIAL				
Single-family dwellings	X	X	X	
Mobile/manufactured homes	X	X	X	X
Mobile home park			X	
Duplex dwellings		X	X	X
Multi-family dwellings			X	X
Planned developments	X	X	X	X
Group residential		X	X	X
SECONDARY USES				
Small residential care facilities	X	X	X	X
Accessory (2 nd) dwelling unit (ADU)	X	X	X	X
Bed and Breakfast Inns				
Home Occupations	X	X	X	X
	X	X	X	X
DENSITY				
Density Range*	2 or fewer primary units per acre	From 2 to a maximum of 7.25 units per acre	From 7.26 to a maximum of 15 units per acre	From 15.01 to a maximum of 32 units per acre

[Revised by Ordinance No. 1377, September 2008] All Residential Land Use Classifications Subject to State Base Density does not include additional density allowed by state ADU, density Lawbonus law.

~~LU-2c Specific considerations for certain residential areas (Not applicable in Coastal Zone). Certain specific parcels have unique characteristics and/or limitations that require careful consideration when development or a change of use occurs. The considerations for the identified parcels shall be as described in Table LU-3.~~

TABLE LU-3 SPECIFIC CONSIDERATIONS FOR CERTAIN RESIDENTIAL AREAS

AREA	SPECIFIC CONSIDERATIONS
Plum Village	Residential development shall be limited to twelve units of which three (25%) will be affordable units and the wetlands area shall be retained. [Wedemeyer/Lewis property]
Spear Ave. & St. Louis Rd.	Residential development shall include a mix of housing types and shall be clustered to maintain creek course and riparian areas as open space. [Sorensen property]
Sunset at Baldwin APN's 505-121-021-026 & 505-121-019	Residential development shall be clustered to preserve Jolly Giant Creek course and wetland areas as open space, and to reserve right of way for the future extension Avenue of Foster Street to Sunset Avenue. Baldwin should be extended to intersect with the extension of Foster. Access to residential development should be from Foster and Baldwin. The City shall allow an alternative unit density as approved through a Type A Planned Development permit for a maximum of 143 one bedroom and/or efficiency units. Density bonus shall not apply to this alternative development option. [Franke Twin Parks property] The eastern portion of the site may be used for a public facility use to allow for a new Fire Station on this site. A public facility specific consideration shall be placed on the Sunset Avenue sites per PC Resolution 08-07.
Giuntoli Lane at Hwy 299	Residential development shall be designed to attenuate noise impacts from Hwy 299. Multifamily units shall be the predominant unit type. Special consideration should be given to internal and external access. This property has a Planned Development (PD) overlay. [Peugh-King property]

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[Revised by Ordinance No. 1377, September 2008]

LU-2d2c **Planned Development - Residential developments.** ~~On vacant sites of one acre and larger designated for residential use, the Planned Development combining zone shall be required.~~ The purpose of a Planned Development shall be to: incorporate a mix of residential types, unit sizes, affordability levels, and styles in a coordinated manner to encourage housing for all, including currently unhoused people; ~~to~~ allow clustering of units; ~~to~~ provide larger, more usable areas of common open space; and ~~to~~ protect natural resources or site features, such as creekside riparian areas, wetlands, and significant vegetation such as trees to incorporate health-promoting design. Planned Development may also incorporate non-residential uses ~~Where planned residential developments are adjacent to non-residential uses~~ with, appropriate visual and noise buffers ~~shall be~~ provided between the uses. ~~Other provisions in the General Plan would assure affordable housing.~~

POLICY LU-3 –COMMERCIAL LAND USE

Objective. Grow a pattern of activity centers that meet the daily needs of the community and visitors and with a variety of retail goods, food, and services that are convenient, safe, accessible, and affordable to all. Provide sufficient land areas and locations for a variety of retail and commercial services, to serve the consumer needs of the community and visitors.



LU-3a **Commercial use classifications.** The following land-use plan categories are applicable to commercial lands. Table LU-4 defines permitted uses, densities, lot sizes, and coverages for each classification. Large scale retail uses shall require a use permit due to evaluate:

~~All proposed retail uses with either: (1) a floor area greater than 30,000 square feet; or (2) physical alteration of eight or more acres; or (3) generation of 1,000 or more vehicle trips per day, shall require a use permit. The use permit review shall include, at a minimum:~~

- Potential impact on existing and projected traffic conditions.
- Impact on municipal utilities and services.
- Impact on the physical and ecological characteristics of the site and surrounding area.
- Impact on the community-
- ~~Fiscal impacts of the use.~~

Commercial – Central [C-C]. This designation covers most of the nineteen square blocks surrounding the Plaza and includes retail, professional office, civic, hotel, theater, residential, and similar uses. The Central-Commercial Area forms the center of the City and is designed to be a high density, pedestrian-oriented activity area, with shops and services, banks, offices, restaurants, and entertainment supporting a variety of day and night activities. [The Commercial-Central Zone will continue to have no upper density limit.](#)

TABLE LU-34 COMMERCIAL LAND USE CLASSIFICATIONS

ALLOWABLE USES	C-C	C-G	C-M
SALES OF GOODS & SERVICES			
General retail sales and services	X	X	X
Food retail sales and services	X	X	X
Specialty retail sales and services	X	X	X
Personal service establishments	X	X	X
Shopping centers	X	X	X
High-impact retail commercial uses	X	X	
Wholesale trade and warehousing		X	
Transient lodging	X	X	X
Travel trailer [RV] park		X	
Animal sales and services		X	X
MOTOR VEHICLE RELATED SALES & SERVICE			
Motor vehicle sales and rentals		X	
Motor vehicles services		X	
Gas sales		X	
RESTAURANTS, BARS, TAVERNS AND PUBS	X	X	X
NIGHTCLUBS	X	X	
COMMERCIAL RECREATION / ENTERTAINMENT			
Indoor recreation services	X	X	X
Outdoor recreation uses and services		X	
Theaters	X	X	X
BUSINESS AND PROFESSIONAL OFFICES			
Offices designed to serve customer traffic	X	X	X
Offices designed to attract little customer traffic	X	X	X
Health services	X	X	X
EDUCATIONAL, CULTURAL & RELIGIOUS USES			
Libraries, museums, art galleries, and similar uses	X	X	X
Mortuaries and funeral homes		X	
PUBLIC & SEMI-PUBLIC USES			
Government administrative offices	X	X	X
Post offices	X	X	X
RESIDENTIAL			
Multi-family residential	X	X	X
URBAN AGRICULTURE		X	X

[Revised by Ordinance No. 1377, September 2008]

Commercial – General [C-G]. This designation provides the full range of retail, entertainment, and service commercial uses primarily in [Uniontown, Valley West, Westwood/Sunset, Sunny Brae, Greenview, and Bayside neighborhood centers, and](#)

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~~other areas, such as Northtown, Giuntoli Lane, and Samoa Boulevard. C-G development must be compatible with the surrounding residential uses and provide convenient access for patrons arriving by bicycle, public transit, motor vehicle, or on foot. A primary difference between the C-G areas and the Plaza Area is parking. Businesses in the C-G area will be expected to provide sufficient on-site parking.~~ C-G areas are intended to have convenient access from residential areas in order to provide for day-to-day shopping and service needs. Residential densities allow up to 50 units per acre, but commercial uses shall constitute a predominant use of the first floor.

~~**Commercial – Visitor Serving [C-VS].** This land use designation permits hotels, motels, recreation vehicle parks, theaters, restaurants, auto sales centers, gas stations, mini-marts, and similar uses which attract or serve the needs of travelers, tourists, and local patrons.~~

~~The C-VS designation is not intended for general retail sales. C-VS uses are appropriate at highway interchanges where they are visible from the road and easily accessible by travelers and tourists. Similarly, C-VS uses are appropriate at locations near natural amenities or other attractions for visitors. The area designated for C-VS is at the State Route 101 and Giuntoli Lane interchange, west of Valley West Boulevard, and along Janes Road/Heindon Road.~~

~~**Commercial – Mixed Use [C-M].** This land use designation allows high-density infill development with a high degree of flexibility around uses. This land use designation is applied to parcels suitable for infill development, redevelopment, and densification. Residential densities allow up to 50 units per acre.~~

~~**LU-3b – Revitalization of existing commercial areas.** Reinvestment to upgrade and/or expand existing commercial centers at Sunny Brae, Westwood, Valley West, Greenview, and Uniontown shall be encouraged. Upgrades should include improvement of parking and landscaping areas; provision of facilities to accommodate bicycles, pedestrians, and transit and to reduce vehicle trips; and residential units on upper floors or in new structures.~~

LU-3c Northtown Commercial Mixed Use Area. (Not applicable in Coastal Zone). ~~The Northtown Commercial Area includes the area along “G” and “H” Streets from 11th Street to Sunset Avenue. If existing residential structures located along north “G” Street are converted to commercial use, some residential use should be required to be maintained on each site. Any conversions to commercial use should be required to provide on-site parking where feasible, in accordance with the parking standards of the Arcata Land Use Code. The design of parking areas shall maintain, or not detract from, the historical character of the existing structure. The continuation of mixed commercial and residential uses in the Northtown area should be encouraged; including allowing redevelopment to maximize housing opportunities at the highest densities, up to 50 units per acre.~~

~~LU-3d **Inclusion of residential uses.** Commercial developments should include residential units where feasible. Where Residential residential units are incorporated, some ground floor commercial should generally be maintained, subordinate to commercial uses, for example located on upper floors or at the rear, to maintain uninterrupted commercial uses at the street level. This shall not apply to the C-VS land use category. Any residential development in new structures shall comply with the development standards of the R-M applicable land use category.~~

LU-3e **Primary residential uses in Commercial – Central [C-C] Infill Opportunity Zone.** To maximize housing opportunities, residential uses are allowed as the primary use on vacant sites designated C-C, ~~at the highest densities consistent with the R-H designation, with a use permit.~~

~~TABLE LU-5 SPECIFIC CONSIDERATIONS FOR CERTAIN COMMERCIAL AREAS~~

AREA	SPECIFIC CONSIDERATION
Giuntoli Lane at State Route 101	Commercial development should include consolidated access points, parking that is screened, and setbacks from the Mad River. This property is within the urban services boundary, must be annexed, and has a Planned Development (PD) overlay [Graham Property].

POLICY LU-4 INDUSTRIAL LAND USE

~~**Objective.**~~ Provide for a variety of industrial uses ~~which that~~ will retain and generate living-wage jobs, including labor-intensive manufacturing, processing, assembly, warehousing, services, and complementary non-industrial uses, in appropriate locations. Build on the City's 150-year history of supporting industrial jobs, businesses, and product generation by supporting new and existing users in identified industrial nodes.

LU-4a **Industrial uses.** The following land use designations are applicable to industrial lands. Table LU-6 defines permitted uses, densities, lot sizes, and coverages for each classification.

Industrial – Limited [I-L]. This land use designation is intended to provide attractive industrial areas suitable for light manufacturing and limited commercial uses. High impact industrial uses more prone to produce noise, odors, heavy truck traffic, or dust such as lumber mills and recycling centers are not permitted in limited industrial areas. The I-L land use designation includes auto sales; service and repairs; sales of mobile homes, trucks and tractors; warehousing and wholesaling establishments; outdoor sales and storage lots; light industrial activities (when conducted within a building); and similar uses. Some retail sales uses and services, particularly those involving sales of products made on the premises, will be allowed as an accessory use. Residential uses may also be permitted where they are compatible with the nature of the production process, or the related sales of products made on the premises (such as artists'

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live/work space).

The major I-L areas are the ~~West Samoa Boulevard Employment Center, and the portion of the~~ West End Employment Center (near the State Route 299/Giuntoli Lane interchange ~~and including~~ the Aldergrove Industrial Park ~~and Happy Valley; and areas south of Samoa Blvd. including Little Lake and along south G Street~~), ~~the area west of “K” Street, South “G” Street, and South “I” Street.~~ These two industrial areas will be the primary focus of industrial and mixed-industrial development and investment during the timeframe of this General Plan.

Industrial – General [I-G]. This land use designation indicates areas which are appropriate for manufacturing; large-scale wood processing and storage; auto wrecking and storage yards, and all other general industrial operations. Some industrial operations generate noise, odors, or traffic which make them incompatible neighbors with residential or most commercial uses.

LU-4b Little Lake. ~~Development of a new industrial “business park” at the site of the The former Little Lake Industries lumber mill on South “I” Street shall developed for industrial or business park uses. be a priority of the City.~~ The site shall be planned as a mixed-use development including passive recreational uses and regional a dog park. Development shall be consistent with the adopted Long Range Property Management Plan.

LU-4c Happy Valley. Development of a new “business park” at Happy Valley shall be a priority of the City. The site shall be planned as a mixed-use development. Development shall be consistent with the adopted Long Range Property Management Plan.

TABLE LU-46 INDUSTRIAL / PUBLIC FACILITY LAND USE CLASSIFICATIONS

ALLOWABLE USES	I-L	I-G	PF
SALES OF GOODS AND SERVICES			
Personal services establishments	X		
High impact commercial uses	X		
Outdoor sales and storage of merchandise	X		
Wholesale trade and warehousing	X	X	
Animal sales and services	X	X	
MOTOR VEHICLE-RELATED SALES AND SERVICES			
Motor vehicle sales and rentals	X		
Motor vehicle services	X		
Gas stations	X		
RESTAURANTS AND NIGHTCLUBS	X		
COMMERCIAL RECREATION / ENTERTAINMENT			
Indoor recreation services	X		
BUSINESS AND PROFESSIONAL OFFICES			
Offices designed to serve customer traffic	X		
Offices designed to attract little customer traffic	X		
EDUCATIONAL, CULTURAL & RELIGIOUS USES			X

RESIDENTIAL	X		
PUBLIC & SEMI-PUBLIC USES			X
HIGH IMPACT MANUFACTURING & PROCESSING		X	
MODERATE IMPACT MANUFACTURING & PROCESSING	X	X	
LOW IMPACT MANUFACTURING & PROCESSING	X	X	
<u>URBAN AGRICULTURE</u>	<u>X</u>	<u>X</u>	<u>X</u>

[Revised by Ordinance No. 1377, September 2008]

LU-4**eb** **Conversion and reuse of old industrial sites.** The City shall encourage the conversion and reuse of ~~abandoned or inactive~~ industrial sites such as closed lumber mill sites. An environmental site assessment will be required for sites where prior uses may have caused soil contamination. Manufacturing uses may be allowed on older I-G sites, where activities are conducted in enclosed spaces and noise, light, air quality, or traffic impacts do not significantly impact adjacent uses.

LU-4**fe** **Incubator industrial sites.** Encourage small business incubator sites and cottage industries as part of the City’s mix of industrial uses.

LU-4**gd** **Industrial performance standards.** The City shall ~~develop periodically review and update and~~ adopted performance standards ensuring that new and upgraded industrial uses are attractively designed, keep noisy uses in enclosed spaces, do not emit light or glare off site, and contain other features that make them compatible with adjacent uses.

~~LU-4e **Specific areas and parcels.** Certain land areas and parcels have unique characteristics, limitations, and/or opportunities that require careful consideration when development or a change of use occurs. The considerations described below shall be made for the identified parcels.~~

~~LU-4f **South “I” Street.** Development of a new “business park” at the site of the former Little Lake Industries lumber mill on South “I” Street shall be a priority of the City. The site shall be planned as a mixed use development. A master plan shall be prepared for the site which includes the specific considerations found in Table LU-7.~~

LU-4**g4h** **Petroleum Coastal related resource extraction and processing Energy development, production, and use.** The City of Arcata recognizes the national need for the responsible exploration, recovery, and processing of the country’s energy resources. ~~T~~However, the City also recognizes the potential adverse impacts such activities ~~may can~~ have. Petroleum extraction is of particular concern on the North Coast. On- or offshore petroleum extraction will result in negative impacts on the climate and our coastal scenic resources, as well as our sensitive land and marine environments. on sensitive land and marine resources and on the scenic quality of coastal resources. Therefore, the City finds that, in order to minimize adverse impacts to such resources, on- and off-shore petroleum product exploration, recovery, and

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processing should be confined to those geographic areas which now accommodate these uses and activities. Consistent with this policy, the City shall prohibit on-shore petroleum exploration, production, and processing within its boundaries, and shall oppose the use of off-shore areas south and west of Arcata and in Humboldt County in general for such uses.

TABLE LU-7 SPECIFIC CONSIDERATIONS FOR CERTAIN INDUSTRIAL AREAS

AREA	EXISTING USE	CONSIDERATION
"K" Street Industrial Area	I-L	Provide live-work spaces; revitalize older industrial uses.
West End Road Corridor	I-L and I-G	Promote more intensive industrial uses.
Aldergrove Industrial Park	I-L	Coordinated light industrial development, with increased landscaping.
Giuntoli Lane/ Valley East area (Zanzi) APN's 507-141-037 & 507-141-042	I-L and I-G	Planned Development for entire property to minimize access points on Giuntoli, and coordinate land uses, interior circulation, shared parking, and overall building layout. Constraints include setback area along Mad River. APN 507-141-042 may allow for a public facility type use to accommodate a future City park along the Mad River. A special consideration for this parcel shall include dual (Industrial Limited [I-L] and/or Public Facilities [PF]) land use activities.
Northcoast Hardwoods site (Samoa Blvd.)	I-G	Planned Development for entire property to minimize access points on Samoa, and coordinate land uses, interior circulation, shared parking, and overall building layout.
Winkel Property - Little Lake Industries (South I Street)	I-L	Planned development for entire site to encourage: 1. An area not less than 25-foot wide adjacent and parallel to "I" Street shall be a landscape buffer between heavy manufacturing uses to the west and internal development on the site. Landscaping within the buffer strip shall include a dense planting of trees of species appropriate to the coastal environment. 2. A multi-use coastal access pathway shall be included along the entire landscape buffer to connect the Samoa Boulevard area to the Arcata Marsh and bay shoreline. 3. The former marsh areas at the southern portion of the property (south of the railroad tracks) shall be restored or recreated. 4. A small portion of the site adjacent to the Marsh Commons area, or along Butcher Slough, may be suitable for limited residential development. 5. A small portion of the site adjacent to the restored marsh area may be suitable as a site for an "eco-lodge" or other eco-tourism uses focused on coastal recreation. 6. The northern portion of the site east of the landscape buffer may be developed as a "business park" with light industrial and business-service uses. Comprehensive development standards for the business park area, including sidewalks, landscaping, and building design, shall be included in the master plan for the site. 7. A creekside conservation area shall also be required parallel to the top of the bank of Butcher Slough.
Happy Valley	I-L	XX

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AREA	EXISTING USE	CONSIDERATION
		XX XX

[Revised by Ordinance No. 1377, September 2008]

POLICY LU-5 PUBLIC FACILITIES

Objective. Provide appropriate locations and sites for water storage and delivery; wastewater collection and treatment; drainage; solid waste management; fire protection; parks and recreation; civic and institutional uses; and education (public and private) facilities.

LU-5a **Public Facility [PF] uses.** Public Facility land use category is applicable to those lands which are to be used for the various types of public facilities, except that some public facility uses are also allowed in other land-use categories. Public facility uses include certain uses which may be owned by private individuals, private organizations, or private institutions, as well as by government entities.



Uses include schools, public services and administrative offices, wastewater and solid waste management facilities, public parking lots, parks and non-commercial recreation uses, golf courses, auditoriums, and other public assembly spaces, hospitals, cultural facilities, community gardens, communication (including telecommunications) and transportation facilities, and utilities. The types of uses allowable within this category shall be specified in Table LU-6.

LU-5b **Development standards for public facilities.** Standards applicable to development of public facility uses shall be as specified in Table LU-6.

LU-5c **Limitation of corporation yard expansion.** Development of the City corporation yard facilities shall be restricted to its existing boundaries. A landscape screen shall be maintained along with northern and eastern perimeter of the oxidation pond.

[Corporation yard relocation in response to climate change shall be considered during the timeframe of this General Plan.](#)

TABLE LU-58 SPECIFIC CONSIDERATIONS FOR CERTAIN PUBLIC FACILITY AREAS

AREA	SPECIFIC CONSIDERATION
Mad River Hospital Area APN's 507-191-033, 507-191-076, 507-291-	Hospital development on APN's 507-191-077 and 507-291-032, in conjunction with an overall shall be developed consistent with the Master Site Plan adopted in 2011, shall retain 50% contiguous open space, have adequate provisions for on site retention of stormwater, be compatible in scale with surrounding uses, provide permanent open space or conservation easements to the City of Arcata or a land trust, and limit extent

AREA	SPECIFIC CONSIDERATION
032 & 507-191-077	of impervious surfaces (e.g. parking lots) as much as possible through a Planned Development. A conservation easement shall address provisions to: preserve prime agriculture soils; encourage limited agriculture activities; and promote trail systems, wellness gardens, and health related outdoor activities. On APN 507-291-032, north of Wiyot Way, a residential specific consideration shall be placed on this site that would allow for development of hospital related housing, including, but not limited to: life care facilities, assisted living facilities, medical related group quarters, medical services extended care facilities, temporary housing for family members of hospital patients, or housing for doctors, nurses, and hospital staff per PC Resolution 08-11. If a Master Site Plan for the Mad River Hospital Area is not approved by the year 2020 for a Public Facility (PF) Land Use Designated areas on APN's 507-191-077 and 507-291-032, then consideration should be given to reverting the PF designated lands back to an Agriculture Exclusive (A-E) designation.
Sunset Fire Station Site APN 505-121-030	Development of a fire station compatible with the proposed Foster Avenue extension shall be allowed on this Public Facilities (PF) designated parcel.
M-Street Fire Station Site APN's 020-127-004 & 005 & 020-128-002	Provide for a residential overlay to this site to make it compatible with adjacent residential low density neighborhood to the west. The special consideration should be considered if a new fire station is constructed south of the intersection of Sunset Avenue at Baldwin Street. Residential overlay considerations shall be placed on the "M" Street site per PC Resolution 08-07.

[Revised by Ordinance No. 1377, September 2008]

POLICY LU-6 AGRICULTURAL AND NATURAL RESOURCE LANDS

Objectives. Preserve and promote the sustained production of natural resources; preserve and promote the agricultural, forest, and aquaculture lands; and protect public natural resource/open space lands, including stream courses, wetlands, tidelands, and open space areas. Provide for complementary uses including farm [worker](#) housing, processing of agricultural and aquaculture products, and access for timber harvesting, in designated areas.

LU-6a **Agricultural and Natural Resource classifications.** The following land use categories are applicable to agricultural and natural resource lands. Table LU-9 defines permitted uses, densities, lot sizes, and coverages for each category.

Natural Resource [NR]. This designation is applied to public or private lands where protection of unique and/or sensitive natural resources, or managed production of resources, are the primary objectives. The resources element describes three subdistrict zones within the NR district which are designated: Wetland Stream Protection Zone (NR-WSPZ), Timber Production Zone (NR-TPZ), and Public Trust Zone (NR-PTZ). Examples of lands designated NR include the Community Forest (NR-TPZ), Janes Creek /McDaniel Slough Linear Park (NR-WSPZ), and the Arcata Marsh and Wildlife Sanctuary (NR-PTZ). Recreation may be considered as a secondary use when there are no adverse impacts to the protected resources. This designation is also applicable to productive resource lands, such as timber-producing forested areas (NR-TPZ) and aquaculture in Arcata Bay (NR-PTZ). The land between [Cal Poly](#)

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~~Humboldt~~[Humboldt State University](#) and the Community Forest is an example of productive forest lands designated (NR-TPZ).

The NR designation is not applied to small or "pocket" wetlands, that exist on parcels large enough to accommodate development without adversely impacting the wetlands. The designation is also not applied to wetlands used as grazed agricultural lands, or riparian areas in other zones. These resource areas are protected by applicable stream and wetlands standards.



Agriculture Exclusive [A-E].

This designation is intended to preserve land for agricultural production. The A-E designation is appropriate for lands with prime agricultural soils and wetlands that could be used as grazed agricultural lands. Structures associated with agricultural production, such as barns and farmhouses, are appropriate uses in A-E areas. [Agricultural and aquacultural product processing facilities for products originating outside of Arcata, and which are essentially industrial and require large-scale industrial buildings over 10,000 square feet are not appropriate for the A-E zone.](#)

Agriculture Residential [A-R]. This designation allows very [low density/low-density](#) residential development on agricultural lands. Structures associated with agricultural production, such as barns and farmhouses, would be appropriate uses in these agricultural areas.

TABLE LU-69 AGRICULTURAL AND NATURAL RESOURCE LAND USE CLASSIFICATIONS

ALLOWABLE USES	A-E	A-R	NR
RESIDENTIAL			
Single-family dwellings	X	X	X
Mobile / manufactured homes	X	X	
Secondary residential uses	X	X	
Bed & Breakfast (as secondary use with use permit)	X	X	
COMMERCIAL RECREATION & ENTERTAINMENT			
Coastal-dependent recreation	X		X
AGRICULTURE, MINING, SILVICULTURE & AQUACULTURE			
General agriculture, except keeping confined animals	X	X	
Agriculture, including keeping of confined animals	X		
Agriculture-related processing facilities	X	X	X
Commercial greenhouse operations	X	X	
Aggregate and other mining			X
Silvicultural operations			X
Aquacultural operations			X

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NATURAL RESOURCE CONSERVATION USES	X	X	X
DENSITY			
Density Range	1 primary residential unit per parcel (and a secondary unit)	1 primary residential unit per parcel (and a secondary unit)	1 unit per existing parcel

[Revised by Ordinance No. 1377, September 2008]

LU-6b **Compatibility between agricultural and adjacent non-agricultural uses.** Agricultural practices can include ~~spraying of herbicides~~, application of fertilizer, operation of farm equipment, and use of local roads by slow moving and large vehicles. These practices can cause noise, health, light, odor, and travel impacts for residents in adjacent non-agricultural areas. To minimize these impacts, development of new non-agricultural uses that locate adjacent to existing agricultural uses shall maintain setbacks and establish buffers. The potential impacts of adjacent agricultural practices shall be required to be disclosed to future residents. Where new or expanding agricultural uses locate adjacent to existing non-agricultural areas, the agricultural user shall be responsible for maintaining setbacks and establishing buffers.

LU-6c **Protection of agricultural lands and uses within the City.** Agricultural lands represent an important natural resource within the City. The protection of agricultural lands shall include the following:

1. Lands designated Agricultural Exclusive [A-E] with Grade* 1 and 2 soils are the City’s prime agricultural resource; lands designated [A-E] with Grade* 3 and 4 soils support less intensive uses than lands with higher grade soils, but are still viable for resource production. A-E lands shall not be developed, except for agricultural-related uses.
2. Agricultural uses on lands designated other than A-E shall be allowed and encouraged, consistent with other General Plan policies.
3. Existing agricultural practices on seasonal wetlands shall be allowed to continue, consistent with other General Plan policies.
4. The minimum lot size for lands designated A-E shall be twenty acres, ~~except in the coastal zone where the minimum lot size shall be sixty acres. Designated uses for agricultural lands within the coastal zone shall include the following:~~
 - ~~The "Permitted Uses" section shall include: "Agricultural Structures, including greenhouses or other nursery structures erected over exposed soil."~~
 - ~~The "Conditionally Permitted Uses" section shall include: Greenhouses or other nursery structures erected on concrete perimeter foundations may be permitted if no less environmentally damaging alternate is available."~~
 - ~~Greenhouses on slab foundations are prohibited.~~

Private and public non-vehicular recreational activities such as hiking, horseback riding, fishing, hunting, and other compatible, low-impact recreational activities which do not require permanent structures, facilities, or foundations may be permitted in areas

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designated A-E if they do not interfere with adjacent agricultural uses, or limit potential of the site to return to agricultural use, or displace the wildlife utilizing the area, especially in seasonal wetlands. This shall be implemented in the City's land use code.

LU-6d **Uses allowed in diked/reclaimed former tidelands.** Allowable uses and development in grazed or ~~farmed-diked former tidelands wetlands~~ are limited to uses compatible with the Public Trust. These uses are summarized below:

1. Agricultural operations limited to accessory structures, apiaries, field and truck crops, livestock raising, greenhouses (provided they are not located on slab foundations and crops are grown in the existing soil on site), and orchards.
2. Farm-related structures, including barns, sheds, and farmer-occupied housing, necessary for the performance of agricultural operations. Such structures may be located on an existing grazed or farmed wetland parcel only if no alternative upland location is available for such purpose and the structures are sited and designed to minimize adverse environmental effects on Public Trust resources and uses. No more than one primary and one secondary residential unit shall be allowed per parcel.
3. Restoration projects.
4. Nature study, aquaculture, and similar resource-dependent activities compatible with the Public Trust resources and uses.
5. Incidental public service purposes which may temporarily impact the resources of the area (such as burying cables or pipes).

[LU-6e Restoration of former tidelands. The City of Arcata recognizes the need to restore former tidelands to salt marsh in order to adapt to rising sea levels and promote biodiversity and a safe environment. The City shall encourage and support the restoration of former tidelands, currently zoned Agricultural Residential \[A-R\] or Agricultural Exclusive \[A-E\].](#)

LU-6fe **Relationship with the Open Space and Resource Conservation and Management Elements.** The Open Space and Resource Conservation and Management Elements (General Plan Chapter 4) address the natural resource values of agricultural and natural resource lands. Lands designated Agriculture Exclusive [A-E] and Natural Resource [NR] are important components of City's open space, as defined in the Open Space Element. The conservation and management of these lands for their natural resource values, as well as their biological, hydrological and soil resources, are specifically addressed in the Resource Conservation and Management Element. The policies of both these elements shall apply to future agricultural and natural resource land use decisions.

[TABLE LU-10 SPECIFIC CONSIDERATIONS FOR CERTAIN AGRICULTURAL AREAS](#)

AREA	SPECIFIC CONSIDERATION
Arcata Bay Storage APN 021-121-010	Provide for agriculture related manufacturing due to aggregate fill deposits on the Arcata Bay Storage site per PC Resolution 08-09.

[Revised by Ordinance No. 1377, September 2008]

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2.3 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
LU -1	Land Use Code Zoning Amendments Revise the City's Land Use Code <u>and other zoning codes so (formerly the Land Use and Development Guide, or LUDG)</u> zoning districts and land use regulations to are be consistent with the General Plan land use designations, densities, primary and secondary uses, building intensity, lot sizes, and coverages. <u>Add inclusionary zoning provisions for affordable housing.</u>	Community Development Department/ Planning Commission	Year 1 <u>21</u>
LU -2	Conversion and Reuse of Inactive Mill and other Industrial Sites Solicit property owner cooperation and pursue funding for environmental site assessments and clean-up of abandoned or inactive mill sites. Encourage conversion of these sites when proposed reuse is consistent with General Plan goals and policies.	Community Development Department/ Property Owners	Ongoing beginning in Year 2 <u>4</u>
LU -3	Maintain Economic Development Strategic Strategies-Plan The Economic Strategy Report <u>Development Strategic Plan</u> is intended to be a guide for City actions pertaining to economic improvement. <u>Update the</u> The report should be updated periodically for compliance with General Plan policies <u>and the vision and needs of the community.</u>	City Manager's Office <u>Community Development Department</u>	Every five years beginning in Year 2 <u>3</u>
LU-4	<u>Pedestrian-friendly activity centers</u> <u>Update zoning to improve the walkability of commercial activity centers using such strategies as:</u> <ul style="list-style-type: none"> <u>• Establish street-facing entrances as the norm through design standards and/or guidelines.</u> <u>• Update land use plan and zoning to concentrate retail land use designations in areas that are less than ½ mile across, and ideally along a single street that is less than 1000 feet from end to end.</u> <u>• Require maximum pedestrian block sizes of less than 1,600 or 2,000 feet in primary areas of activity.</u> 	<u>Community Development Department</u>	<u>Year 2</u>
LU - 5 <u>4</u>	<u>New Business Park Master Plans</u> Seek funding for and prepare a <u>business park master plans</u> for the reuse of the South "I" Street <u>Little Lake</u> property for mixed-use development <u>and the Happy Valley business park.</u>	Community Development Department	Year 2 <u>3</u>
LU-5 <u>LU-5</u>	<u>Neighborhood Centers Tribal Housing Partnerships</u> <u>The City shall continue to coordinate with the Wiyot Tribe to explore opportunities for housing partnerships intended to serve members of the Wiyot Tribe within the City of Arcata.</u> City staff will communicate City policy regarding mixed use to neighborhood center owners and managers, and work proactively with them to maximize use of the centers.	Community Development Department	<u>Ongoing beginning Year 1</u>
LU-6	<u>Industrial Performance Standards</u> City staff will communicate City policy regarding performance standards to industrial area owners and managers and work proactively with them to improve appearances, and reduce noise, light, and glare from operations.	<u>Community Development Department</u>	<u>Year 1</u>

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#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
LU-75	Planned Development Overlay Re-evaluate application of purpose and need of Planned Development overlays throughout the City. Consider revising or removing PD overlays based on purpose and need.	Community Development Department/ Planning Commission	Year 2
LU-86	Commercial Visitor Serving Zoning Overlay Evaluate the need for a Commercial Visitor Serving (CVS) zoning overlay to cover the area (or a portion) of the previous CVS land use designation in Valley West.	Community Development Department/ Planning Commission	Year 2
LU-7 LU-9	Affordable Housing Include inclusionary zoning measures and/or incentives in the next update of the Housing Element. City shall Consider a rezone to consider allowing mixed uses and more housing in current R-L neighborhoods within walking distance of the Plaza and Cal Poly Humboldt, including: The Bayview, Northtown, Arcata Heights, and Sunset neighborhoods. (MAKE SURE THIS SHOWS UP ON FINAL VERSION!)	Community Development Department	2002 Year 2

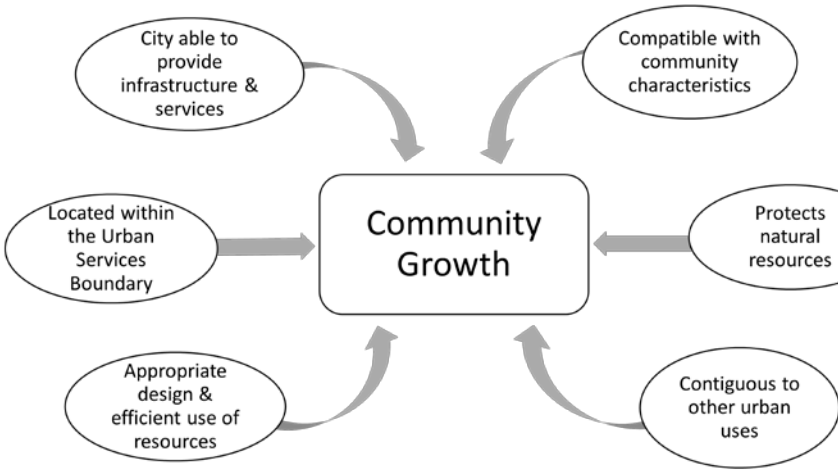
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GROWTH MANAGEMENT ELEMENT

2.4 INTRODUCTION

Growth Management Overview. ~~Since it was originally incorporated in the late 1850s, Arcata's population has grown to an~~ estimated ~~1998 2000 population of about to be~~ 18,900 ~~16,500 persons~~ in the 2020 Census. The General Plan ~~includes an projects~~ anticipates approximately 1% year over year population growth with a maximum ~~assumption of low ambitious~~ population growth, ~~to about of~~ 20,000 ~~27,000~~ persons, by the year ~~2020~~ 2045.

The majority of the City's growth has been, and is planned to be, ~~located~~ within the present City boundary, and concentrated within identified Infill Opportunity Areas (see Land Use Element); including the Gateway Area as well as around the downtown area, within existing neighborhood commercial centers, and near Cal Poly Humboldt Humboldt State University. Growth is directed to these areas because they have existing urban services and infrastructure. In addition, infill implements the regional Climate Action Plan and the City's sea level rise adaptation plan, preserves the natural and open working lands surrounding the City, and provides needed reinvestment in existing developed areas.



Growth Management Considerations

Cal Poly Humboldt is a major driver for both economic and population growth. There are currently about 5,739 students enrolled at Cal Poly Humboldt, with projected enrollment of 11,000 (full time equivalent) students by 2028^a. While a proportion of these students are anticipated to be remote learners, and Cal Poly will be housing a large proportion of newly enrolled students, Cal Poly's growth will impact the population.

Planning Areas.

The City of Arcata ~~has an~~ incorporated area is defined by its City limits. ~~The City also has an~~ Urban Services Boundary (USB) that defines the anticipated limits for extending City services and infrastructure ~~so as to accommodate new development anticipated~~ within the General Plan time frame ~~of this General Plan;~~ There is also a Sphere of Influence (SOI) ~~boundary~~ that ~~indicates~~ includes unincorporated lands which ultimately may be annexed to the City and

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developed ~~and annexed to the City~~; and a Planning Area which also includes nearby unincorporated lands. Collectively, these planning features/areas and the policies that guide related development activities, such as annexation, constitute much of the City’s growth management program. The City’s Sphere of Influence/SOI is shown on Figure LU-a, the and Planning Area are shown on Figure LU-b, and the Urban Services Boundary on Figure GM-a. Each of these features is described in more detail below.

Annexations/ Expansion of City Boundaries.

The Cortese/~~Knox~~/Hertzberg (CKH) Local Government Reorganization Act of 1985-2000 sets forth ~~the rules and~~ regulations under which Arcata may apply to the Local Agency Formation Commission (LAFCo) to expand its ~~adopted~~ City limits/boundary. The ~~legislative~~ intention ~~of this law~~ is to encourage orderly growth and development, which is considered essential to the social, fiscal, and economic well-being ~~of the state~~. It recognizes that the formation of local agency boundaries is an important factor in assuring orderly that development and the efficient delivery of services is orderly. ~~Within the legislation~~ The CKH Act gives, the Local Agency Formation Commission (LAFCo) review is given the sole and exclusive authority for the annexations of additional territory to a city. ~~The Coastal Land Use Plan applies only to the land areas which are within the city boundary.~~

Sphere of Influence.

Before LAFCo may approve territorial an annexation, ~~it must define and adopt that territory must be within the City’s SOI~~ Sphere of Influence for the City. A Sphere of Influence/SOI is described as a plan for defines the City’s probable ultimate physical boundaries and service area as determined by LAFCo of a local agency.



The ~~Sphere of Influence boundary/SOI~~ includes all City lands and unincorporated lands (under County jurisdiction) that may be annexed ~~as part of the ultimate development of the City. This ultimate development extends beyond in the twenty-year General Plan~~ time frame ~~of the General Plan~~. Designating unincorporated lands (under County the jurisdiction of the County) lands within the ~~Sphere of Influence is done for two reasons/SOI~~:

- 1. ~~It a~~ Allows the City to plan for the eventual annexation and extension of services as part of community growth.
- 2. Allows the City to ~~It~~ designates uses ~~for those lands that are~~ compatible with adjacent City lands.

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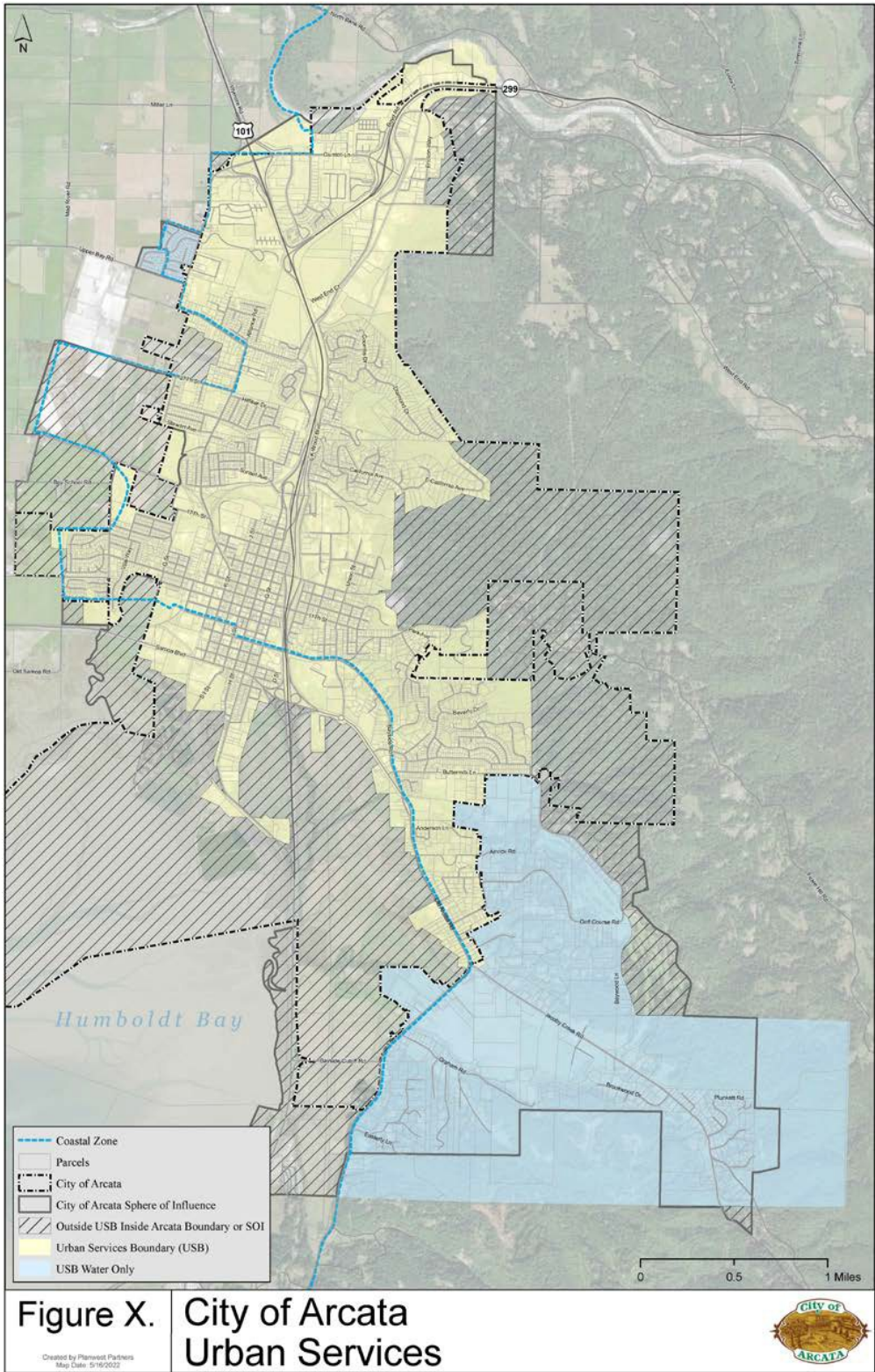
The City's Sphere of Influence SOI is adopted, as required by state law, by ~~the~~ Humboldt County LAFCo, a county-wide agency.

Arcata Planning Area.

California planning law requires the City to adopt a general plan for lands within its limits and also for any land outside its boundaries which bears relation to its planning. Defining a Planning Area is a means by which the City can communicate its concerns for the future of surrounding lands under the jurisdiction of the County or neighboring cities. The Planning Area may extend beyond the City's Sphere of Influence SOI.

The Planning Area includes the City, the Sphere of Influence and surrounding county lands ~~that are beyond the SOI~~ intended to remain unincorporated, where uses and activities impact the City. The Planning Area boundary is determined by geographic features, such as the Pacific Dunes, Pacific Ocean, Mad River, Fickle Hill ridgeline, and hydrological features such as the watersheds of Arcata's creeks ([Figure LU-b](#)). Other considerations for determining the Planning Area include preserving viewsheds and maintaining a greenbelt around the City.

FIGURE GM-a URBAN SERVICES BOUNDARY

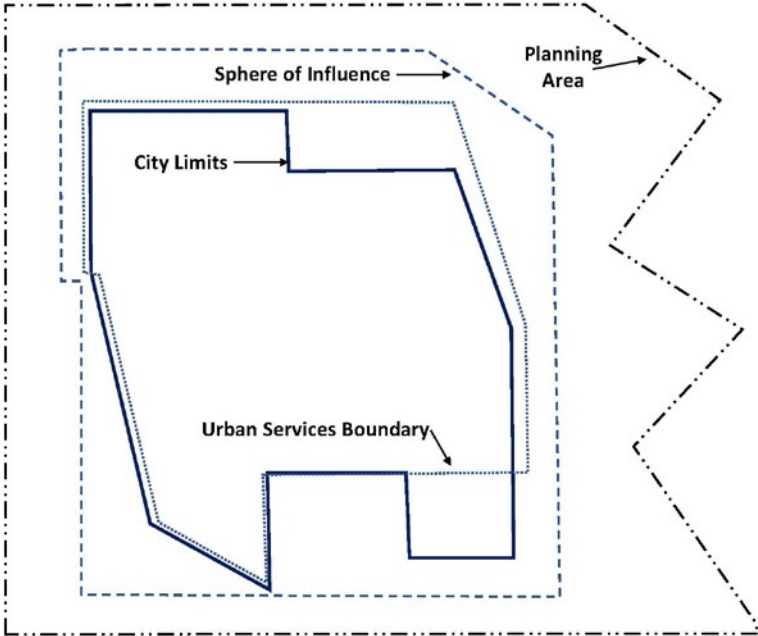


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The Urban Services Boundary.

The Urban Services Boundary is a mapped limit line surrounding the City’s developed and developing areas. It can also include areas of the Sphere of Influence SOI where urban

development is planned. Inside the City limits, the boundary excludes areas inappropriate for urban development, such as agricultural lands and the community forest. Where it extends beyond the City limits, it does not extend outside the City’s Sphere of Influence SOI. The boundary is determined by the City’s interest in extending services and infrastructure (water, wastewater collection, police services, etc.) to urban uses. The City provides certain areas, such as Jacoby Creek, with “water only” services. The Urban Services Boundary encompasses the area wherein urban development may occur during the General Plan’s twenty-year time frame.



Illustrative of City Limit, Urban Services Boundary, Sphere of Influence and Planning Area Boundary

Relationship with Other General Plan Elements and OPR General Plan Guidelines.

The Growth Management Element includes guiding principles, policies and implementation to ensure municipal services and management operations are coordinated concurrently with anticipated development and consistent with Land Use, Circulation, Housing and other element plans and policies. The Growth Management Element guides and sets parameters under which future development may occur, consistent with the Land Use Element. The Growth Management Element provides guidelines for analyzing future boundary changes, development project proposals and mitigation. The City can apply Growth Management guidance to prioritize public facilities and services access that enhances quality of life, including transportation, housing, parks, open space, trails, greenbelts, recreational facilities (including senior and youth centers), community centers, grocery stores, health care facilities, childcare centers, libraries, and cultural centers. Ensuring sufficient community facilities and support services is important to maintain social equity.

The Growth Management Element is consistent with state planning priorities (California Government Code section 65041.1) to promote equity, strengthen the economy, protect the environment, and promote public health and safety, as summarized below:

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- (a) To promote infill development and equity by rehabilitating, maintaining, and improving existing infrastructure that supports infill development and appropriate reuse and redevelopment of previously developed, underutilized land presently served by transit, streets, water, sewer, and other essential services, particularly in underserved areas, and to preserving cultural and historic resources.
- (b) To protect environmental and agricultural resources by protecting, preserving, and enhancing natural resources, including working landscapes such as farm, range, and forest lands, natural lands such as wetlands, watersheds, wildlife habitats, and other wildlands, recreation lands such as parks, trails, greenbelts, and other open space, and landscapes with locally unique features and areas deserving special protection.
- (c) To encourage efficient development patterns by ensuring that any infrastructure associated with development, other than infill development, supports new development that does all of the following: uses land efficiently; is built adjacent to existing developed areas to the extent consistent with the priorities specified pursuant to subdivision; is located in an area appropriately planned for growth; is served by adequate transportation and other essential utilities and services; and minimizes ongoing costs to taxpayers.

Guiding Principles and Goals.

- A. Lands in the Sphere of Influence should remain rural and under the land use authority of the County until it can be demonstrated that urban services and annexation by the City is necessary until they are annexed to the City.
- B. Preserve the rural existing community character and promote resource protection in the Planning Area.
- C. Maintain an Urban Services Boundary that serve includes only the existing urban area, and areas appropriate for development by the year 20202045.
- D. ~~Locate new urban~~ Prioritize infill development in areas within or contiguous to existing urban uses and services and concentrate growth around existing activity centers. -
- E. ~~Protect-Exclude~~ flood-prone, steeply sloped, and other natural hazard areas, streamside buffers, ~~areas~~ and productive natural resource, agricultural, and forest lands from urban development.
- F. Incorporate greenbelts, designated natural areas, and other open space into the planning area and Sphere of Influence in order to maintain an identity separate from surrounding communities.
- G. Manage the timing and amount of growth in accordance with the ability to maintain acceptable levels of service and quality of life for existing and new residents.

2.5 POLICIES

The Growth Management Element contains the following policies:

- GM-1 Planning Area
- GM-2 Sphere of Influence
- GM-3 Annexation
- GM-4 Urban Services and Urban Services Boundary

POLICY GM-1 PLANNING AREA

Objective. Advocate for appropriate Planning Area land uses and management of for Planning Area land outside the City boundary, including Arcata's surrounding creek watersheds, the Arcata Bottoms, and coastal areas, in recognition that they will affect Arcata's the future community form of the Arcata community. The intent is that land uses shall be limited to timber and agricultural production and natural resources management and development only at rural densities be approved within the Planning Area.

GM-1a **Planning Area.** The Planning Area shall include all lands within the boundary shown on Figure LU-ab. The Planning Area includes lands where any future changes in use or management practices are likely to have an impact on the City of Arcata. The City shall provide mapping showing the planning area boundary and request that all land use and development applications received by the County, or resource management plans received by other agencies, be referred to the City. The City shall for review and comment prior to entitlements on these applications and plans. The City shall also advocate that the County implement policies and that property owners manage for the conservation of these unincorporated lands. This policy applies to unincorporated lands within the Planning Area but outside the Sphere of Influence.

GM-1b **Referrals by Humboldt County.** The City shall review all development and infrastructure proposals affecting the Planning Area and communicate the City's position on these matters to the applicable decision-making body. Humboldt County shall be requested to refer all planning studies and applications for development in the Planning Area to the Arcata Community Development Department, for review and comment, prior to taking action.

GM-1c **Land-use designations within the Planning Area.** The City shall request that the County of Humboldt retain timber, agricultural, and rural and agricultural designations on lands located within the Planning Area. The City shall request that the County adopt consistent land use designations for lands within the planning area but outside the Sphere of Influence, as shown on Figure LU-b.

GM-1d **Greenbelt.** The rural and agricultural lands within the Planning Area are designated by the City as open space or greenbelt. The intent is that such lands shall not be developed with urban densities or uses and that land uses shall be limited to [timber and agricultural production and natural resources conservation management and development only at rural densities be approved.](#)

GM-1e **Resource Protection in the Planning Area.**

Proposals which could affect the Planning Area's resources, including those resources addressed in the Resources Conservation and Management Element, shall be reviewed by the City. City review shall address the proposal's compatibility with applicable General Plan policy. The City shall communicate the intent of this policy to agencies with jurisdiction within the Planning Area (for example, the California Department of Forestry and Fire Protection for timber management, and the Humboldt Bay Harbor, Recreation and Conservation District for Humboldt Bay tidelands management) together with the policies in the Resource Conservation and Management Element.



POLICY GM-2 SPHERE OF INFLUENCE

Objective. Designate an appropriate amount of urban reserve [and open space](#) land in the Sphere of Influence (SOI) to provide for the ultimate development of the City.

GM-2a **Sphere of Influence.** ~~The proposed Sphere of Influence boundary, which must be adopted by LAFCo for the City of Arcata, is shown on Figure GM-a.~~ The SOI boundary defines land ~~parcels that are~~ eligible for annexation to the City in the future, [shown on Figure GM-a.](#) The City shall maintain an adequate amount of land in the [Sphere of Influence SOI](#) to accommodate future growth, consistent with the goals and policies of this plan, ~~as well as to protect open space and productive resource uses.~~ Annexation of property may not proceed unless or until such lands are within the [SOI Sphere of Influence boundary.](#)

GM-2b ~~Changes to the Sphere of Influence~~ [Updates and Amendments](#) ~~boundary.~~ [The City shall comprehensively evaluate the LAFCo updates the](#) Sphere of Influence [boundary at least](#) every five years, [as necessary,](#) but more frequently if appropriate. [The City can also apply directly to LAFCo for a SOI amendment.](#) ~~Any boundary amendments~~ [Updates and Amendments to the SOI](#) shall be considered by the Planning Commission and recommended to the City Council, prior to ~~any LAFCo action by~~

~~LAFCo~~. Any City decision to amend change the SOI boundary shall be based on the following:

1. The resulting area has an adequate supply of land to accommodate projected housing needs allocated by the Humboldt County Council of Government Association of Governments (HCAOG).
2. Any property owner ~~of property located adjacent the Sphere of Influence boundary~~ may request that the City ~~apply to LAFCo~~ add his or her/their lands to the Sphere of Influence SOI.
3. Any such proposal shall also identify the requested land use designations and any other necessary or appropriate General Plan Element amendments ~~for consistency and social equity to the various elements of the General Plan~~.
4. In considering such requests, and at during each periodic comprehensive evaluation SOI Update, the City Council, upon Planning Commission recommendation ~~of the Planning Commission~~, shall determine whether it would serve the public interest to designate additional lands ~~for which to provide to~~ receive municipal services and be developed with urban uses.
5. An amendment to the Sphere of Influence SOI Update, to include additional lands, shall be subject to environmental review pursuant to CEQA.
6. A proposal to amend the Sphere of Influence SOI Update may be considered concurrently with an annexation request.
7. Any SOI adjustment ~~to the sphere boundary~~ must incorporate adequate open space and resource management provisions ~~for open space~~.

GM-2c **Areas in Sphere of Influence intended only for partial services.** Some SOI areas are ~~included in the Sphere of Influence~~ only for the purpose of receiving City water service, such as the Jacoby Creek residential area. Such areas are not to be considered for annexation during the 20-year time frame of this Plan.

GM-2d **City land-use designations.** The City's land use designations for parcels ~~located within~~ the Sphere of Influence SOI are shown in Figure LU-a. These designations have legal force and effect only upon annexation ~~of particular land areas~~ to the City.

GM-2e **Prezoning of Sphere of Influence parcels ~~within the Sphere of Influence~~.** Lands in the Sphere of Influence SOI shall not be prezoned until ~~the City considers particular~~ annexation requests is proposed. ~~Such p~~Prezonings shall ~~be consistent with the apply~~ City land-use designations ~~for the Sphere area~~, or a General Plan Amendment would be required. The purpose shall be to provide maximum latitude to the City to determine the appropriate timing of expansion of the City boundaries, extension of services, and urban development.

GM-2f **County Land-use and Zoning designations in Sphere of Influence.** The City shall request that Humboldt County adopt agricultural, timber, natural resource, or other rural land use designations, within the Sphere of Influence SOI, as a holding zone to

prevent premature development prior to annexation to the City. This shall not apply to areas ~~which are~~ already ~~fully~~ developed such as Pacific Manor. The County's land-use designations are legally binding until annexation to the City is completed.

GM-2g **Resource lands in the Sphere of Influence.** The City shall periodically review the SOI boundary, especially to the west, to determine whether resource lands are being adequately protected.

POLICY GM-3 ANNEXATIONS

Objective. Provide for logical annexations of unincorporated areas, within the City's Sphere of Influence and/or Planning Area, only when ~~the~~ existing or proposed development is consistent with community character and City services can be adequately provided.

GM-3a **City annexation procedure.** The City prefers to consider annexation requests prior to LAFCo consideration. If ~~area~~ property owner(s) ~~or residents~~ request that the City initiate an annexation request to LAFCo, the following City review procedures shall apply:

1. Initiation and City review prior to LAFCo application filing:
 - a. ~~If lands are inhabited, a~~ petition must be signed by no less than ~~fifty twenty-five~~ percent of the resident registered voters, or at least twenty-five percent of property owners who also own twenty-five percent of the assessed value of land within the territory proposed for annexation of property located within the annexation area;
 - b.
 - c. ~~If lands are uninhabited but consist of more than one parcel and owner, a~~ petition must be signed by a majority of owners representing more than fifty percent of the annexation area. ~~If a single parcel, a petition must be submitted by owner(s).~~
2. The Planning Commission shall review and make a recommendation on the requested-proposed annexation to City Council.
3. ~~Final~~ Action shall be taken by Council regarding to pass a Resolution of Intention Application to LAFCo for annexation, or advise property owners that a City application will not be filed. Property owners may apply directly to LAFCo by petition in accordance with petition thresholds in the Cortese-Knox-Hertzberg Local Government Reorganization Act of 2000.
4. Following City Council approval ~~of annexation request~~, including any accompanying General Plan amendment, pre-zoning ordinance and/or environmental document, the City shall prepare and transmit ~~the an~~ annexation request-application to LAFCo for its consideration and decision.

GM-3b **Required materials for consideration of non-urbanized land area annexations ~~of non-urbanized land areas~~.** The following shall apply to annexation ~~requests-proposals~~

where the land proposed to be ~~added~~ annexed to the City is not developed with urban land uses ~~prior to annexation~~:

1. The City, ~~or experts under contract to the City~~, shall prepare an annexation study addressing items "a" through "f" listed below. The annexation study costs, including City administrative costs, shall be borne by the property owner(s) proposing the annexation.
 - a. A ~~comprehensive and detailed analysis of the~~ fiscal impacts analysis of the annexation, addressing the full range of projecting revenues and expenditures for proposed development in the annexed territory. One-time capital costs of facilities, as well as ~~recurring~~ operating costs and revenues, including tax revenues and parcel assessments, shall be evaluated;
 - b. A study and/or proposal for tax-sharing agreements with other taxing entities, such as the County;
 - c. An accompanying General Plan Amendment, if requested or appropriate;
 - d. A proposed preliminary development plan, including phasing if appropriate;
 - e. An assessment of the City's capacity to provide facilities and services, including: wastewater collection and treatment; stormwater management; water supply and distribution; streets and circulation; fire protection; police services; parks; and others as appropriate;
 - f. A rezoning ordinance. ~~The costs of preparing the annexation study, including City administrative costs, shall be borne by the property owner(s) requesting the City to consider the annexation.~~
2. An environmental document pursuant to CEQA.
3. A Planned Development, master plan, or other comprehensive plan for development ~~or Specific Plan~~ may be required for any land area greater than five acres.

GM-3c **Criteria for annexation of undeveloped land areas.** ~~All undeveloped lands proposed for annexation shall be added to the~~ The City shall apply for annexation of undeveloped lands only if the following criteria are met. The proposed annexation area shall:

1. Be within Urban Services Boundary and adjacent to existing urban development; unless the annexation area is City owned land for the purposes of resource/habitat management or for the purpose of fulfilling the City's greenbelt policies.
2. Not exceed the City's capacity to provide services and infrastructure to accommodate proposed development.
3. Have annexation timed so that availability of services and infrastructure is concurrent with need.
4. Have a positive or neutral fiscal impact, or other overriding public benefits.
5. Be in compliance with General Plan policies.
6. Not include prime agriculture land (Storie Index 60 or higher) other than with designation and prezone as Agriculture-Exclusive [A-E], Natural Resources [NR], or

Public Facility [PF]. The Natural Resource and Public Facilities designation may be applied for uses consistent with the Open Space Element, the Resource Conservation Element, and City plans adopted pursuant to these Elements.

GM-3d **Criteria for annexation of areas with existing urban development.** ~~All lands with existing urban development proposed for annexation shall be added to~~The City shall apply for annexation of existing urban development only if the following criteria are met. The proposed annexation area shall:

1. Be within the Urban Services Boundary and adjacent to existing city boundary.
2. Have facilities brought up to City standard prior to or concurrent with annexation.
3. Have costs of service extensions borne entirely by owners of annexing properties and not by existing City taxpayers or ratepayers, or, have a financing mechanism in place prior to annexation.

POLICY GM-4 URBAN SERVICES AND URBAN SERVICES BOUNDARY

Objective. Define the boundary ~~that limits the extent of~~ City services and urban uses, within the City's incorporated area and Sphere of Influence SOI. Extend City services ~~to urban uses within the urban services boundary when provision of~~ only when these services they will not exceed the City's planned system capacities.

GM-4a **Urban Services Boundary.** The City shall maintain an Urban Services Boundary, (USB) beyond which urban services shall not be provided (except as provided for in Policy GM-4b), and urban development shall not be approved (see Figure GM-b). Rural residential development may be approved outside the Urban Services Boundary only if the development would not require the extension of water, sewer, and other public facilities. The area within the Urban Services Boundary shall be annexed at the time of development.

Any USB changes ~~to the Urban Services Boundary~~ shall be based on an development capacity analysis of site characteristics such as soils type, vegetation, topography (slope) and natural hazards, availability of public water, ~~and sewer~~ and other municipal services, existing property lines, existing land use, and development potential ~~for development~~. The boundary-USB shall not be extended into the prime agricultural land or flood-prone areas on the west and south, nor extend past the Mad River on the north. To the east, the boundary-USB shall not be extended into the steeper portions of Fickle Ridge, as designated on the General Plan Map. Parts of the Jacoby Creek and Bayside areas are included within the USBurban services boundary.

Only the Agriculture- Exclusive [A-E], Natural Resource [NR], and Public Facilities [PF] land use designations shall be applied to areas outside the Urban Services Boundary and within the Coastal Zone.

GM-4b **Services outside City boundary.** The City shall not extend sewer ~~mains~~ or ~~new~~ water ~~mains infrastructure~~, or provide new service connections ~~to portions of the Planning Area~~ outside the City Limits except under the following conditions:

1. **Emergency sanitary sewer and/or water connections.** The City may provide an emergency sewer or water line extension, when approved by LAFCo, provided under the following conditions ~~are met~~:
 - a. The property is located within the City Urban Services Boundary for water and sewer.
 - b. The property is adjacent to the City limits.
 - c. The on-site sewage disposal system has failed.
 - d. It is not feasible to replace or repair the on-site sewage disposal system as evidenced by a letter from the County of Humboldt Division of Environmental Health.
 - e. The on-site sewage disposal system failure is considered a health hazard by the County of Humboldt Division of Environmental Health.
 - f. The owner has submitted a complete application to the City of Arcata for annexation of the property ~~within 18 months from the date that sanitary sewer service was provided~~.
 - g. LAFCo has approved the emergency sanitary sewer connection.
 - h. The sewer connection shall be sized to only accommodate the failed system.
2. The City may contract to provide sewer services to other service districts subject to the following guidelines:
 - a. Only those areas with existing contracts as of December 31, 1998 shall be served.
 - b. No new contracts for services shall be approved.
 - c. No new connections shall be allowed to the sewer lines in the area between the City Limits and the Arcata Planning Area Boundary.
[Revised by Ordinance No. 1377, September 2008]

GM-4c **Requirements and procedures for modifying Urban Services Boundary.** The following findings must be made to modify the Urban Services Boundary:

1. There are existing or planned urban uses ~~or urban uses designated on the land use plan map~~ for the area being considered.
2. There is sufficient existing or planned infrastructure capacity to extend water, sewer, police protection, and other services, without reducing service standards for other areas.
3. The area to be served is adjacent to existing urban development.
4. The area to be served is within the City limits or Sphere of Influence SOI.
5. City services extensions will not adversely impact natural resources in the area.

2.6 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURE	RESPONSIBLE PARTY	TIME FRAME
LU-1	Annexation Procedures (LUC Amendment) Add Update City annexation criteria and procedures to in the City's LUC (formerly LUDG) Land Use Code for consistency with General Plan policies.	Community Development Dept./ Planning Commission	Year 1
GM -1	Sphere of Influence/Planning Area Application Review Develop memorandum of understanding (MOU) between City of Arcata, Humboldt County, and LAFCo for review of development and use permit applications. Continue to participate in the County application review process that provides for City review and input on all proposed actions in Sphere of Influence/Planning Area lands. Submit mapping showing the Planning Area boundary and request that all land use and development applications received by the County be referred to the City.	Community Development Dept.	Year 1 & Ongoing
GM-2	Jacoby Creek Water Service Area Develop a memorandum of understanding (MOU) between the City of Arcata and the Jacoby Creek County Water District that addresses how services will be provided long term once the Davis-Grunsky loan payoff occurs.	Community Development Dept.	Year 5
GM-2	Memorandum of Understanding (MOU) with County Negotiate a MOU with Humboldt County that provides for City review and input on all proposed actions on Sphere of Influence/Planning Area lands.	Community Development Dept.	Year 1
GM-3	Urban Services Boundary Update Title 5 and Title 7 of the Arcata Municipal Code to define and describe criteria for modifying Urban Services Boundary, consistent with General Plan policy.	Community Development Dept./ Planning Commission	Year 1
GM - 43	Update Sphere of Influence Boundary Coordinate with LAFCo to comprehensively evaluate the Sphere of Influence Boundary to ensure that City growth and resource protection objectives are being met. In coordination with LAFCo as part of the next municipal service review and SOI update process evaluate potential SOI reductions in the following areas: east of Aldergrove/Ericson Way, Bay School Road, east of Buttermilk Lane near Baywood golf course, and Bracut.	Community Development Dept.	Every 5 Years
GM-5	Prezoning Ordinance Prepare and adopt a prezoning ordinance that does not allow resource lands to be prezoned, but requires consideration of land use designations at time of annexation.	Community Development Dept.	Year 2
GM-6	Local Agency Formation Commission Submit changes in Sphere of Influence and updates of maps to LAFCo, and request that maps be updated.	Community Development Dept.	Year 5

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^a Cal Poly Humboldt Enrollment 2022: <https://irar.humboldt.edu/data-enrollment>. Polytechnic Prospectus: <https://www.humboldt.edu/sites/default/files/hsupolytechnicprospectus.pdf>

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~~TRANSPORTATION CIRCULATION AND MOBILITY ELEMENT~~ 6/20/23

2.7 INTRODUCTION

~~Transportation concerns~~ Circulation and mobility consider how people and goods move through and around the City. The ~~transportation element~~ Circulation and Mobility Element addresses how a comprehensive, integrated transportation network roadway, transit, rail, freight, bicycle, and pedestrian systems can be planned to ~~achieve maximum~~ maximize individual mobility in a manner consistent with community character and environmental protection. The City is committed to providing a complete, connected, multimodal transportation network. California law requires that transportation and land use policies be closely correlated. The Arcata General Plan accomplishes this correlation in two ways. First, travel demand has been forecasted based on the amount and distribution of growth ~~allowed~~ anticipated by the land use plan. Second, the ~~policies of the transportation~~ Transportation, Land Use and Air Quality ~~elements policies are linked~~ have been interwoven to ~~provide a~~ balance ~~between~~ land uses and the transportation facilities that serve them. The overall theme of this element is achieving a balanced transportation system that is safe, accessible, comfortable, accommodating, and welcoming to all users. Transportation and mobility planning and policies in Arcata will put the safety of people first, both outside of vehicles and in them, recognizing that mobility goes beyond vehicular circulation patterns.

Protecting and improving safety must come first in our transportation, circulation, and mobility policies and planning decisions. This is in line with a “Vision Zero” approach to transportation planning that strives to eliminate traffic-related death and injury as the highest priority in transportation planning, above and beyond speed, convenience, and financial cost.

Overview of Existing ~~and Future~~ Transportation Circulation and Mobility Conditions

Existing Roadway System. Arcata’s pattern of highways and streets is similar to many small and rural communities. The central business district has a traditional grid pattern of streets, with a one-way couplet system comprising the primary arterial. A non-grid series of arterial and collector streets surrounds the central business district and serves outlying residential subdivisions, neighborhood shopping centers, Cal Poly Humboldt State University, and industrial areas. On the outer edges of Arcata, the transportation system is comprised of



rural roads and highways serving isolated farms and residences. Arcata is bisected by the State Route 101 freeway, the main state route serving the North Coast of California from San Francisco to Oregon.

Functional Classifications of the Street System. Arcata’s existing and planned primary streets and their functional classifications are shown in Figure T-a. The functional classification system is described in the following paragraphs. All streets within Arcata city limits, with the exception of access-controlled segments of Highways 101 and 299 and certain rural roads, are lined with homes and businesses and will be managed primarily to provide safe access and high-quality public space, regardless of functional classification. Slow speeds and traffic calming will be prioritized on all city streets.

Freeways and Highways. Freeways are high speed facilities with restricted access that move traffic on an intercity or regional basis. Access to freeways is limited to grade-separated interchanges. Routes 101 and 299 are designated as freeways. Highways are also high-speed facilities, but with fewer restrictions on access and at-grade intersections. Route 255 is designated as a highway.

Arterial Streets. The primary function of arterial streets is to provide intracity mobility as efficiently as possible. In addition to interconnecting the various parts of the city, arterial streets also provide some access to abutting lands. Compared to other communities, arterials in Arcata have fewer traffic control devices at intersections. ~~As of 1998, all of the traffic signals in Arcata~~ are ~~were~~ located on Samoa Boulevard, which is State Route 255 state facilities. Examples of arterials include the “G” and “H” Street one-way couplet, Alliance Road, Samoa Boulevard and L.K. Wood Boulevard.

Minor Arterials. Local streets, while providing access to development on adjacent lands, primarily provide mobility between arterial and collector streets. Examples include Buttermilk Lane and West End Road within City limits, and, Jacoby Creek Road and Upper Bay Road(within the Sphere of Influence) ~~West End, Union, and Upper Bay Road.~~

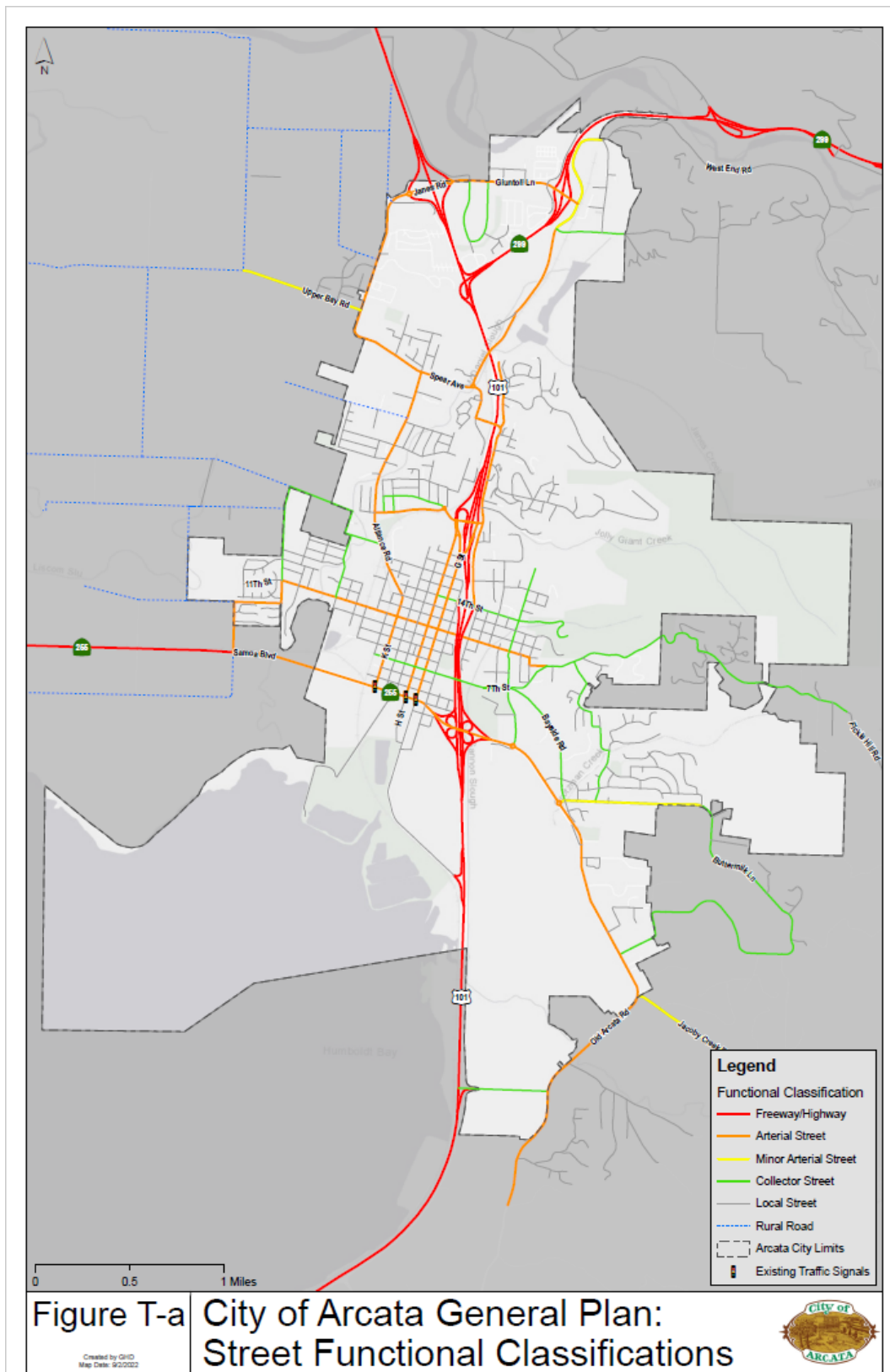
Collector Streets. Collector streets provide both mobility and access to land in about equal proportions. These roadways move vehicular, pedestrian, and bicycle traffic within and between residential, commercial, and industrial areas. As the name implies, collector streets are intended to collect traffic from local streets and channel it to the arterial street system. Examples of collector streets include 7th Street, 14th Street, Union Street, Buttermilk Lane, and Fickle Hill Road.

Local Streets. Local streets mainly serve to provide access to development on abutting parcels of land. These low-speed roadways provide access between land uses and collector streets. Local streets serve all types of land use including residential, commercial, and industrial. Often, local streets in residential areas are utilized by through traffic, resulting in complaints from residents about speeding and high traffic noise volumes.



Rural Roads. Rural roads are generally two-lane unimproved facilities located on the outer edges of the community, not within the City. Their primary function is to provide connection and access to farms, isolated residential areas, and industrial uses. Rural roads usually do not have typical urban improvements such as underground drainage, lighting, sidewalks, or curbs and gutters. Examples of rural roads in the Arcata area include Mad River Road, Upper Bay Road, Jackson Ranch Road, the western portion of Foster Avenue, and Jacoby Creek Road.

FIGURE T - a STREET FUNCTIONAL CLASSIFICATIONS



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Operational analysis and intersection level of service (LOS) Summary. Deprioritize LOS as a management consideration for City streets, and shift focus to methods of analysis that better measure a project's transportation-related environmental impacts, such as use of Vehicle Miles Traveled (VMT). Decades of research and experience show that projects that attempt to relieve congestion and improve LOS simply attract more traffic and are ultimately unsuccessful at relieving congestion. Furthermore, congestion is often desirable from a safety standpoint, as it results in slower traffic speeds. To that end, the City will use LOS to reduce speeds and encourage mode shift. Appendix A of this Element describes existing and projected traffic volumes and LOS for key City intersections. Although several unsignalized locations are projected to operate at LOS C or better, locations which experience higher volumes such as US 101/Sunset Avenue interchange, Alliance Road at Foster Avenue, Alliance Road at "M" Street/15th Street, and locations on 14th Street at "G" and "H" Street couplets are projected to operate at LOS D, E or F. Improvements anticipated by this plan (see Figure T-k) are expected to improve the LOS to acceptable levels for all intersections while balancing the priorities of active transportation goals. See appendix A for the complete analysis.

Existing and Projected Traffic Volumes and Intersection Levels of Service. Existing and projected future average weekday traffic volumes are shown for selected street locations in Figure T-c. Existing volumes were established with traffic counts conducted in 1996. Projected traffic volumes were developed using the Arcata Citywide Traffic Model. The model was created to evaluate three potential year 2020 growth or "buildout" scenarios for Arcata. The traffic volumes reported in Figure T-c represent the buildout scenario associated with the land use plan. Table T-1 provides levels of service definitions for intersections. Existing and projected future volume/capacity ratios and afternoon peak hour levels of service for key intersections are shown in Table T-2. The locations of key intersections analyzed are shown in Figure T-b.

Traffic Volumes. Arterial streets with the highest daily traffic volumes are Samoa Boulevard, Alliance Road, Spear Avenue, "K" Street, and 11th Street. Collector and local streets carry considerably less traffic than arterial streets. The highest projected traffic volume on a surface street is on Samoa Boulevard west of State Route 101, with an average daily volume of over 17,000 vehicles. The largest percent increases in daily traffic volumes are on Alliance Road, "K" Street, and 11th Street east of "K" Street. Projected future traffic volumes on State Route 101 range from nearly 41,000 vehicles per day north of Arcata to 43,000 vehicles per day south of the City. Traffic volumes on Highway 299 east of Arcata are projected to increase from about 10,000 vehicles per day to over 16,000. Traffic volumes on these freeways reflect continuing growth in areas outside Arcata that will result in increased through traffic, particularly McKinleyville to the north along State Route 101.

Intersection Levels of service. Level of service (LOS) is a qualitative and quantitative description of intersection operations defined in terms of a letter grade and average either the volume to capacity ratio or total stopped delay per vehicle during the peak hour. Levels of

service range from LOS “A”, representing free flow conditions to LOS “F” which signifies excessive delays, long vehicle queues, and generally unacceptable conditions. The level of service criteria, defined in the most recent version of the 1995 Highway Capacity Manual prepared by the Transportation Research Board, are used by local agencies nationwide to establish standards of acceptability for traffic operations. What is considered acceptable may vary from one jurisdiction to another.

Level of service for a signalized intersection is defined by its volume to capacity ratio. A ratio of 1.00 indicates that the intersection’s volume equals its capacity. At unsignalized intersections, the total stopped delay is applied only to vehicles required to stop.

TABLE T-1 LEVEL OF SERVICE DEFINITIONS FOR INTERSECTIONS

LEVEL OF SERVICE	SIGNALIZED INTERSECTIONS	UNSIGNALIZED INTERSECTIONS
A	Uncongested operations; all queues clear in a single cycle. Volume to capacity ratio of 0.00 to 0.60.	No delay for stop-controlled approaches. Delay equals 0 to 5 seconds.
B	Uncongested operations; all queues clear in a single cycle. Volume to capacity ratio of 0.61 to 0.70.	Minor delay for stop-controlled streets. Delay equals 5.1 to 10 seconds.
C	Light congestion; occasional backups on critical approaches. Volume to capacity ratio of 0.71 to 0.80.	Moderate delay for stop-controlled approaches. Delay equals 10.1 to 20 seconds.
D	Significant congestion of critical approaches but intersection remains functional. Some vehicles required to wait through more than one cycle during brief periods. No long queues formed. Volume to capacity ratio of 0.81 to 0.90.	Long delay for stop-controlled streets. Delay equals 20.1 to 30 seconds.
E	Severe congestion with long standing queues on critical approaches. Blockage of intersection may occur if intersection does not provide protected left turns. Queues may extend into adjacent intersections. Volume to capacity ratio of 0.91 to 0.99.	Very long delays for stop-controlled intersections, reaching level of tolerance for average driver. Delay equals 30.1 to 45 seconds.
F	Total breakdown; stop and go operations. Volume to capacity ratio of 1.00 or greater.	Extreme congestion, intolerable delay for stop controlled vehicles. Delay equals 45.1 seconds or greater.

Source: 1994 Highway Capacity Manual (Special Report 209) & Circular 212, Transportation Research Board. Delay for unsignalized intersections is based on average stopped delay in seconds per vehicle.

Table T-2 identifies the existing and projected service levels at the two signalized and twenty-one unsignalized key intersections within the City. Projected service levels are based on the estimated buildout of the land use plan by the year 2020.

TABLE T-2 AFTERNOON PEAK HOUR INTERSECTION SERVICE LEVELS

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Th	EXISTING		PROJECTED	
	V/C RATIO	LOS	V/C	LOS
SIGNALIZED INTERSECTIONS¹				
23) G Street / Samoa Boulevard	<1	A	<1	A
22) H Street / Samoa Boulevard	<1	A	<1	A
Unsignalized Intersections¹				
	Delay	LOS	Delay	LOS
1A) Giuntoli / 101 SB Ramps	8	B	>45	F
1B) Giuntoli / 101 NB Ramps	3	A	>45	F
2) Alliance Road / Spear Ave.	5	A	6	B
3) Alliance Road / 17th Street	1	A	14	C
4) Bayside Road / Crescent Way	4	A	7	B
5) Alliance Road / Foster Ave.	1	A	11	C
6) Sunset Ave. / "G" & "H" Streets	4	A	9	B
7) Alliance Road / 14th Street	1	A	4	A
8) Janes Road / 11th Street	2	A	5	A
9) K Street / 11th Street	6	B	10	B
10) K Street / 7th Street	1	A	9	B
11) K Street / Samoa Boulevard	>45	F	>45	F
12) H Street / 11th Street	7	B	15	C
13) G Street / 11th Street	10	C	23	D
14) Union Street / 14th Street	2	A	5	A
15) H Street / 14th Street	7	B	12	C
16) G Street / 14th Street	12	C	16	C
17) H Street / 7th Street	4	A	7	B
18) G Street / 7th Street	4	A	11	C
19) Union Street / 11th Street	2	A	3	A
20) Union Street / Samoa Boulevard	4	A	>45	F
21) Union Street / 7th Street	3	A	4	A
24) L.K. Wood Blvd. / 14th Street	5	A	5	B
25) Sunset Ave. / US 101 NB Ramp	2	A	10	B
26) Sunset Ave. / L.K. Wood Blvd.	8	B	11	C

¹ See Figure T-1b for intersection locations. V/C Ratio = volume to capacity ratio.
 < less than, > greater than.

All of the intersections analyzed presently operate at LOS C or better with an average delay of twelve seconds or less per vehicle in the afternoon peak hour. The one exception is the intersection of "K" Street with Samoa Boulevard, which operates at LOS F for the southbound to eastbound left turn. This movement must wait for gaps in both directions of traffic on the four-

lane segment of Samoa Boulevard.

Table T-2 also presents the projected afternoon peak hour intersection levels of service for the buildout scenario. At buildout, the two signalized intersections ("G" and "H" Streets at Samoa Boulevard) continue to operate well under capacity at LOS A. Two of the City's unsignalized intersections are projected to operate at LOS F. These are:

1. ~~Giuntoli Lane/US 101 Southbound Ramps~~ failed service level is for left turn movement from the southbound off-ramp approach onto Giuntoli Lane. This intersection meets the Manual of Uniform Traffic Control Devices' (MUTCD's) peak hour volume warrant for signalization. This intersection should be monitored for possible installation of a traffic signal, all-way stop control, or traffic roundabout.
2. ~~Giuntoli Lane/US 101 Northbound Ramps~~ failed service level is for left turn movement from the northbound off-ramp approach onto Giuntoli Lane. This intersection meets Manual of Uniform Traffic Control Devices (MUTCD's) peak hour volume warrant for signalization. This intersection should be monitored for possible installation of a traffic signal, all-way stop control, or traffic roundabout.

Other intersections which operate near or over capacity include the unsignalized intersection of "K" Street/Samoa Boulevard. This intersection currently operates at LOS F for the southbound left turn onto Samoa Boulevard, and continues to operate poorly for this movement with buildout under the land use plan. All of the remaining unsignalized intersections are projected to operate at LOS D or better. Most intersections, however, operate at LOS A or LOS B. Existing and projected peak hour service levels are based on existing intersection control and lane configurations as of 1998. Improvements anticipated by this plan will likely improve the LOS to acceptable levels for all intersections.

Existing and Projected Transit Ridership. The Arcata & Mad River Transit System (A&MRTS) is a service provider of the Humboldt Transit Authority (HTA) that offers public bus service for the City and is operated by their Public Works Department. and Humboldt Transit Authority are the two transit systems providing service in the City of Arcata. Fixed service routes include the Red, Gold and Orange Route(s) running along major streets in the city to destinations including City Hall, Uniontown Shopping Center, and Mad River Hospital and Transit services are offered along major streets in the city and to major inter-route transfer points including the Arcata Transit Center and Cal Poly Humboldt State University (HSU). The Willow Creek route offers travel between the communities of Arcata and Willow Creek, located east of Arcata. HTA also provides regional public transportation for Humboldt County including service through Arcata via the Redwood Transit System (RTS), a fixed route system serving cities along the Highway 101 corridor from Trinidad to Scotia. The RTS has four stops in Arcata including Cal Poly Humboldt and the Arcata Transit Center. The Arcata Transit Center, located on "F" Street between 9th and 10th Streets, provides a centralized transit facility for buses operated by A&MRTS, RTS, Greyhound, and Amtrak. The Transit Center provides a park-and-ride lot and secure bicycle facilities. HSU student ridership is significant during the school season when extra shuttles are provided to accommodate overflows in the morning peak hour. Table T-3

~~shows existing and projected A&MRTS bus ridership.~~

~~During the school season, A&MRTS ridership increases significantly, by more than 150%. As of 1998, HSU subsidized student bus fares by \$0.60 resulting in increased ridership and reduced vehicle travel to the university. The subsidy is funded through parking fines at the University. A 1995 survey indicated that 75% of A&MRTS riders are traveling to and from school, 12% for work, and the remaining 14% for various purposes including shopping, recreational, and personal trips.~~

Cal Poly Humboldt student ridership is significant during the school season. Cal Poly Humboldt provides unlimited free ride access on several HTA routes, including A&MRTS, through the Jack Pass program. The Jack Pass program aims to encourage mass transit and reduced travel via single-occupant vehicles. Staff, faculty, and Extended Education participants are also granted unlimited rides on these HTA routes for \$60 a semester. A&MRTS ridership over the past several years is included in Figure T-e of Circulation Element Appendix A. Figure T-f presents the existing transit routes and stops.

TABLE T-3 A&MRTS RIDERSHIP SUMMARY: EXISTING AND PROJECTED

PASSENGER TYPE	AVERAGE SUMMER MONTHLY RIDERSHIP	AVERAGE SUMMER DAILY RIDERSHIP	AVERAGE SCHOOL SEASON-MONTHLY RIDERSHIP	AVERAGE SCHOOL DAILY RIDERSHIP
All Passengers	5,900	300	14,900	850
Students			11,000	750
DAILY STUDENT RIDERSHIP BY PERIOD				STUDENT RIDERS DURING PERIOD
7 to 11 AM				400
11 to 3 PM				250
3 to 7 PM				200
PM Peak Hour—All Passengers				70
Source: A&MRTS superintendent Larry Pardi.				
Note: A&MRTS provides extra shuttles to HSU during school season to accommodate overflow in the morning peak, extra shuttles are not required in the afternoon peak, but buses have standing room only.				
PROJECTED A&MRTS INCREASE IN RIDERS (AT 2020 LAND USE PLAN BUILDOUT)				
	1% Increase in Riders		3% Increase in Riders	5% Increase in Riders
Passengers	62		183	304
Note: Increase in daily ridership based on projected land use in traffic analysis zone's [TAZ's] within 1/4 mile of transit corridor stops and 1990 census mode split within each TAZ.				

~~In 1997, fares represented 20% of the A&MRTS capital and operating costs. The balance of the costs are funded through Transportation Development Act (TDA) funds (70%), State Transit Assistance (STA) funds (5%), and Federal Transit Administration (FTA) Section 18 funds (5%).~~

The two A&MRTS fixed routes serve most of the City, and most points are within 1/3 mile of a bus stop. As of 1998, the system runs on weekdays from 7:00 AM to 7:00 PM with sixty minute headways. Saturday service is from 9:00 a.m. to 5:00 p.m. with 120 minute headways. In addition to fixed routes, A&MRTS provides "demand responsive" dial a ride service. This service accommodates about fifteen to twenty passengers per day. The majority of these passengers are elderly or disabled with destinations to the Mad River Adult Day Health Center. The Humboldt Transit Authority provides regional public transportation through the Redwood Transit System (RTS). This fixed route system serves cities along the Highway 101 corridor from Trinidad to Scotia. The RTS has four stops in Arcata including Humboldt State University and the Arcata Transit Center. The City of Arcata contributes to the funding for RTS. The Arcata Transit Center, located on "F" Street between 9th and 10th Streets, provides a centralized transit facility for buses operated by A&MRTS, RTS, Greyhound, and Amtrak. The Transit Center provides a park and ride lot and secure bicycle facilities.

The A&MRTS 1995 Transit Development Plan projected a 16% increase in ridership between 1995 and the year 2000 based on an equal projected increase in population. The plan recommends service improvements comprised of redesigning the present "Red Route" creating a more direct and faster route with consistent thirty minute headways between downtown and Humboldt State University, and thirty minute headways between downtown and Sunny Brae. If additional resources become available, the plan recommends providing evening service on the Red Route during the school season and providing a third bus on the Red Route during the school season to achieve thirty minute headways.

Table T-3 also presents year 2020 projected increases in average school daily riders. This information is derived from growth in population and employment in Traffic Analysis Zones (TAZs) within transit corridors and 1/4 mile from bus stops, and a 1%, 3% and 5% increase in riders over 1990 census mode split information. While relatively small percentages, the increases represent a large increase in riders for Arcata. A sensitivity analysis indicates that these increases in riders could reduce vehicle trips between 0.5% and 2.3% and reduce annual vehicle miles of travel between 0.2% and 1.1%.

FIGURE T - f Existing Transit Routes

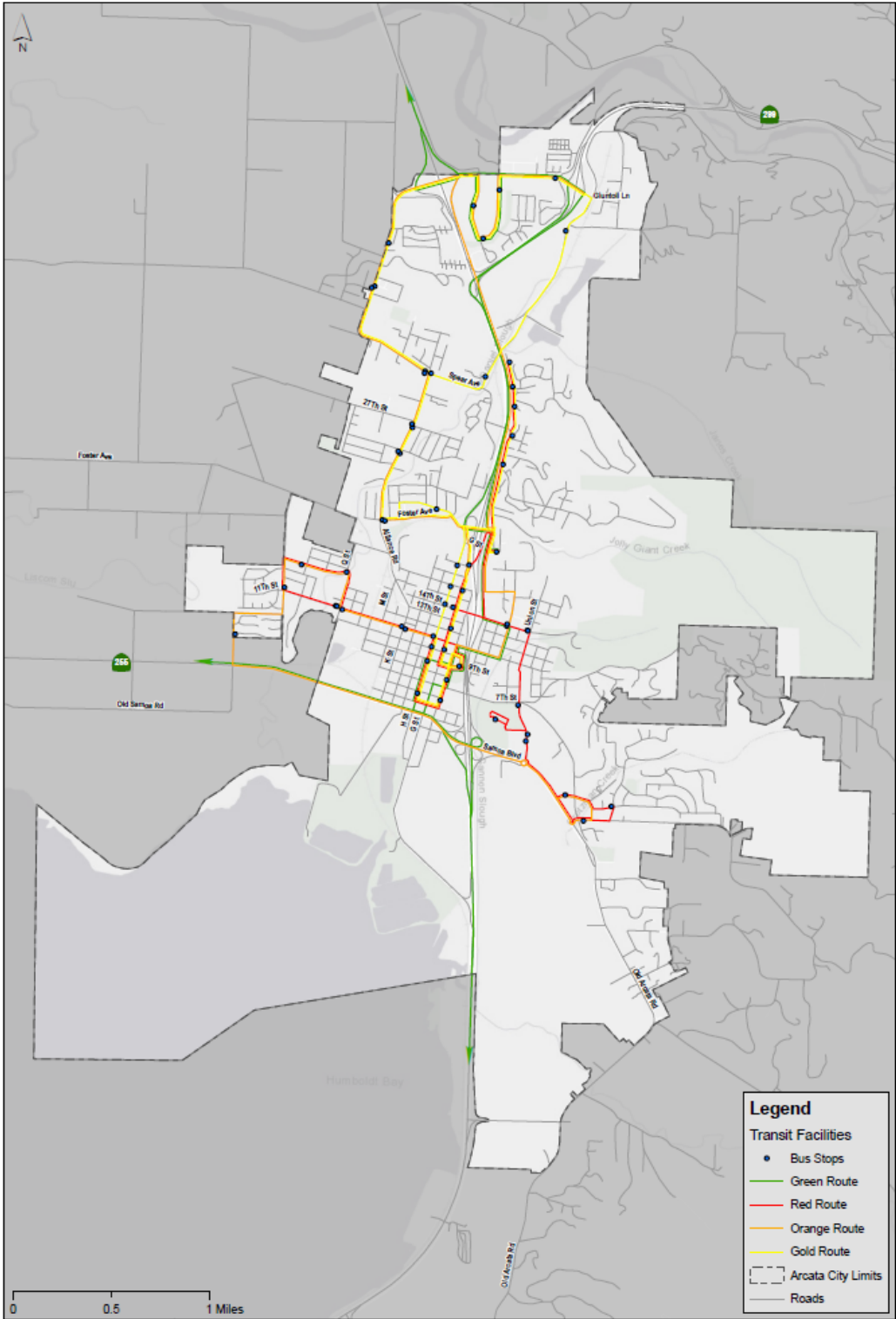


Figure T-e

City of Arcata General Plan:
Existing Transit Routes

Created by GRID
Map Date: 9/2022



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

The Humboldt County ~~2017-2022~~ Transit Development Plan (TDP) ~~was prepared for the Humboldt County Association of Governments (HCAOG) to help provide~~ guidance to local agencies on service programs, capital improvements and financial strategies to improve the public transit services in Humboldt County over a five-year period. Recommended alternatives in the TDP include the following and the City shall make an effort to follow the recommendations in updated TDPs as they become available. :

- Adjust Schedule to Better Match University Class Schedules / Increase Trip Choices – The TDP identified adjustments to transit schedules that allowed more time for students to get to class from campus stops would encourage more transit use.
- Make the Community Center and “On Demand” Stop – Low ridership at the Community Center stop was identified and recommended for “on demand” service. Procedures include passengers telling operators at boarding to be dropped off and to call a service helpline in advance for pick-up.
- Extend Transit Service to South G Street – Higher density housing and commercial activities are identified south of Samoa Boulevard on H and G Streets and potentially capture additional ridership for the Red Route.
- A&MRTS Services Recommended Contingent on Funding: Provide a High Frequency Shuttle between Cal Poly Humboldt and Downtown in Peak Periods. The TDP also recommended considering new shuttle service during peak periods so that students and university staff would be better served as highest transit demand was noted between Downtown Arcata and Cal Poly Humboldt.

Existing Bicycle and Pedestrian Facilities. Arcata’s bicycle transportation system consists of Class I off-street shared use paths, Class II bike lanes, and Class III bike routes, and bicycle boulevards on public streets. Class I facilities are multi-use paths that provide a completely separated right-of-way for the exclusive use of bicycles and pedestrians with cross flows of motorized traffic minimized. Class II bike lanes provide a striped and signed lane for one-way bicycle travel on a street or highway within the paved area of a roadway. are on street facilities delineated from motor vehicle travel lanes by pavement striping and markings. Class III bike routes are specially designated corridors in which the travel lanes are shared by motor vehicles and bicycles. Class III bike routes are specially designated corridors in which the travel lanes are shared by motor vehicles and bicycles and are usually marked with on-street pavement stencils. Research has shown that Class III bike routes do not provide adequate safety or comfort for bicyclists unless significant additional design features are included. Bicycle boulevards are a type of Class III facility on low-volume roadways which prioritize the use of bicycles with traffic controls, signage, roadway markings, and traffic calming measures, including bicyclists having the right-of-way. Class IV bike lanes are protected from traffic by a vertical barrier. Arcata does not currently have any Class IV bike lanes, but research has shown that most people will not bike on busy streets without them.

Arcata presently provides a comprehensive bike bikeway network lane system connecting most major areas of the City on primary arterial streets but many of the current facilities do not provide adequate protection for the comfort and safety of bicyclists. The primary Class I shared use path along the L Street rail alignment provides a north-south connection from the southern

City limits and to the Humboldt Bay Trail south to Eureka, connecting to Alliance Road north of the Gateway area, and connects to Foster Avenue at Sunset Avenue. Additional Class I facilities provide brief connections between existing roadways and on-street bicycle facilities. Most Class II bike lanes are located on north-south streets, while Class III bike routes provide east-west connection on key streets. The western portion of the City (west of Alliance Road) is least served by bike lanes, providing an opportunity to expand the bike lane system to encompass more residential areas. Figure T-h presents the existing bicycle and trail facilities. The City of Arcata adopted a Pedestrian and Bicycle Master Plan, last updated in 2010 that identifies pedestrian and bicycle conditions and various proposed improvements.

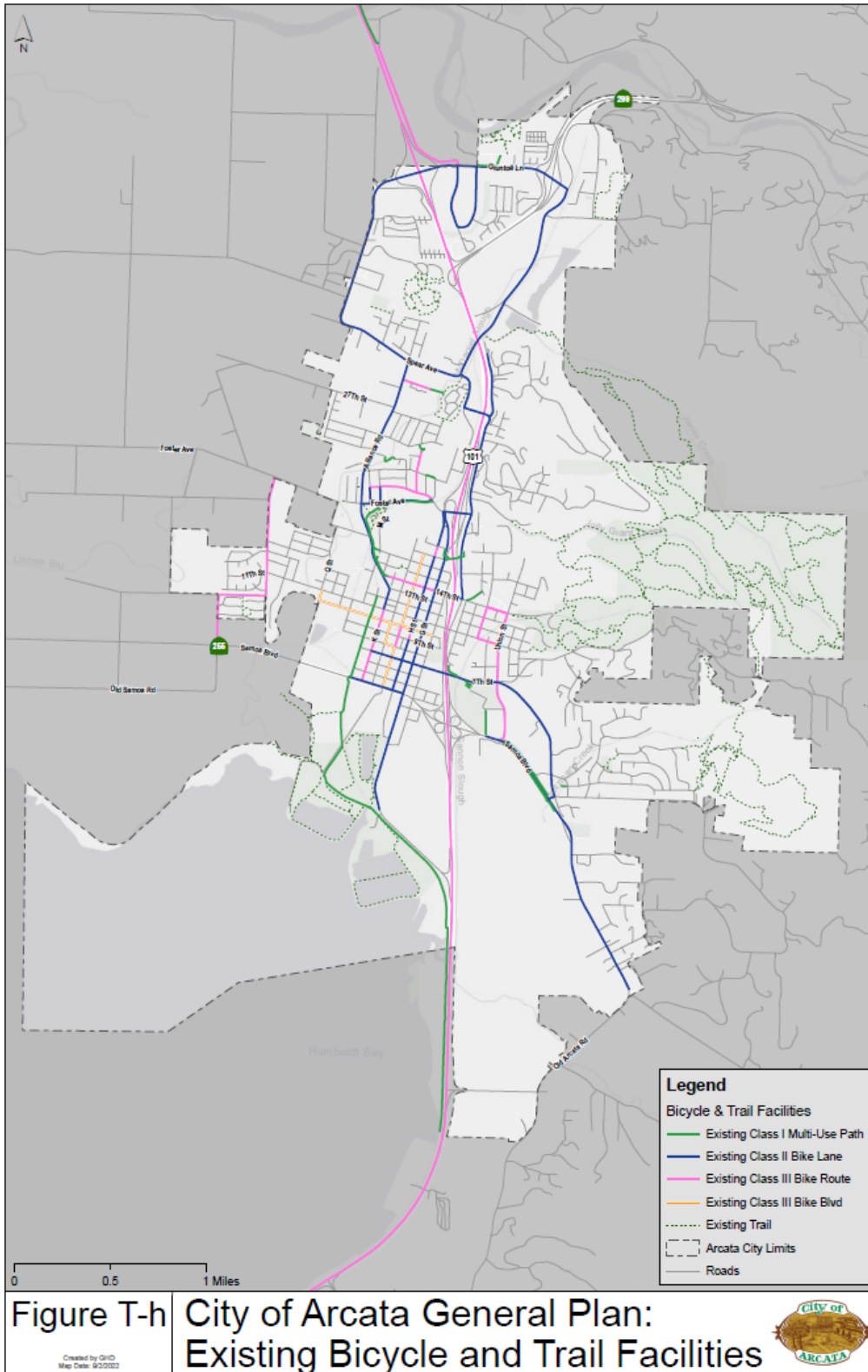
Regional trail needs are assessed when HCAOG updates the Regional Bike Plan and the Regional Transportation Plan (RPT). The 2018 Regional Bike Plan identifies the following proposed bikeways for short-term regional priority projects (not yet completed):

- 11th Street Corridor (Janes Road to Bayview Street) – Class II / Class III
- F Street (7th Street to 14th Street) – Class I / II
- Sunset Avenue east (L.K. Wood Boulevard to Jay Street) – Class I

Improvements since 2020 General Plan. Below is a list of bikeway and trail improvements which have been implemented since the last update of the General Plan and since the 2010 Pedestrian and Bicycle Master Plan:

- Class I Shared-Use Paths/Trails:
 - Humboldt Bay Trail – Arcata Segment, Arcata Skate Park to Bracut Marsh
- Class II Bike Lanes:
 - Foster Avenue Extension (east) – from Alliance Road to Sunset Avenue (also includes adjacent Class I trail)
 - G and H Streets
 - Old Arcata Road – Hyland Street south city limit
 - Samoa Boulevard – Union Street to Crescent Way
- Class III Bicycle Boulevards:
 - Q Street – 11th to 10th
- Class III Bicycle Routes:
 - 11th Street – B Street to Union Street
 - 14th Street – K Street to L.K. Wood Boulevard
 - Baldwin Street – Cahill Park to Sunset Avenue
 - Union Street – Samoa Boulevard to 14th Street
 - Westside Corridor (includes Janes Road, Vaissade Road, V St.) from Foster Avenue to Samoa Boulevard

FIGURE T - hf Existing Bicycle and Trail Facilities



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Pedestrian facilities are provided throughout Arcata in the form of sidewalks on public streets and along Class I shared use paths which also accommodate bicyclists. Refer to The City's design standards s for ~~streets includes five-foot wide sidewalk widths on both sides of the street with a fifty-foot wide and~~ right of way. Many streets, particularly local, collector, and rural roads, do not have curb and gutters or sidewalks – forcing pedestrians to walk on unpaved shoulders or within the travel lanes. While the downtown and areas surrounding Cal Poly Humboldt State University provide a continuous sidewalk system in other areas of the City, there are many gaps in the sidewalk system. The City's standard ~~five-foot wide~~ sidewalk meets the minimum Americans with Disabilities Act (ADA) requirements, but wider sidewalks are desirable for high-traffic pedestrian locations and to encourage walking. Narrow sidewalks are often obstructed with utility poles, signs, and street furniture, further reducing their effectiveness. In addition, the City's street standards lack sufficient width for a planting strip or street trees, which are important elements in promoting walking as an alternative mode of transportation. Opportunities exist, however, within the standard fifty-foot wide right of way to provide street trees in planter boxes located in the parking lane, or to add a planting strip between the sidewalk and travel lanes when new development projects are considered.

Existing Freight ~~and Railroad~~ Transportation Systems. Arcata has designated truck routes on several key arterial and collector streets including Giuntoli Lane, Valley West/Valley East Boulevard, West End Road, Alliance Road, "K" Street, Spear Avenue, L. K. Wood Boulevard, 11th Street, Fickle Hill Road, Union Street, Old Arcata Road, Vaissade Road, Heindon Road, South G Street, Janes Road, and Samoa Boulevard. These streets provide intracity connections for freight travel and serve most of the industrial areas of the City. All state facilities including Routes 101, 299, and 255 are designated truck routes. US Highway 101 is considered an STAA Terminal Access Route within Humboldt County, apart from Richardson Grove at the southern border with Mendocino County where access is limited as a California Legal Truck Route. SR 299 is considered an STAA Terminal Access Route between US Highway 101 in Arcata and Interstate 5 in Redding, and SR 255 is considered a California Legal Truck Route between Eureka and Arcata.

Railroad Right of Way Transportation Systems. Arcata has railroad ~~mainline right-of-way formerly~~ managed by the North Coast Railroad Authority (NCRA), with spurs into several industrial properties. Although most rail service was suspended following damage to tracks caused by storms in 1997, the ~~mainline main-line~~ and many spurs in Arcata were active prior to that time. They served several industrial uses in the northeast and southwest areas of the City and were used to move freight between Arcata and Eureka. The Great Redwood Trail Agency, established in March 2022, took over railroad corridor management from NCRA. The Great Redwood Trail is a proposed multi-use rail-to-trail project connecting San Francisco to the Humboldt Bay area. ~~Service consisted of one round trip at night between the hours of 7:30 p.m. and 7:30 a.m. The North Coast Railroad Authority has permitted passenger service between Arcata and Eureka on certain holidays each year as special event excursions. There has been discussion about initiating regular passenger rail service between Arcata and Eureka, but no plans have been developed.~~

Several rail corridors in Arcata have already been converted into Class I trails with others planned. The Arcata Rails with Trails Project was completed connecting Foster Avenue and Alliance Road south to SR 255 along the L Street rail alignment (Phase 1 of the Humboldt Bay Trail).

FIGURE T - i Existing Truck Routes & Rail Right of Way

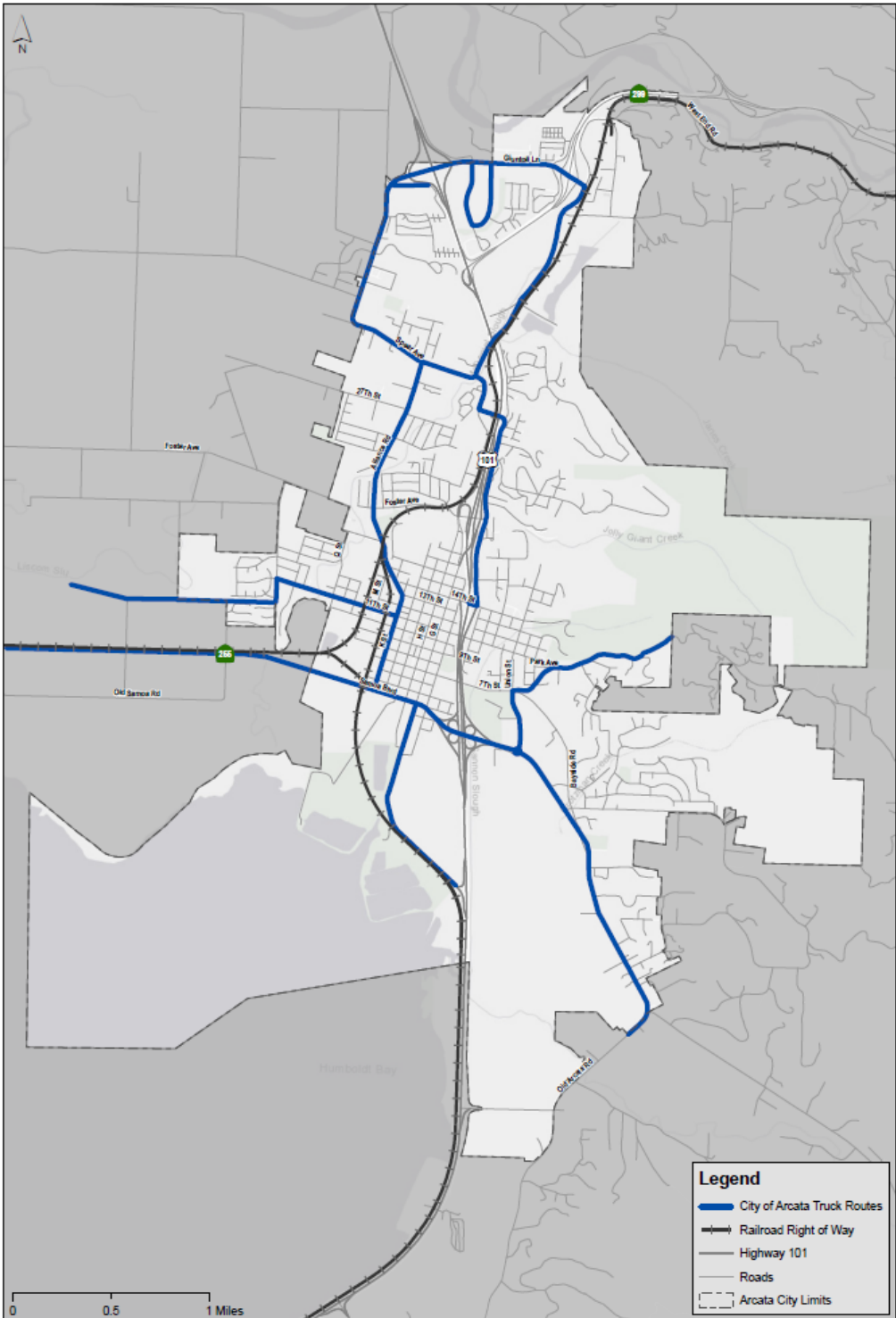


Figure T-j City of Arcata General Plan:
Existing Truck Routes and Rail

Created by GHG
Map Date: 6/20/23

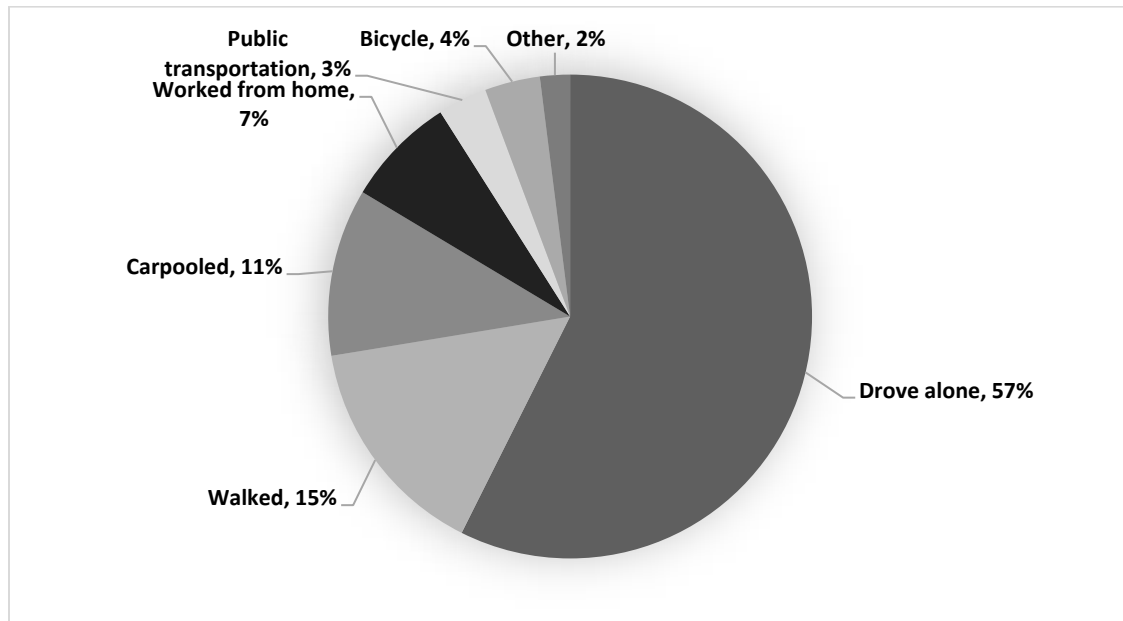


Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

The rail to trail corridor south of SR 255 at this location to US 101/Bracut has also been constructed (City of Arcata Rail with Trail Connectivity Project) and is part of the Humboldt Bay Trail connecting to Eureka. The Annie and Mary Rail Trail and Trail Connectivity Project are planned trails that will connect Sunset Avenue to the Aldergrove Industrial Park along West End Road in Arcata, and then continue east to the City of Blue Lake along the inactive rail corridor.

Existing Modes of Travel (note: the City is working with HCAOG to explore if this data can be further parsed to show travel within and outside of City limits per TSC request). Based on ~~1990~~ 2020 census data, the majority of Arcata residents drive alone to work (~~66~~57%) as shown in the accompanying figure. Walking and bicycling modes make up ~~15~~12% and 4% respectively. About ~~7~~5% of Arcatans work at home and 3% commute via public transportation. ~~Public transit is the least utilized mode of travel at 1%.~~ While low on a citywide basis, public transit usage is higher in some areas of the City when examined at the census block level using Replica. ~~Transit mode of travel in the downtown area, for example, is about 8%. Similarly, walking and bicycling modes are high in certain areas, up to 16%. Travel modes for people who work in Arcata are similar to those of residents, with the exception that more employees drive alone (71%) and carpool (10%). Compared to residents, fewer employees walk (11%) and bike (3%), while the same amount (1%) use public transit. In the downtown area, the split for walking increases to up to 37%.~~

Figure T-ij Existing Modal Split



Source: US Census, 2020 ACS 5-Year Estimates.

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Existing Travel Demand Management. The most comprehensive use of Transportation Demand Management (TDM) measures is by the City's largest employer, Cal Poly Humboldt, HSU, which has the following programs offered by the University:

- ~~1. HSU subsidizes free travel on A&MRTS buses for students, faculty, and staff. This subsidy covers \$3.00 for every \$11.00 spent for transit service.~~
 - ~~2. The University's "Ease the Crunch" campaign offers information to students explaining the proximity of the campus to residential areas and the convenience of transit use. The information includes transit routes and subsidy programs available.~~
 - ~~3. HSU provides approximately 2000 bike racks on campus to ensure safety of bicycles.~~
 - ~~4. HSU purchased bike racks for buses, enabling members of the university community to combine bus and bicycle commutes to and from the campus.~~
 - ~~5. HSU provides the fee for bicycle licensing.~~
- Jack Pass – utilizes student ID cards and reduced rates for staff and faculty to ride local bus system.
 - Zipcar – car-sharing program offered to students as alternative to car ownership or rental with two cars on campus.
 - Humboldt Bikeshare – bike-sharing program with stations on campus and in and around Downtown Arcata.
 - Bicycle Learning Center -campus bike shop run by students offering parts, tools and instruction on bike repair, maintenance, and safety.
 - Carpool Preferential Parking - allows commuters by car with additional passengers between designated hours (7am and 11am) to receive permission to park in preferred locations for the day.
 - Homeward Bound Bus Charter – Program offered during school year that provides students discounted round-trip fare for travel between Arcata and San Francisco or Los Angeles. (Note program offered for free during 2022 school year supported by funding to meet students with basic needs, subject to funding availability in the future.)



Proposed Circulation Network

Arterial, collector, and local roads will provide access to new and established residential, commercial, and industrial areas, connecting those areas with the existing local and regional transportation system. Buildout of the General Plan land uses to year 2045 will increase multimodal, access and parking demands and will result in areas already under stress to exceed acceptable limits for safety, and delay. As presented in Appendix A Table T-3, forecasted traffic operations at several intersections are projected to degrade to LOS D, E, or F.

In order to accommodate the existing and planned land uses

within the City, a robust network of multimodal safety capacity improvements will be needed. Based on buildout of the General Plan land uses and forecasted traffic operations, Several improvements are planned for most of the intersections projected to operate deficiently, mainly installation of roundabouts. At the US 101/Sunset Avenue interchange, the City is currently undergoing the Project Approval and Environmental Document (PA&ED) phase of the interchange improvement, which proposes to install two roundabouts at the interchange including pedestrian and bicycle facilities.

Additionally, implementation of the mobility improvements within the Gateway Area Plan, including the “K” and “L” Streets couplets, and the 8th and 9th Street couplets extension, will alleviate traffic congestion within the Gateway and will ensure all transportation modes remain comfortable, convenient, safe, and attractive to residents, workers, students, and visitors, with an emphasis on mode shift away from single-occupancy vehicles. The City should fully investigate and publicly assess detailed alternatives to provide access to the west side of the Gateway Area.

the following Table T-5 presents the proposed circulation improvements have been identified to meet City goals. Figure T-k presents the proposed Vehicular Circulation Plan on the following page.

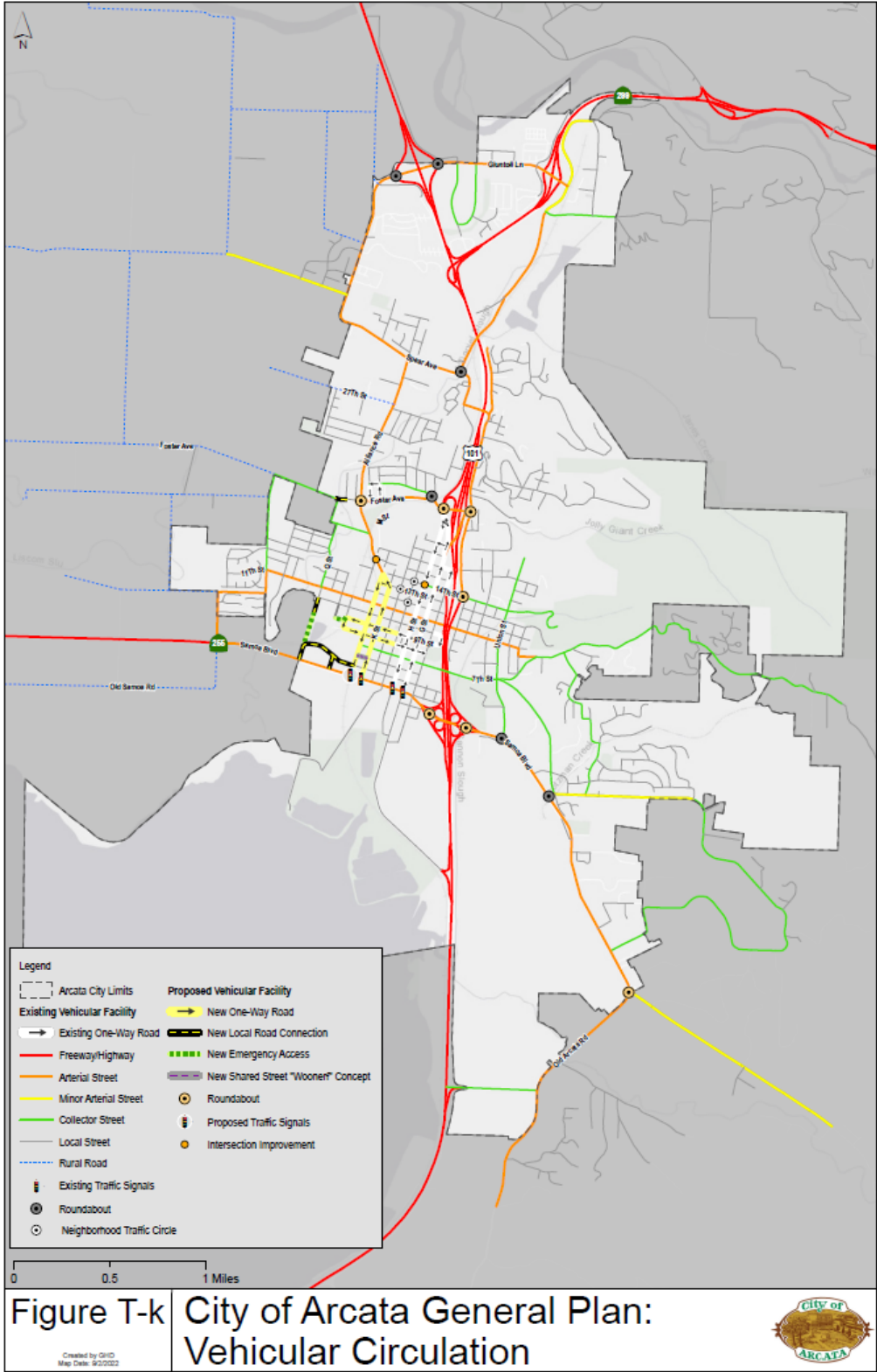
Table T-5 Proposed Vehicular Circulation Improvements

<u>Location</u>	<u>Improvement</u>	<u>Notes</u>
<u>Sunset Avenue Interchange</u>	<u>Dual Roundabouts at both ramp termini. Easternmost roundabout will be 5-legged combined with ramps and L.K. Wood Boulevard. Class I path on south side of overpass.</u>	<u>Traffic operation improvements (LOS deficiency). Safety improvements for all modes, with bicycle and pedestrian safety addressed through design phase.</u>
<u>Samoa Boulevard (SR 255) at US 101 Interchange</u>	<u>Full Interchange redesign with two roundabouts via “diamond” ramp configuration.</u>	<u>Improved pedestrian and bicycle connections across US 101 overpass. Improve interchange access.</u>
<u>14th Street at L.K. Wood Boulevard</u>	<u>Roundabout</u>	<u>Large intersection, will provide safer access for all modes. Entry feature for campus and City at US 101.</u>
<u>14th Street at H Street</u>	<u>Restripe southbound left lane to be dedicated left turn lane. Also provide improved bicycle access.</u>	<u>Traffic operation improvements (LOS deficiency).</u>
<u>Alliance Road at M Street/15th Street</u>	<u>Intersection improvements including channelization.</u>	<u>Traffic operation improvements (LOS deficiency). Ensure bicycles</u>

<u>Location</u>	<u>Improvement</u>	<u>Notes</u>
		<u>and pedestrians are a priority in design.</u>
<u>Alliance Road at Foster Avenue</u>	<u>Roundabout (or mini-roundabout)</u>	<u>Traffic operation improvements (LOS deficiency).</u>
<u>Foster Avenue Extension</u> <u>Connection</u>	<u>Extend roadway west across McDaniel Slough</u> <u>Connect Foster Avenue west of Alliance</u>	<u>Circulation improvement. Traffic will be diverted from 17th Street and some from M Street at Alliance Road.</u>
<u>Giuntoli Lane / SR 299 Interchange</u>	<u>Dual Roundabouts at both ramp termini</u>	<u>Traffic operation improvements (LOS deficiency). Safety improvements for all modes.</u>
<u>Giuntoli Lane at Valley West Boulevard</u>	<u>Roundabout</u>	<u>Traffic operation improvements (LOS deficiency). Safety improvements for all modes.</u>
<u>Gateway Area Plan Improvements</u>	<u>Improvement</u>	<u>Notes</u>
<u>K Street & L Street One-Way Couplets</u>	<u>Redesign “K” and “L” Streets to be one-way couplets south of 14th Street. Traffic Signal coordination at Samoa Boulevard.</u> <u>Class I Bikeway along L Street retained as much as possible.</u> <u>Class IV protected bikeway or Class II Buffered Bikeway along K Street.</u>	<u>Improve traffic flow through the Gateway Area while providing safer pedestrian crossings at intersections, and enhanced access</u>
<u>Barrell District Roadways</u>	<u>New roadway connection through Barrell District.</u> <u>New emergency access roads along southern end of Q Street and between N Street and O Street west of 9th Street.</u>	
<u>8th Street and 9th Street One-Way Couplet</u>	<u>Extend existing couplets west to N Street.</u>	

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

FIGURE T - e-k GENERAL PLAN VEHICULAR CIRCULATION



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

As part of the Gateway Area Plan, the City is exploring mobility concepts and proposing circulation patterns to convert two-way streets into one-way couplets on K Street, L street, 8th Street and 9th Street (continuation west of I Street). This would allow new opportunities for implementing Active Transportation (bicycling and walking) elements as part of the Gateway Area Plan including the addition of Class IV separated bikeways. Class IV bikeways facilities are designed within the roadway using barriers such as bollards, raised medians, vehicle parking and other devices creating a physical separation between vehicle traffic and people riding bicycles. For example, K Street, between 13th Street and Samoa Boulevard, is characterized as a two-way street with one lane in each direction, parking on both sides and designated as a Class III bike route. Changing K Street to a one-way couplet maintains a travel lane and parking but would then allow the street to be upgraded with a Class IV facility through implementation of the Gateway Area Plan.

Additional Active Transportation ideas included in the Gateway Area Plan include the Shared Street, “Woonerf” concept proposed on 6th Street between K Street and L Street. Through this concept walking and bicycling are the primary modes emphasized and vehicle traffic is de-emphasized. Through traffic calming, lowered speed limits and enhanced streetscapes, Shared Streets allow more public space opportunities that prioritize people over vehicles. The potential to apply these concepts outside of the Gateway Area depends on context but the City should consider context-specific implementation via Policy.

Table T-6 presents the proposed bikeway improvements identified to meet City goals and are consistent with the Gateway Area Plan and the City of Arcata Pedestrian and Bicycle Master Plan (2010). This list may be superseded based on subsequent updates of the Pedestrian and Bicycle Master Plan or a similar planning document. Figure T-1 presents the proposed Active Transportation Circulation Plan on the following page.

Table T-6 Proposed Bikeway and Trail Improvements

<u>Bicycle Facility</u>	<u>Roadway/Name</u>	<u>Location</u>
<u>Class I Trail / Shared-Use Path</u>		
	<u>Annie & Mary Rail Trail</u>	<u>West End Road to Arcata Skate Park</u>
	<u>Hammond Trail</u>	<u>Arcata Bottoms to west of Foster Avenue Extension</u>
	<u>Sunset Avenue</u>	<u>L.K. Wood Blvd to Jay Street (south side of 101 overpass)</u>
	<u>Giuntoli Lane</u>	<u>West End Road to Janes Road</u>
	<u>Samoa Blvd.</u>	<u>Union to G street</u> <u>K Street to V Street (and eventually to Manila)</u>
	<u>10th and/or 11th Streets</u>	<u>Bayview to Janes Road</u>
	<u>Spear and St. Louis</u>	<u>Janes Road to L.K. Wood Blvd.</u>
	<u>Class I Paths in Gateway Area Plan:</u>	<u>South of Q Street alignment to Barrell District Roadway</u> <u>Along Barrell District Roadway</u> <u>Along rail line southwest of 9th Street</u> <u>Along N Street alignment from 9th St to Alliance Road</u>

<u>Bicycle Facility</u>	<u>Roadway/Name</u>	<u>Location</u>
		<u>Along L Street north along rail line to M Street then north to Alliance Road</u> <u>14th Street west of M Street to proposed trail along N Street alignment</u> <u>Pedestrian path along 12th Street west of M Street to proposed Class I path</u> <u>Pedestrian path south of O Street to proposed Class I path</u>
<u>Class II Bike Lanes</u>		
	<u>11th Street</u>	<u>Janes Road to B Street</u>
	<u>7th Street</u>	<u>Between L Street and K Street</u>
	<u>8th Street</u>	<u>N Street to J Street (Gateway Area Plan)</u>
	<u>9th Street</u>	<u>J Street to N Street (Gateway Area Plan)</u>
	<u>F Street</u>	<u>7th to 11th Streets</u>
	<u>Foster Avenue Extension (west)</u>	<u>West of Alliance Road to Foster Avenue</u>
	<u>K Street</u>	<u>Samoa Blvd to 11th Street (Gateway Area Plan)</u>
	<u>N Street</u>	<u>9th Street to 8th Street</u>
	<u>Sunset Avenue</u>	<u>Jay Street to G/H Streets</u>
<u>Class III Bicycle Boulevard</u>		
	<u>F Street</u>	<u>11th Street to 14th Street</u>
<u>Class III Bicycle Route (insert Class IV facilities. Per 05/09/23 PC Approval)</u>		
	<u>11th Street</u>	<u>Union Street to Bayview Road</u>
	<u>14th Street</u>	<u>L.K. Wood Blvd to B Street, then Union Street</u>
	<u>16th Street</u>	<u>M Street to G Street</u>
	<u>Alder Grove Road</u>	<u>West End Road to Ericson Way</u>
	<u>Bayview Street</u>	<u>13th Street to 11th Street</u>
	<u>Boyd Road</u>	<u>Giuntoli Ln to Sierra Way</u>
	<u>Buttermilk Lane</u>	<u>Samoa Blvd east to Arcata city limit</u>
	<u>D Street</u>	<u>11th Street to ped. trail south of 9th</u>
	<u>Ericson Way</u>	<u>West End Road to Aldergrove Road</u>
	<u>Foster Avenue</u>	<u>Janes Road to Alliance Road</u>
	<u>Q Street</u>	<u>17th Street to 11th Street</u>
	<u>Stromberg Ave/Maple Ln</u>	<u>Alliance Rd to Janes Creek Linear Trail</u>
	<u>South G Street</u>	<u>Arcata Corp. to Yard Highway 101</u>
	<u>Union Street</u>	<u>14th Street to 17th Street</u>
	<u>West End Road</u>	<u>Giuntoli Ln to Ericson Way</u>
	<u>Wyatt Lane</u>	<u>27th Street to Stewart Avenue</u>
<u>Shared Street</u>	<u>6th Street</u>	<u>Between L Street and K Street</u>

FIGURE T - ~~f-i~~ GENERAL PLAN ACTIVE TRANSPORTATION CIRCULATION

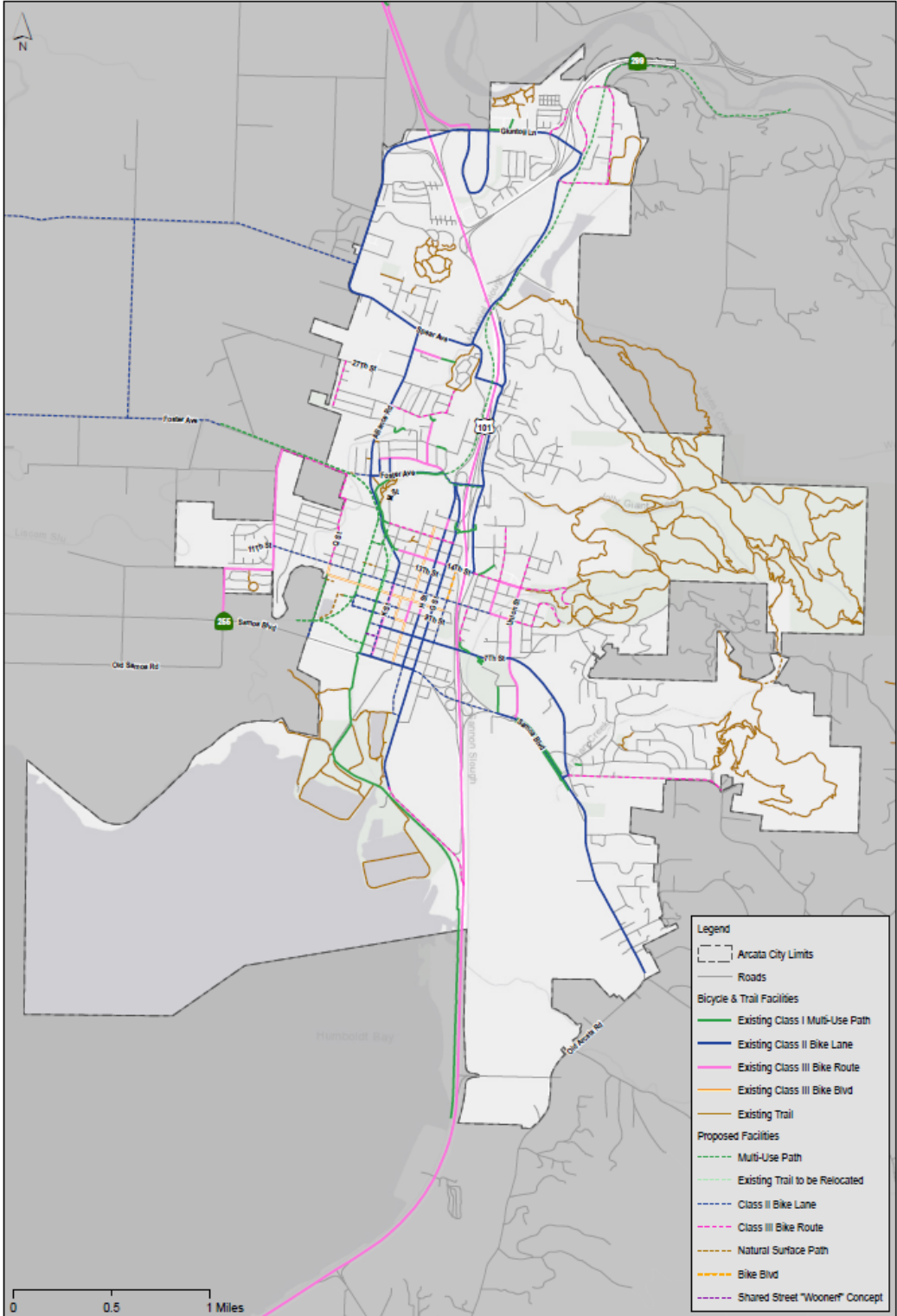


Figure T-i City of Arcata General Plan: Active Transportation Circulation

Created by GHD
Map Date: 9/2/2022



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Guiding Principles and Goals.

- A. Provide a connected multimodal transportation and mobility system ~~which allows that contributes directly to the~~ safety, health, economic vitality, and quality of life of all residents ~~people in Arcata, and efficient travel.~~
- B. Create a transportation system ~~which provides that incentivizes~~ a choice of travel modes and is safe, accessible, comfortable, accommodating, and welcoming to all users. Put safety first in all mobility planning, policies, and projects.
- C. Provide for increased use of active and shared transportation modes as alternatives to the single-occupant vehicle, including walking, rolling, bicycling, public transit, carpooling/vanpooling, and ridesharing.
- ~~C.~~D. Prioritize investment and transportation planning which shifts the City's transportation system from being car-centric to one in which transit and active transportation are competitive or superior to single-occupancy vehicles in terms of convenience, perceived and actual safety, and accessibility for all residents.
- ~~D.~~E. Manage the street and highway system to promote more efficient use of existing facilities ~~capacities~~ rather than increase the number of travel lanes or make other capacity enhancements.
- ~~E.~~F. Create a multimodal transportation system for people of all mobility levels ~~which that~~ will improve the livability of residential neighborhoods, including use of methods to calm or slow traffic and reduce through-traffic on local neighborhood streets.
- ~~F.~~G. Educate residents, employees, and students about the importance of using alternative forms of transportation and mobility instead of the single-occupant automobile.
- ~~G.~~H. Promote land use patterns that encourage walking, rolling, bicycling, and public transit use.
- I.~~H.~~ Establish a set of curb fee-based parking prices that are high enough to ~~maintain an adequate supply of available spaces~~ drive more active and shared transportation.
- J. The City recognizes that safe mobility is a right of all people in Arcata.

2.8 POLICIES

The ~~Transportation~~ Circulation Element includes the following policies:

- T-1 Balanced Transportation System with Choice of Modes
- T-2 Travel Demand Management
- T-3 Bus Transit System
- T-4 ~~Streets and Highways Plan~~ Circulation Maps and Context Sensitive Street Design
- T-5 Bicycle and Pedestrian Facilities
- T-6 Parking Supply and Parking Management
- T-7 ~~Rail and~~ Freight Transportation
- T-8 Financing Transportation Improvements

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

POLICY T-1 BALANCED TRANSPORTATION SYSTEM WITH CHOICE OF MODES

Objective. Create and maintain a balanced transportation system with choice of bus transit, bicycle, and pedestrian as well as private automobile modes. Reduce the percentage of trips that are made by automobile and provide the opportunity and facilities to divert trips from automobiles to other modes.

T-1 a Complete Streets. The City shall direct the design, construction, reconstruction, repair and maintenance efforts on the City's streets, bridges, pathways, and sidewalks, creating a comprehensive, integrated transportation network that is safe, accessible, comfortable, accommodating, and welcoming to users of all ages, races, ethnicities, incomes, and physical abilities, and all modes of transportation, particularly those walking, rolling, biking, and using transit, and in doing so the City shall apply a Complete Streets framework in all applicable and feasible transportation projects to allow the safe, comfortable, convenient and accessible use of streets for all street users.

T-1ba Investment in alternative modes. In order to provide a realistic and cost-effective balance between travel modes, the City shall emphasize investment in alternative modes (bikeways, etc.) as a priority over increasing vehicular capacities of streets.

T-1cb Interconnections and transfers between travel modes. The City shall provide and maintain a Transit Center to facilitate interconnection and transfers between bus routes and systems. As funding permits, Transit Center facilities shall be improved to encourage its use as a multi-modal transfer point. Pedestrian and bicycle amenities shall be provided at other locations which serve as modal transfer points such as bus stops and park-and-ride lots.

T-1de Intercity travel. The City shall coordinate with Humboldt County and Caltrans to provide adequate facilities for vehicles, buses, and bicycles to serve intercity demand. Joint efforts may include transportation improvements outside of Arcata which serve intercity travel, such as bicycle links, timed-transfer bus stops, park-and-ride lots, and regional transit service and development of park-and-ride lots in Arcata to reduce intercity vehicular travel.

T-1ed Critical transportation facilities. Critical transportation facilities for emergency vehicle access and emergency evacuation shall be maintained and improved as a priority need. However, when determining needed improvements, ease and speed of emergency vehicle access shall at all times be weighed against safe design for all street users. Critical transportation facilities include the major routes into and out of the City such as Highways 101, 299, and 255, their interchanges with City streets and primary intra-city street connections including Samoa Boulevard, 11th Street, "G" and "H" Streets, Sunset Avenue, L.K. Wood Boulevard, Alliance Road, Janes Road, and Giuntoli Lane. Due to the potential for structural failure of these facilities in a seismic emergency, alternative

routes and procedures for their use shall be identified.

T-1e **Parking and public transit service study.** The City shall undertake a comprehensive study of parking and public transit service options for the downtown/uptown area and HSU Cal Poly Humboldt, with cost/revenue implications presented for each option. This study shall be undertaken jointly with HSU Cal Poly Humboldt.

T-1f **Mobility equity.** The City will adopt policies, pursue plans, and implement programs and projects that further transportation and mobility equity. Safe mobility is a right of all people in Arcata.

POLICY T-2 TRAVEL DEMAND MANAGEMENT

Objective. Reduce the percentage of automobiles and reduce the annual vehicle-miles of travel.

T-2a **Land use development patterns.** The City encourages and supports travel demand management efforts. The City shall promote land use and development patterns that encourage walking, bicycling and transit use. In recognition of the link between land use and transportation, the land use plan shall discourage low density, homogenous land-use patterns that foster automobile travel and are impractical to serve with transit. Land use planning shall emphasize high density and mixed land-use patterns which translate into higher transit and pedestrian travel in the downtown and neighborhood commercial areas. Infill, redevelopment, and reuse of underutilized property at higher densities shall be encouraged prior to outward expansion of City boundaries. The following land use measures are emphasized:

1. Mixed-use neighborhood centers within transit corridors which include housing and commercial services near employment.
2. Land use patterns which maximize linking trip opportunities by assembling uses, thus allowing people to take care of a variety of daily needs with a single trip.
3. Clustering of higher density housing and incorporation of residential apartments units on upper floors of commercial buildings in the downtown area.
4. Integration of new housing into neighborhood shopping centers, including Sunny Brae, Westwood, and Valley West.
5. Pedestrian-oriented land use and urban design, including the following elements:
 - a. Pedestrian-scale block patterns.
 - b. Incorporate pedestrian and bicycle amenities into public and private projects.
 - c. Design streets for multi-modal use.
 - d. Integrate transit stop facilities into public and private projects.
 - e. Orient buildings and houses to the street.
 - f. Provide attractively landscaped streets and buffers.
 - g. Preserve existing and historic urban fabric.
 - h. Eliminate blank wall facades.

- i. Incorporate bicycle routes and enhancements in public and private projects.
- 6. A fixed urban services boundary to reduce sprawl and infrastructure costs.
- 7. Focused growth along existing or planned transit corridors rather than extension of transit to serve new isolated development.
- 8. Prevention of large areas of single uses. Isolated single-use developments at the edge of the City could encourage automobile travel for commuting and errands.
- 9. Provision of convenience retail and services in ground floor space in the downtown to accommodate the needs of employees and reduce the need for mid-day automobile trips.

POLICY T-3 BUS TRANSIT POLICY

Objective. Maintain a bus transit system which connects and serves major commercial and employment areas within Arcata, Cal Poly Humboldt State University, public schools, and higher density residential areas. Increase average citywide transit mode share of daily person trips to 5—12% from the 1998-2020 level of 13% by 2040.

T-3a **5-year transit plans.** The City shall improve ~~maintain~~ the existing A&MRTS routes (as shown in Figure T-~~de~~), frequency, and level of service as funding permits. ~~until increased demand, additional development, and transit planning studies identify the need for either route modification, an expanded route system, or increased service on existing routes. The transit planning studies should evaluate the cost effectiveness and feasibility of increased routes and service based on projected capital and operating costs, fare box recovery, and state and federal subsidies (see Policy T-3c for planning criteria).~~



T-3b **Regional transit service.** Short- and long-range transit plans shall be coordinated with the regional transit service provided by the Redwood Transit System. The City supports regional transit plans which improve service and timed transfers, and reduce headways for intercity travel. In the interest of enhanced coordination and efficiency for local and regional service, the City shall continually evaluate alternatives to existing services including potential mergers with the Humboldt Transit Authority or other service providers.

T-3c **Bus route system.** Public transportation is both a civil right and a critical climate solution, and should be designed to provide service competitive with automobile travel in terms of access, convenience and comfort. Potential improvements to the transit system should be assessed according to the best available evidence of both need and existing and induced demand. ~~an enterprise activity and its services must be designed to be as efficient and productive as possible. As a transit operator, the City must balance demand with resources for a sustainable system. The City shall consider adding transit~~

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~~routes or modifying existing transit routes and level of service based on the transit planning efforts described in Policy T-3a. Criteria to evaluate and identify thresholds for changes to the A&MRTS system shall be developed. General guidelines for planning future routes and service include:~~

- ~~1. Accessibility of route to residents and employees. Calculate the number of people living or working within walking distance of the route (typically 1,000 feet). Assuming 1% to 8% of that population would use transit (based on existing transit mode share by census block), determine if the route will serve an adequate population for cost effective service.~~
- ~~2. Review the housing density within the proposed route corridor. Minimum densities of at least seven dwelling units per acre are necessary to support local transit service. Ideally, the average housing density within a transit corridor or transit served nodes should range between eighteen to twenty dwelling units per acre, depending on the proximity to stops.~~
- ~~3. Evaluate the efficiency and directness of future routes. Compare bus travel time with automobile travel time to avoid a disproportionality which favors automobile use. Determine if the route requires inefficient loops which take riders out of their way and discourages transit use. Design routes to be as direct as possible with turnarounds at endpoints.~~
- ~~4. Evaluate the diversity of the destinations served. Efficient routes serve a diversity of land uses including residential, employment, schools, and shopping. Evaluate the number of activity centers connected by the route and the transfer opportunities provided.~~



T-3d **Transfers between routes and systems.** The public transit system shall provide convenient transfers between routes, other transit services, and other modes of travel such as bike share locations with racks for one-way trip use. The Arcata Transit Center shall serve as the primary multi-modal transfer station. Bus stops should be located near municipal parking lots or future park-and-ride lots. The A&MRTS and Redwood Transit System schedules shall be coordinated to provide a timed-transfer system at key stops.

T-3e **Bus stops.** Existing bus stops should be improved and new bus stops on future routes should be designed with appropriate amenities and features. Design elements include either bus stop lanes or bus turnouts. Bus stop design amenities which increase rider comfort and feeling of safety and encourage walking and bicycling are emphasized, including shelters, benches, lighting, shade trees, signs, information kiosks, waste receptacles, paved surfaces, facilities for disabled and alter-abled riders, and secure bicycle parking. Prioritize covered seating at all bus stop locations wherever feasible. Bus stop areas should be consistently maintained and cleaned, including vandalism repair and graffiti removal. Developers shall be required to provide bus stops and amenities on their frontage if the property is located on an existing or future bus route and is an appropriate location for a stop. Pedestrian, and bicycle, and handicapped rider access should be provided to neighborhood bus stops.



T-3f **Transit subsidies.** The City supports continued A&MRTS contract services with Cal Poly Humboldt State University to provide subsidized fares to its students and employees. This subsidy revenue source, which allows these users to ride without cost to the individual, is the single most important Transportation Demand Management strategy for Arcata.

T-3g **Transit implications of new development.** The public works engineering department and A&MRTS shall evaluate proposed new development projects and make recommendations prior to project approval regarding transit improvements and road designs.

~~T-3h **Increased weekend transit service.** The feasibility and cost-effectiveness of providing weekend bus service to Eureka should be studied.~~

T-3h The City shall study investigate the possibility of pairing its traditional fixed-route bus system with an on-demand micro-transit system which could serve lower density areas and feed into the fixed route system to increase transit mode share.

POLICY T-4 STREETS AND HIGHWAYS PLAN AND CIRCULATION MAPS AND CONTEXT SENSITIVE STREET DESIGN POLICY

Objectives. Plan an internal street system the circulation network consistent with Figure T-k and Figure T-i and Arcata's small town, non-metropolitan character to create Complete Streets solutions that are appropriate to individual contexts; that best serve the needs of all people using streets and that support the land-use, climate, safety, and environmental quality targets and policies of the City and which: 1) efficiently utilizes existing facilities and reduces need for investment in new or expanded street and highway facilities or

capacities; 2) improves connectivity of streets to provide for direct routes between origins and destinations; 3) has a high quality of regular maintenance and repair; and 4) to the extent feasible, maintains a level of service which minimizes delays, acknowledging LOS is not a high priority of the City in street design but allows for higher levels of congestion during the short peak periods on weekdays.

T-4a **Freeways and Highways.** ~~State Routes 101 and 299 are designated as freeways for their entire length in the City. State Route 255 is designated as both an arterial and a highway within the City.~~ The following standards shall apply to State Routes 101 and 299 and State Route 255:
these classifications:

- ~~1. Function. The function of freeways is to provide for high-speed automobile and freight movement for intercity and regional travel. Freeway access is highly controlled to achieve this function. Freeway operations, design, and maintenance are under the jurisdiction of the State. Highways (Route 255) also function to move automobiles and freight at relatively high speeds with little friction from intersections and conflicting traffic. Access is controlled on highways, but not as restrictive as freeways. [See functional classification map in Figure T-a.]~~
1. No additional travel lanes. The City does not support development of any additional through-travel lanes to State Routes 101, 299, or 255 in Arcata or nearby areas. Existing and projected traffic volumes do not warrant additional lanes on these facilities.
2. Auxiliary lanes. The City does not support construction of auxiliary lanes between existing interchanges, or any new interchanges, on State Route 101.
3. Interchange improvements. The City supports interchange improvements that increase safety and reduce potential conflicts created by unrestricted access from freeway off-ramps.
4. Landscaping. The City encourages Caltrans to maintain and improve landscaping along freeway corridors in Arcata and surrounding areas to improve aesthetics, and provide a visual and noise buffer, ~~and maintain the rural and small-town character of the region.~~
- 4.5. Undesignated right of way. All public rights of way with no land use designation (i.e. freeways, highways and associated interchanges) shall be used for transportation purposes only, including multi-modal use. All land uses within these rights of way shall be for transportation or related (i.e. lighting, drainage, utilities, pedestrian and bicycle) purposes.

T-4b **Arterial Streets**~~Vehicular Circulation.~~ ~~Routes designated as arterial streets are shown on the functional classification map in Figure T-a.~~ The following shall apply to vehicular these routes~~circulation routes:~~

1. Functional classification and designated routes. Arterial streets are intended to

- ~~provide a high degree of mobility and serve longer trips within the City. Arterials connect various neighborhoods within Arcata and provide direct connections to the state highway system. Arterials are intended to emphasize traffic movement over access to property.~~
2. ~~Alternative street cross-sections for arterial streets.~~ The ~~Engineering Department~~ ~~Department of Public Works~~ shall prepare alternative cross-sections for ~~new arterial existing and proposed new arterial, collector, and local~~ streets utilizing a smaller right-of-way, and ~~prepare alternative cross-sections for existing rights of way that reduce~~ traffic speed ~~and shall be designed to allow the safe, comfortable, convenient and accessible use of streets for all roadway users~~ and safely accommodate bicycle and pedestrian traffic.
3. ~~Arterial street connectors.~~ Extend existing roads to increase the City's arterial connectivity if proposed development creates significant traffic congestion or overwhelms existing neighborhoods. The Foster Avenue to Sunset connector is a planned road extension if feasible. This project will extend Foster Avenue east of Alliance Road to connect with Sunset Avenue near the State Route 101 interchange to create an east-west facility between Spear Avenue and 14th Street. This extension would bypass the residential neighborhoods on Sunset Avenue, provide a direct arterial connection from Alliance Road to State Route 101, and improve and facilitate bus routing.
- No additional ~~automobile-vehicular travel lanes on arterial streets.~~ Street projects shall not be designed solely to improve vehicular traffic flow and LOS shall be de-emphasized in street capacity planning and design ~~shall emphasize intersection improvements and facility maintenance.~~ If congestion occurs, it shall be managed using alternative methods such as diversion of trips to other travel modes or intersection improvements. Construction of additional ~~arterial street vehicle~~ travel lanes shall not be considered ~~only when no other feasible congestion management methods are available and if unless~~ it supports the land-use, climate, safety, and environmental quality targets and policies of the City.
3. Improvements at intersections. Improvements at intersections shall be designed to allow the safe, comfortable, convenient and accessible use of streets and walkways for all roadway users.
- a) ~~Minor improvements at intersections.~~ Minor projects to improve traffic safety include redistributing lane allocations and coordination of traffic signals. ~~Improvement projects shall be designed to accommodate the needs of pedestrians and bicyclists.~~ The City shall consider developing traffic signals and signalized pedestrian crossings to accommodate new or denser land uses, traffic patterns, and safety concerns, especially in the Downtown, Gateway Area, and the Giuntoli/Valley West neighborhood.
- b) ~~6.~~ Minimize the installation of new traffic signals. New traffic signals shall be provided only in instances where there is no feasible alternative to relieve a demonstrated safety problem at an intersection (based on documented collisions ~~accidents~~). Alternatives which shall be studied prior to signals include roundabouts or installation and monitoring of all-way stop signs.

~~6. Minor improvements at intersections. Minor projects to improve traffic safety include redistributing lane allocations and coordination of traffic signals. Improvement projects shall be designed to accommodate the needs of pedestrians and bicyclists.~~

~~T 4c Collector Streets. Routes designated as collector streets are shown on the functional classification map in Figure T a. The following shall apply to collector routes:~~

- ~~1. Functional classification and designated routes. Collector streets serve to provide access to land use and movement of traffic, pedestrians, and bicycles within residential, commercial, and industrial areas. Collectors generally penetrate, but should not have continuity through residential neighborhoods. Collector streets collect traffic from local streets and distribute it to the arterial street system.~~
- ~~2. Alternative street cross sections for collector streets. The Department of Public Works shall prepare alternative cross sections for new collector streets utilizing a smaller right-of-way, and prepare alternative cross sections for existing rights-of-way that reduce traffic speed and safely accommodate bicycle and pedestrian traffic.~~
- ~~3. No additional automobile travel lanes on existing collector streets. No additional travel lanes are planned on collector streets. If congestion occurs, it shall be managed using alternative methods such as intersection improvements or diversion of trips to other travel modes.~~
- ~~4. Intersection Improvements. No new traffic signals are planned on collector streets. Other alternatives that may be considered to improve safety at intersections include stop signs, roundabouts, or other traffic calming measures.~~

~~T 4d Local Streets. All streets within the city not classified in another category in Figure T a are designated as local streets. The following standards apply to these streets:~~

- ~~1. Functional classification and designated routes. Local streets function to provide access to adjacent land use and exist in any land use setting such as residential, commercial, and industrial areas. Movement on local streets is intended to involve traveling to and from a collector facility. Therefore, the trip length on a local street is intended to be short, volumes should be low, and speeds slow.~~
- ~~2. Alternative street cross sections for local streets. The Department of Public Works shall prepare alternative cross sections for new local streets utilizing a smaller right-of-way, and shall prepare alternative cross sections for existing rights-of-way that reduce traffic speed and safely accommodate bicycle and pedestrian traffic.~~

~~T 4ec Rural Roads. Routes designated as rural roads are shown on the vehicular circulation functional classification map in Figure T a. The following standards shall apply to these roads:~~

- ~~1. Functional classification Description. Rural roads serve very low density land uses (mostly agricultural and rural residential) outside of the urbanized area of Arcata. Rural roads are usually not intended to serve through traffic, but often accommodate truck traffic related to the land uses served.~~
- ~~2. Maintain rural character. Rural roads shall be maintained in a manner which will retain~~

~~their rural character and discourages use as alternatives to arterials and highways for longer distance travel.~~

T-4f4c **Slowing Traffic-calming.** The City shall employ ~~the following~~ a range of measures to reduce speeds and “calm” traffic ~~throughout the city in the various neighborhoods to improve safety and comfort for those walking, rolling, biking, and taking transit:~~

- ~~1. Transportation Safety Committee. Neighborhood Traffic Management. A Neighborhood Traffic Management Program (NTMP) shall be developed to respond to problems in a consistent and methodical approach. The NTMP should be a two-phase program, with the first phase involving education and community driven measures, and the second phase involving installation of restrictive physical devices in appropriate circumstances. Neighborhood residents and businesses should be invited to participate in the program so that they can evaluate the benefits and trade-offs of various measures and be involved in the decision-making process. The Transportation Safety Committee holds regular public meetings and reviews matters related to traffic safety in Arcata and make recommendations to the Council, Commissions, or City staff as appropriate. Measures requested by residents or property owners, or initiated by City staff, that intended to slow traffic shall/should be presented to the Transportation Safety Committee for recommendations. The Transportation Safety Committee shall make recommendations after a public meeting where any public comments are heard.~~
- ~~1-2.~~ Measures should be context sensitive and may include the installation of physical infrastructure, such as street trees, speed bumps, speed humps, narrowing streets, mid-block crossings, and bulb outs, while ensuring that the techniques employed have the effect of slowing traffic without compromising emergency access.
- ~~3.~~ The installation of speed tables, humps and lumps shall adhere to the then current City of Arcata policy regarding installation of speed tables, humps and lumps for residential and local streets administered by the Department of Public Works.
- ~~2-4.~~ All neighborhood streets shall remain open to through vehicle travel unless there is a ~~demonstrated~~ warranted application of safe streets policy ~~safety problem that cannot be adequately addressed through the measures identified above, as determined by the City Engineer.~~

T-4g **Street closures.** ~~All neighborhood streets shall be kept open unless there is a demonstrated safety problem. The following traffic-calming measures will keep streets open and safe, and will reduce through traffic:~~

- ~~1. Full or partial diverters or closures of streets.~~
- ~~2. Median barriers at intersections.~~
- ~~3. Diagonal diverters at intersections.~~
- ~~4. Entrance barriers at beginning of street.~~
- ~~5. Conversion of street to one way.~~

TABLE T-4-7 PASSIVE AND RESTRICTIVE TRAFFIC CALMING MEASURES

<u>PHASE I PASSIVE MEASURES</u>	<u>PHASE II RESTRICTIVE MEASURES</u>
<u>Neighborhood campaigns for traffic safety or speed watch reporting</u>	<u>Traffic circles or roundabouts</u>
<u>Passive traffic controls such as stop signs</u>	<u>Medians</u>
<u>Parking restrictions or modifications</u>	<u>Raised intersections and raised crosswalks</u>
<u>Active police enforcement</u>	<u>Speed humps/speed tables</u>
<u>Pavement markings and signage</u>	<u>Curb extensions at intersections or midblock</u>
<u>Neighborhood gateway features</u>	<u>Chicanes or slow points</u>
<u>Visual cues at neighborhood entries</u>	<u>Narrowing travel lanes</u>
<u>Emphasis on visual rather than physical deterrent</u>	<u>Reduced curb radii</u>
<u>Textured crosswalks</u>	

T-4h4d **Street maintenance.** The Pavement Management System shall be maintained to identify and prioritize street maintenance projects in the City’s Capital Improvement Program (CIP). The maintenance program shall include regular street cleaning and repair of pavement, sidewalks, multi-use paths, and bicycle lanes, and pay particular attention to conditions that discourage bike usage.

~~TABLE T-4 PASSIVE AND RESTRICTIVE TRAFFIC CALMING MEASURES~~

<u>PHASE I PASSIVE MEASURES</u>	<u>PHASE II RESTRICTIVE MEASURES</u>
<u>Neighborhood campaigns for traffic safety or speed watch reporting</u>	<u>Traffic circles or roundabouts</u>
<u>Passive traffic controls such as stop signs</u>	<u>Medians</u>
<u>Parking restrictions or modifications</u>	<u>Raised intersections and raised crosswalks</u>
<u>Active police enforcement</u>	<u>Speed humps/speed tables</u>
<u>Pavement markings and signage</u>	<u>Curb extensions at intersections or midblock</u>
<u>Neighborhood gateway features</u>	<u>Chicanes or slow points</u>
<u>Visual cues at neighborhood entries</u>	<u>Narrowing travel lanes</u>
<u>Emphasis on visual rather than physical deterrent</u>	<u>Reduced curb radii</u>
<u>Textured crosswalks</u>	

POLICY T-5 BICYCLE AND PEDESTRIAN FACILITIES

Objective. Create a complete, interconnected bikeway system and pedestrian circulation system network. Increase the percentages of person trips via walking and bicycling. ~~Provide a pedestrian and bicycle system,~~ which serves commuter as well as recreational travel ~~the full range of mobility needs.~~

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T-5a **Overall bicycle route system and connectivity.** ~~The bicycle trails and facilities route system plan is are~~ shown in Figure T-ej. ~~The bikeway cycle route system shall be improved and expanded as necessary consistent with the City of Arcata Pedestrian & Bicycle Master Plan and applicable Regional Transportation Plans prepared by the Humboldt County Association of Governments (HCAOG) to serve new development and activity centers. Routes that provide access to and between major destinations including public facilities, schools, parks and open space, employment, and shopping, shall be the highest priority. Future improvements may be made which upgrade bike routes to a higher class. The City shall:~~



1. Regularly (at least every ~~two-five~~ years) update the City of Arcata Pedestrian & Bicycle Master Plan and coordinate planning efforts with Caltrans and the Humboldt County Association Council of Government's bicycle plans and advocacy groups to provide continuous bicycle routes.
2. Maintain existing bicycle routes and provide additional routes where feasible connecting the various neighborhoods with Cal Poly Humboldt State University. Class IV+ bike lanes shall be provided on routes with the highest bicycle demand, or where there is sufficient right of way; in areas with insufficient right of way, Class II lanes shall be considered.
3. Improve and maintain bicycle infrastructure including removal of height differences between pavement and gutter pans, smooth pavement on street edges, drainage inlet grates, and street cleaning to remove debris from street shoulders.
4. Continue to implement Pedestrian & Bicycle Master Plan bicycle boulevard project, including the public awareness campaign about the form, functions, and routes of the bicycle boulevards, with messages that bicycle boulevards are preferred routes for bicyclists and pedestrians and do not exclude motor vehicle traffic. Consider developing standards for a "Bicycle Boulevard," a low-volume and low-speed through-street where bicycles have priority over vehicles, conflicts between vehicles and bicycles are minimized or eliminated, and bicycle travel time is reduced by removal of stop signs and other impediments.

T-5b **Bikeway system and pedestrian network standards.** ~~Class I bikeways.~~ The City of Arcata Pedestrian & Bicycle Master Plan and Humboldt County Association of Governments (HCAOG) Humboldt Regional Bicycle Plan contain appropriate design standards and guidelines for the proposed bikeway system and pedestrian network improvements in the City of Arcata. Continue to work with regional partners and HCAOG to plan improvements to the bikeway system and pedestrian network. Class I bikeways are within completely separated right of way for exclusive use of non-

~~motorized modes. They generally serve corridors not served by streets and provide a recreational opportunity or a high-speed commuter route. Class I bikeways can be multi-use trails serving bicyclists, pedestrians, rollerbladers, and equestrians. A Class I bikeway shall be included on the proposed Sunset Foster arterial. The following standards shall apply to development of Class I bikeways:~~

~~1. Bikeway continuity. Off-street bikeways do not need to be continuous but need to connect to other types of facilities at each end of the bikeway to provide an interconnected system.~~

~~1.2. Right of way opportunities. As opportunities arise, the City shall utilize existing or acquire new easements or right of way for Class I bikeways. Such opportunities may include connecting dead-end streets in new developments with existing neighborhoods, along streets with excess width and unpaved right of way, along drainage channels or creeks, or along abandoned railroad rights of way.~~

~~3. Design standards. Two-way Class I bikeways shall be constructed with a minimum width of eight feet and a preferred width of ten feet (five feet for one-way travel). Caltrans design standards shall be used for other design elements such as drainage slope, clearance, signing and striping, and control where bikeways intersect streets.~~

~~T-5c Class II bikeways. Class II bikeways are lanes located on the outside edge of roadways, including all arterial streets, and delineated from vehicle travel lanes with striping and pavement markings. The following standards apply to Class II bikeways:~~

~~1. Design standards. Caltrans design standards shall be used for Class II facilities. Minimum widths are five feet adjacent to on-street parking or vertical curb without on-street parking, and four feet on streets without curb and gutter. Appropriate signing and pavement markings shall be provided to identify the bicycle lane. Caltrans standards shall be used for bike lane markings or transitions at intersections.~~

~~2. Required street width. The standard street width of forty feet curb-to-curb can accommodate Class II bike lanes in both directions if parking is eliminated from one side of the street and vehicle travel lanes are reduced to eleven feet. Bike lanes should be provided in both directions, if feasible, unless the street is one-way. Streets appropriate for Class II bike lanes include those where on-street parking needs are not critical. Alternatively, prohibition of parking on one side of the street during certain hours of the day may be considered to accommodate bicyclists.~~

~~3. Bike lanes in new development areas. New collector streets in new development areas should have a cross-sectional standard with a minimum curb-to-curb width of forty-eight feet, which can contain two twelve-foot vehicle travel lanes, seven-foot wide parking lanes, and five-foot wide bike lanes.~~

~~T-5d Class III bikeways. Class III bikeways are unmarked bicycle routes which share the street with other vehicles. This type of facility is usually established on low-volume local neighborhood streets, but can be located on any type of street. Many of the existing City designated bicycle routes consist of this type of facility. Any Class III bike routes on~~

~~routes to school with younger bicyclists should have wider outside lane widths (fourteen to sixteen feet). Prohibition of parking during school hours may be considered to achieve the desired width.~~

- T-5e **Bicycle parking facilities.** Secure bicycle parking facilities shall ~~be~~ provided at important activity centers, civic facilities, apartment complexes, employment centers, shopping centers, major bus stops, and schools. Bicycle parking facilities include racks, lockers, and bollards.

Developers shall be required to provide a minimum number of bicycle parking devices at convenient and visible ground-floor locations within the development. Bicycle parking should be located in an interior location near an entrance whenever feasible. The required number of bicycle parking spaces shall be calculated as a proportion of the number of ~~users.~~ vehicle parking spaces.

- T-5f **Pedestrian network enhancements.** Prioritize implementation of improved pedestrian facilities and enhancements linking residential areas with important destinations such as health care, education, employment, shopping, and recreation with priority given to neighborhoods with the greatest need (disadvantaged communities, etc.) in areas of the city with the greatest need including the Arcata Plaza, Westwood Center area, Valley West area, the Sunset Avenue neighborhood, Samoa Boulevard, Alliance Road, Spear Avenue, Janes Road in the vicinity of the Pacific Union School, and Bayside Road in the vicinity of Jacoby Creek School. The following pedestrian improvements and safety enhancements should be considered in future planning for these areas:

1. Close sidewalk gaps.
2. Install vertical curbs to keep vehicles from parking on sidewalks.
3. Reduce street crossing distance with curb extensions and smaller curb radii.
4. Use on-street parking as a pedestrian buffer.
5. Install textured crosswalks.
6. Provide adequate street lighting focused on crossings.
7. Restrict parking near crosswalks to improve sight distance.
8. Install rumble strips on approaches to crosswalks.
9. Plant street trees or place street trees in planters in the parking lane.
10. Relocate intersection stop bars five feet back from crosswalks to improve driver and pedestrian visibility.
11. Install wayfinding signage where appropriate.

- T-5g **Pedestrian pathways and multi-use trails.** Pedestrian pathways or multi-use trails for the exclusive use of non-motorized transportation modes should be provided. Pathways may be long facilities located along corridors or short facilities providing direct access through development projects or connecting areas not directly accessible by streets. Pathways should be planned to serve the full range of mobility both recreational and commuter needs for people of all ages, races, ethnicities, incomes, and

physical abilities. The following shall apply to pedestrian pathways or multi-use trails:

1. Easement or right of way dedication. Dedication of easements or rights of ways for pathways through new private developments may be required.
2. Cooperation with local and regional agencies and jurisdictions. The City shall cooperate with other agencies to establish and maintain off-street pathways and trails utilizing creek, utility, and railroad right of way.
~~—Foster Avenue Extension. Multi-use paths or trails shall be included in the Foster Avenue extension to Sunset Avenue.~~
3. Other Locations. Other potential locations for multi-use paths are within the North Coast Railroad right of way from Giuntoli Lane to Samoa Boulevard, along the west side of Samoa Boulevard/Old Arcata Road east of State Route 101, and along the perimeter of Arcata Bay towards Manila.

T-5h **Sidewalks**. A continuous and interconnected system of sidewalks shall be provided throughout the City. The existing standard right of way of most arterials, collectors, and local streets (fifty feet) permits a five-foot sidewalk in each direction, the minimum width to comply with Americans with Disabilities Act (ADA) requirements. Some commercial areas in downtown Arcata should have wider sidewalks to accommodate higher levels of pedestrian traffic and window-shopping. The following standards shall apply to sidewalks:

1. Sidewalk continuity. Gaps in existing sidewalks should be closed to provide a continuous pathway. Cul-de-sacs should be discouraged because they disrupt pedestrian connectivity.
2. Sidewalk widths. New development projects shall be required to construct or reconstruct sidewalks along the property frontage in accordance with adopted City standards. ~~Required widths for new or reconstructed sidewalks are shown in Table T-5.~~
3. Sidewalk Requirements. Where adequate width exists to maintain ADA ~~minimum~~ clearance, sidewalk pedestrian amenities should be provided in the downtown commercial area. These include benches, bicycle parking, pedestrian-scale lighting, street trees, flower boxes, trash receptacles, drinking fountains, and awnings. Private development projects shall be required to include sidewalk improvements; other landowners are encouraged to provide improvements.
4. Sidewalk Maintenance. Sidewalk facilities shall be systematically inspected and maintained to clean and repair damaged surfaces and remove impediments such as poles, newspaper racks, and other ~~paraphernalia~~ obstructions that interfere with pedestrian flow.

~~TABLE T-5 SIDEWALK FUNCTIONAL WIDTH REQUIREMENTS~~

DESCRIPTION	WIDTH
Low density residential area for two-way pedestrian traffic	6 feet

DESCRIPTION	WIDTH
Low intensity commercial area for two-way pedestrian traffic and window shopping	8 feet
Higher density commercial and residential area for two-way pedestrian traffic, window shopping, and street furniture allowance	10 feet
Minimum width of sidewalk at bus stop with bench on sidewalk, without a shelter	8 feet
Minimum width of sidewalk at bus stop with a shelter on sidewalk	12 feet
High intensity commercial area with high pedestrian traffic and a variety of outdoor sidewalk use such as shopping and dining	12 to 15 feet

T-5i Retention of railroad right of way. The Great Redwood Trail Agency, as the holder of the former North Coast Railroad Authority right of way, is encouraged to maintain railroad rights-of-way through railbanking for interim use as a multi-purpose trail. The City may consider purchase of right of way should the Authority decide to sell. Railroad right of way may potentially be used for creation of multi-use trails. Long range potential uses of railroad right of way include an exclusive bus transitway or passenger rail service.

T-5j Rails to trails conversions. The City supports plans to convert abandoned railroad rights-of-way to provide multi-use trails. Planning efforts shall be coordinated with federal, state, and regional agencies to obtain funds to purchase or lease abandoned lines if the railroad authority selects not to dedicate the right of way. If feasible, non-abandoned railroad lines should also be explored for multi-use trail purposes.

POLICY T-6 PARKING SUPPLY AND PARKING MANAGEMENT

Objective. ~~Manage parking to reduce the incentive for single occupancy vehicle use. Provide an adequate supply of parking in perimeter lots downtown. Minimize the impacts of Cal Poly Humboldt State University parking into adjacent neighborhoods. Ensure that new development provides an adequate but not excessive supply of parking.~~

T-6a **Downtown parking.** The following shall apply to parking within the ~~Plaza Area Commercial land use category~~ Downtown area:

- ~~The City shall explore implementing a smart parking meter system in the Downtown area to manage parking demand while generating revenue to support public transit and/or active transportation. Develop additional public parking lots. Assess and plan for future parking needs. Municipal parking lots shall be provided in the perimeter of downtown to create an adequate parking supply to serve existing businesses, future development, and to replace on-street parking removed for pedestrian, bicycle, and landscaping improvements. One municipal lot is planned to complete the City's parking system, but additional parking lots may be provided if additional demand or opportunities arise.~~
- In-lieu fee for on-site parking. Payment of a fee in-lieu of providing required parking

- spaces ~~may be permitted~~ should be required in the Central Commercial District or for Landmark Historic Structures. Fees collected shall be used exclusively to fund municipal off-street parking lots or alternative travel mode facilities.
3. On-site parking standards. The City ~~shall consider~~ should ~~reducing or eliminate~~ the parking standards applicable within the ~~Plaza Area Commercial land use category~~ downtown area. New development is encouraged to pay in-lieu fees rather than provide parking on-site within the immediate Plaza area. Any on-site parking in the downtown should be located to the rear or side of buildings. Park and ride, car shares, downtown parking benefits districts, and other measures to encourage alternative transportation shall be considered.
- T-6b **Parking in neighborhoods impacted by ~~Cal Poly Humboldt State University~~.** The City shall employ the following measures to reduce the impacts of ~~HSU University~~ related parking on the surrounding neighborhoods:
1. Management of on-street parking. Metered on-street parking shall continue to be provided along local streets in the neighborhoods south of ~~Cal Poly Humboldt State University~~ to prevent all-day parking by students.
 2. Preferential parking zones. The restrictive residential permit parking program shall be maintained for neighborhoods severely impacted by ~~Cal Poly Humboldt HSU~~ to provide residents and their visitors more on-street parking and to discourage students from driving to campus.
 3. Other parking management approaches. Alternative parking management approaches shall be considered if the student population and parking demand increases. Alternative approaches include time limit parking without meters, increasing no-parking zones to decrease supply of spaces, and implementing a strictly enforced tow-away policy. The City encourages ~~Cal Poly Humboldt State University~~ to reduce parking impacts on the City.
- T-6c **Parking standards for new development.** The City's ~~should continue parking standards shall be revised~~ to specify ~~a maximum parking requirements ratio as well as a minimum parking ratio~~ for new development and eliminate minimum parking requirements. Parking lots should be located, where feasible, to the rear or side of commercial and multi-family residential buildings.
- T-6d **Shared or joint-use parking for commercial development.** ~~A 25% Reductions~~ in the individual use parking requirements ~~may be allowed~~ should be considered where two or more ~~non-residential~~ uses provide joint parking. Developers of projects with appropriate land uses for effective shared parking are encouraged to provide joint parking facilities. Examples of compatible land uses include office buildings and any use that generates primarily an evening parking demand such as restaurants and theaters. ~~The Urban Land Institute (ULI) Shared Parking manual shall be used to establish criteria for the parking generation characteristics of land uses.~~

POLICY T-7 ~~RAIL AND~~ FREIGHT TRANSPORTATION

Objective. Provide a transportation system which adequately serves the freight shipment needs of the City's industrial and commercial uses. Recognize that freight transportation via truck ~~or railroad if rail service is re-established in the future~~, is an essential element of the area's economic base.

~~T-7a **Retention of railroad right of way.** The future re-establishment of rail service would be a valuable resource for industrial uses in Arcata. The North Coast Railroad Great Redwood Trail Authority, as the holder of the former North Coast Railroad Authority right of way, is encouraged to maintain railroad rights of way through railbanking for interim use as a multi-purpose trail, subject to possible future reconstruction and reactivation of the right of way for rail service, even if service is abandoned. The City may consider purchase of right of way should the Authority decide to sell. Railroad right of way may potentially be used for creation of multi-use trails. Long range potential uses of railroad right of way include an exclusive bus transitway or passenger rail service.~~

~~T-7b **Train service.** Existing or improved levels of freight train service to industrial uses is encouraged as demand increases. The City supports improvements to facilities and operations and increases in freight service as a necessity for maintaining a viable industrial economy. However, freight train service during the day, particularly in the peak morning and afternoon hours, is discouraged. The possibility of providing passenger train service between Arcata and Eureka using renovated historic trolleys should be considered.~~

~~T-7e~~ **T-7a Truck routes.** The ~~transportation-circulation~~ system shall be planned to provide truck mobility to serve all commercial and industrial land uses in Arcata. Specific truck routes are designated in Figure T-f, although other highways, arterials, and collector streets may be designated in the future. The City shall actively enforce truck routes and speed limits.

~~T-7d **Rails to trails conversions.** The City supports plans to convert abandoned railroad rights of way to provide multi-use trails. Planning efforts shall be coordinated with federal, state, and regional agencies to obtain funds to purchase or lease abandoned lines if the railroad authority selects not to dedicate the right of way. If feasible, active railroad lines may be used for multi-use trail purposes.~~

POLICY T-8 FINANCING TRANSPORTATION IMPROVEMENTS

Objective. Ensure that adequate funding is available to implement transportation improvements required to adequately serve the amount of growth allowed by the land use plan. Ensure that private development provides on-site transportation improvements and contributes an appropriate share of funding for off-site improvements.

T-8a Developer responsibilities and exactions. Developers shall be required to construct transportation improvements along their property frontages. Where appropriate, a traffic impact study shall be required which identifies on-site and off-site impacts and mitigation

measures.

The developer shall be required to provide all necessary access and circulation facilities within the property and such facilities shall be designed to meet City standards. The following improvements may be required, based on the individual context and the needs of all people using streets and the right-of-way; and that support the land-use, climate, safety, and environmental quality targets and Complete Streets policies of the City:

1. If development is located on an existing street:
 - a. dedication of right of way;
 - ~~b. widening of street along property frontage to provide for a travel lane;~~
 - ~~e-b.~~ bicycle lane and parking lane;
 - ~~d-c.~~ reconstruction of curb, gutter and sidewalk;
 - ~~e-d.~~ transit facilities and landscaping within the right of way.
2. If development is located in a new growth area not served by streets:
 - a. dedication of right of way to construct a street to connect the project site to a public street, which accommodates all modes of transportation, particularly those walking, rolling, biking, and using transit;
 - b. construction of the street and connecting intersection(s) to City standards;
 - c. after the dedication is accepted, the City will maintain the street.
3. In all instances, the developer shall be responsible for mitigating any off-site ~~mobility~~ ~~traffic~~ impacts of the proposed development in a manner consistent with the policies of this plan. Measures may include ~~a reduction in the size or density of the development;~~ installation of additional pedestrian, bicycle and transit amenities to encourage alternative travel modes; or implementation of Transportation Demand Management measures.

T-8b **Subdivision improvements.** All on-site transportation infrastructure shall be constructed using standards approved by the City. Developers are required to establish mechanisms, such as homeowners associations, to provide future maintenance of on-site streets and intersections that are not dedicated. The City may elect to require streets connecting to a public street to be dedicated to the City.

T-8c **Traffic/Vehicle Miles Traveled impact fees.** The City may adopt a citywide traffic impact fee ~~in accordance with the requirements of AB 1600~~ to fund transportation improvements to mitigate the ~~traffic~~ mobility impacts of new development based on a Vehicle Miles Traveled (VMT) or similar analysis. The traffic impact fee may substitute in whole or in part for the off-site mitigation requirements described in Policy T-8a, but would be in addition to the developer's responsibility for on-site and frontage improvements. The traffic impact fee may be used to fund roadway extensions, intersection improvements, safety improvements, transit facility improvements, and pedestrian and bicycle facilities or amenities.

T-8d **Transit finance.** A&MRTS should continue to fund capital and operating expenses

through fare box revenue, [Cal Poly Humboldt State University subsidies](#), and state and federal subsidies. The City will explore the possibility of new development contributing a one-time fee towards A&MRTS capital expenses through the citywide traffic mitigation fee ordinance [and funding transit through parking meter revenues where feasible](#).

2.9 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
LU-1	Amend LUC to Incorporate Street Standards Add Street Standards to City's LUC (formerly LUDG)	Community Devel. Dept.	Year 1
T-1	Create Neighborhood Traffic Management Program Prepare and adopt a two-phase traffic management program. Phase 1 will involve education and community-driven measures, including developing a handbook describing procedures for residents to initiate a local NTMP. At a minimum, the handbook should define the procedures for initiation, types of data to be collected, a toolbox of measures, a method for establishing priorities, and potential funding mechanisms. Phase 2 will involve installing traffic calming devices in appropriate circumstances.	Public Works Dept.	Year 1
T-1	Reducing Vehicle Miles Traveled Consider application of vehicle miles traveled (VMT) as a metric for evaluating impacts of new development at such time as a methodology is available that is suitable for use in Arcata. Work with Humboldt County Association of Governments (HCAOG) when evaluating potential regional applications both to evaluate and to reduce vehicle miles traveled.	Community Development/ Engineering Dept.	Ongoing
T - 2	Pavement Management Program A pavement management program will evaluate roadway conditions, and schedule and complete needed maintenance and repair in a timely manner.	Public Works Dept. Engineering Dept.	Ongoing
T - 3	Capital Improvements Program (CIP) Include transportation improvements, including bicycle and pedestrian facilities, in the City's CIP.	Public Works Dept. Engineering - Dept.	Annually
T - 4	Adoption of Traffic Mitigation-Impact Fee Ordinance Program Adopt a citywide traffic impact fee in accordance with AB 1600 to mitigate the traffic impacts. Assess an equitable share of costs associated with cumulative traffic impacts to all development projects on facilities for all modes of travel.	Public Works Dept. Engineering Dept.	Year 15
T-5	Develop Additional Public Parking Lot on West Side of Downtown	Community Devel. Dept.	Year 2
T-65	Develop Comprehensive Pedestrian and Bicycle Master Plan and Priorities Periodically review and update Pedestrian and Bicycle Master Plan priorities including collaborating with Humboldt County Association of Governments on Humboldt Regional Bicycle Plan updates. Seek sidewalk improvement program funding to implement priority projects.	Public Works Dept. Engineering Dept. and Transportation Safety Committee	Ongoing
T-76	Bicycle Boulevards Provide primary bicycle corridors between major activity centers. Clearly sign all bicycle boulevards and include traffic calming measures to discourage automobiles.	Public Works Dept. Engineering Dept.	Year 1

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
T-7	Rail Right of Way Coordination with Great Redwood Trail Agency Coordinate with the Great Redwood Trail Agency in planning for use of the former NCRA rail right of way for a multi-use trail.	Engineering Dept.	Ongoing
T-8	Weekend Transit Service Continue to monitor demand for weekend bus service to Eureka in coordination with HCOAG and transit providers and ensure that planning for weekend transit service from Arcata to Eureka is appropriately addressed in the Humboldt County Transit Development Plan.	Engineering Dept.	
T-8 T-9	Foster Avenue Connection Secure funding for the Foster Avenue connection, including bicycle paths. City shall consider implementing a slow streets program.	Public Works Dept. Engineering Dept.	Year 3 Year 5
T-109	Improve accessibility and mobility. The City shall undertake a comprehensive program to assess and improve accessibility and mobility for people of varied physical abilities and disabilities.	Engineering Dept.	Year 2

[Appendix T-A City of Arcata Operational Analysis and Intersection Level of Service](#)

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

PUBLIC FACILITIES & INFRASTRUCTURE ELEMENT

2.10 INTRODUCTION

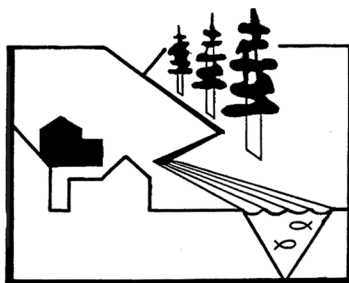
The Public Facilities and Infrastructure Element covers: Water Supply and Delivery; Wastewater Collection, Treatment, and Disposal; Stormwater Management; Integrate Waste Management; Educational Facilities; and Community Facilities. Policy addressing these topics is included in this section. There are many other kinds of infrastructure, physical and social, that this element does not address. See the Parks and Recreation Element, Open Space and Resource Conversation Element, and Health Element for infrastructure-related material more specific to those topic areas.

Overview of Arcata's Water Supply and Delivery System. Community water systems divert free-flowing and subsurface water sources for domestic ~~use~~, commercial and industrial uses. The City of Arcata recognizes that water and other natural resources are vital to the community, ~~and but~~ also must be used efficiently and conserved. -The City provides economic incentives and educational materials for water conservation, and also integrates water reclamation ~~withand~~ wastewater treatment at the Arcata Marsh and Wildlife Sanctuary.

The City of Arcata uses a system of trunk lines and mains, above ground water tanks, and booster pumps to deliver potable domestic water to residents, businesses, industry, and other facilities within the City's Urban Services Boundary.- The City water supply, drawn from Ranney subsurface collectors wells located in the bed of the Mad River, is provided by the Humboldt Bay Municipal Water District, which pumps, treats, and sells the water to the City. The City's Urban Water Management Plan defines water sources, conservation measures, usage, projections and shortage contingencies. In addition to the existing Mad River water source, the Management Plan also identifies a moderately deep groundwater aquifer in the north Arcata area as an additional water source. The City's ~~Water Master~~Urban Water Management Plan addresses the function and capacity of the water system, including equipment efficiency and life expectancy, water storage, pumping, storage, and fire flows.



Overview of Arcata's Stormwater and Wastewater Collection and Treatment Systems.



Arcata's best known public facility is the Arcata Marsh and Wildlife Sanctuary (~~AMWS~~) (Wildlife Sanctuary) where municipal wastewater is treated and re-used for wetlands, ~~ponds~~, and related wildlife habitat. The ~~AMWS Wildlife Sanctuary~~ complies with California State-North Coast Regional Water Quality Board and California Coastal Zone Wetlands Enhancement Program policies and standards. But more importantly, the ~~AMWS Wildlife Sanctuary~~ employs natural systems as part of the wastewater treatment system to enhance water quality before final discharge to Arcata Bay while providing water reuse to support beneficial uses associated with constructed freshwater wetlands ~~to successfully treat and reuse wastewater that most other communities dump or export.~~ ~~The Arcata's wastewater treatment system uses traditional mechanical treatment systems, oxidation ponds, and treatment wetlands to purify the city's wastewater. The t~~reated wastewater effluent then flows through three enhancement flows through five marshes wetlands in the ~~170-acre sanctuary~~ AMWS Wildlife Sanctuary, where natural processes further polish the effluent organisms filter the water before it is disinfected and released into Arcata Bay. Stormwater ponds, distinct from the enhancement wetlands, provide habitat related benefits separate from the enhancement wetlands. The wetland resulting nutrient laden water and habitat attracts more than 200 species of birds, as well as other species. The City has a Wastewater Treatment Facility Plan ~~Plant Master Plan~~ to guide the rehabilitation and upgrade of the treatment system and enhancement wetlands to meet up to date regulatory requirements. ~~plant operations.~~



The City also manages a stormwater drainage system; it ~~and~~ has a Drainage Master Plan and Stormwater Management Program Storm Water Management Program to guide management practices of the system.

The Drainage Master Plan includes a hydrological analysis, drainage management alternatives, a capital improvement program, needs-assessment and financial summary, and a recommended operational plan. ~~The Plan's objectives are~~ to: 1) to identify and quantify the existing stormwater and drainage system, including channels, wetlands, creeks, culverts and pipes in order; to determine the available capacity of the system; and 2) to identify design, maintenance, and repair alternatives to improve the capability of the system.

The Stormwater Management Program was developed in 2005 to comply with the City's Municipal Separate Storm Sewer System (MS4) Permit (Water Quality Order No. 2003 – 0005 – DWQ), which regulates stormwater runoff within City limits. As part of the MS4 Permit, the City was required to develop a Storm Water Management Program (SWMP) in 2005 that included a variety of measures to reduce pollutants discharged into receiving water. The program continues to apply, and has been augmented through the City's compliance with the Phase II MS4 Permit (Water Quality Order No. 20913-0001-DWQ) that was issued by the State Water

Resources Control Board in 2013.**DRAINAGE MASTER PLAN MANAGEMENT GOALS**

- Minimize increases in the volume and the flow of stormwater runoff associated with new development so as to minimize an increase in the hazards and the costs associated with flooding.
- Minimize the erosion potential from a development or construction site so as to prevent deposition of sediment into streams and other receiving water bodies.
- Maintain the integrity of stream hydrology by preventing stream channel erosion so as to sustain the hydrologic functions of streams.
- Reduce the pollutant load in stormwater runoff from developing and urbanizing areas so as to preserve the natural biological functions of streams and other receiving water bodies (and flood management and stream habitat quality)
- To the extent practical, acquire easements and properties necessary for effective drainage management.

Overview of Arcata's Educational and Public Facilities. The educational opportunities available in Arcata include public and private schools for kindergarten through twelfth grades, vocational training programs, and [Cal Poly Humboldt State University](#). These facilities have been integrated into the community and represent an important facet of Arcata's identity as a place to live, work, and learn.

There are three public school districts (Pacific Union, Arcata, and Jacoby Creek Elementary School Districts) and several private schools that provide kindergarten through eighth grade education. High school education is provided by the Northern Humboldt Union High School District, which also serves the community of McKinleyville to the north. The College of the Redwoods Community College, located approximately fifteen miles south of Arcata, offers occupational, transfer and Associate degree programs.

[Cal Poly Humboldt State University](#), the northernmost of California State University's system of twenty-three campuses, offers undergraduate and graduate degrees in more than fifty subject areas. The University also offers cultural activities, music, art, theater and athletic events that are open to the community. ~~The University's Master Plan includes a maximum enrollment limit of 8,500 full time equivalent students.~~

Public facilities offer locations for community interaction and events, ranging from community-wide celebrations to group meetings, instructional classes, and weddings. The City has enclosed spaces such as the Community Pool, Community Center, City Hall, schools, and places of worship. The City also has twenty-four separate parks including Redwood Park, the Marsh and its interpretive center and the Arcata Ballpark. Collectively, they provide gathering places for all manner of social, cultural, political, recreational, religious, educational, and entertainment events. Their physical form and design are also important in representing recognizable features that strengthen community identity. Arcatans have come to recognize that their public facilities are important components of community character.

Overview of Arcata's integrated waste management program. Arcata residents have a long-standing tradition of active commitment to resource conservation. Being far from the production centers of consumer goods, local ~~native-indigenous~~ peoples and the City's early settlers were resourceful and "made do" with what they had. The civic and church-led drives for scrap metal and newspapers during WWI and WWII, and the youth and volunteer drives of the 1950s and 1960s set the stage for the citizen recycling activism following Earth Day 1970.

Over the years~~Since 1990~~, the City's role in ~~source reduction~~ waste prevention, waste disposal, recycling, and composting components of ~~integrated~~ waste management has shifted from facilitative to managing contracts for collection and processing of solid waste and recyclable materials, composting organic ~~debris from the City's operations~~ materials, and ~~direct performance of~~ public education responsibilities.

The State of California has an extensive history of solid waste laws resulting in significant waste-related legislation. In 1989, the State of California passed Assembly Bill (AB) 939, requiring every city and county in the State to develop an integrated waste management planning document called a Source Reduction and Recycling Element (SRRE). AB 939 and its accompanying regulations specified the content for the City's SRRE Source Reduction and Recycling Element and required ~~that the SERE provide~~ a detailed plan for achieving "landfill diversion" goals of 25% by 1995, and 50% by 2000. ~~The plan includes a "Waste Generation Study," (WGS) which reports the composition and quantity of solid waste disposed and diverted from disposal by Arcata. It is from this study, and its baseline projections of solid waste generation, that the City of Arcata is accountable for 25% and 50% diversion targets. In 2017, the City adopted a Zero Waste Action Plan that built upon the Source Reduction and Recycling Element, with a goal of achieving 90% landfill diversion by 2027. The Zero Waste Action Plan included guiding principles, goals, and near-, mid-, and long-term implementation programs.~~



The City developed integrated waste management goals from the Source Reduction and Recycling Element, as well as a list of goals from the City's Zero Waste Action Plan, which are summarized as follows:

ARCATA INTEGRATED WASTE MANAGEMENT GOALS:

1. Reduce the quantity of waste generated.
2. Educate Arcata residents to reduce, re-use, repair, compost, and recycle.
3. Maximize public involvement.
4. Minimize negative environmental impacts of solid waste management.
5. Increase economic incentives for source reduction, re-use, repair, composting, and recycling.
6. Improve measurement and standardize accounting of source reduction, re-use, composting, recycling, waste hauling, and disposal activities to increase knowledge and create a database for long term use.
7. Integrate source reduction, re-use, repair, composting, and recycling programs into all City activities.

ZERO WASTE ACTION PLAN GOALS:

1. Zero waste public education will create behavioral change and a citywide zero waste culture.
2. City operations and policies will integrate zero waste strategies.
3. Waste prevention is Arcata's top priority for sustainable materials management.
4. Materials reuse is Arcata's second priority for zero waste strategies.
5. Recycling drop-off and collection options are fully used by residents.
6. Increase effective and efficient food and organics waste prevention and reuse options.
7. Reduce construction and demolition waste and promote materials reuse.
8. Zero waste and recycling market development is an integral component of local economic development.
9. Support State and Federal legislation that results in waste reduction in rural communities and Arcata.
10. City will work cooperatively with strategic partners and key stakeholders to accomplish ZWAP goals.

Building on AB 939, AB 1826 became effective on January 1, 2016 and required businesses and multi-family complexes (with 5 or more units) that generate specified amounts of organic waste (compost) to arrange for organics collection services. On January 1, 2022 the requirements for AB 1826 were eclipsed by those of SB 1383.

The most recent major waste-related legislation is SB 1383, which was signed by Governor Jerry Brown in September 2016 and addresses short-lived climate pollutants. The final implementing regulations were adopted in November 2020 and require a 50% reduction in the level of statewide disposal of organic waste from the 2014 level by 2020 and a 75% reduction in the level of statewide disposal of organic waste from the 2014 level by 2025. Included in the regulations is a goal of rescuing at least 20% of currently disposed edible food. SB 1383 is the largest overhaul of the State's solid waste and recycling structure in over 30 years and requires significant action by local jurisdictions, including the City, residential and commercial organics generators, haulers, and facilities to reduce organics disposal.

SB 1383 includes requirements within six main categories:

1. Provide organics collection services to all residents and businesses
2. Establish an edible food recovery program
3. Conduct education and outreach to the community
4. Procure recyclable and recovered organic products
5. Secure access to recycling and edible food recovery capacity
6. Monitor compliance and conduct enforcement

~~Prior to AB939, municipal solid waste management focused on collection and landfilling. With the passage and implementation of AB939, California cities and counties are required to adopt the "Integrated Waste Management Hierarchy". Emphasizing the "highest and best use" of secondary resources, the IWM Hierarchy establishes the following priority order for dealing with discarded materials:~~

- ~~1. Waste Prevention or "source reduction" systems to prevent waste at the source.~~
- ~~2. Recycling and Composting systems for collecting, processing, and manufacturing with discarded materials. Organic material is composted for soil amendment.~~
- ~~3. Transformation—a term for incineration disposal.~~
- ~~4. Landfill Disposal.~~

~~The City of Arcata has been in the forefront of recycling and has the oldest community-owned recycling center in the State. The Arcata Source Reduction and Recycling Element has eight main components are:~~

- ~~1. The **Source Reduction Component** identifies programs to be implemented by the City to reduce the quantity of waste generated.~~
- ~~2. The **Recycling Component** defines programs to be implemented to increase the type and quantity of materials recycled.~~
- ~~3. The **Composting Component** identifies programs to be implemented to increase the composting of organic wastes.~~
- ~~4. The **Special Waste Component** identifies wastes requiring special handling and disposal, and programs to manage special wastes which cannot be handled at current or future permitted disposal facilities.~~
- ~~5. The **Education and Public Information Component** describes educational and informational programs to be implemented to educate and increase public participation in the adopted Source Reduction and Recycling Element's programs.~~
- ~~6. The **Disposal Facility Capacity Component** identifies disposal capacity needed to meet the community's needs for fifteen years.~~
- ~~7. The **Funding Component** identifies costs and funding for the City's Source Reduction and Recycling Element plan implementation and how the City will fund those programs.~~
- ~~8. The **Integration Component** demonstrates that the programs to be implemented are sufficient to achieve a 25% diversion rate by 1995 and a 50% rate by the year 2000.~~

~~(Note: The Arcata Source Reduction and Recycling Element is not part of the General Plan.)~~

~~Each of these components includes the following integrated waste management goals:~~

ARCATA INTEGRATED WASTE MANAGEMENT GOALS:

1. Reduce the quantity of waste generated.
2. Educate Arcata residents to reduce, re-use, repair, compost, and recycle.
3. Maximize public involvement.
4. Minimize negative environmental impacts of solid waste management.
5. Increase economic incentives for source reduction, re-use, repair, composting, and recycling.
6. Improve measurement and standardize accounting of source reduction, re-use, composting, recycling, waste hauling, and disposal activities to increase knowledge and create a database for long term use.
7. Integrate source reduction, re-use, repair, composting, and recycling programs into all City activities.

Guiding Principles and Goals.

- A. Provide an adequate water supply and delivery system for day-to-day and emergency needs.
- B. Maintain the Arcata Marsh and Wildlife Sanctuary as an exemplary model of how natural systems can be effectively and efficiently used to treat and reclaim wastewater.
- ~~C. Utilize natural systems and processes for managing stormwater.~~
- ~~D. Promote lifelong learning by supporting educational facilities and programs at all levels.~~
- ~~E.C. Recognize that public facilities are the primary gathering places for social, cultural, political, educational and entertainment events/celebrations, and that these facilities are important components of the community's identity.~~
- D. Advance waste diversion goals set forth in state mandates and the City's Zero Waste Action Plan. Publicly advocate reducing solid waste as the first priority for waste management; and promote recycling based manufacturing through: City purchase of recycled products, education, and community support. Support new technology and education programs that reduce solid waste by an additional 10% every five years and maintain a long term strategy for achieving "Zero Waste".
- E. Promote lifelong learning by supporting educational facilities and programs at all levels.
- F. Recognize that public facilities are gathering places for social, cultural, political, educational, and entertainment events and celebrations, and that these facilities are important components of the community's identity. Protect public civil and Constitutional rights in Arcata's public sphere, and especially on City property and public rights of way, and in any facility that receives City support.

2.11 POLICIES

The Public Facilities and Infrastructure Element contains the following policies:

- PF-1 Water Supply and Delivery
- PF-2 Wastewater Collection, Treatment and Disposal
- PF-3 Stormwater Management
- PF-4 Educational Facilities
- PF-5 Community Facilities
- PF-6 Integrated Waste Management

POLICY PF1-1 WATER SUPPLY AND DELIVERY

Objective. Manage the City’s potable water resources to ensure adequate quantities for community use, to promote water conservation, to maintain water quality, and not to deplete source supplies.

PF-1a **Water supply.** Surface and subsurface water quantities that supply the City are dependent on rainfall and adequate upstream storage. -The City shall continually monitor the water quantity and quality in its system and adhere to the Humboldt Bay Municipal Water District’s rationing system to ensure that adequate supplies reach all users. ~~The City shall also develop additional water sources to meet current peak use and future use demands.~~

PF-1b **Capacity and management of City water delivery system.**

The City shall update its Urban Water Management Plan, at least every five years, to maintain current projections, management, and contingency programs for water delivery. ~~The Plan shall identify needed water delivery system improvements and anticipated extensions so that they can be budgeted for in the City’s Capital Improvement Program.~~ The City water system shall not be extended beyond the Urban Services Boundary (except as provided for in Policy GM-4b of the Growth Management Element).

~~The City shall update its Water Master Plan, at least every five years, to assess system efficiency and ensure that there is adequate storage capacity and fire flows to meet City needs.~~ continue to assess through its Capital Improvement Program needed water delivery system improvements and storage capacity to meet demand and capacity to respond to emergencies including fire and disruption in water service from Humboldt Bay Municipal Water District.

PF-1c **Water****conservation.**

The City shall use a combination of economic incentives, educational programs, and auditing to promote water conservation and shall remain in compliance with Humboldt Bay Municipal Water District policy regarding its Five-Stage Water Rationing System.:

HUMBOLDT BAY MUNICIPAL WATER DISTRICT'S FIVE STAGE RATIONING SYSTEM

1. ~~In effect at all times to assure best use of water in storage. Goes into effect when the storage reservoir reaches 90% of capacity. No use reductions are required.~~
2. ~~Goes into effect when the storage reservoir reaches between 60% and 5580% of capacity and industrial and retail customers will be required to reduce usage by 5%, and Ruth Lake area rainfall is 70% or less of historical rainfall.~~
3. ~~Goes into effect when Ruth Lake reaches 4030% of capacity and rainfall is 60% or less of historical average. All wholesale industrial and retail customers will be required to reduce usage by 1050 to 15% over the previous two year average. and 10%, respectively.~~
4. ~~Goes into effect when Ruth Lake reaches 3060% of capacity and rainfall is 50% or less of historical average. All wholesale industrial and retail customers will be required to reduce usage by 16 to 70% and 230%, respectively over the previous two year average.~~
5. ~~Goes into effect when Ruth Lake reaches 2550% of capacity and rainfall continues at 50% or less of historical average. Industrial All wholesale and retail customers will be required to reduce by usage up to 95% and 3050%, respectively as may be determined by the rate of use of available supply and weather conditions.~~

Water rates will continue to be higher for consumption above baseline usage. Information about conservation devices such as flow restrictors, and practices such as off-peak irrigation, will be made available to the public. -Building and site development permits that require connections to the City's water system shall incorporate water conservation design features and best management practices.

The City shall also implement water conservation measures through the water, wastewater, and drainage master plans, and through leak detection and inflow and infiltration (I/I) reduction programs.

In response to water shortages, the City will work cooperatively with the regional water supplier and the supplier's other municipal customers to implement an effective water shortage contingency plan that defines response actions based on the severity of the water shortage. End use prohibitions shall be determined based on current conditions, recommendations of a regional task force, and historic water use data and trends, and will be considered for mandatory penalties, charges and other escalating enforcement actions including education and outreach, issuing a verbal/written warning, penalty assessment, and water service termination. The City may consider imposing sharply graduated excessive use rates and/or excessive use fines, additional forms of water rationing, warnings, and physical flow restrictions to water users who fail to respond to less severe sanctions.

PF-1d **Water quality.** The City shall perform periodic testing and, if necessary, treatment of its domestic water supply to ensure that it meets all state and federal safe drinking water standards, as required by the federal safe drinking water act.

PF-1e **Water Loss.** The City shall perform annual water loss audits to identify and better understand the type of and quantity of water losses occurring in the water distribution system. The City shall ensure that it meets any water conservation/water loss standards promulgated by the State Water Resources Control Board.

POLICY PF-2 WASTEWATER COLLECTION, TREATMENT, & DISPOSAL

Objective. Collect and treat wastewater to achieve safe water quality standards, utilizing the City's internationally renowned marsh treatment facility.

PF-2a **Capacity and management of City wastewater collection system.** The wastewater collection system is designed to transport community sewage to the treatment plant. The City shall update its Sewer System Management Plan Collection System Maintenance Program, at least every five years, to maintain current projections, management, and contingency programs for wastewater collection. The Plan shall identify needed collection system improvements and anticipated extensions, so that they can be budgeted for in the City's Capital Improvement Program. The City shall continue to monitor groundwater infiltration and surface water inflow (I/I) and take necessary action to ensure that these sources do not cause the collection system or the treatment plant to exceed capacity. The City shall also consider adopting building and land use code policies that provide incentives for design, operation, and technology for buildings and sites to minimize wastewater as well as stormwater loads. The City wastewater collection system shall not be extended beyond the Urban Services Boundary except as provided in Policy GM-4b.

PF-2b **Arcata ~~Marsh~~ wastewater treatment system.** The City shall continually review and update its Arcata Wastewater Treatment Facility Operations and Maintenance Manual Wastewater Treatment Plant Master Plan, at least every five years, to evaluate the entire system; reflect any changes in treatment standards; ensure wastewater treatment is meeting current standards; verify that there is adequate treatment system capacity; and ensure adequate water flows to maintain habitat. The City shall ensure ongoing treatment system planning and investments are consistent with mid- and long-range climate adaptation goals, which balance preserving the City's existing investments with habitat restoration and sea level adaptation priorities. Goals, priorities, planning assumptions, and the best available science on which they are based, shall be reviewed publicly through City committees and the Planning Commission at the discretion of the City Council.

~~The City shall maintain the existing facilities of the Arcata Marsh and Wildlife Sanctuary and construct new facilities consistent with the Marsh Enhancement Plan adopted by~~

~~the City Council.~~

- PF-2c **Protecting, improving and restoring water quality:** Protecting surface and ground water quality, preventing water pollution, restoring water quality in waterways and wetlands within the City and in receiving waters of California and the United States shall guide design, construction, and operation of the City's water management infrastructure. The City shall use necessary resources to comply ~~Compliance~~ with State California-Regional Water Quality Control Board wastewater treatment and discharge standards. The City shall regularly test its wastewater discharges and make necessary adjustments in treatment ~~processes~~levels, to ensure that effluent it meets the California State Regional Water Quality Control Board and standards. ~~The City shall also keep its~~ National Pollution Discharge Elimination System Permit (NPDES) permit standards. ~~current and in compliance with U.S. Environmental Protection Agency standards.~~
- PF-2d **Composting and beneficial reuse of biosolids ~~disposal of sludge~~ and other system byproducts.** The City includes biosolids ~~sewage sludge~~ in compost processed at the treatment plant. The City uses this composted material as a soil amendment. The City shall continue this practice as an efficient means of recycling treatment plant by-products and shall investigate the possibility of selling excess compost to generate revenue. This requires the City to protect the quality of its sludge by implementing an industrial and high-volume discharger wastewater pre-treatment program. (See Policy PF-2g Source Control Program, below.)
- PF-2e **Treatment of wastewater from other communities.** The City, ~~until 2012,~~ accepts for treatment wastewater from the ~~unincorporated community of~~ Fieldbrook Glendale Community Services District. This practice may continue as long as there is adequate treatment system capacity. The City shall not enter into any new agreements for processing wastewater from other communities, nor shall the City accept additional loadings from any connection from other communities through the Fieldbrook Glendale system.
- PF-2f **Maintain the Joint City/ Cal Poly Humboldt ~~State University~~ Wastewater Utilization Program.** Recognize that Cal Poly Humboldt ~~State University~~ faculty and students were instrumental in the design, testing, and development of the Arcata Marsh and Wildlife Sanctuary. The City and the University jointly participate in a wastewater utilization program, which provides ongoing research projects for students and faculty studying wastewater, stormwater, and water quality issues. The City and University maintain an five-year agreement to operate the program, ~~with the City providing the funding and the University providing the student research and faculty advisors.~~ The City shall renew the program with the University when the current agreement ends, and should collaborate with Cal Poly faculty in seeking funds for future research. ~~as long as there are funds available to compensate the University.~~

PF-2g Source Control Program. The City shall maintain a source control and pretreatment program that provides the legal authority to regulate non-domestic sewer use to control discharges of industrial waste. The source control and pretreatment program shall include legal authority, local limits, pretreatment permitting, and an enforcement response plan.

POLICY PF-3 STORMWATER MANAGEMENT

Objective. Implement the City's Drainage Master Plan and abide by the MS4 Permit to utilize natural drainage systems; minimize increases in stormwater runoff, flooding, and erosion; maintain the integrity of stream hydrology; and reduce pollutant loads, and acquire easements and properties for effective drainage management.



PF-3a Utilization of City streams and

watercourses as natural drainage systems. Arcata's network of creeks provides a natural drainage system, however, this system is very susceptible to damage from urban pollutants carried by stormwater runoff, and from drainage facilities that alter creek flows and natural functions. The City shall utilize creeks for urban drainage only when the basic natural functions of the creeks will not be degraded.

PF-3b Control of stormwater runoff, flooding, and erosion. Stormwater runoff, especially at peak flows, can cause significant flooding and erosion if adequate precautions have not been taken. As stated in the Drainage Master Plan, the City shall manage the storm and surface water system in Arcata to maintain a hydrologic balance in order to protect water quality, prevent property damage, provide for the safety and enjoyment of citizens, and preserve and enhance habitat and sensitive areas.

PF-3c Stormwater quality. Implement the City's Phase II MS4 Permit to reduce the discharge of pollutants to the maximum extent practicable and protect water quality. Manage surface water controls, facilities such as detention basins and natural infiltration areas, and education programs to protect surface and ground-water quality.

PF-3d City drainage system. The City shall take a comprehensive approach to drainage system management in order to effectively control the quantity of stormwater runoff, protect water quality, and reduce potential flood damage from peak flows. As stated in the City Drainage Master Plan, the City shall gradually expand the City managed drainage system to:

1. Continue maintenance of all drainage facilities within public right-of-way, regardless

- of size.
2. Extend responsibility onto private property only when permanent easements are dedicated or otherwise available from the private property owner, and need is established based on technical criteria.
 3. Define service limits upstream of the City as the point at which runoff from a publicly (not County) dedicated street enters the drainage system, or when a drainage feature needs repairs/improvements which have public benefits that exceed the cost of said repairs/improvements.

PF-3e **Easements and properties for drainage management.** The City shall secure the easements and properties necessary to complete and maintain the drainage system identified in the Drainage Master Plan.

POLICY PF-4 EDUCATIONAL FACILITIES

Objective. Value Arcata's educational institutions and facilities as keys to achieving the high educational standards that will lead to prosperity and community wellbeing. Partner with schools and the University in support of Arcata's future prosperity, and our diverse and tolerant cultures. Seek the cooperation of Arcata's educational institutions to achieve City goals. Identify student enrollment increases, based on the projected future population of the City, and coordinate with local school (public and private) districts, ~~Cal Poly Humboldt State University,~~ and other education providers to maintain and improve educational facilities and services. Coordinate with Cal Poly Humboldt to project demand for City services and facilities based on anticipated increases in enrollment and employment. ,while preserving established community/student ratios.

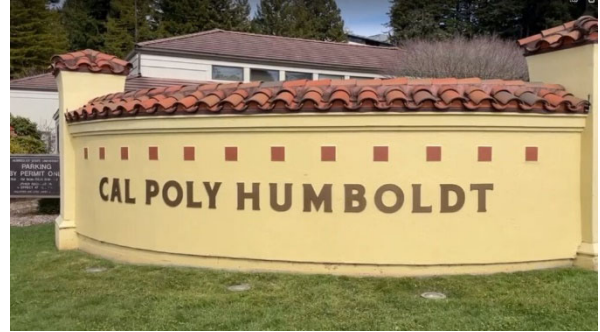


ARCATA HIGH SCHOOL

PF-4a **Coordination with Arcata, Pacific Union, and Jacoby Creek school districts, the Northern Humboldt Unified High School District, and Charter School Operators.** The City shall provide demographic information to assist the School Districts and charter schools in projecting future student enrollments. The City shall encourage the school districts and charter schools to expand existing schools rather than designating new sites for this purpose. Public and charter schools are allowed in all residential zones, in the Gateway Area, and in Public Facilities zoned parcels.

PF-4b **Coordination with private and specialized education providers.** The City shall accommodate providers of private, vocational, and specialized education that fills an identified community need, when they desire to locate in the City.

PF-4c **Coordination and development review with Cal Poly Humboldt State University (~~Not applicable in Coastal Zone~~).** The City shall designate at least one member of the City Council ~~and one Planning Commission member~~ as liaisons to Cal Poly Humboldt State University and request that the designated Council member be appointed to the University President's Advisory Council.



PF-4d **Joint use of school facilities for community events and recreation.** School facilities are primary locations for neighborhood level events and recreational activities. The City, school districts, and community organizations shall develop and maintain partnerships for the joint use of school facilities.

Insert Updated Figure PF-a City of Arcata School Districts

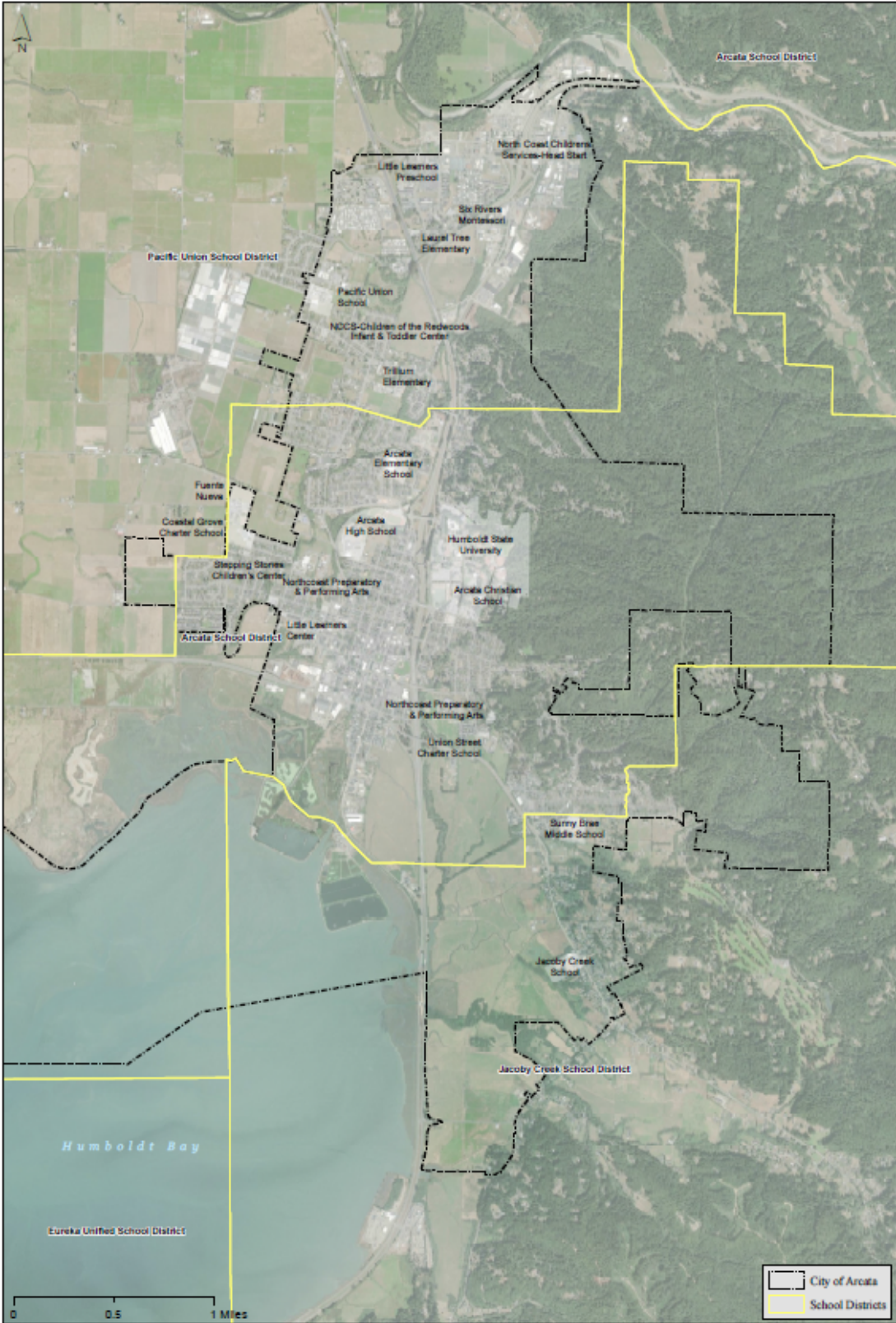


Figure X. City of Arcata School Districts
Created by Planning Partners
Map Date: 4/16/2023

City of Arcata School Districts



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POLICY PF-5 PUBLIC FACILITIES

Objective. Provide adequate facilities for services and programs administered by the City and other public service providers, including City administrative and meeting facilities (City Hall), police and fire departments, libraries, and community centers.

PF-5a Facilities for community service and private organizations.

Community service organizations, as well as non-profit and private organizations serve an important and vital role in the health of our community and offer shelter, assistance, training and other



human services. These organizations also offer places for religious, cultural, social, entertainment and recreation activities. The City shall allow facilities, operated by community service and private organizations, to be located in incorporated areas designated General Commercial [C-G]; Central Commercial [C-C]; Residential High Density [RH]; Limited Industrial [I-L]; and Public Facility [P-F]. Operations and functions of these facilities may be subject to a use permit, to be granted and revocable at the discretion of the City.

PF-5b ~~City Corporation Yard administrative and operations facilities and community centers.~~

The City shall limit development of the Corporation Yard facilities to within existing boundaries, and shall maintain a landscaped screen along the northern and eastern perimeter of the oxidation pond. The City shall continue to explore opportunities for relocation of the corporation yard outside of the tsunami inundation zone and sea level rise vulnerability zone in collaboration with local, state, and federal regulatory and funding partners.

PF-5c Public libraries and civic facilities operated by other agencies.

The City shall coordinate with Humboldt County to provide public library facilities in the City. The City shall also coordinate with other agencies, such as the Humboldt Transit Authority, Redwood Regional Transit System, to maintain joint-use facilities in the City.

PF-5d Telecommunications facilities.

Telecommunication towers, commercial dishes and antenna, monopoles, and other transmitting and receiving facilities shall be co-located (grouped together) to minimize the number of facilities and shall be screened to reduce impacts. Placement of commercial (serving more than a single user) telecommunication facilities shall require be limited to lands designated Public Facility [P-F], Industrial [I-L & I-G], and Commercial [C-G, C-C, & C-VS], with a use permit and shall be limited to the zoning designations outlined in the City's Zoning Ordinance.

Cellular, broadcast, and receiving towers shall not exceed ten feet in height, unless it can be demonstrated that additional height (up to thirty feet) would not create adverse

~~visual or safety impacts.~~ These facilities shall be screened from view and associated equipment rooms and switching devices shall be designed and landscaped to blend with their surroundings. In approving a use permit, findings must be made that the proposed location is the most appropriate for the neighborhood, that the facility is of the minimum size necessary for the intended use, and that it is set back and screened to reduce visual and safety impacts. Any proposed city construction projects involving trenching shall be reviewed for opportunities to extend high speed networking infrastructure.

PF-5e **Maintenance of City streets and rights-of-ways.** The City's streets and ~~right-of-ways~~rights-of-way shall be adequately maintained for public use. Utilities within rights-of-way shall be placed underground, when feasible, to reduce obstructions such as poles and above-grade utility boxes on sidewalks. Pavement and landscape management programs shall be periodically reviewed and prioritized. The City shall comply with Americans with Disabilities Act requirements as a minimum, and seek to develop City rights-of-way beyond those requirements to safely accommodate mobility by people of all abilities and disabilities.

PF-5f **Energy conservation and decarbonization of City facilities.** Continue reducing City facility energy consumption, including conservation and weatherization measures exceeding building code standards, electric only appliances/ HVAC/ water heating systems and renewable energy sources. Consider Greenhouse Gas Reduction measures in all new facility, renovations, site design and connectivity decisions. Develop and implement City practices to increase electrification equity and affordability for all residents.

POLICY PF-6 INTEGRATED WASTE MANAGEMENT

Objective. Reduce solid waste generation at the source; maximize re-use and repair of appropriate items and material; promote composting and recycling; and properly transport non-recyclable solid waste to approved disposal sites. Coordinate with regional bodies to develop effective regional solid waste management systems. eEnsure new development is adequately esited and designed, including adequate physical space for solid waste bins.

PF-6a **Source reduction.** Source reduction and materials re-use are the most ~~cost~~effectivecost-effective ways to minimize solid waste. Source reduction, or waste prevention, reduces the growing costs of collection, recycling, and disposal systems. Source reduction and re-use shall be promoted through educational programs and incentives. Examples of effective source reduction and re-use activities that shall be promoted are:

1. Backyard composting, landscaping with low water needs, and grass mulching.
2. Purchasing durable re-usable goods instead of disposable items (e.g., cloth diapers, rechargeable batteries).
3. Repairing equipment and appliances.

4. Purchasing goods from second-hand stores, flea-markets and swap meets.
5. Reducing the use of packaging by buying in bulk or purchasing fresh food at farmers markets.
6. Moving away from using paper copies for as much City business as possible by working with electronic mail, forms, and agendas, and re-using of scrap paper if possible, if copying is necessary. Electronic mail and forms, double sided copying and re-use of scrap paper.
7. Reusable coffee cups and beverages provided in dispensers.
8. Termination of unwanted mail.
9. Incentives such as on-call garbage collection and differential solid waste fees shall be used to encourage source reduction.



~~—The Source Reduction and Recycling Element shall be updated every five years and shall incorporate the most efficient and cost-effective source reduction programs.~~

PF-6b **Recycling.** The City's recycling program shall continue and expand, unless a more efficient and cost-effective method of collecting and reusing materials is identified. The City shall continue to contract for recycling, collection, and processing, in order to help meet and exceed the State diversion goal. The following programs shall also be promoted:

1. Collection of commercial corrugated cardboard.
2. Collection of office paper.
3. Collection of organic waste.
- ~~4~~3. City procurement policies and practices reflecting State requirements for purchasing recovered organic waste products and recycled content paper products, favoring reusable and recycled products.
- ~~5~~4. Implementation of density bonuses for buildings designed to promote recycling.
- ~~6~~5. The City's active involvement in the Humboldt County Recycling Market Development Zone.
- ~~6~~ 7. Public education Educate the public to "close the loop" and buy locally-made products with recycled content.
- ~~7~~ 8. Public education on recycling Recycling education that includes self-haul services for recyclable materials not collected curbside.
- ~~7~~ 9. Encourage the Support for development and expansion of recycling-based manufacturing.

~~After the year 2000, i~~n order to minimize increases in solid waste volumes and maximize the amount of material returned to productive use, the City shall continue these programs with a target of reducing landfill volumes 10% every five yearsas

specified in the Zero Waste Action Plan.

- PF-6c **Collection, transport and disposal of non-recyclable solid waste.** The City shall continue to contract for solid waste and recycling collection, transport, and disposal and will amend relevant Franchise Agreements to include the collection and transport of organic waste in the future to comply with state waste and organics regulations (e.g. SB 1383). ~~Solid waste collected for disposal shall be transported to an approved landfill, or other approved solid waste processing or disposal facility. The City is supportive of organic processing and will work with regional partners to develop an in-County organics processing facility.~~ The City will provide educational materials about proper use and disposal of household hazardous waste, non-toxic alternatives to household hazardous waste, and recycling of materials (e.g., motor oil, anti-freeze, paint, batteries) in conjunction with ~~recycling centers and~~ local governmental and businesses partners.

2.12 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
PF -1	Urban Water Master-Management Plan Update the City Urban Water Master-Management Plan, at least every five years, to assess system efficiency and ensure there is adequate storage capacity and fire flow. The Master-Management Plan will also include economic incentives, education programs, and monitoring measures to promote water conservation.	Environmental Services Department	Every five years starting in Year 2
PF -2	Wastewater Collection System Maintenance Program Update the City Wastewater Collection System Maintenance Program, at least every five years, to assess collection system capacity and condition, ensure there is adequate treatment and disposal capacity, and recommend improvements necessary to reduce groundwater infiltration and surface water inflow. The Maintenance Program Master-Plan will also include economic incentives, education programs, and monitoring measures to reduce wastewater generation.	Environmental Services Department	Every five years starting in Year 3
PF -3	Stormwater Management Drainage Master Plan and Phase II MS4 Permit Program Update the City's Drainage Master Plan , at least every five years, to implement current provisions for minimizing increases in stormwater runoff, maintaining the integrity of stream hydrology, and reducing pollutant loads. Implement the City's MS4 permit requirements through Ordinance No. 1463, which sets forth standards for discharge into the stormwater drainage facilities for the City of Arcata, and establishes a stormwater pollution control program in compliance with the Clean Water Act. The Master Plan will also include economic incentives, education programs, and monitoring measures to promote on-site retention and reduce flooding and erosion impacts.	Environmental Services Department	Ongoing Every five years starting in Year 1
PF -4	Source Reduction and Recycling Element Zero Waste Action Plan Update the Source Reduction and Recycling Element (SRRE) Zero Waste Action Plan , every ten years, to implement the most current technology for reducing solid waste generation at the source, maximize re-use and repair of goods, promote composting and recycling, and properly transport non-recyclable solid waste to approved disposal sites. The SRRE Zero Waste Action Plan will also include economic incentives, education programs, and monitoring measures to achieve the City's goal of reducing solid waste volume by an additional 10% every five years a defined amount, and the overall long-term strategy for "zero-waste."	Environmental Services Department	Every ten years starting in Year 10
PF-5	Arcata Wastewater Treatment Facility Operations and Facilities Plan. <u>Review and update the WWTP operations and facilities plan periodically to take into account changes associated with sea level rise and climate change.</u>	<u>Environmental Services Department</u>	<u>Ongoing</u>
PF-5	Telecommunication Facilities Ordinance <u>Prepare a Telecommunication Facilities Ordinance that implements policy PF-5d and specifies findings required for a use permit.</u>	<u>Community Development Dept.</u>	<u>Year 2</u>

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#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
PF-6	Marsh Enhancement Plan. Update the Marsh Enhancement Plan to reflect the City's acquisition and addition of the Hunt Property.	Environmental Services	Year 3

CITY OF ARCATA

10/1/22 REVISED DRAFT Gateway Area Plan 2022

This Revised Draft incorporates Committee and public input received as of 9/30/22 that comports with the draft plan. Recommendations for changes to the Plan that are either in conflict with the draft or that have competing recommendations are included in a table that will be published separately. Proposed changes in conflict with the draft have not been made at this stage because there are important discussions around several topics ongoing—i.e. building height/massing and transportation circulation. Staff requires further input and direction from the Planning Commission on these topics before the policy related to them in the current draft is revised. The conclusion of these conversations and resulting direction from decision makers will produce in a subsequent revised draft.

This document is designed to be printed double-sided.



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

GATEWAY AREA

AT-A-GLANCE

To grow opportunity and build community equitably.

OVERVIEW

The Arcata Gateway Area is a dense residential and mixed-use neighborhood that reflects the community's commitment to equity and sustainability.

RACIAL EQUITY AND SOCIAL JUSTICE

The Gateway Area fulfills Arcata's aspiration to be an equitable community providing the same high quality of life and equitable housing/employment options to everyone, regardless of skin color, economic standing, or national origin.

EQUITABLE HOUSING

The Gateway Area provides a substantial solution to the City's unmet and future housing needs, with thousands of housing units that are environmentally sustainable and affordable to people in all income ranges. Residents live within a broad range of housing densities and types, including rental and owner-occupied options, in a vibrant, walkable, near-downtown neighborhood.

ARTS, CULTURE, AND SENSE OF PLACE

Vibrant, authentic, and diverse public art and performing art spaces complement a distinctive and attractive sense of place that is built through creative placemaking. Artist and entrepreneurs prosper.

OPEN SPACES

A connected series of green spaces, trails, protected greenbelts, and daylighted creeks provide pockets of active and passive recreational opportunities complement the Gateway Area's urban environment and welcome the entire community. Concentrating residential development in the Gateway Area also preserves the cherished greenbelts around the perimeter of the City by reducing the need and demand for sprawl.

SUSTAINABILITY and AND CLIMATE ADAPTATION

The Gateway Area furthers the Community's focus on sustainability with enhanced natural resources lands, greener streetscapes, rehabilitated coastal resources, and a range of strategies to reduce energy use, greenhouse gases, and vehicle miles traveled.

ECONOMY AND JOBS

Offering a good housing supply that is within walking distance of the core Downtown, the Gateway Area expands the City's vibrancy by increasing economic growth and stability of downtown Arcata. Being within walking distance of Humboldt State University, Cal Poly Humboldt, the Gateway Area also supports the growth of students and jobs at the University. Simultaneously, the Gateway Area also provides some neighborhood commercial uses where people gather and socialize, as well as a few pedestrian-oriented commercial corridors.

MOBILITY AND ALTERNATIVE TRANSPORTATION

A realigned roadway network offers more efficient vehicular circulation, while simultaneously providing more extensive bicycle, pedestrian, and transit facilities. The transportation needs of residents can be fully met via pedestrian and bicycle infrastructure that connects seamlessly to key destinations throughout the City to allow truly car-free lifestyles.

BUILT ENVIRONMENT AND ARCHITECTURE

While new development is taller and denser than has occurred in past decades, new buildings reflect the architectural values, livability, and aesthetic expectations of the community.

City of Arcata

Gateway Area Plan v 11c

10/1/22 REVISED DRAFT

Adopted 

Key to Revisions (Font Color/Highlighting)

Proposed changes recommended by each committee are shown in underline/strikeout text with color coded committee text as follows:

Transportation Safety Committee 9/2/22
 Parks and Recreation Committee
 Creeks and Wetlands Committee
 Forest Management Committee
 Historic Landmarks Committee
 Energy Committee
 Economic Development Committee
 Public member/entity
 Staff edits

Prepared for:
City of Arcata
 736 F Street, Arcata, CA 95521



Prepared by:
Planwest Partners, Inc.
 1125 16th St. Suite 200, Arcata, CA 95521



PLANWEST
 PARTNERS, INC.



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THE PEOPLE'S SUMMARY

The Gateway Area (as defined in the next section) is an optimal location for residential and mixed-use development. The Area is within easy walking distance to the core Downtown, Arcata High School, and Humboldt State University (soon to be Cal Poly Humboldt). A paved Class I bike trail passes through the heart of the Area, connecting the northern half of the City to the Arcata Marsh & Wildlife sanctuary. The Area is also a bustling business hub, as well as a vibrant arts and culture node. Yet, at the time of this Plan's adoption in 2022 the Area is underutilized with pockets of blighted properties despite all its inherent assets. Legacy uses such as mini-storage and natural gas distribution centers consume entire City blocks and many developable sites sit vacant despite the Area's remarkably strategic location. This Plan envisions a revitalized Gateway Area that achieves its potential and better serves the people of Arcata, largely by providing substantial residential growth in complete neighborhoods that provide a range of community amenities.

The Gateway Area is an optimal location for residential and mixed-use development.

Given the Gateway Area's incredible potential and optimal location, coupled with the massively unfulfilled demand for housing in Arcata, this Plan allows for residential growth of up to 3,500 new residential units in the Gateway. To accommodate this much growth within a relatively small area, new housing will be provided primarily through new high density mixed-use multi-story buildings. Consistent with the City's values, that growth will be equitable and environmentally sustainable. While the majority of new development will be residential, this is a mixed-use part of town that supports a car free lifestyle by providing shops, cafes, and jobs within walking distance of homes. Non-residential land uses include retail personal services, restaurants, professional offices, arts and entertainment uses, and light industrial manufacturing. Non-residential uses serve residents, provide employment opportunities, and enhance the City's economic vitality. The Area is also a vibrant hub for arts and culture. For more on these topics, see Chapter 1: Land Use.

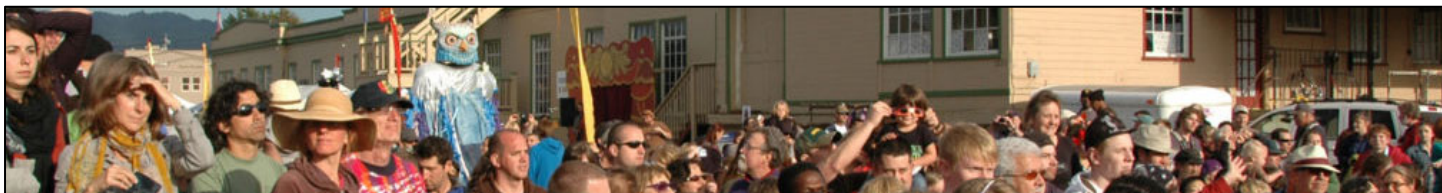
This Plan allows growth of up to 3,500 new residences in the Gateway Area.

This Plan presents a new "community benefits program" that utilizes a system of tiered incentives in which projects that provide higher levels of community benefits are permitted greater building heights, increased residential density, and streamlined approval processes. Thus, a proposed development project that includes electric vehicle charging stations, street trees, murals, on-site bike parking, rooftop solar, enhanced architectural features, or other defined community amenities can be larger than a building without those features. Given the need for housing at all income levels, this Plan considers the production of housing, in and of itself, to be a community benefit. So too are projects that daylight creeks, enhance the vibrancy of City streets, minimize sprawl by contributing to a strong bike and bus networks, or improve the quality of life for community members in other ways. While the Plan provides many core amenities throughout the Gateway Area, the incentives program encourages developers to provide additional amenities. For more details regarding Community Amenities, see Chapter 2.

Projects that provide higher levels of community benefits are permitted greater intensities.

Multiple strategies are baked into this Plan to make housing in the Gateway Area affordable to the full range of Arcata household incomes. This includes promoting a range of residential unit sizes and types, including studios, which are affordable because of their size, student housing, deed-restricted affordable housing, single room occupancies, and family sized dwellings with three or more bedrooms. As a result, the Gateway Area is a mixed-income neighborhood, with housing options available for all income groups, ranging from 200 square foot deed-restricted micro units to luxury condominiums for high-income households. To these ends, residential density and various forms of affordable housing are considered community amenities that qualify for the Community Benefits Program. For more on housing, see Chapter 3.

There are multiple strategies to ensure housing is affordable to the full range of incomes.



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THE PEOPLE'S SUMMARY

Equitable, sustainable, and efficient mobility systems are highly valued in Arcata. Thus, this Plan also prepares for substantially realigned transportation network that significantly enhances bicycle and pedestrian facilities while also creating a more efficient vehicular circulation pattern. Fulfilling the "Gateway" name, the Plan's newly configured circulation network provides much improved mobility, with better north-south and east-west connections. Once implemented, the Gateway will seamlessly link the Gateway Area to the Plaza. Traveling north from Samoa Boulevard to Alliance Avenue will be much more fluid and enjoyable, especially for bicycles and pedestrians. The comprehensive update to the mobility infrastructure may require a reconfiguration of the cherished Class I trail on L Street, and a new supplemental Class I trail is also planned. This is all detailed in Chapters 7 and 8 (Mobility and Streetscape).

This Plan significantly enhances the bicycle and pedestrian network.

A driving motivation in the new mobility infrastructure is to ensure the Gateway Area supports a car-free lifestyle. This is accomplished in part by planning for non-motorized connectivity to other parts of the City with pedestrian and bicycle friendly corridors that draw residents and visitors to enter the Gateway via means other than motorized vehicles. A large part of promoting a car-free lifestyle is providing safe and attractive pedestrian friendly walking routes that focus on safety and aesthetics by incorporating art and street lighting. Off-street parking is de-emphasized in support of more valuable use of land, while the streetscape prioritizes human activity and movement. Where viable, sidewalk widening strategies create unobstructed accessible pedestrian pathways. Providing adequate bike infrastructure, including secure parking at destinations will ensure usage meets the goals of a car-free lifestyle. In pursuit of these values, development projects that enhance active transportation are considered community amenities that qualify for the Community Benefits Program.

The Gateway Area supports a car-free lifestyle.

The process of preparing this document included extensive community engagement to identify the concerns and aspirations of existing and prospective residents and workers. The City hosted over 25 100 public engagement activities and events throughout Plan development and initial review. The City also conducted detailed analyses of real estate market conditions and demands. For more on public engagement activities, see the Introduction Section, which also provides a summary of administrative topics, such as defining the regulatory authority of this document. The Challenges and Opportunities section provides a detailed analysis of the factors that may hinder realization of this Plan. The Vision section provides a detailed preview of the Plan's guiding principles and some visualizations of how the Gateway Area may look in the future. The remaining sections (Chapters 1 through 11) provide topic-specific strategies and policies ranging from Land Use and Employment to Historic Resources and Mobility. The final section provides a summary of the implementation measures designed to ensure this Plan is achieved.

The Vision section provides some speculative forecasts of how the Gateway Area may look when this Plan is realized.



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SECTION A: INTRODUCTION



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

ADMINISTRATION AND CONTEXT

Definitions

The following terms are used throughout the remainder of this document:

Gateway Area Plan: The document that you are currently reading, which is an Element of the City’s General Plan. This definition also applies to the terms “Area Plan” and “Gateway Plan,” both of which are used throughout this document. This Gateway Area Plan contains the goals, strategies, and policies relevant specifically and exclusively within the “Plan Area” (see below for definition of “Plan Area”).

Gateway Zoning Code: A section of the City’s zoning ordinance that applies specifically and exclusively to the “Plan Area” (see below for definition of “Plan Area”). The Gateway Zoning Code implements the policies of this Area Plan with detailed and specific development standards, allowances, and requirements. The Gateway Zoning Code specificity supersedes more general development standards in the Land Use Code.

Gateway Plan Area: The 138-acre portion of the City within which the “Area Plan” and the “Gateway Zoning Code” are applicable. The Gateway Plan Area is also referred to as the “Gateway Area” and the “Plan Area.” The Plan Area is located in the southwest corner of the developed portion of the City as shown in Figure 1 on the next page.

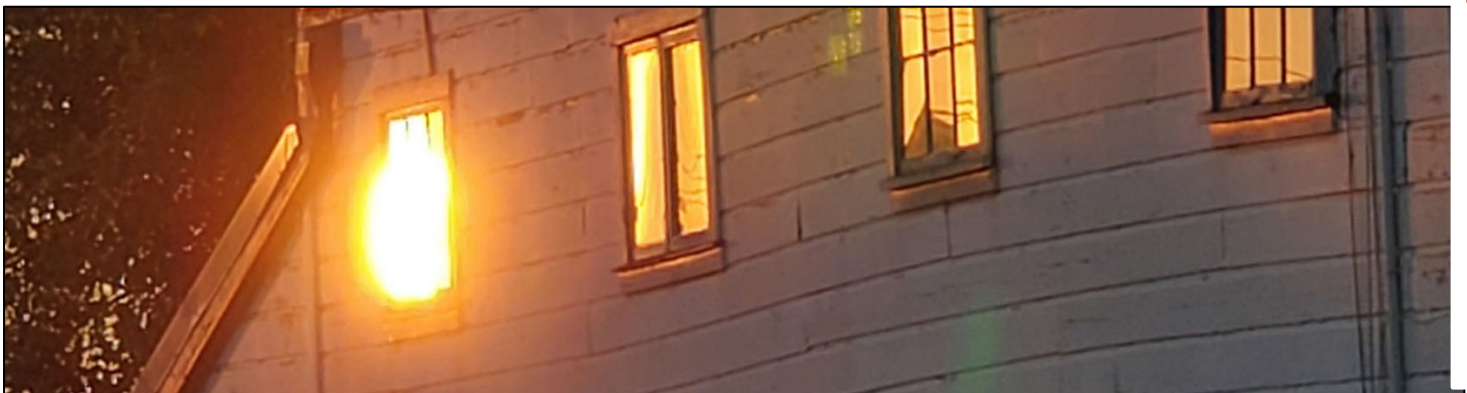
Gateway Sub-areas: The Land Use designations within the Gateway Plan Area, each of which provides for different types and intensities of development. The various sub-areas are described in detail under the Land Use section of this document. See Figure 4: General Plan Land Use Designations with the Gateway Area.

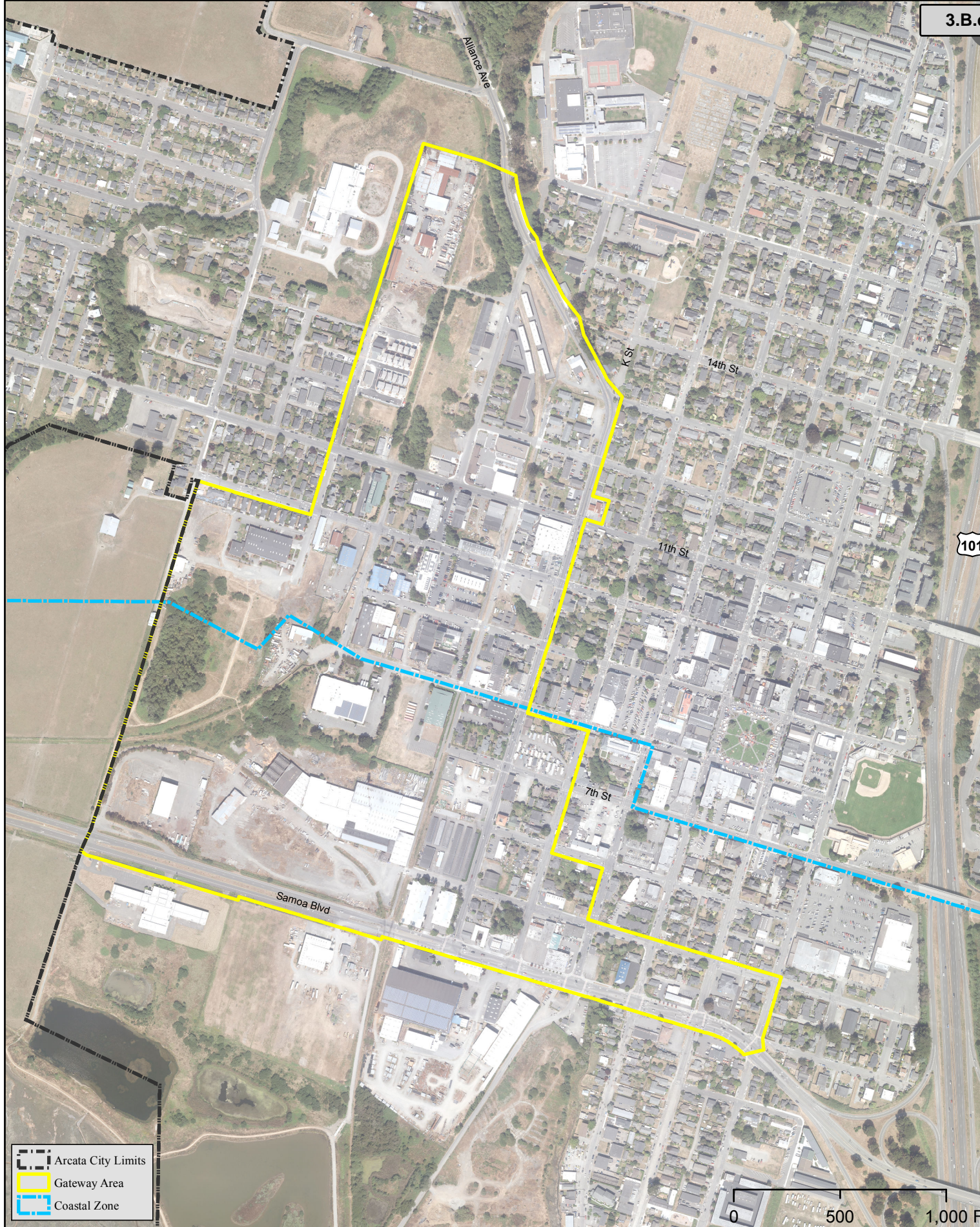
Document Purpose




This Plan satisfies a City Housing Element requirement to rezone the Gateway Area to “high density infill development along with other requirements. Thus, this Plan is a tool for implementing the City’s Housing Element and seeks to provide a means to achieve the regulatory and social demand for more housing. In line with Housing Element requirements, this Plan seeks to create a dense residential, mixed-use neighborhood that reflects the community’s racial equity and sustainability commitment while providing streamlined permit review and project approval for designated community amenities and that satisfy the City’s vision for the Plan Area.

Regulatory Authority

Per California Government Code § 65300 et seq., the City must adopt a General Plan, which must include several mandatory elements and which may also include other elements important to achieving the community’s vision, including area plans. An area plan is a part of the general plan that focuses on a particular region within the overall City and provides greater detail regarding policies affecting development in the defined area. Per California Government Code § 65458 (2), “the plan serves as the land use element for the area covered by the plan” and refines the policies of the general plan as they apply to a smaller geographic area. An area plan need not address all the mandatory components of the overall general plan. This document is an area plan of the City of Arcata General Plan and applies with the Gateway Plan Area or Plan Area as defined above.





-  Arcata City Limits
-  Gateway Area
-  Coastal Zone

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Figure 1: Gateway Area Plan Boundary



Public Engagement and Community Participation

Beginning in late 2020, the City initiated an extensive community engagement program. Development of this plan was a iterative process, with multiple points of public input from the earliest steps through the completion of the plan. Through **four** community workshops dedicated specifically to this plan, residents and other stakeholders developed a shared vision of the future, identified community issues, evaluated conceptual development standards, and identified desired community amenities. Multiple public walking tours of the Plan Area were also hosted by City staff. Including these walking tours and study sessions with various City committees/commissions, the City hosted over **25100** public engagement events and activities for throughout plan development. In addition, an on-line survey was open to the public from February through December of 2021. The following is a chronological summary of the participation opportunities that were available to the public. Items in bold were surveys and public meetings directly relevant to this Plan, while the others were committee/commission meetings with public comment periods:

- For related public engagement activities prior to December 2020, see the City's Housing Element and the and the "Arcata Infill Market Study Community Engagement Report"

1. **12/17/20: Community Visioning and Listening Session (virtual)**
2. 1/21/21: City Council Special Study Session: Strategic Infill Redevelopment Program Report
3. **2/1/21 through 12/31/21: Visioning Survey (on-line; English and Spanish versions)**
4. **2/21/21: HSU Associated Students Board Strategic Infill Redevelopment Program presentation**
5. **2/25/20: Community Visioning and Listening Session (virtual)**
6. **4/20/21: Community Lecture hosted by HSU (virtual) (Victor Schuab Lecture Series)**
7. **6/4/21: Walking Tour with Arcata High School (in person)**
8. **6/19/21: Public Walking Tours (in person)**
9. 6/24/21: City Council and Planning Commission Special Study Session: Strategic Infill Redevelopment Program Report
10. 7/6/21: Economic Development Committee (virtual): Discussion of the Gateway Area Plan
11. 7/14/21: Parks and Recreation Committee (virtual): Discussion of the General Plan Update and the Gateway Area Plan
12. 7/15/21: Historic Landmarks Committee (virtual): Discussion of the Gateway Area Plan
13. 7/19/21: Energy Committee (virtual): Discussion of the General Plan Update and the Gateway Area Plan
14. 7/20/21: Traffic Safety Committee Meeting: Discussion of the General Plan Update and the Gateway Area Plan
15. 7/20/21: Wetlands and Creeks Committee meeting: Discussion of the General Plan Update and the Gateway Area Plan
16. 8/10/21: Planning Commission (virtual): Study Session for the Strategic Infill Redevelopment Program (including Gateway Area Plan)
17. 8/17/21: Traffic Safety Committee (virtual): Discussion of the Gateway Area Plan
18. 8/30/21: Letter mailed to all property owners within Gateway Area encouraging outreach to City staff with questions regarding pending plan
19. 9/14/21: Planning Commission Walking Tour (in person)
20. 9/21/21: City Council Special Study Session Walking Tour (in person)
21. **10/15/21: Public Walking Tour (in person)**
22. **10/16/21: Public Walking Tour (in person)**
23. **12/9/21: Public Workshop (virtual)**
24. **12/11/21: Public Walking Tour (in person)**
25. 12/14/22: Planning Commission (virtual)
26. 12/15/21: City Council (virtual): Report of Public Draft Document
27. 01/04/22: Eco-News Report Interview
28. 01/06/22: "We'll Come to You" Walking Tour with Local Bankers
29. 01/11/22: Planning Commission Meeting
30. 01/18/22: Transportation Safety Committee Meeting
31. 01/19/22 and 01/20/22: Meeting with local area tribes Tribal Historic Preservation Officers
32. 01/20/22: Historic Landmarks Committee Meeting
33. 01/20/22: "We'll Come to You" Presentation Coalition for Responsible Transportation Priorities
34. 01/21/22: Gateway Area Plan Open House, Day 1
35. 01/22/22: Gateway Area Plan Open House, Day 2
36. 01/22/22: Farmer's Market Tabling
37. 01/24/22: "We'll Come to You" Presentation North Coast Health Leadership Team
38. 01/25/22: "We'll Come to You" attendance and Q+A Artists in Action
39. 01/29/22: "We'll Come to You" North Gateway Walking Tour (Public)
40. 01/31/22: "We'll Come to You" tabling-El Centro Open House
41. 02/01/22: Economic Development Committee
42. 02/04/22: "We'll Come to You" tabling at Cal Poly Humboldt
43. 02/08/22: Planning Commission Meeting
44. 02/12/22: Farmer's Market Tabling
45. 02/15/22: "We'll Come to You" Walking Tour-Cal Poly ES 360 Course
46. 02/15/22: Parks and Recreation Committee Meeting
47. 02/17/22: "We'll Come to You" Presentation Cal Poly Humboldt Community Psychology
48. 02/17/22: "We'll Come to You" tabling at Cal Poly Humboldt
49. 02/24/22: "We'll Come to You" tabling at Cal Poly Humboldt
50. 02/17/22: "We'll Come to You" Presentation Cal Poly Humboldt Research Methods
51. 02/17/22: "We'll Come to You" Presentation 350 Humboldt
52. 02/26/22: Farmer's Market Tabling
53. 02/28/22: "We'll Come to You" Presentation Timber Heritage Society
54. 03/01/22: "We'll Come to You" Presentation Artists in Action
55. 03/02/22: "We'll Come to You" Presentation Cal Poly Humboldt Conflict Resolution
56. 03/09/22: Parks and Recreation Committee Meeting
57. 03/10/22: Forest Management Committee Meeting

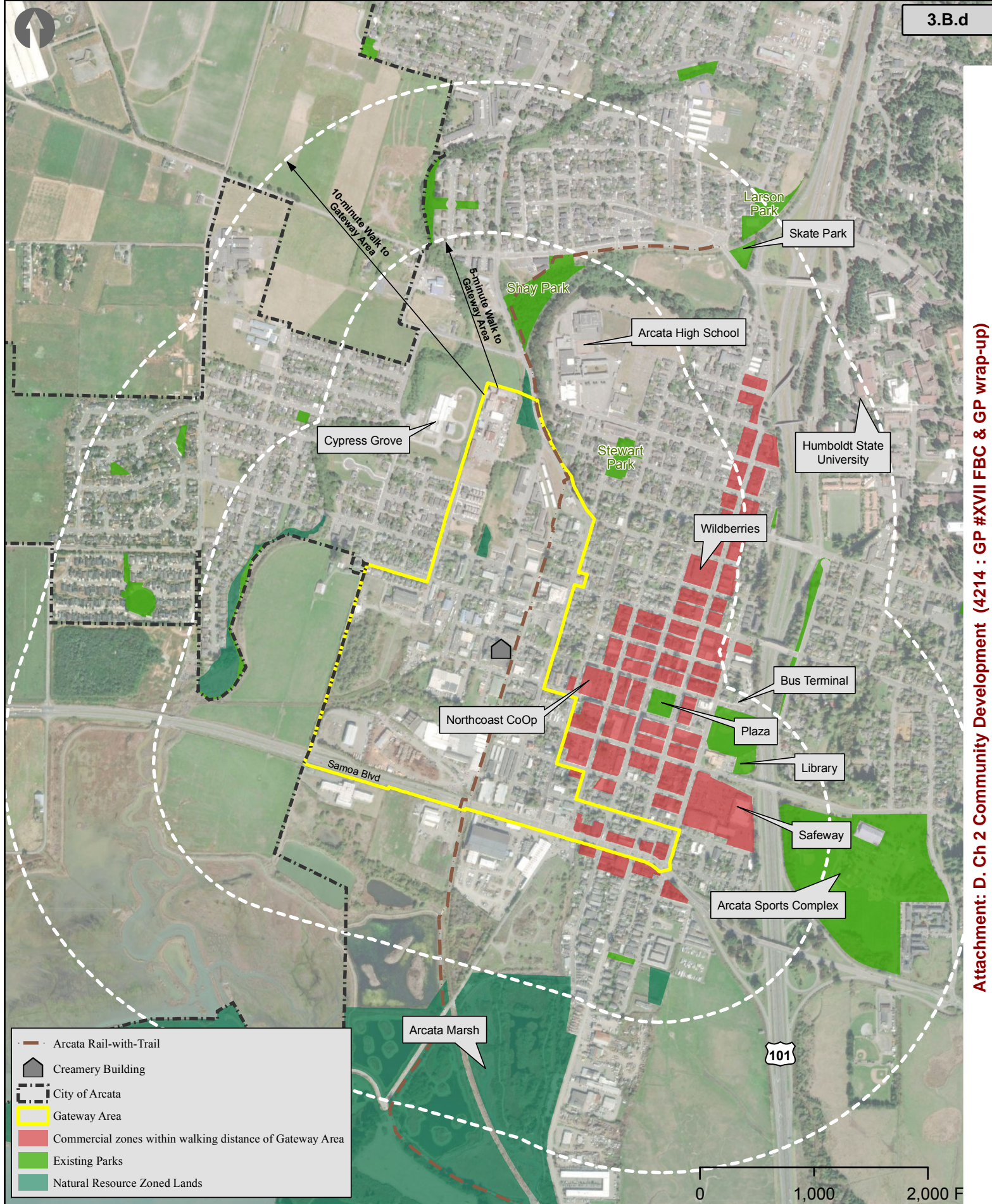
INTRODUCTION: ADMINISTRATION AND CONTEXT

- | | |
|---|---|
| 58. <u>03/10/22: Environmental Impact Report Scoping Meeting</u> | 77. <u>05/19/22: Historic Landmarks Committee Meeting</u> |
| 59. <u>03/12/22: Farmer's Market Tabling</u> | 78. <u>05/24/22: Planning Commission Meeting</u> |
| 60. <u>03/15/22: Creeks and Wetlands Committee</u> | 79. <u>06/01/22: City Council Meeting</u> |
| 61. <u>03/16/22: "We'll Come to You" Presentation Greenway Partners</u> | 80. <u>06/14/22: Planning Commission Meeting</u> |
| 62. <u>03/21/22: Energy Committee</u> | 81. <u>06/16/22: Historic Landmarks Committee</u> |
| 63. <u>03/21/22: "We'll Come to You" Presentation Local Architects</u> | 82. <u>06/22/22: City Council Meeting</u> |
| 64. <u>03/23/22: "We'll Come to You" Local Government Commission</u> | 83. <u>06/29/22: Form-Based Code Information Session</u> |
| 65. <u>03/24/22: Cal Poly Humboldt Student Forum with Cal Poly Humboldt Housing Liaison</u> | 84. <u>07/05/22: Economic Development Committee</u> |
| 66. <u>03/26/22: Farmer's Market Tabling</u> | 85. <u>07/13/22: Parks and Recreation Committee Meeting</u> |
| 67. <u>03/28/22: "We'll Come to You" Presentation Osher Lifelong Learning Institute</u> | 86. <u>07/18/22: Energy Committee Meeting</u> |
| 68. <u>04/08/22: "We'll Come to You" Cal Poly Humboldt Associated Students Board</u> | 87. <u>07/19/22: Wetlands and Creeks Committee Meeting</u> |
| 69. <u>04/10/22: "We'll Come to You" Presentation Universalist Fellowship Climate Action Campaign</u> | 88. <u>07/21/22: Economic Development Committee Meeting</u> |
| 70. <u>04/12/22: Planning Commission Meeting</u> | 89. <u>07/26/22: Planning Commission Meeting</u> |
| 71. <u>04/14/22: Gateway Area Plan Open House-Spanish Language</u> | 90. <u>07/27/22: Historic Landmarks Committee Meeting</u> |
| 72. <u>04/21/22: "We'll Come to You" Presentation True North</u> | 91. <u>08/02/22: Economic Development Committee Meeting</u> |
| 73. <u>04/21/22: Cal Poly Student Forum with North coast Environmental Center</u> | 92. <u>08/09/22: Planning Commission Meeting</u> |
| 74. <u>04/21/22: Historic Landmarks Committee Meeting</u> | 93. <u>08/11/22: Forest Management Committee Meeting</u> |
| 75. <u>04/26/22: Planning Commission Meeting</u> | 94. <u>08/15/22: Parks and Recreation Committee Meeting</u> |
| 76. <u>05/12/22: Economic Development Committee Meeting</u> | 95. <u>08/16/22: Form-Based Code Information Workshop #1</u> |
| | 96. <u>08/18/22: Historic Landmarks Committee Meeting</u> |
| | 97. <u>08/23/22: Planning Commission and City Council Joint Study Session</u> |
| | 98. <u>09/13/22: Planning Commission Meeting</u> |
| | 99. <u>09/20/22: Wetlands and Creeks Committee Meeting</u> |
| | 100. <u>09/27/22: Planning Commission Meeting</u> |

Existing Conditions and Context

For many locals and visitors from the south and west, the Plan Area is the first impression of the City due to its easy access from Highway 101 via Samoa Boulevard. K Street, a main arterial through and into the City, also offers easy accessibility to many parts of the City including Arcata High School and surrounding residential neighborhoods. The Plan Area includes several notable landmarks and well-known buildings/features, including the Creamery Building, a former industrial building that has been successfully repurposed to house a restaurant, dance and fitness studios, artist galleries, and community theatre. The Plan Area also includes the Arcata Rail-to-Trail along the L Street Corridor, a paved Class I bike/pedestrian trail.

Currently, the Plan Area is a mixture of uses, including heavy- and light-industrial, retail, residential, and others, with a disjointed sense of identity. The Plan Area once provided easy access to railways connecting Arcata to lumber mills along the Samoa Peninsula and the City of Eureka. As such, it was a prime location for industrial uses and continues to be the location of various industrial companies such as Industrial Electric, Humboldt Wholesale, Wing Inflatables, AmeriGas, and numerous auto repair and detailing shops. However, interspersed with these industrial uses are historic homes, boutique shops, condominiums, theaters, gyms, pubs, and restaurants. A general summary of the existing context around the Gateway Area is presented in Figure 2 below. These legacy industrial lands offer substantial growth opportunities that also have the benefit of being the most fiscally responsible pathway to provide for the City's needs now and into the future.



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 2: Gateway Area Context



Relationship to Other Plans

This page and the next page show how this document relates to the four other City regulatory documents that apply to the Plan Area. Table 1 below shows which documents apply City-wide and which apply exclusively in the Gateway Area. Table 2 summarizes the objectives of each of the regulatory documents and which document to consult for various purposes. The next page provides details regarding each document, their relationship to one another, and their geographic limitations. As described on the next page, the City's Local Coastal Program is contained within the City's General Plan and Municipal Code.

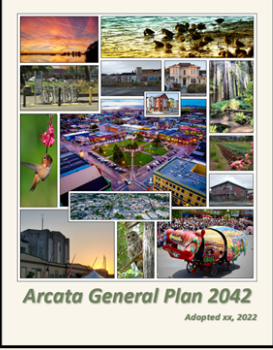
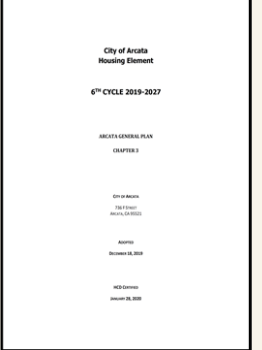
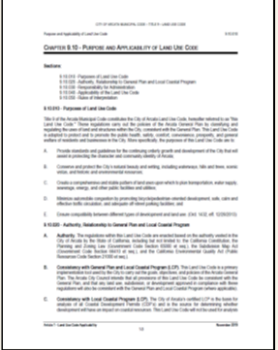
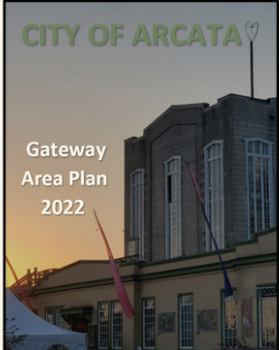
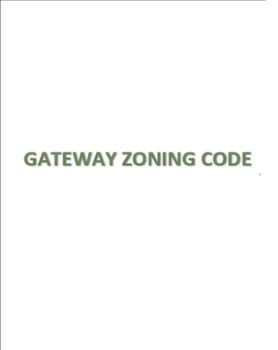
Applies to Entire City, including the Gateway Area			Applies Exclusively to the Gateway Area	
General Plan	Housing Element, 6th Cycle	Land Use Code	Gateway Area Plan	Gateway Area Code
				
	Must be consistent with General Plan.	Must be consistent with General Plan and Housing Element.	Must be consistent with General Plan and Housing Element.	Must be consistent with General Plan, Housing Element, and Gateway Area Plan.

Table 1: Summary of City Regulatory Documents Applicable within the Plan Area

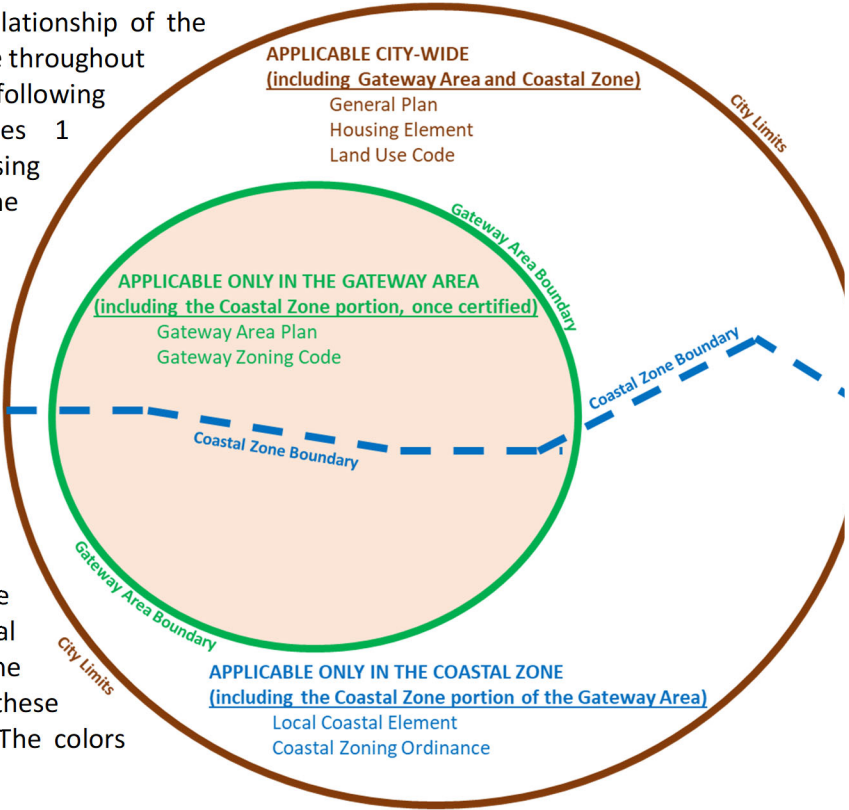
OBJECTIVE	RELEVANT DOCUMENTS				
	General Plan			Municipal Code	
	General Plan	Housing Element	Gateway Area Plan	Gateway Zoning Code	Land Use Code
Establish an overall vision for the entire City.	X				
Establish broad policies that apply City-wide, including within the Gateway Area. These policies are crafted to support the vision for the overall City.	X	X			
Analyze the City's housing stock and outline broad policies for meeting future housing needs, including within the Gateway Area.		X			
Establish a vision for the Gateway Area that are more detailed than the vision presented in the General Plan and Housing Element.			X		
Establish specific policies for the Gateway Area that are more detailed than the policies presented in the General Plan and Housing Element.			X		
Outline general strategies for implementing the future vision for the Gateway Area.			X		
Define the specific detailed development and design standards for the Gateway Area. These standards are meant to enact the vision and policies of the Gateway Area Plan.				X	
Identify permit requirements and processes for development activities.				X	X
Define general development standards (e.g. building heights, subdivision standards, environmental requirements, administration adjustment standards, etc).					X

Table 2: The Primary Objectives of City Regulatory Documents Applicable within the Plan Area

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Image 1: Geographic Applicability of City Land Use Regulatory Documents

The graphic to the right (Image 1) shows the relationship of the various regulatory documents that control land use throughout the City. The colors of this image and the following paragraphs match the color code in Tables 1 and 2. As Image 1 shows, the General Plan, Housing Element, and Land Use Code apply throughout the entire City, including within the Coastal Zone portion of the City, within the Gateway Area, and within the Coastal Zone portion of the Gateway Area. Two documents apply exclusively within the entire Gateway Area, including the Coastal Zone portion of the Gateway Area: the Gateway Area Plan (this document) and the Gateway Zoning Code. Neither of those two documents apply outside of the Gateway Area. Then, two documents apply through the entire Coastal Zone portion of the City, including within the Coastal Zone portion of the Gateway Area: the Local Coastal Element and the Coastal Zoning Ordinance. As the following paragraphs explain, several of these documents are contained within one another. The colors correspond to Tables 1 and 2 as well as Image 1.



General Plan

The City of Arcata’s General Plan outlines the comprehensive objectives the community seeks to achieve in the areas of lan use, growth management, transportation, open space, health and safety, housing, and related topics. Applies City-wide.

Housing Element

Includes policies to help remove barriers to housing development. Applies City- wide. The General Plan Policy Area are contained within General Plan Elements. The following Elements are discussed in detail here because the closely relate to the subject matter covered in this Element.

Gateway Area Plan

This document, which is an Element of the General Plan that applies specifically and exclusively to the Plan Area.

Local Coastal Element

An Element of the General Plan that contains the land use policies within the Coastal Zone portion of the City.

Land Use Code (a part of the zoning ordinance contained within the Municipal Code under Title IX)

A section of the City’s Municipal Code that implements the overall goals and policies of the General Plan with detailed an specific development standards, allowances, and requirements.

Coastal Zoning Ordinance

Regulates development standards in the Coastal Zone portion of the City. The Coastal Zoning Ordinance is a combinatio of standards specifically designed for regulating activities in the coastal zone consistent with the Coastal Act as well as are or City-wide regulations certified in the coastal zone to streamline permitting.

Gateway Zoning Code

A section within the City’s zoning ordinance with detailed and specific development standards, allowances, an requirements applicable specifically and exclusively to the Arcata Gateway Plan Area.

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Overriding Authority of Regulatory Documents

The documents outlined above are interrelated with one another and some govern over others in specific circumstances: As a General Plan Element, the Area Plan refines the citywide goals and policies by addressing relevant Plan Area issues for more specific guidance on other Plan elements. Therefore, in the case of an Area Plan and General Plan conflict, the Area Plan prevails within its boundaries. However, the General Plan provides guidance on all issues not covered in the Area Plan.

Likewise, the Gateway Zoning Code's specificity supersedes more general development standards in the Land Use Code. However, the Land Use Code provides guidance on matters not covered in the Gateway Zoning Code, such as permit processing and subdivision regulations. The Land Use Code is overruled where there is more specificity in the Coastal Zoning Ordinance and/or the Gateway Zoning Code.

The Coastal Land Use Element rules where there is conflict among standards between the Gateway Area Plan and the Coastal Land Use Element. Similarly, the Coastal Zoning Ordinance supersedes the Gateway Zoning Code when conflicts exist.



The Gateway Area Boundary, looking south (see Figure 1 for more detail).



The southern half of the Gateway Area, looking south (see Figure 1 for more detail).

How To Use This Plan

This Area Plan is designed to be easily understood and referenced by a range of users, including property owners, developers, business owners, general community members, elected/appointed officials, and City staff. To help navigate the main components of the document, the following steps are a quick way to understand the different sections of the plan.

Step 1: Confirm the project site or your site of interest is within the Gateway Area

The Plan provisions only apply within its boundaries.

Step 2: See if your project or desired undertaking fits the vision of the Gateway Area

Briefly review the vision chapter to see if your project satisfies or complements the City's vision for the Gateway Area. With the multitude of mixed-use possibilities and emphasis on equity, it is likely there is place for you in the Gateway.

Step 3: Identify the appropriate Land Use Designation (sub-area)

The Gateway Area is sub-divided into four sub-areas or Land Use Designations. Locate your proposed project location or site of interest on Figure 4 to determine which land use designation applies.

Step 4: Evaluate your project's contribution to targeted types and mixes of land use

Review "Table 5: Target Land Use Mixes" to evaluate if or how your project contributes the City's targeted volume and mix of land uses. This Plan heavily favors a diversity of high-density residential development.

Step 5: Determine if your project provides any designated community benefits and adjust project as needed

Review the program outlined in the "Community Benefits and Development Standards" chapter to determine how your project could offer desired community benefits. Consider modifying your project to add more community benefits in order to receive greater development potential and streamlined permitting processes.

Step 6: Determine your project's allowed and required development standards

Review "Table 7: Development Standards for Base Level Projects and Community Benefit Projects" to determine the allowed building height of your project, as well as the required residential density and other basic development standards. Projects that provide more community benefits are granted enhanced levels of development.

Step 7: Review the policies throughout the Plan

Determine the appropriate policies throughout this document that apply to your project. Policies have been established for land use, equitable housing, employment, arts & culture, historic resources, building design, mobility, streetscape design, open space & conservation, and infrastructure. Some policies will need to be assessed on a parcel-by-parcel basis, such as creek daylighting, but others apply to the all properties in the Area, such as form-based design standards.

Step 8: Review the Gateway Code and, if applicable, the Local Coastal Program/Element

Review the Gateway Zoning Code, a City zoning code (a.k.a. "Land Use Code") section that applies specifically and exclusively to the Plan Area. The document implements the policies of this Area Plan and outlines the specifics of what is required for Gateway Area projects.

Step 9: Speak with a City Planner

The City encourages you to speak with a planner for any questions about how this Area Plan applies to your project or the application process.

Step 10: Follow the Appropriate Application Process

Your planner will inform you as to next steps and required application materials. Submitting a complete and detailed application package to the City that follows the policies, development standards, and design guidelines presented in this plan will ensure timely processing so you can get started on your project.

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CHALLENGES AND OPPORTUNITIES

The process of preparing this document included extensive community engagement to identify the concerns and aspiration of existing and prospective residents, workers, business owners, and property owners. The public input gathered during Plan development was an extension of similar work conducted under the City's Housing Element, which also included substantial public engagement. That document revealed a collective interest in conserving Arcata's natural resources and creating more opportunities for housing by using infill redevelopment as a primary solution to the City's housing shortage. The Housing Element subsequently mandated that the Gateway Area is to be rezoned to allow "high density infill development." To understand the feasibility of utilizing the Gateway Area as a focused redevelopment area, the City conducted detailed analyses of real estate market conditions and Area demands (see the Arcata Infill Market Study), which also included broad public engagement. Both of those documents confirm the City's overall drive towards utilizing infill redevelopment to produce housing. This is consistent with the long-standing principles and values of Arcata, which include the preservation of open spaces around the periphery of the City, an aversion to sprawling low density development, efficient use of the existing built environment, and a commitment to sustainability and low-impact development.

In addition to confirming a consistent interest in infill redevelopment, the public processes of developing this plan, the Housing Element, and the Infill Market Study also uncovered eight key challenges that concern the current and prospective populace of Arcata. A primary motivation of this Gateway Area Plan is to guide development within the Plan Area as a means of resolving these challenges and/or turning them into opportunities.

The primary challenges identified by the community are:

1. **Housing Needs and Limited Land:** Fulfilling the community's housing needs with a limited amount of available land.
2. **Racial Equity and Social Justice:** Building a community based on genuine racial equity and social justice.
3. **Market Constraints:** Producing new housing that is affordable at all income levels and constructing new buildings for job-creating uses within a challenging economic market.
4. **Growth Management:** Facilitating community-supported high-density development that retains what people love about Arcata and is consistent with the character and style of Arcata while also providing preservation of historical resources.
5. **Climate Change and Sustainability:** Reducing greenhouse gas emissions and contributing to solutions to climate change.
6. **Unfulfilled Identity:** Reducing blight, fulfilling the potential of underutilized properties, and transitioning away from antiquated land uses, all while satisfying the community's desire to create a part of town that is rich in vibrant art and culture.
7. **Jobs and Entrepreneurial Opportunities:** Providing the right balance of employment and entrepreneurial opportunities for existing and future residents.
8. **Infrastructure, Circulation, and Parking:** Addressing the cost and complexity of planning, designing, and constructing new infrastructure, including a transportation system that serves everyone across all modes of transportation, while also supplying sufficient parking spaces.



HOUSING NEEDS AND LIMITED LAND

Challenges Related to Housing Needs

The State of California has an inadequate housing supply at all income levels, which is also true of Humboldt County and the City of Arcata. In Arcata, the housing shortage could increase in the coming years, especially with projected HSU expansions and related student, faculty, and administration housing demands. In addition, the area is experiencing in-migration due to climate, economic pandemic, and other related causes. These trends may increase in the future. There is also a limited variety of housing types throughout Humboldt County in general, with single-family and low-density multi-family residential serving as the dominant forms of housing. While Arcata provides some degree of higher-density and mixed-use residential housing options, housing variety is still dominated by single-family development patterns and relatively low-densities. The housing shortage is also not unique to any income level; finding housing is a challenge for unemployed students as well as for doctors moving to the area, though the shortage is inherently more consequential for low-income individuals and families. The most significant component of this challenge is that Arcata has a limited amount of developable land remaining with which to plan for housing growth. The City has reached its horizontal growth limits around its periphery, surrounded by forested hillsides, coastal wetlands, coastal agricultural lands, and protected greenbelts. The City can only provide a meaningful amount of new housing through vertical infill.

More specific to the Plan Area, while it is centrally located within the City and directly adjacent to the Downtown core, the Plan Area currently has very little housing and contains many vacant and underutilized lots. While the Plan Area is geographically ideal for housing, the Area is currently dominated by legacy industrial land uses (such as warehousing and storage), though there are pockets of high value job centers, some residential clusters, and a sprinkling of commercial businesses. Transitioning from the current patchwork-development pattern to a denser, more cohesive mixed-use-residential pattern with high quality community amenities will require a clear vision and actionable implementation strategy. Additionally, the existing residents and businesses of the Plan Area could be displaced if the Plan Area is not planned appropriately, presenting additional challenges that will need to be addressed.

Opportunities to Address Housing Needs

From a City-wide perspective, the Gateway Plan Area has by far the best potential to create new housing units of any priority infill area identified in the City's 6th Cycle Housing Element. The Plan Area also has the greatest potential to accommodate a much-needed increase in the diversity of housing types, such as mixed-income, high-density, multi-story residential towers, owner-occupied affordable housing, affordable-by-design studios, student housing, mixed-tenure condominiums, market-rate units, and luxury penthouses. Many of the City's unmet housing needs challenges are intended to be addressed with strategically planned Gateway Area residential growth as discussed and analyzed in the City's 6th Cycle Housing Element. The Plan Area can provide volumes of new housing types that have historically been underdeveloped Countywide. The envisioned volume and range of new housing types will also provide an opportunity to minimize the impacts of gentrification and displacement from redeveloped properties.



GUIDING RACIAL EQUITY & SOCIAL JUSTICE

Challenges Related to Racial Equity & Social Justice

The United States has suffered from and perpetuated centuries of racial injustice and social inequity. Both locally and nationally, people of color fare worse than their white counterparts across quality of life metrics, including housing, employment, education, criminal justice, and health. Arcata strives to be a just and equitable community, but substantial work is needed to make that vision a reality. While acknowledging historical context and the systemic harm done to specific racial groups in Arcata, Humboldt County, and the United States more broadly, the City intends to aid in the creation of equitable outcomes for people of all races. This work began in part in 2017 with the formation of Equity Arcata, a racial equity partnership between the City, Cal Poly Humboldt Humboldt State University, local businesses, and members of the community that strives to make Arcata a more inclusive and community. Those who implement this Plan will be tasked with carrying forward the work of creating an anti-racist community that is already underway.

Opportunities to Address Racial Equity & Social Justice

This Plan provides the City with opportunity to stimulate development designed to decrease the role that race plays as a determinant of life outcomes. It has been shown that racialized, inequitable systems stifle opportunities and depress outcomes across all groups; therefore, addressing racial inequity directly in this Plan will ultimately benefit all Arcata residents. Through implementing measures that identify equity as their central tenet across various sectors—including housing, employment, and recreation—the City can lift all populations by paying specific attention those historically excluded. Proposed anti-racist policies and implementation tools will be elaborated on throughout the Plan.



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MARKET CONSTRAINTS

Challenges Related to Market Constraints

The cost of construction, and specifically the cost of housing development, is at an all-time high. But construction cost has out leveraged local wages, the ability to pay for a cost effective return on investment, for decades. This is major contributor to the housing shortage because the cost of construction limits housing supply even while the demand for new housing continues to grow. While a diversity of high-density housing types could contribute to the solution, the majority of local construction companies have little experience with high-density housing. In addition, community members have been reticent in the past to support higher densities and taller buildings, making such development potentially risky and therefore less appealing to developers. As Identified in the City’s Infill Market Study, City permitting processes can also be lengthy and complex, thereby making development even more expensive. The potential for environmental contamination at key opportunity sites within the Plan Area also has the potential to increase development costs, adding to the challenge.

Opportunities to Address Market Constraints

The City has little ability to reduce the regional cost of construction. However, the City can ease development costs by lessening the time and complexity of permit approval processes and providing a path to streamlined environmental review. In exchange, the City can require a range of community amenities that will ensure development meets the needs and objectives of the broader community while simultaneously allowing higher densities than what has historically existed in the Gateway Area, modifying the scale and feel of the Area to more closely match Arcata’s downtown Plaza core.



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GROWTH MANAGEMENT

Challenges

A notable number of properties within the Plan Area are either vacant or underutilized. The residential development that does exist within the Plan Area is well below the currently allowed density, while some of the industrial properties consist of largely vacant former mill yards that may present issues with historic contamination. Very low intensity uses dominate large areas, such as storage units, utility corporation yards, and a car wash. While the City's Rail-to-Trail project traverses through the heart of the Plan Area, the Area has no formal parks or official public gathering spaces. Partially-daylighted segments of Jolly Giant Creek cut through the Plan Area and are unsafe to access where obscured by dense vegetation that lacks a sense of safety. Unmaintained extant creeks and wetlands can attract unauthorized camping. This all makes the Plan Area ideal for new development. However, resistance to infill growth has been a factor in the City in the past and could be a challenge moving forward.

Opportunities

Underutilized Plan Area properties offer substantial new mixed-use development opportunities, including high density mixed-tenant residential, vibrant community-oriented commercial, and neighborhood-scale light manufacturing that provide employment opportunities. There is also an opportunity to create pedestrian-friendly public spaces, including enhanced creek daylighting and a full realization of the Rail-to-Trail project. There are also opportunities to enhance wetland/creek restoration in the northern corner of the Plan Area as well as at the southwestern boundary. Growth can be managed within reasonable community expectations and according to design standards compatible with Arcata's character. Perhaps most important, dense infill redevelopment in the Plan Area will relieve development pressure in other parts of the City, especially within the forested hillsides, coastal wetlands, coastal agricultural lands, and protected greenbelts around the community's periphery, all of which are highly valued by the public.



CLIMATE CHANGE AND SUSTAINABILITY

Challenges

New growth and new development come with an environmental cost. More people typically means more energy use, more cars, more waste generation, and more greenhouse gas emissions. As the climate warms due to human activities, the next generation of Arcatans will face challenges at a scale that no generation has previously faced. As an example, sea level rise will displace local residents, furthering other challenges listed above. Such displacement will exacerbate the housing shortages, will intensify market demands, and put remarkable pressures on critical infrastructure. These types of challenges need to be actively acknowledged and mitigated as the community plans for growth in the Gateway Area. This includes prioritizing non-motorized transportation, electric vehicle charging infrastructure, and green buildings. This also means that the City must find ways to reduce greenhouse gas emissions while simultaneously adding population and new buildings. As a related challenge, the Gateway Area also includes several contaminated and potentially contaminated sites that will need to be cleaned up.

Opportunities

Arcata has the opportunity to prove its commitment to sustainability by demonstrating how infill redevelopment can provide housing, jobs, and entertainment, with a minimal environmental footprint. The City can also showcase how infill in the already-developed Gateway Area can relieve future development pressure in other parts of the City, thereby protecting the greenbelts and agricultural areas around the community's periphery.

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UNFULFILLED IDENTITY

Challenge

A great deal of heart, social capital, and strategic planning efforts have been invested into envisioning an identity for the “Creamery District,” which is a “special arts and commerce district surrounding the former California Central Creamery” building. The Creamery District is contained entirely within the Gateway Area. A Creamery District vision has been established, and there’s been remarkable progress towards achieving that vision. This includes repurposing the Creamery Building into a cherished entertainment destination. The rest of the Plan Area has yet to realize its own unique potential and there is still much to be accomplished. The Plan Area has a lack of aesthetic cohesion and a fragmented sense of identity; building forms, land uses, and infrastructure systems all vary. There are also concerns regarding both blight and safety, which is partially a reality and partially a problem with perception. And, while the Plan Area contains the heart of the City’s Rail-to-Trail, the Plan Area overall is not as pedestrian-friendly as has been envisioned by the general community with few clear connections to the Downtown. An antiquated set of land uses (such as mini-storage and warehousing) hold back the Area’s potential, while blight and underutilized properties bring down property values. In addition, the City is actively working on finalizing and then implementing a City-wide Strategic Arts Plan, much of which will include expectations for the Plan Area.

Opportunity

Implementation and realization of the Creamery Plan Area vision as an “Arts District” will not only substantially transform this part of town, but will also contribute greatly to the spirit of Arcata and the overall culture of the region. The Plan Area can be thoughtfully-designed with a cohesive sense of identity, including features and amenities that minimize any perceived lack of safety while transitioning the Plan Area to become pedestrian-dominated. To some degree, new growth within the Plan Area should radiate outward from the Rail-to-Trail and the Creamery Building. In addition, streamlined permitting processes, modernizing land uses, and creating custom design standards can stimulate the construction of new buildings and the filling of key opportunities sites, all of which will eliminate blight and contribute to a cohesive neighborhood identity.

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JOBS AND ENTREPRENEURIAL OPPORTUNITIES

Challenges

The majority of the Plan Area is currently zoned for light industrial uses and provides over 100 middle-income job opportunities. Currently, the Plan Area is home to several major employers, with a robust business mix. However, the envisioned amount of residential growth could quickly outpace the number of businesses, skewing the balance of housing and jobs. Along with the growth in residential and commercial uses, the Plan Area will also need to see growth in office-based and light-industrial jobs to provide living wage career opportunities for both blue-collar and white-collar professionals who choose to work and live in the Plan Area. As envisioned residential growth occurs, the Plan Area could not support a “car-free lifestyle” without growth of commercial, retail, and entertainment uses. For the Plan Area to be truly “mixed use” there will need to be more neighborhood-scale commercial and job-generating uses. In addition, the existing Plan Area uses are largely disconnected from one other with very little sense of unity and primary arterials are not being utilized as the vibrant neighborhood circulation features they could be.

Opportunities

Abundant employment opportunities will be necessary for the Plan Area to offer a truly “car-free lifestyle.” Fortunately, the entrepreneurial spirit among residents and businesses in the Plan Area is strong, with several Arcata gems such as the Creamery Building, the Kinetic Lab, Holly Yashi, Wing Inflatables, Café Mokka - Finnish Country Hot Tubs, and many others. There is a great opportunity to build on this spirit and facilitate more home-grown businesses, both by encouraging new entrepreneurs and protecting/supporting the interests of pre-existing business owners within the Plan Area. The Plan Area could be highly walkable and easily-accessible via transit, making employment opportunities easier to access. This plan envisions the Plan Area to become a neighborhood-scale shopping and entertainment destination as well as a place where people can both live and work. The Area has the opportunity to provide a robust nightlife, ample food choices, and many things to do for residents and the broader community. This should occur in a well-planned and compatible manner that increases the market value of neighboring properties while contributing to the vibrancy of the City overall. Such community-oriented mixed-use will allow the Plan Area to be more walkable and will decrease Vehicle Miles Traveled. The street has the opportunity to become a welcoming place, with public restrooms, trash receptacles, pedestrian-scale street lighting, street trees, and wide sidewalks.



INFRASTRUCTURE, CIRCULATION, AND PARKING

Challenges

While the current Plan Area physical infrastructure is adequate to serve current demands, substantial growth would require infrastructure upgrades. This is true of the bicycle and pedestrian network, the vehicular circulation system, parking infrastructure, and various utilities (water, wastewater, etc.). The infrastructure upgrade costs may further hinder development and hold back the ability of Plan Area to address housing needs. In addition, a relatively large volume of traffic currently utilizes K Street (via Samoa Blvd) to enter the City from the south, while 11th Street serves as a primary access to the City from the residential neighborhoods to the west. Circulation upgrades must be planned and designed for complete streets with multi-modal features. Additionally, there is underdeveloped connectivity from the Plan Area to the core Downtown, the Plaza, Cal Poly Humboldt, Humboldt State University, Arcata High School, and other key nodes throughout the City. Some community members have also expressed concerns about parking availability into the future. Finally, detailed utility studies will be needed to understand if and how water, wastewater, storm water, communications, and electrical utilities will need to be upgraded to serve new growth.

Opportunities

If substantial growth is going to occur in the Gateway Area, and if such growth is going to lead to the development of new infrastructure, then Arcata has the opportunity to create cutting-edge, sustainable, equitable, and efficient new infrastructure about which the community can be proud. New non-motorized circulation systems can be created that provide improved connections to the Plaza, Valley West, and downtown Eureka via trails and rapid transit bus lines. With intentional design, the Plan Area can look and feel like a more welcoming and vibrant “gateway” into the City from the south and west, while also improving street and pedestrian networks. Upgrades to all modes within the circulation system and efforts to establish “complete streets” along the new gateway corridors can also be expanded into other parts of the City, thereby increasing multi-modal connections to key destinations, including the Creamery District and larger Gateway Area. There is also an opportunity to prioritize non-motorized transportation, reduce vehicle trips from other parts of the City, and fulfill the potential of the Rail-to-Trail project. Reconfiguring the street network also provides opportunities to increase the quantity and quality of on-street parking. Utility upgrades can be master planned. And the future stormwater system can consist of daylighted creeks and groundwater recharge basins.



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SECTION B: VISION



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GATEWAY AREA VISION STATEMENT

The Arcata Gateway Area is a vibrant, mixed-use, community-oriented blend of transitional neighborhoods with the Creamery District at its physical core and entrepreneurial heart in its community center.

The Gateway Plan lifts these values **to grow opportunity and build community equitably.**

It is mixed-use, mixed-tenure, high-density, mixed-income, thoughtfully-designed, pedestrian-friendly, community-facing, sustainable, equitable redevelopment.

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GUIDING PRINCIPLES

To overcome the challenges presented in the previous section and to achieve the vision outlined above, the Arcata Gateway Area Plan is built on six guiding principles. The principles connect the overarching vision with the Plan's policies and the subsequent regulations. These principles will serve as a reference point for stakeholders and decision-makers as projects are reviewed. As Table 3 below outlines, each guiding principle addresses one or more of the challenges.

Table 3: Relationship of Guiding Principles and Challenges

GUIDING PRINCIPLES	CHALLENGES AND OPPORTUNITIES							
	Housing Needs and Limited Land	Market Constraints	Growth Management	Racial Equity and Social Justice	Climate Change and Sustainability	Unfulfilled Identity	Infrastructure	Jobs and Entrepreneurial Opportunities
1. Stimulate the Creation of a Volume and Variety of Housing	X	X	X	X		X		X
2. Promote Racial Equity, Social Justice, and Diversity	X	X	X	X	X	X	X	X
3. Promote Thoughtful Development Through Form-Based and Streamlined Development Standards	X	X	X	X	X			
4. Promote a Balance of Mixed Uses to Create a Neighborhood District That Offers a Car-Free Lifestyle	X	X	X	X	X		X	
5. Design Circulation Improvements to Accommodate Planned Growth and Minimize Vehicle Trips				X	<u>X</u>	X		
6. Expand the Arts and Celebrate Cultural Identity				X		X		X
7: Plan for Environmental Restoration and Sustainability Features				X	X		X	

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Each of the six Guiding Principles is outlined in detail in the following pages.

GUIDING PRINCIPLE 1: STIMULATE THE CREATION OF A VOLUME AND VARIETY OF HOUSING

Overcoming the housing shortage that Arcata is facing will require bold plans and actions. This first guiding principle is intended to overcome these challenges by generating a substantial amount of new housing units at a scale, density, and a variety previously unseen in the City. This will be accomplished by ensuring that the City:

- Plan for up to 3,500 new residential units with the Gateway Area that are available to a wide range of income level and that include a balance of renters and owners.
- Incentivize a wide range of affordable and equitable housing types, including affordable-by-design studios, student housing, mixed-tenure condominiums, and housing for low-income families.
- Encourage the creation of market-rate units for singles and families as well as upper income units, such as luxury penthouses. Promote the benefits of truly mixed income neighborhoods.
- Require that all new development requires at least some minimum quantity of housing units.



GUIDING PRINCIPLE 2: PROMOTE RACIAL EQUITY, SOCIAL JUSTICE, AND DIVERSITY

This Guiding Principle outlines a framework for the City to intentionally integrate fairness and justice into all planning and development activities within the Gateway Area so that factors of identity – such as race, ethnicity, gender, age, disability, sexual orientation or expression – have no detrimental effect on the distribution of resources, opportunities, or outcome for anyone. Accordingly, this Plan has policies and implementation tools structurally designed as anti-racist with the intention of dismantling racial inequities. The Plan calls on those who will implement these policies to carry this work forward to create an anti-racist community. This will be accomplished by ensuring that the City:

- Acknowledge, support, and collaborate with the First Peoples of the region.
- Recognize past and present inequities.
- Honor cultural, racial, and gender diversity.
- Promote socioeconomic mobility by incentivizing development that grows opportunities.
- Identify and implement measures to counteract gentrification and/or the displacement of residents as a result of intensive redevelopment of the Gateway Area.
- Intentionally integrate the principle of 'fair and just' in all the City does in order to achieve equitable opportunities and outcomes for all people and communities.
- Work to identify and eliminate the root causes of inequity—resulting in more effective and equitable City policies, processes, and services.
- Ensure that all public processes are transparent, easy-to-understand, and accessible through a variety of community-based sources, organizations, and languages.



GUIDING PRINCIPLE 3: PROMOTE THOUGHTFUL AND ATTRACTIVE DEVELOPMENT THROUGH FORM-BASED AND STREAMLINED DEVELOPMENT STANDARDS

As described in the Challenges section, the City is experiencing a shortage of housing that is expected to get worse over time. Market constraints exacerbate this problem, meaning that the imbalance between demand and supply will continue to increase. At the same time, the community clearly demands that new housing comes as a part of well-planned growth. This first guiding principle is intended to overcome these challenges by streamlining development standards that include design standards that meet the community's expectations for new development. This will be accomplished by ensuring that the City:

- Establish ministerial permitting options and streamlined development processes for projects that provide designated community amenities or otherwise facilitate the guiding principles.
- Maximize flexibility in uses, with targeted limitations on land uses that do not contribute to the vibrancy of the Gateway Area.
- Create form-based design standards that allow high-density multi-story buildings while simultaneously requiring vibrant, community-oriented, street-facing built environment designed to fit a "human-centered" scale.
- Plan for custom development standards along key gateway segments and nodes, particularly along Samoa Boulevard, K Street and 11th Street, with the objective of generating a more welcoming and vibrant "gateway" into the City.



GUIDING PRINCIPLE 4: PROMOTE A BALANCE OF MIXED USES TO CREATE A NEIGHBORHOOD DISTRICT THAT OFFERS A CAR-FREE LIFESTYLE

A natural demand for commercial uses and job-generating uses will occur as the City successfully generates new housing units. The Plan Area could be exclusively residential, with the subsequent commercial and job-related uses found elsewhere in the City. However, the public has made clear an interest to create a place where residents have the option to live car free. The Plan Area will compliment other nearby employment centers by including a range of commercial and light industrial compatible manufacturing uses mixed with residential uses. This will be accomplished by ensuring that the City

- Facilitate genuinely "mixed-use" development, with residential, retail, office, and light-manufacturing use thoughtfully blended together with new public open spaces to create a newly cohesive neighborhood that feel complete.
- Cultivate the creation of a distinct neighborhood that offers a truly "car-free lifestyle" by promoting job-creating uses and neighborhood commercial uses to balance the planned growth of residential units. Promote the area a "a district that has it all" as well as a district that will contribute to and support existing businesses in the rest of City
- Strategically integrate new uses harmoniously with existing uses, both within and adjacent to the Plan Area.
- Protect and complement the historic charm of legacy structures while allowing for substantial growth and development on vacant and underutilized parcels.



GUIDING PRINCIPLE 5: DESIGN CIRCULATION IMPROVEMENTS TO ACCOMMODATE PLANNED GROWTH AND MINIMIZE VEHICLE TRIPS

Promoting dense and diverse redevelopment and new development in the Plan Area will require an updated circulation system that supports increased demand for all forms of mobility – vehicles, trucks, transit, bicycles, and pedestrians, each of which will allow for accessibility. All of these mobility improvements will support and facilitate many of the other guiding principles and can help ease several of the challenges outlined above. A new, sustainable circulation system presents the opportunity to minimize new vehicle trips and parking demand in the Plan Area. This will be accomplished by ensuring that the City:

- Modify and update the circulation system to expand availability of safe and comfortable bicycle and pedestrian facilities, while managing vehicular travel to minimize adverse impacts from traffic noise and congestion.
- Reduce vehicle trips and parking demand from other parts of the City by creating convenient and safe pedestrian and bicycle corridors that draw residents and visitors into the Plan Area via means other than motorized vehicles.
- Connect Gateway Area to the Downtown/Plaza core with safe and attractive walking and bicycling routes suitable for all ages and abilities, incorporating wayfinding, public art, and street lighting, furniture, and amenities.
- Fulfill the potential of the Rail-to-Trail project by planning for expanded perpendicular connections that invite exploration and draw recreational cyclists, runners, and pedestrians from beyond the Plan Area.
- Where appropriate, close gaps in street blocks with bicycle and pedestrian trails accessible to people of all ages and abilities. For community cohesion and public safety purposes, maintain lines of-sight for visual continuity. Whenever vehicular roadways are eliminated, be sure to retain the pedestrian and bicycle facilities to avoid creating new gaps for non-vehicular travel.
- Provide enhanced ADA accessible and universal accessibility designs when possible.



GUIDING PRINCIPLE 6: SUPPORT THE ARTS AND CELEBRATE CULTURAL IDENTITY

Since 2012, the “Creamery District” has been a program of the non-profit organization “Arcata Playhouse” which also house Playhouse Arts, the official “arts agency” for the City of Arcata. The organization’s mission is to build community through the arts and “create a vibrant, diverse community where artists and entrepreneurs prosper,” while the tagline of the Creamery District is “Where Arts and Commerce Meet.” As a budding arts and culture commerce cluster that falls entirely within the Gateway Area, the Creamery District is locally recognized as a primary driver for expanding the role and recognition of the arts as an integral part of a healthy and vibrant community. This Area Plan seeks to build on already established momentum. This will be accomplished by ensuring that the City:

- Build upon past planning efforts to create the Creamery Arts & Culture District and honor the well-established entrepreneurial spirit and powerful identity that has been nurtured and organically cultured for years.
- Incentivize the integration of all forms of art into new development, including murals, sculptures, performing arts spaces, and anything that is funky, eye-catching, or creative.
- Promote, prioritize, integrate, and incentivize housing integrated with or nearby workspaces for artists.
- Identify opportunities to integrate arts and cultural amenities into streetscape designs and open space plans.



GUIDING PRINCIPLE 7: PLAN FOR ENVIRONMENTAL RESTORATION AND SUSTAINABILITY FEATURES

The City of Arcata values environmental sustainability, meaning protecting natural resources and requiring efficient use of land, water, and energy. This Plan includes strategies to reduce sprawl, minimize energy use, reduce vehicle trips, decrease waste generation, and reduce greenhouse gas emissions. This includes prioritizing non-motorized transportation, electric vehicle charging infrastructure, and “green” buildings. Another sustainability component is contaminated sites cleanup and where possible, habitat restoration. This will be accomplished by ensuring that the City:

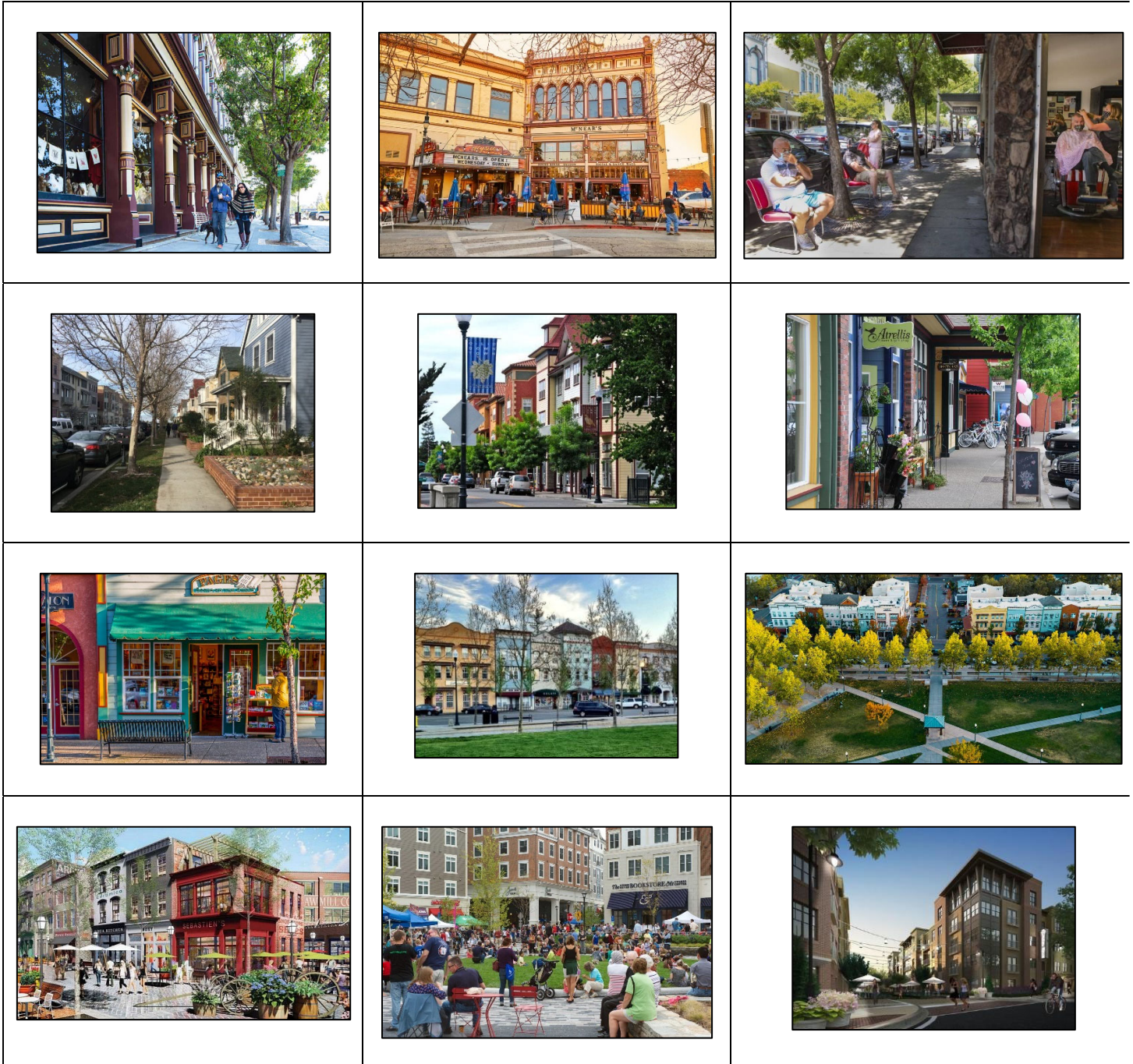
- Evaluate the possibility of daylighting Jolly Giant Creek within street rights-of-way, which could include eliminating or reducing vehicular traffic and parking on specific opportunity blocks.
- Incentivize the daylighting of Jolly Giant Creek on private properties.
- Incentivize the creation of privately-owned public spaces that can serve as community parks.
- Programs to assist private property owners with hazardous waste cleanup.
- Wetland mitigation banking/connection to Arcata Bay Trail.
- Form-based code standards for green buildings, electric vehicle charging, rainwater management, and incentive for open space, creek daylighting, and community gardens.



INSPIRATION FROM OTHER COMMUNITIES

The following images are examples of streetscape designs, architecture, building heights, and densities from other communities that this Plan considers as inspiration for the types of development that is envisioned for the Gateway Area.

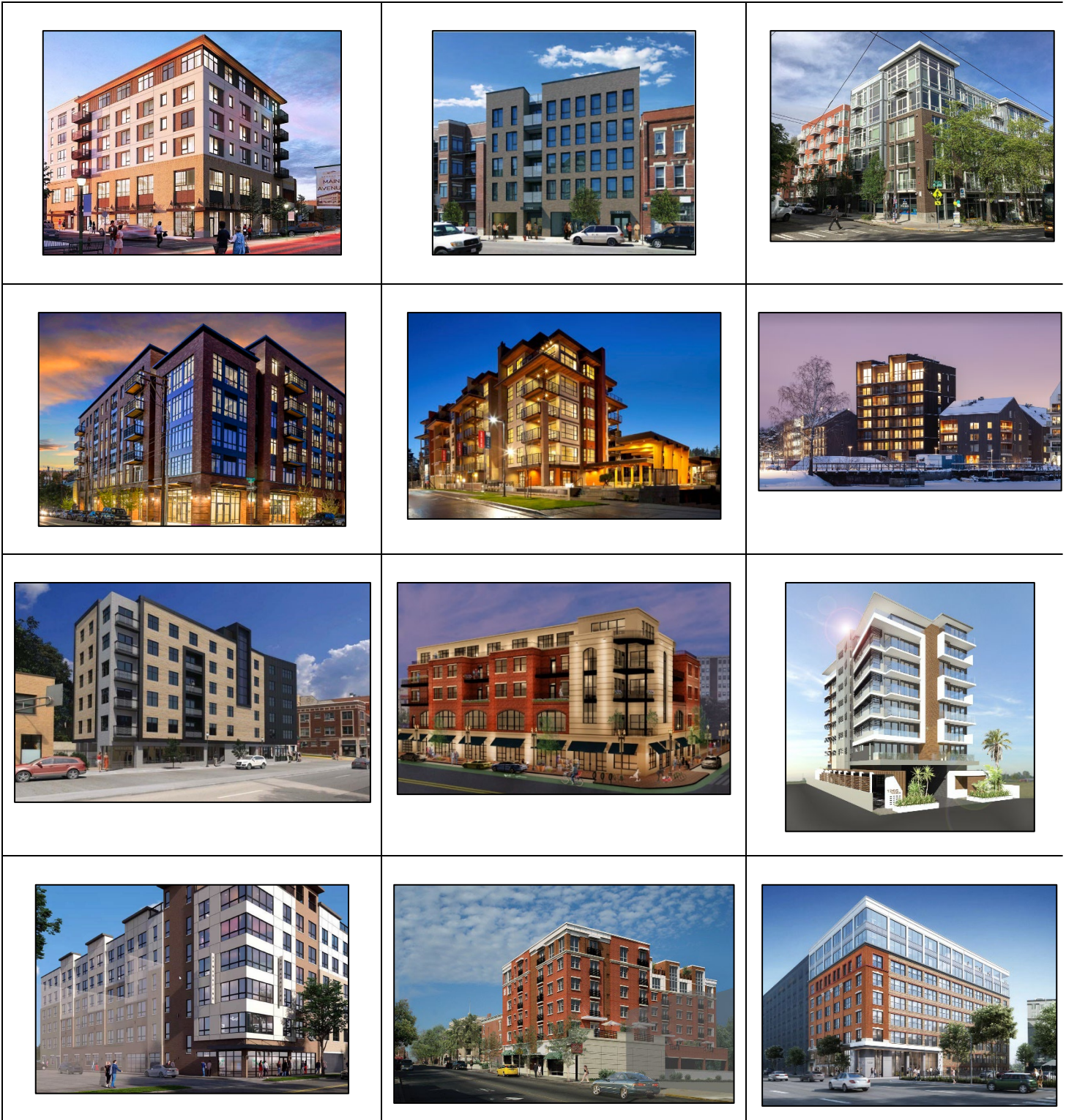
EXAMPLES OF QUALITY STREETSCAPES



These images are provided for illustrative purposes only to display the types of development envisioned for the Gateway Area.

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

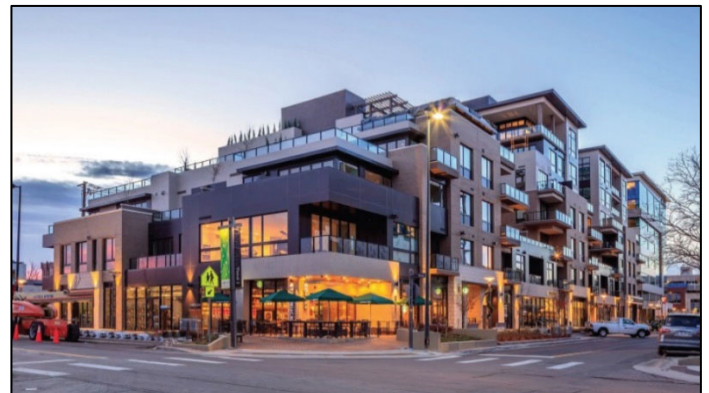
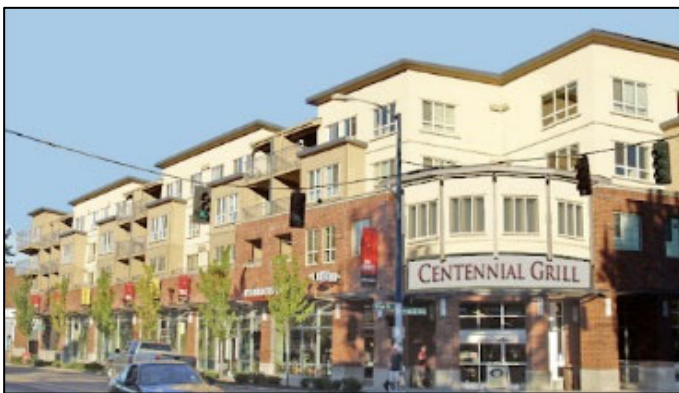
EXAMPLES OF BUILDINGS ON THE UPPER END OF ENVISIONED HEIGHTS AND DENSITIES:



These images are provided for illustrative purposes only to display the types of development envisioned for the Gateway Area.

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

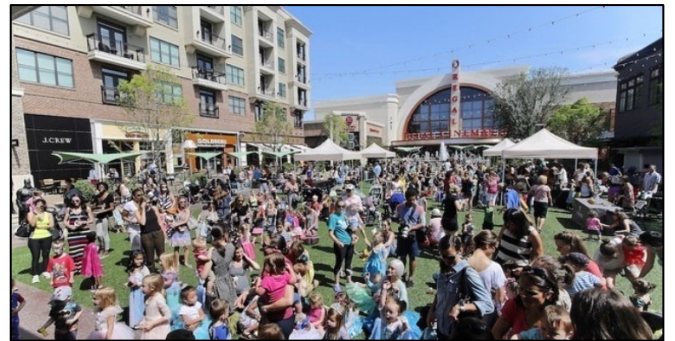
EXAMPLES OF UPPER FLOOR SETBACKS



These images are provided for illustrative purposes only to display the types of development envisioned for the Gateway Area.

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

EXAMPLES OF QUALITY OPEN SPACES INTEGRATED INTO DENSE DEVELOPMENT



These images are provided for illustrative purposes only to display the types of development envisioned for the Gateway Area.

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

KEY OPPORTUNITY SITES

As discussed above, the Gateway Area is optimally located for redevelopment. The Area is within walking distance to the Downtown core, Arcata High School, Cal Poly Humboldt Humboldt State University, and several other key destinations making it an ideal location for high density residential development. Future residents of the Gateway Area will have easy access to essential services and job opportunities. Remarkably, despite its optimal location, the Area contains many vacant underutilized and blighted sites. Many parcels contain legacy single-story industrial or residential buildings. Table 4 and Figure 3 below evaluate the sites most readily available for redevelopment. Given the importance of generating a volume of new residential units, each site is analyzed for its residential development potential under the “Opportunities” column. The range of residential units that each site could generate is calculated at a medium density residential standard (1 dwelling units per acre) and at a moderately high-density standard (50 dwelling units per acre). While this plan does not envision establishing minimum residential density standards, the 18 du/ac baseline provides a conservative estimate of what may actually be constructed since not all developers seek to achieve the maximum allowed units. For sites where wetland or other constraints could reduce the potential for residential units, the table assumes that the City would allow each property to retain its overall gross density on the net developable portions of the site. It is also important to note that there are many other sites that could potentially be listed as opportunity sites. These were selected because they either have characteristics that make them particularly well-suited to redevelopment or because they are somewhat blighted and redevelopment would generate much-needed enhancements to the Plan Area.

Table 4: Primary Opportunity Sites

Site Ref.	Approx. Acres	# of Parcels	Existing Conditions, Constraints, and Assets	Opportunities	Residential Unit Capacity	Other Potential Uses
A	16.7	30 (multiple owners)	Located north of 11 th Street, south of Alliance Ave, and west of M Street. Includes rights-of-way for 14 th and 15 th Streets. This somewhat blighted former mill site largely consists of open green space. While mostly vacant, the site does contain some legacy single-story metal industrial buildings. Jolly Giant Creek passes through the site in intermittent underground and above-ground segments. A railroad right-of-way runs north-south through the site.	This is an excellent site for environmental restoration, cleanup of contamination, formally-developed public open space, creek daylighting, bike/ped trails, and medium to high-density residential development. To maximize the development potential of the site, it could be developed as a single master planned “campus” with minimal internal roadways. Along the east edge of the site (along M Street) the site could contain some ground-floor non-residential uses.	300 to 835+	<ul style="list-style-type: none"> •Creek daylighting •Restoration •Open Space •Bike/Ped •Non-residential mixed use
B	3.9	6 (multiple owners)	Located south of Alliance Ave, west of K Street, and east of M Street. Much of the site contains a single-story mini-storage business and a single-story automotive-service business, though nearly an acre is entirely vacant green space. A railroad right-of-way runs north-south through the entire site. The north end of L Street terminates at this site, preventing L Street from reaching Alliance Ave. The existing Arcata Rail-to-Trail bike/ped facility passes through the site.	This is an excellent site for environmental restoration, cleanup of contamination, an extension of L Street, and medium to high-density residential development. Along its street frontages, the site could contain some ground-floor non-residential uses.	70 to 195+	<ul style="list-style-type: none"> •Bike/Ped •Street connection •Non-residential mixed use
C	0.7	1	Located north of 12 th Street and west of K Street. The site includes approximately five single-story wood and metal industrial buildings and a small structure of potential historic significance. Over 50% of the site is undeveloped. The site currently contains an automotive-service business.	This site is well suited to residential uses, with some potential for non-residential ground-floor uses.	12 to 35+	<ul style="list-style-type: none"> •Non-residential mixed use
D	0.75	1	Located south of 11 th Street and west of L Street. The site includes one single-story metal industrial building that is home to a wholesale and equipment rental business. Approximately 50% of the site is undeveloped, most of which contains outdoor storage of automotive parts and equipment.	The site is in a prominent location, with 11 th Street providing substantial vehicular and foot traffic daily. In addition, the site is also directly adjacent to the Rail-to-Trail, making it ideally suited for upper floor residential and ground-floor non-residential uses.	14 to 38+	<ul style="list-style-type: none"> •Non-residential mixed use

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

VISION

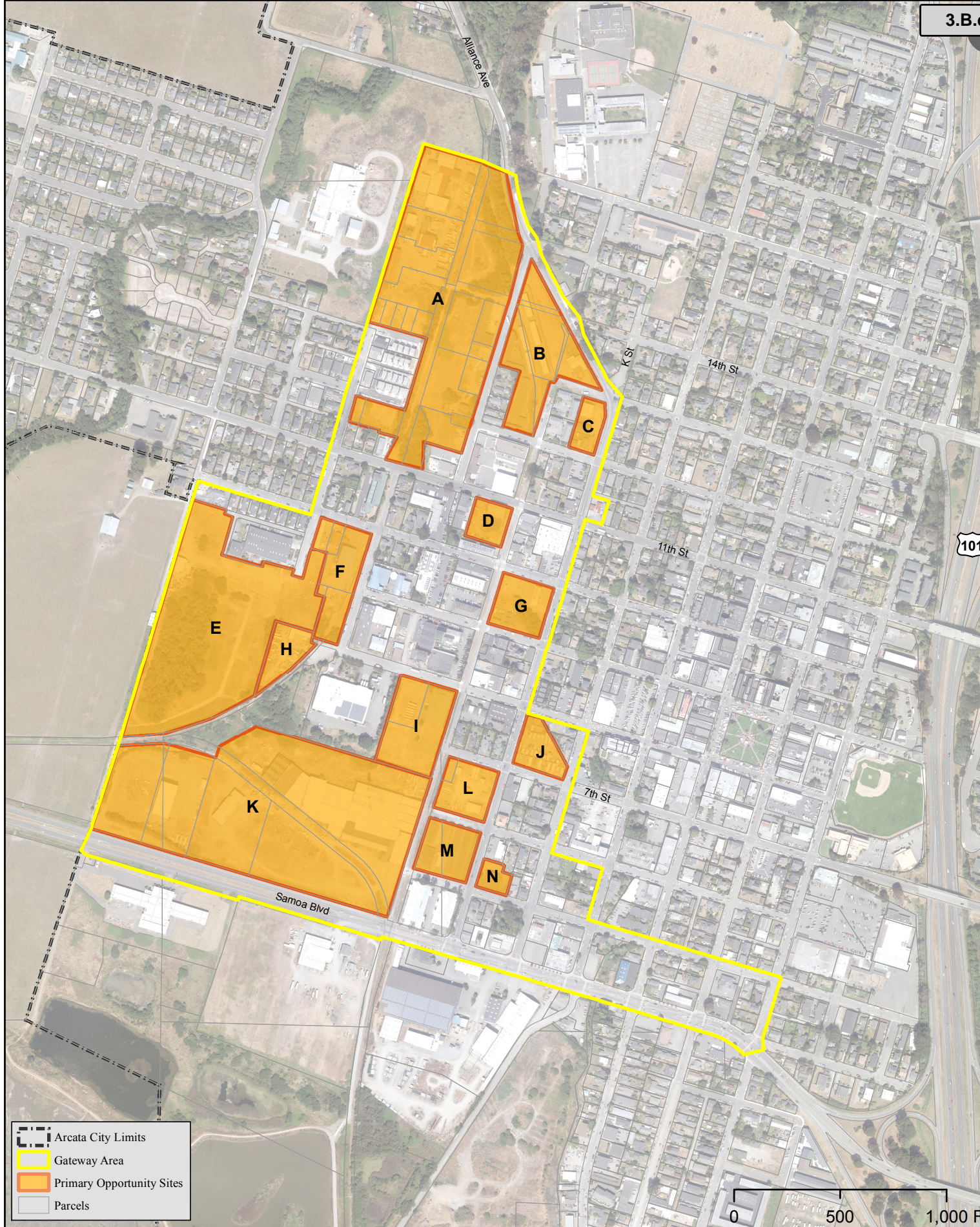
Site Ref.	Approx. Acres	# of Parcels	Existing Conditions, Constraints, and Assets	Opportunities	Residential Unit Capacity	Other Potential Uses
E	12.2	1	Located east of the far southern extent of Q Street, south of 10 th Street, and west of N Street. Until recently, the site was mostly undeveloped, containing just one small single-story metal industrial building and an automotive-services business. Within the past two years, an approximately 20,000sf two-story mixed-use building was constructed in the northeast corner, leaving the vast majority of the site still undeveloped. The site is partially within the Coastal Zone.	At the edge of the Plan Area and directly adjacent to planned open space, the site is well suited for residential development.	112 to 311+	<ul style="list-style-type: none"> •Bike/Ped •Non-residential mixed use
F	2.19	5 (multiple owners)	Located south of 10 th Street and east of N Street. The site includes three single-story metal industrial buildings. Over 65% of the site is undeveloped. The site currently contains an automotive-service business, a recycling business, and contractor yards. It is possible that development on the south end of the site may be restricted due to the possible presence of wetlands.	This site is well suited to environmental restoration, cleanup of contamination, and residential uses, with some potential for non-residential ground-floor uses.	39 to 110+	<ul style="list-style-type: none"> •Non-residential mixed use
G	1.42	1	The site is a single parcel and also the entire block contained by 10 th Street, K Street, 9 th Street, and L Street. The site does not contain any finished structures, but does contain a single canopy structure and several shipping containers. The site is also bisected diagonally by Jolly Giant Green from the northwest corner to the southeast corner. The northern half of the creek is underground while the southern half is aboveground and includes a substantial strip of riparian vegetation. The site contains a car wash business.	The site is optimally located for environmental restoration and new multi-story mixed use development. Being an entire City block also adds to the site's development potential. The underground portion of Jolly Giant Creek is the single best opportunity for creek daylighting south of 11 th Street. The site's proximity to the CoOp grocery store and other downtown commercial amenities makes the site an ideal location for residential development that can provide a car-free lifestyle for residents. Fronting on the high-volume K Street also makes the site prime for some ground-floor non-residential development that can provide jobs and retail opportunities.	26 to 71+	<ul style="list-style-type: none"> •Creek daylighting •Open Space •Bike/Ped •Non-residential mixed use
H	1.0	1	Located at the far west end of 8 th Street, the site contains a one-story metal industrial building and a substantial volume of scrap metal, vehicle parts, boat parts, and shipping containers. The site is entirely within the Coastal Zone.	The site is optimally located for environmental restoration and new multi-story mixed use development.	18 to 50+	<ul style="list-style-type: none"> •Non-residential mixed use
I	2.65	1	The site contains several shipping containers, a parking lot, and a one-story metal industrial building that currently serves as professional office space and hand-assembly light manufacturing. Approximately 66% of the site is currently vacant. The site is entirely within the Coastal Zone.	This site is well suited to residential uses, with some potential for non-residential ground-floor uses.	48 to 133+	<ul style="list-style-type: none"> •Open Space •Bike/Ped •Non-residential mixed use
J	1.0	1	The site consists of 70% of the block contained by 8 th Street, J Street, 7 th Street, and K Street. The site is currently home to approximately 27 trailers/RVs, a single-family residence, and a garage. The site is presumably home to over two dozen low-income individuals/families. The site is entirely within the Coastal Zone.	The site presents an excellent opportunity to create an multi-story affordable housing development that provides substantially upgraded residential accommodations for the site's current residents.	18 to 50+	<ul style="list-style-type: none"> •Non-residential mixed use

VISION

Site Ref.	Approx. Acres	# of Parcels	Existing Conditions, Constraints, and Assets	Opportunities	Residential Unit Capacity	Other Potential Uses
K	20.3	8	Located north of Samoa Blvd and west of the L Street right-of-way, this site consists of a large industrial site that once contained a barrel factory and a mill. While the site contains nearly 200,000 sf of legacy metal and wooden industrial buildings, this site is most vacant open area that once served as log deck. The site contains no internal public roadways or rights-of-way, though a railroad right-of-way does cut through the site. Though the site is home to several important businesses, the site is also relatively blighted in appearance. The site is entirely within the Coastal Zone.	This site is among the most developable sites within the City and has the potential to provide many of the housing units that the Gateway Area Plan seeks to create. To maximize the development potential of the site, it could be developed as a single master planned "campus" with minimal internal roadways. The site also has the potential to provide open space and recreation areas. Some non-residential mixed use may be appropriate along Samoa Blvd.	365 to 1,015+	<ul style="list-style-type: none"> • Restoration • Open Space • Bike/Ped • Non-residential mixed use
L	1.44	2	The site is the entire block contained by 7 th Street, K Street, 6 th Street, and the L Street right-of-way. The site contains three small structures, several pressurized above-ground-storage-tanks, and dozens of stockpiled residential-scale propane tanks. The site is home to Amerigas and is largely empty being primarily used for storage. The site is entirely within the Coastal Zone.	The site is optimally located for new multi-story mixed use development. Being an entire City block also adds to the site's development potential. The site's proximity to various downtown commercial amenities makes the site an ideal location for residential development that can provide a car-free lifestyle for residents. Fronting on the high-volume K Street also makes the site prime for some ground-floor non-residential development that can provide jobs and retail opportunities.	26 to 72+	<ul style="list-style-type: none"> • Non-residential mixed use
M	1.44	2	The site is the entire block contained by 6 th Street, K Street, 5 th Street, and the L Street right-of-way. The site is entirely occupied with small-bay low-profile single-story buildings serving as a mini-storage business. The site is entirely within the Coastal Zone.	The site is optimally located for new multi-story mixed use development. Being an entire City block also adds to the site's development potential. The site's proximity to various downtown commercial amenities makes the site an ideal location for residential development that can provide a car-free lifestyle for residents. Fronting on the high-volume K Street also makes the site prime for some ground-floor non-residential development that can provide jobs and retail opportunities.	26 to 72+	<ul style="list-style-type: none"> • Bike/Ped • Non-residential mixed use
N	0.43	1	The site is east of K Street and north of 5 th Street. The site contains a 1.5 story building that takes up less than 50% of the lot. The site is entirely within the Coastal Zone.	The site is optimally located for new multi-story mixed use development. The site's proximity to various downtown commercial amenities makes the site an ideal location for residential development that can provide a car-free lifestyle for residents. Fronting on the high-volume K Street also makes the site prime for some ground-floor non-residential development that can provide jobs and retail opportunities.	8 to 22+	<ul style="list-style-type: none"> • Non-residential mixed use
TOTAL COLLECTIVE POTENTIAL RESIDENTIAL UNITS					Approximately 1,000 to 3,000+	

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 3 below corresponds to Table 4 above.



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 3: Primary Opportunity Sites



VISUALIZATIONS

Following public feedback regarding draft Gateway Area Plan, several pages will be added here to display photo-realistic visualizations of selected opportunity sites within the Plan Area. Each visualization will show existing conditions and the envisioned future improvements on the same site. These renderings will be illustrative, will be conceptual, and will not be prescriptive.

PLACEHOLDER

SECTION C: STRATEGIES, POLICIES, AND IMPLEMENTATION

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)



1. LAND USE

Overview

The Land Use strategies and policies in the Plan allow for, and actually encourage, a broad range of residential and mixed-use redevelopment with an emphasis on housing creation. The Plan supports a broad diversity of uses as long as they support a pedestrian-friendly environment and other Plan goals.

Land Use Designations

Figure 4 shows the land use designations that apply within the Gateway Plan Area. As described below, the designations allow for a high-intensity mix of residential and non-residential land uses. The Gateway Zoning Code zoning districts implement these designations with additional specificity on permitted land uses and development and design standards. Each of these designations are also known as “Gateway Sub-areas.”

Gateway Barrel District (G-B)

The G-B district (approximately 35-acres) provides a high-density walkable residential campus on a former barrel manufacturing site and neighboring former industrial sites. Internal circulation is based primarily on bicycle and pedestrian modes of travel. Vehicular infrastructure is minimal, producing a campus-like district. Building and site design supports a pedestrian-friendly public realm compatible with the internal high-density residential character of the District. While land uses include retail, office, personal service, restaurant, and light [industrial manufacturing](#) uses, the District is primarily residential. The G-B district allows building heights up to eight stories for development providing community benefits.

Gateway Hub (G-H)

The G-C district is a vibrant, high-intensity mixed-use area allowing for a broad range of residential, retail, office, personal service, restaurant, entertainment, light [industrial manufacturing](#), and other similar uses. The G-C district allows building heights up to seven stories for development providing community benefits. Building and site design supports a welcoming, vibrant, and pedestrian-friendly public realm.

Gateway Corridor (G-C)

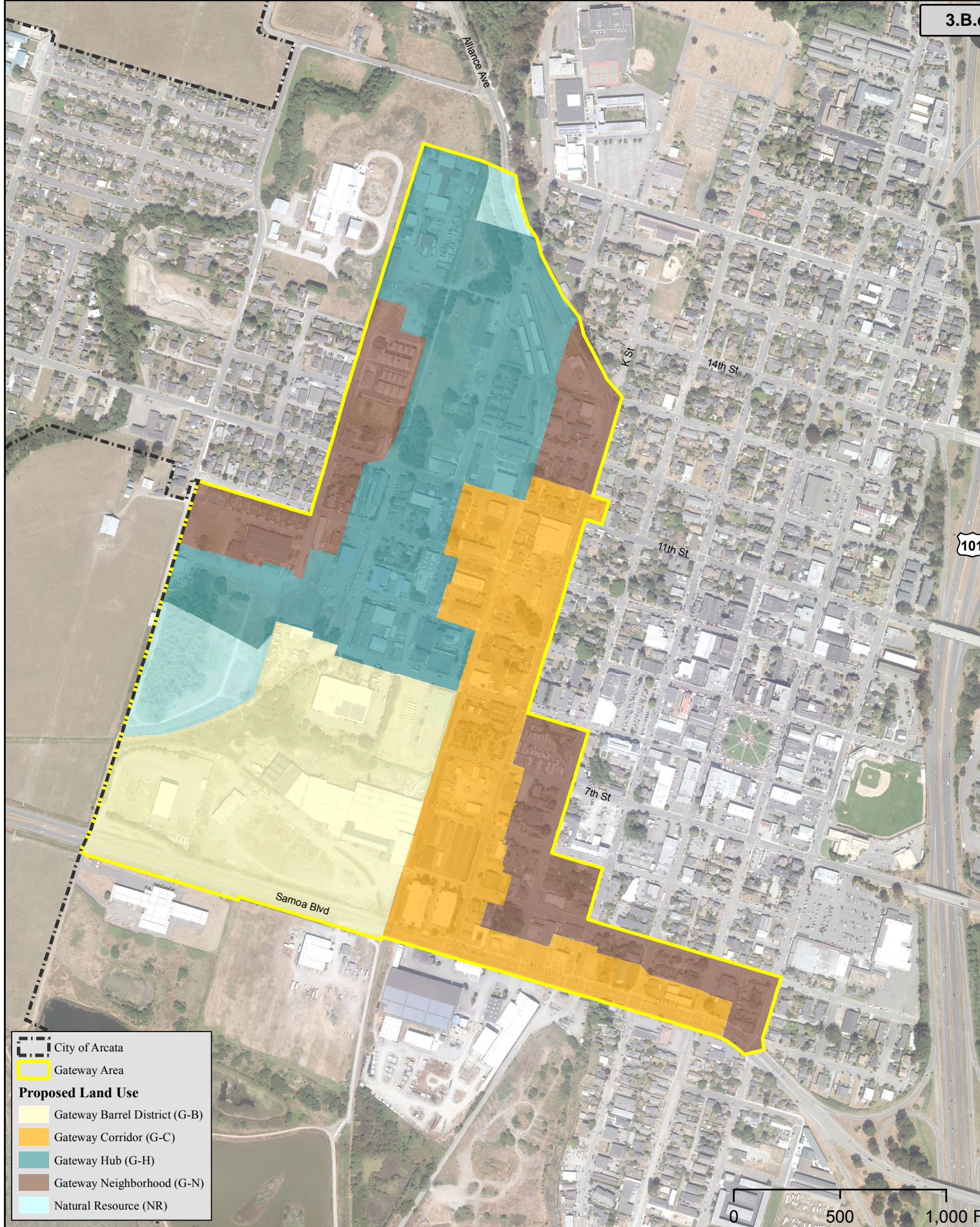
The G-C district is a linear area along Samoa Boulevard, L Street, and K Street, each of which serve as major bicycle and vehicular gateways into the City. This active, inviting, high-intensity, mixed-use designation requires a pedestrian-oriented ground floor design aesthetic intended to slow passing vehicular traffic and encourage drivers to park and walk. Street-facing uses primarily consist of non-residential uses, with residential allowed in limited cases as defined in the corresponding zone district. Upper floors primarily have residential uses, with some non-residential allowed in limited cases. Overall allowed uses include residential, retail, office, personal service, restaurant, entertainment, light [industrial manufacturing](#), and other similar uses. The G-C district allows building heights up to six stories for development providing community benefits.

Gateway Neighborhood (G-N)

The G-N district provides a transition from the high-intensity G-C district to lower intensity residential uses outside of the Gateway Plan Area. Allowed land uses include residential, retail, office, personal service, restaurant, light [industrial manufacturing](#), and other similar uses. The G-C district allows building heights up to five stories for development providing community benefits. Building and site design supports a pedestrian-friendly public realm compatible with the residential character of surrounding areas.

Other Designations

Other Designations that occur in the Gateway Area, such as Natural Resource, are described in the General Plan.



City of Arcata
 Gateway Area
Proposed Land Use
 Gateway Barrel District (G-B)
 Gateway Corridor (G-C)
 Gateway Hub (G-H)
 Gateway Neighborhood (G-N)
 Natural Resource (NR)

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 4: General Plan Land Use Designations Within the Gateway Area



Land Use Vision

The Gateway Area Plan envisions a mix of residential and non-residential uses, but does not mandate specific land uses on specific properties other than requiring residential uses. Instead, the Plan expects that a mix of land uses will evolve organically over time in response to changing market conditions, social demand, and development opportunities. Given this approach, the Gateway Area Plan identifies broad “land use targets” for each Land Use Designation (a.k.a. “subarea”). The Land Use Designation Diagram above (Figure 4) shows the subarea boundaries. Table 5 below shows the land use targets for each subarea and the overall Plan Area. These targets are not a regulatory requirement for individual development projects, but rather a broad goal. The targets heavily favor the production of new residential units given that market conditions, social demand, and projected growth all overwhelmingly demonstrate a shortage of housing in the City. If market conditions and/or social demand change significantly in the lifetime of this Plan, then these targets may not continue to be applicable.

Table 5: Target Land Use Mixes

	Target Dwelling Units	Estimated Average Unit Size (square feet)	Approximate Total Residential Floor Area	Assumed SF of Non-Residential Floor Area Per Residential Unit	Approximate Total Non-Residential Floor Area	Approximate Total New Floor Area	Approximate Percent of Total Floor Area that is Non-Residential
Barrel District (G-B)	1,200	1,000	1,200,000	85	102,000	1,302,000	8%
Gateway Hub (G-H)	1,300	900	1,170,000	50	65,000	1,235,000	5%
Gateway Corridor (G-C)	500	900	450,000	200	100,000	550,000	18%
Gateway Neighborhoods (G-N)	500	1,000	500,000	20	10,000	510,000	2%
TOTAL/AVERAGE	3,500	949	3,320,000	79	277,000	3,597,000	8%

As shown in Table 5, the Plan envisions that the majority of new floor area will be residential, with a modest amount of new non-residential uses developed to serve Plan Area residents, visitors, and the broader community. The City envisions each subarea will include pockets of non-residential uses integrated within existing and new residential developments. While a mix of land uses in each subarea is essential to create complete neighborhoods and support a car-free lifestyle, the production of residential units is the Plan’s primary objective. This heavy emphasis on residential units over new commercial uses factors into consideration the fact that the Plan Area is within walking distance of the Downtown core, the G Street commercial corridor, several grocery stores, and dozens of existing businesses, all of which can serve the new residents of the Plan Area. If a surge of new residential dwelling units in the Gateway Area generates more demand for non-residential building area than these existing nearby commercial areas can support, then market pressures are anticipated to naturally generate more non-residential uses in the Plan Area. This expectation of evolving market conditions and social demand to produce what is most needed within the Gateway Area is supported by the regulatory structure of this document and the emphasis on mixed uses. Note that Table 5 is builds upon and is complementary to the analysis of opportunity site presented in Table 4: Primary Opportunity Sites.



Land Use Policies

Objective: Establish an overall land use arrangement that promotes mixed-use, multi-story buildings with a clear emphasis on high-density housing that is equitable, environmentally sustainable, and compatible with the envisioned neighborhood character. Allow for a broad diversity of non-residential uses that support a pedestrian/bicycle-friendly environment, contribute to a car-free lifestyle, and meet the physical, social, and economic needs of residents.

GA-1a. Maximum and Minimum Residential Density Standards. Except for in designated natural resource and open space areas, permit all land use designations in the Plan Area to allow residential uses. In each of these land use designations, do not establish a maximum residential density standard. Instead, allow residential density to be naturally restricted through other development standards, such as building height and Building Code requirements for minimum unit size. In addition, establish a minimum residential density standard in each of these land use designations, with exceptions established for some use types (such as theaters) and some building types (such as historically significant structures and the adaptive reuse of existing buildings).

GA-1b. Relationship of Building Height to Open Space. Allow for buildings over four stories using community Benefits Program, but require greater percentages of Open Space to be retained at ratios set in the Form-Based Code to ensure residents of high-density housing retain an excellent quality of life and easy access to high-quality open space.

GA-1c. New Non-residential Uses. Except for in designated natural resource and open space areas, permit all land use designations in the Plan Area to allow mixed uses to complement residential uses. Ensure that all new non-residential mixed-use development supports an active and livable neighborhood, with residential, retail, office, and light-manufacturing uses thoughtfully blended together to create a cohesive neighborhood that feels complete. Allow flexibility in non-residential uses, with targeted limitations on uses that do not encourage street level human activity, livability, or neighborhood identity. Examples of uses that do not encourage human activity include heavy industrial uses, mini-storage, and outdoor automotive sales.

GA-1d. Land Use Targets. Aim to achieve a mix of residential and non-residential uses in each land use subarea that supports the full range of services to Gateway Area residents, workforce, and visitors. Estimated long-term targets to balance uses are shown in Table 5.

GA-1e. Incentivize Projects that Provide Designated Community Amenities. Establish ministerial permitting options and streamlined development processes for projects that provide designated community amenities that contribute to quality of life.

GA-1f. Incentivize Mixed Use as a Community Amenity. Incentivize mixed-use projects that have a ground-floor non-residential (retail, office, light industrial, or other job-creation component) through the Gateway Area community benefit program where not otherwise required by the form-based code.

GA-1g. Form-based Design Standards. Apply form-based design standards that allow high-density, multi-story buildings while simultaneously requiring a vibrant, community-oriented, street-facing built environment designed to fit a "human-centered" scale.

GA-1h. Relocate Existing Uses that are Incompatible with Plan Vision. Facilitate the relocation of nonconforming uses that are incompatible with the Plan Vision and explore opportunities to reduce conflicts between new and existing users (e.g. noise attenuation). Target uses that conflict with or otherwise detract from the intended character of the Plan Area by being incompatible with human activity, livability, or neighborhood identity/cohesion. Support relocation of existing uses with affordable housing, large workforce, or high sales-tax.

GA-1i. Nonconforming Uses, Site Conditions and Structures. Allow existing nonconforming uses, site conditions, and structures to remain unless specifically targeted for relocation in the Zoning Code. When new development occurs, encourage incompatible non-conforming uses to become conforming, and in some cases, require non-conforming uses to relocate as specified in the Zoning Code. Consider offering incentives for some nonconforming uses to either relocate or modify operations or form to become more compatible with the surrounding neighborhood.

Land Use Programs

Imp-GA-1.1. Gateway Zoning Code. Adopt a Gateway Zoning Code as a section of the City's Land Use Municipal Code to implement the ~~land use policies and other policies~~ of this Plan.

Imp-GA-1.2. Relocation Assistance. Develop a formalized business relocation program that identifies funding, program requirements, and critical infrastructure for industrial users, including incompatible Industrial Limited uses and Industrial General uses. (FMC also requested this in addition to EDC).

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Key to Revisions (Font Color/Highlighting)

Proposed changes recommended by each committee are shown in underline/strikeout text with color coded committee text as follows:

Transportation Safety Committee

Parks and Recreation Committee

Creeks and Wetlands Committee

Forest Management Committee

Historic Landmarks Committee

Energy Committee

Economic Development Committee

Public member/entity

Staff edits

2. COMMUNITY BENEFITS AND DEVELOPMENT STANDARDS

Overview

The Plan recognizes the community benefit of compact, thoughtful, infill development. Housing affordable to the full range of Arcatan household incomes is a community benefit. Providing vibrant city streets benefits the whole community. Avoiding sprawl, developing strong bike, pedestrian and bus networks, daylighting creeks, and protecting and enhancing wetlands and other open spaces all improve quality of life for community members. While the Plan provides these core amenities through its implementation, incentives offered through the plan will encourage developers to provide additional amenities.

Regulatory Framework for Community Benefit Program

The community benefits program requires applicants to incorporate designated community amenities (a.k.a. “community benefits”) into proposed development projects in exchange for a streamlined, by-right approval process and increased development potential. This program is intended to facilitate the production of new multifamily and mixed-use residential development with amenities that benefit the general public. Amenities have been specifically targeted for the opportunities in the Plan Area.

Multifamily and mixed-use residential projects throughout the Gateway Area are eligible for participation in the Community Benefits Program. To be eligible for incentives, a project must meet specified architectural standards, meet a minimum density requirement, and include a specified percent of affordable units.

The Gateway Zoning Code, the regulatory document that implements this Plan, identifies specific benefits available to qualify for increased development intensity and ministerial approval. The benefits address the categories outlined below in Table 6.

Table 6: Categories of Community Amenities

Housing Creation	Rapid production of new housing, high-density housing, small units (which are naturally more affordable), multi-bedroom units for families, owner-occupied multi-family development, single-room occupancy, preservation of existing affordable housing, creation of new deed-restricted affordable housing, and related amenities that create valued forms of housing.
Arts and Culture	Public art, murals, pedestals for sculptures, performing arts spaces, arts/culture fee program, artist live-work housing, and related amenities that facilitate the inclusion of arts/culture into development projects.
Open Space	Creek daylighting, dedicated open space, open space fee program, community gardens, and related amenities that support trees, landscaped areas, open space, and public-use areas.
Green Building and Sustainability	LEED <u>Platinum</u> certification (or built to LEED standards), mass timber construction, renewable energy generation, contamination remediation, street trees, rainwater management, light pollution reduction, and related amenities that reduce energy use, reduce carbon output, or facilitate a more sustainable environment.
Active and Alternative Transportation	Electric vehicle charging stations, employee showers, on-site bike parking, bus passes for residents, dedication of parcel frontage to transportation uses, utility undergrounding, and related amenities that stimulate non-motorized and zero-carbon transportation options.
Enhanced Architectural Features and Exterior Design	Roof form variation, detail, and ornamentation; horizontal articulation; cantilevered upper floors; <u>green roofs, rooftop gardens or social space</u> ; and related amenities that enhance the quality and appearance of buildings.
Retail and Job Creation	Ground-floor retail, outdoor dining, rooftop dining, job creation <u>including office and light industrial uses</u> , and related amenities that generate desired non-residential uses; <u>include maker spaces and co-working</u> .

The City may grant incentives only when the community benefits or amenities offered are not otherwise required by the Zoning Ordinance or any other provision of local, state, or federal law.

The community benefits program utilizes a tiered incentives system where projects that provide higher levels of community benefits are permitted greater intensity. As shown in Table 7 and Image 2, projects not directly required to provide community benefits (Base Tier) have the lowest level of allowed intensity and are required to comply with discretionary processes that may require conditions of approval. The additional voluntary bonus tiers each allow increasingly higher levels of intensity and each require community benefits, though none require discretionary processes.

Table 7: Development Standards for Base Level Projects and Community Benefit Projects

	Base Tier	Community Benefits Bonus Tier 1	Community Benefits Bonus Tier 2	Community Benefits Bonus Tier 3	Community Benefits Bonus Tier 4
LAND USE DESIGNATIONS					
Gateway Barrel District (G-B)	Optional	Optional	Optional	Optional	Optional
Gateway Hub (G-H)	Optional	Optional	Optional	Optional	Not permitted
Gateway Corridor (G-C)	Optional	Optional	Optional	Not permitted	Not permitted
Gateway Neighborhood (G-N)	Optional	Optional	Not permitted	Not permitted	Not permitted
DEVELOPMENT STANDARDS					
Minimum residential density (du/ac) ^{1, 2}	18	27	35	44	53
Maximum residential density (du/ac) ³	NA	NA	NA	NA	NA
Building Height, Maximum Height	50	60	70	80	90
Building Height, Maximum Stories	4	5	6	7	8
Building Height, Minimum Stories ²	2	2	3	3	3
Baseline Architectural Standards ⁴	Required	Required	Required	Required	Required
Community Benefits	Conditioned⁵	Required⁶	Required⁶	Required⁶	Required⁶
Conditional Use Permit	Required	Not Required	Not Required	Not Required	Not Required
Design Review approval	Required	Not Required	Not Required	Not Required	Not Required
Other committee approvals	TBD ⁷	Not Required ⁸	Not Required ⁸	Not Required ⁸	Not Required ⁸
Notes					
<ol style="list-style-type: none"> Projects that do not provide the designated minimum residential density will need to obtain a use permit demonstrating projects support Gateway Area vision and objectives. For details, see Gateway Zoning Code. Some use types (such as theaters) are exempt from minimum density requirements and minimum story requirements. Also exempt from minimum density requirements and minimum story requirements are some building types (such as historically significant structures and the adaptive reuse of existing buildings). For details, see Gateway Zoning Code. The maximum number of residential units for any project will be determined through other development standards, such as building height, setbacks, and Building Code requirements for minimum unit size. Determined by applicable zone district. See Gateway Zoning Code. Community benefits are not necessarily required but may be required as Conditions of Approval issued through discretionary processes, which may be required as determined by Gateway Zoning Code. The number of committees that must issue approvals will vary depending on applicable zone district and project type. Required community benefits determined by applicable zone district. See Gateway Zoning Code. Projects within the Base Tier may be required to receive approval from one or more discretionary City committees and may be subject to committee-issued conditions of approval. Requirements to receive approval from City committees will vary depending on location and project type. See Gateway Zoning Code to determine if any discretionary committee approvals are required. Projects that qualify for any of the Community Benefit Bonus Tiers are not required to receive approval from any City committees and are not subject to any conditions of approval issued by any City committees. 					

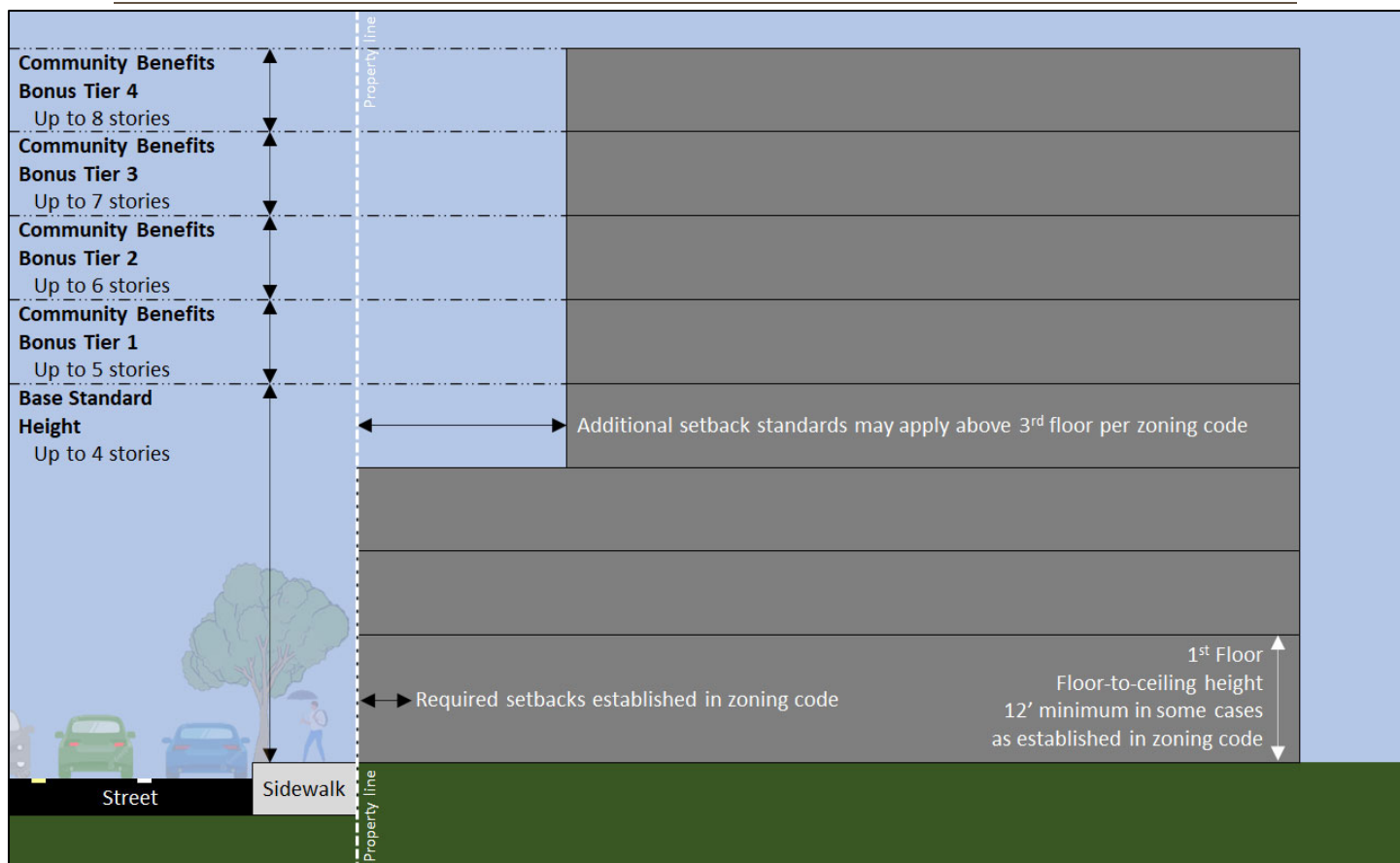


Image 2: Building Height Standards and Community Benefit Program Bonus Tiers

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Key to Revisions (Font Color/Highlighting)

Proposed changes recommended by each committee are shown in underline/strikeout text with color coded committee text as follows:

- Transportation Safety Committee
- Parks and Recreation Committee
- Creeks and Wetlands Committee
- Forest Management Committee
- Historic Landmarks Committee
- Energy Committee
- Economic Development Committee
- Public member/entity
- Staff edits

Community Benefit Policies

Objective: Identify a range of supplemental community amenities that a development project could include but that is not inherently required (such as electric vehicle charging stations, affordable-by-design studio apartments, and murals). Ensure that these amenities improve the quality of life for community members. Allow applicants to incorporate such community benefits into proposed development projects in exchange for a streamlined, by-right approval process and increased development potential consistent with Table 6, Table 7, and Image 2.

GA-2a. Base Standards and Bonus Tier Standards. Utilize the tiered incentives system presented in Table 6, Table 7, and Image 2, where projects that provide higher levels of community benefits are permitted greater intensity.

GA-2b. By-Right Approval. Allow development projects participating in the community benefits program with by-right approvals when the project conforms to all applicable standards and design guidance.

GA-2c. Require Residential Uses for Bonus Tiers. To promote housing production, limit participation in community benefits program to projects that include a minimum standard of residential units.

- GA-2d. Choice of Benefits.** Allow applicant to select community benefits from a menu of available options. The Form-Based Code will reflect the community’s priorities among and relative value of community benefits. All buildings that exceed four stories must include architectural features amenities.
- GA-2e. Housing Production Emphasis.** Ensure that the available community benefits emphasize and support the City’s goal of maximizing housing production in the Gateway Area.
- GA-2f. Value of Benefits.** Ensure that the additional intensity allowed is appropriately calibrated to the value of the community benefits provided. High-cost benefits should allow for a greater increase in allowed intensity than low-cost benefits.

Community Benefit Programs

- Imp-GA-2.1. Create Community Benefits Program.** Within the Gateway Zoning Code, create a “Community Benefits Program,” where projects that provide higher levels of community benefits are permitted greater intensity. Utilize and build upon the tiered incentives system presented above in Table 6, Table 7, and Image 2.
- Imp-GA-2.2. Review of Community Benefits Program.** Periodically review the community benefit program in the Gateway Zoning Code and assess if revisions are needed to improve program effectiveness. This periodic evaluation is intended to allow decision makers to modify and “scale up” community benefits as state regulations change and the urgency of the climate crisis advances.



3. HOUSING

Overview

The City aims to accommodate up to 3,500 new residential units in the Gateway Area, provided primarily through high-density multifamily, townhouses, lofts, work-live units, quads, small space clustered units, and mixed-use development. New housing will feature a range of unit sizes, a mix of renter and owner-occupied units, and housing choices available for students and lower-income households. Infill development to meet the City's housing and economic development needs fulfills other General Plan Elements goals, including Open Space, Recreation, Circulation, and the adaptation goals in the Local Coastal Element. But this Plan is critical tool implementing the Housing Element, which identifies infill redevelopment as vital to the City's sustainable growth and development.

Housing Policies

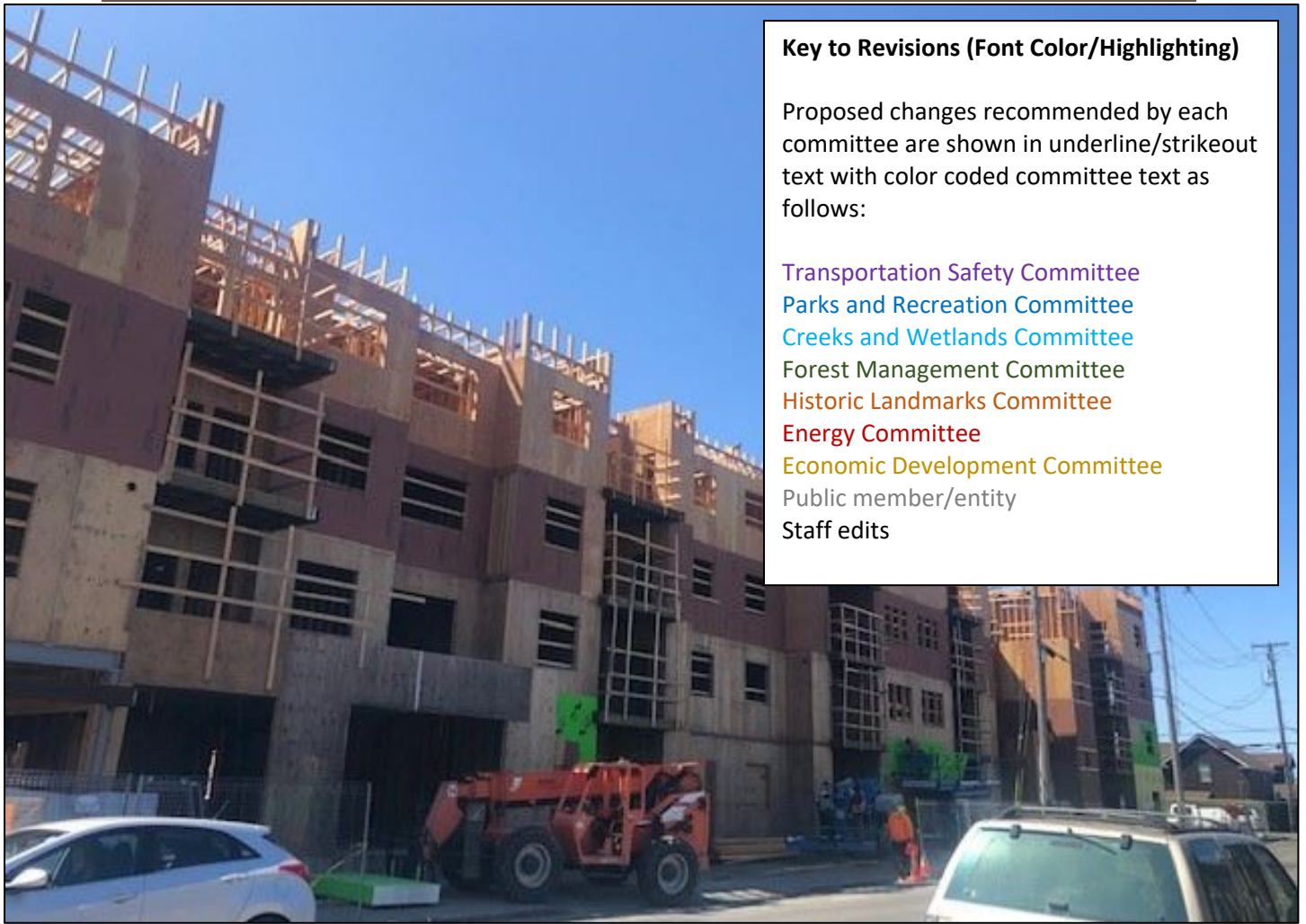
Objective: The City's housing development is regulated by the State of California through the City's adopted housing element, which is updated every eight years. The last three cycles have consistently included policies to encourage efficient growth patterns, including policies supporting mixed use and infill development projects near jobs and transit. This plan delivers on previous implementation measures by creating a new and targeted focus on high density residential projects within the Gateway Area. To implement the Housing Element, substantially increase the volume and variety of housing in the Plan Area by transitioning from the current patchwork light-industrial development pattern to a denser, more cohesive mixed-use and high-density residential pattern. Plan accordingly in recognition that the Gateway Plan Area has by far the best potential to create a volume of new housing when compared to all the other priority infill areas identified in the Housing Element. The City also acknowledges that the Plan Area has the greatest potential to accommodate a much-needed increase in the diversity of housing types.

- GA-3a. New Units.** Plan for an approximate maximum of 3, approximately 500 new residential units in the Gateway Area in the next 20 years, recognizing the full buildout potential in the Area is close to 3,500 units, as shown in Table 5.
- GA-3b. No Maximum Residential Density.** Regulate building bulk and massing through design and community benefit measures; do not directly limit units per acre. Instead, encourage the maximum of dwelling units feasible within the allowed building envelope and allow other standards (e.g. height, setbacks, minimum units sizes) to collectively establish natural limitations on the number of dwelling units that can be developed.
- GA-3c. Minimum Residential Density.** Require that all new development provides at least some minimum quantity of housing units by establishing a minimum residential density (number of units per acre). Projects that do not provide the designated minimum residential density will need a use permit demonstrating they support the Gateway Area objectives.
- GA-3d. Range of Unit Sizes.** Encourage a range of unit sizes, from micro-units (200 square feet or minimum per building standard) to units with three or more bedrooms.
- GA-3e. Student Housing.** Encourage new low-cost housing for students, including single room occupancy housing, group living accommodations, and micro-units.
- GA-3f. Multiple Strategies to Promote Affordability.** Employ multiple strategies to promote the creation of affordable housing, including rent-restricted units affordable to middle and lower-income households, smaller units that are affordable-by-design studios, alternative ownership models including limited equity housing cooperatives, student housing, deed-restricted owner-occupied affordable housing affordable to middle and lower-income households, single room occupancies, and housing for low-income families.
- GA-3g. Mixed-Tenure.** Encourage a mix of both owner-occupied and rental housing.

- GA-3h. Mixed-Income Neighborhoods.** Provide for mixed-income neighborhoods with housing options available for all income groups. Housing in a mixed-income neighborhood should include deed-restricted units affordable to very low-income households, small affordable-by-design units, student housing, moderate income owner-occupied condominiums and townhouses, market-rate rental units, median-priced family-sized dwellings, and penthouse units for high-income households.
- GA-3i. Owner-Occupied Affordable Housing as a Community Amenity.** Encourage new home ownership opportunities affordable to households of all income levels. Include deed-restricted affordable opportunities for low- and moderate-income households. Encourage a range of ownership opportunities including condominiums, townhouses, and other alternative ownership models. ~~for lower-income households including through condominium projects (e.g., deed restricted owner-occupied condominium units and for-sale micro units)~~ Provide strong incentives through community benefits program for owner occupancy.
- GA-3j. Incentivize Residential Density as a Community Amenity.** Through the Gateway Area community benefit program, allow increased development intensity and simplified development processes for projects that provide residential densities above established minimums.
- GA-3k. Incentivize Affordable Housing as a Community Amenity.** Through the Gateway Area community benefit program, allow increased development intensity and simplified development processes for projects that provide deed-restricted affordable units ~~residential densities above established~~ inclusionary zoning minimums.
- GA-3l. Incentivize Adaptable Design.** ~~Through the Gateway Area community benefit program, allow increased development intensity and simplified development processes for projects that provide fully accessible or “adaptably designed” units to facilitate the conversion to housing accessible for people with disabilities or general frailty.~~
- GA-3m. Residential Relocation Displacement.** In addition to relocation support encoded in state law, support the re-housing of existing residents displaced by the redevelopment of properties containing existing ~~dwelling units~~ residential uses.
- GA-3n. Racial Equity and Housing Partner Collaborations.** Continue to collaborate with community housing partners that work to promote racial equity and anti-discrimination in housing for both ownership and rental opportunities within the Gateway Area.
- GA-3o. Partnerships with the Wiyot Tribes.** Seek partnerships with the Wiyot Tribes to support housing projects

Housing Implementation Programs

- Imp-GA-3.1. Housing Monitoring.** Monitor the size, type, and affordability of new housing proposed, approved, and developed in the Gateway Area. Revise policies and programs in the Gateway Area Plan and implementing development regulations as needed to achieve the Plan housing goals. Monitoring will be completed in conjunction with the City’s annual Housing Element Annual Performance Report.
- Imp-GA-3.2. Resident Relocation Assistance.** Establish a program to require developers to assist with the re-housing of low-income residents displaced from their housing as a result of a redevelopment project. Seek funding through a combination of state and federal sources, and partner with nonprofit entities such as Housing Humboldt. Require private financing to contribute to ensure relocation program is fully and sustainably funded.
- Imp-GA-3.3. Business Relocation Program.** Establish a relocation program to support businesses displaced by redevelopment in the Gateway Area. Program funding should be through a combination of private and public sources. The program should use both City owned and privately owned property to retain businesses within the City limits.



Key to Revisions (Font Color/Highlighting)

Proposed changes recommended by each committee are shown in underline/strikeout text with color coded committee text as follows:

- Transportation Safety Committee
- Parks and Recreation Committee
- Creeks and Wetlands Committee
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- Public member/entity
- Staff edits

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

4. EMPLOYMENT

Overview

As reflected in Table 5, the City envisions the majority of new development in the Gateway Area to be residential. However, to provide for complete neighborhoods and support a car-free lifestyle, some additional non-residential development is desired. Non-residential land uses will include retail, personal services (e.g., hair salons and tattoo studios), restaurants, professional offices, hotels, light manufacturing, and arts and entertainment uses. Non-residential uses not only serve residents but also provide new employment opportunities and enhance the City's economic vitality.

Employment Policies

Objective: Provide for uses that retain and generate jobs.

- GA-4a. Type of Non-Residential Uses.** Allow employment-focused uses (e.g., professional office, Research & Development facilities) as well as resident-serving commercial uses. [Encourage tech and other digital careers by incentivizing co-workspaces and business incubators through Community Benefits Program.](#)
- GA-4b. Incentivize Designated Forms of Retail Spaces and Job Creation Uses as a Community Amenity.** Through the Gateway Area community benefit program, allow increased development intensity and simplified development processes for projects that provide designated community-desired forms of retail spaces and job-creating uses, such as outdoor dining, roof-top dining, and job-generating uses compatible with the Plan vision.
- GA-4c. Amount of Non-Residential Uses.** Aim to achieve new non-residential development in the Plan Area in the amounts shown in Table 5 (Land Use Mix).



5. ARTS AND CULTURE

Overview

The Gateway Area is envisioned to be a center for arts and culture anchored by the historic Creamery Building and the thriving Creamery District, both of which are contained entirely within the Gateway Area. New and expanded arts and entertainment uses will promote district vitality, generate economic activity, and enhance the quality-of-life for district residents and the community at large.

Arts and Culture Policies

Objective: Honor the Creamery District’s well-established entrepreneurial spirit and powerful identity that has been nurtured and organically cultured for years. Incentivize the integration of all forms of art into new development, including murals, sculptures, performing art spaces, and anything that is funky, eye-catching, or creative. Also identify opportunities to integrate arts and culture into streetscape designs and open space plans. In addition to being a place where people live and work, facilitate growth and development so that the Plan Area evolves into an arts and culture entertainment destination.

- GA-5a. Arts and Entertainment Uses.** Encourage arts and entertainment uses to enhance the vitality of the Gateway District and promote the arts in Arcata and the broader region. Incentivize the creation of arts and entertainments uses by making them principally permitted (by-right).
- GA-5b. Creamery District.** Continue to use existing placemaking and naming of the Creamery District in City mapping.
- GA-5c. Incentivize the Arts as Community Amenities.** Through the Gateway Area community benefit program, allow increased development intensity and simplified development processes for projects that provide amenities that supports the arts and area artists, such as murals on building exteriors, art installations in public-facing locations, and outdoor pedestals for sculptures. Arts-related community amenities shall be informed by consultation with Arcata’s Local Arts Agency as well as other arts and equity-related stakeholders as determined appropriate by staff (e.g. equity Arcata “Just Arts” group, Cal Poly stakeholders, etc.).
- GA-5d. Incentivize Artist Housing and Cultural Spaces as a Community Amenitiesy.** Through the Gateway Area community benefit program, allow increased development intensity and simplified development processes for projects that provide various forms of artist housing, including live/work units and deed restricted low-income housing units dedicated to artists and/or with amenities to attract artists. Encourage efforts to create a Native Arts and Cultural Space within the Gateway Area boundary.
- GA-5e. Outdoor Spaces.** Encourage larger-scale development to provide public outdoor spaces that can incorporate informal artistic and cultural activities open to the public and integrated with or connected to public space.
- GA-5f. Temporary Events.** Support formal and informal temporary artistic and cultural events.
- GA-5g. Adaptive Reuse for the Arts.** Support the adaptive reuse of existing buildings for artistic purposes.
- GA-5h. Public Art.** Through the Gateway Area community benefit program, encourage new development to incorporate public art that both is creative and reflects the Creamery District identity and history, and speaks to the goals and intent of the Arcata Strategic Arts Plan (Arts Plan). Specifically encourage development of public art that uplifts and support BIPOC artists and narratives as described in the Arts Plan.



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

6. OPEN SPACE AND CONSERVATION

Overview

The Plan Area currently contains no public parks, recreational facilities, or publicly accessible open spaces, other than the Rail-to-Trail project along L Street. However, as shown in Figure 5, there are several parks and recreational facilities within walking distance in the neighborhoods surrounding the Plan Area. Arcata Plaza is just three blocks from the eastern Plan boundary. Shay Park and Stewart Park are north of the Plan Area, and the Janes Creek Linear Park is west of the Plan Area. South of Samoa Boulevard is the Arcata Marsh and Wildlife Sanctuary.

Figure 6 shows the existing extents of open creek channels within and near the Plan Area. As shown, many segments of the creeks are underground in engineered conveyances. Jolly Giant Creek runs through the Plan Area, which is classified as a Class 1 (fish-bearing) stream in the General Plan Resource Conservation and Management Element. The creek is mostly culverted or covered south of 11th Street and north of 5th Street.

This document envisions a significantly enhanced and upgraded open space network throughout the Plan Area, including linear parks and privately-owned public spaces that fit within the envisioned streetscape. This new network of Open Spaces is intended to complement and counterbalance the residential and mixed-use growth planned for the Area by providing natural habitat, recreation opportunities, aesthetic enhancements, and harmonization with the built environment.



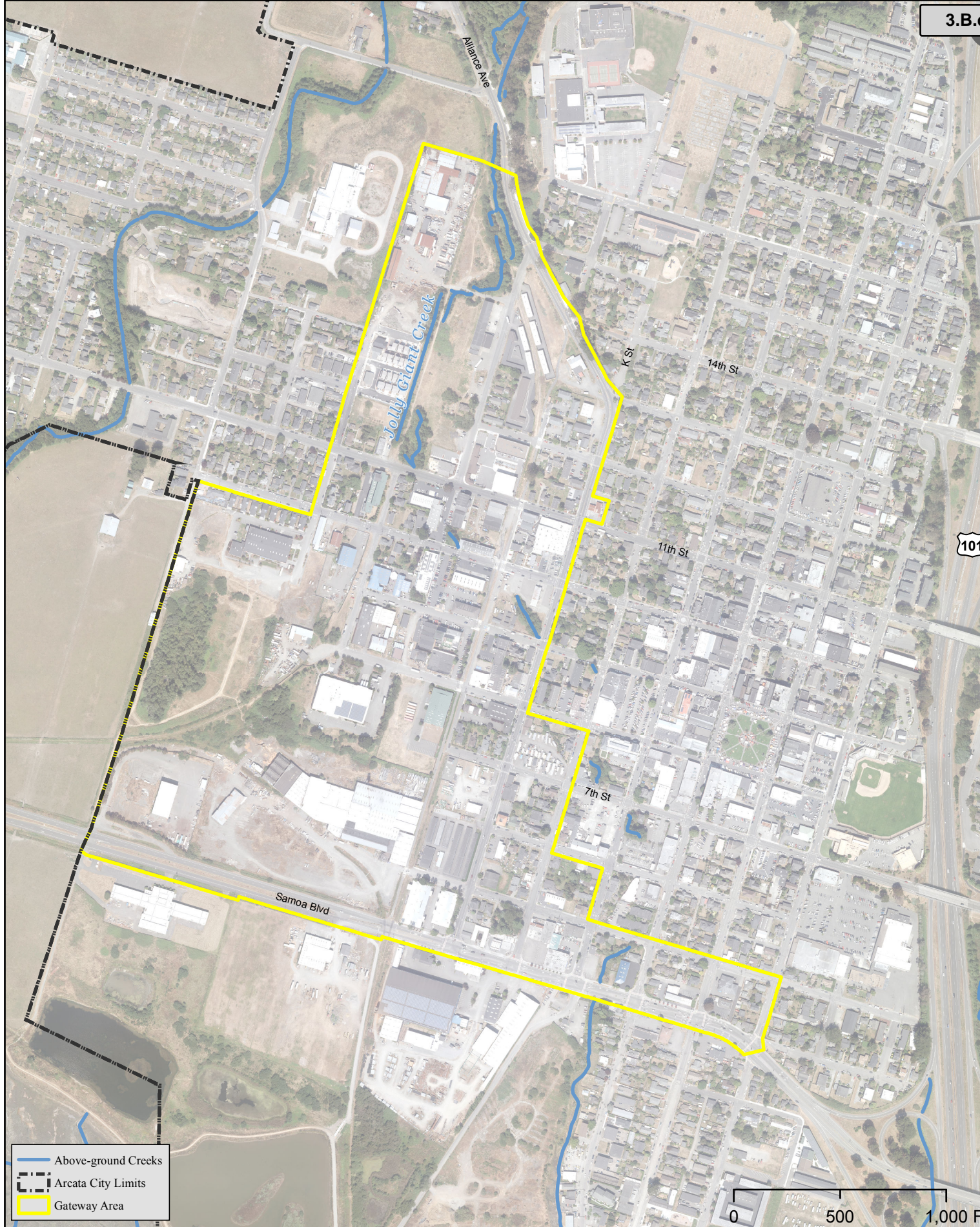
Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 5: Existing Parks and Recreational Facilities





Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 6: Existing Creeks



Regulatory Framework for Open Space and Conservation

The Gateway Zoning Code, the regulatory document that implements this Plan, identifies specific regulatory requirements for open space and conservation in line with the framework established in this Chapter. The City envisions a system of open spaces connected by streets, greenways, and paths throughout the Gateway Area. Open spaces will provide a variety of interesting areas that enhance community interaction and foster the area’s urban environment. These spaces will promote public gathering, enjoyment, and active and passive use by a broad range of the community.

Figure 7 shows a conceptual open space network to guides the general location and character of new open spaces in the Plan Area. Table 8 describes the envisioned types of open spaces to be provided. These open spaces, including new privately-owned publicly accessible open spaces, linear parks, and a public square, will be integrated into new development to support active public spaces and a pedestrian-friendly environment.

The City’s goal is to provide a park, high-quality trail, or open space within 200 yards of every residential unit in the Plan Area. It is anticipated that City Park in-lieu fees collected from residential development in the area will be sufficient for purchase and at least partial development of new parkland facilities. The City will enhance connections to parks and open space destinations near to the Plan area, such as Arcata Plaza, Shay Park, and Arcata Marsh, to leverage these amenities for the use and enjoyment of residents, community, and visitors. This Plan aims to provide open space amenities to residents in a cost-effective manner that does not create a significant new unfunded maintenance burden for City staff.

Publicly-accessible open spaces will be integrated with the protection and preservation of natural resources. New linear parks will be provided adjacent to daylighted and restored segments of the Jolly Giant Creek. Walking trails and other passive recreational activities will be provided in and around protected wetland areas. The Gateway Area Plan envisions developing existing low resource value vacant land to permanently protect high-value natural resources and create high-quality open space amenities for residents.

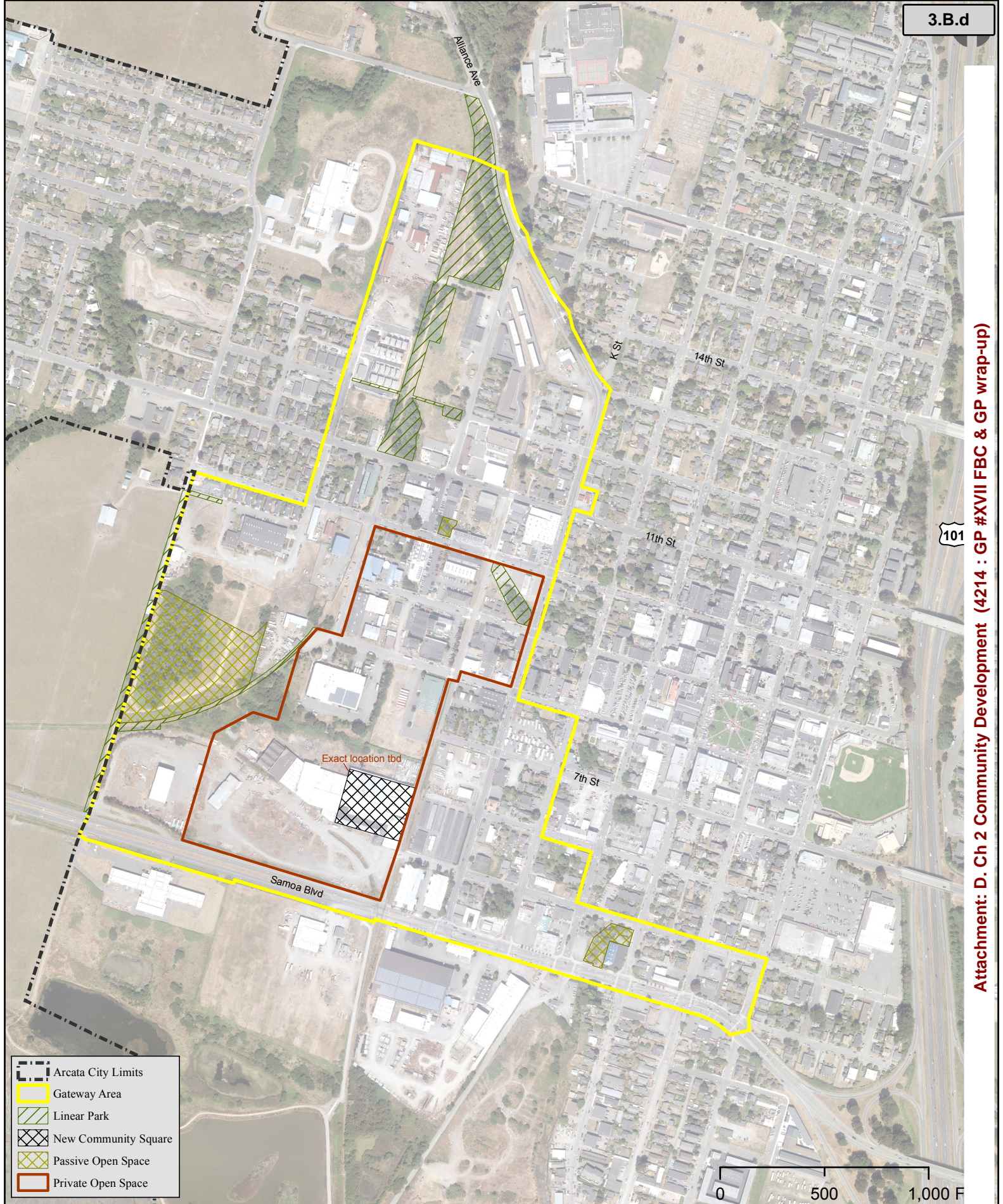
Table 8: Open Space Categories and Characteristics







TYPE	DESCRIPTION	LOCATION	SIZE	CHARACTERISTICS
Community Square (may be named “Barrel Square”)	Outdoor public gathering place; New focal point for civic use social interaction serving residents and workers.	Southwest Industrial Area (precise location to be determined in future with development agreement or similar documentation between property owner and City).	0.5 to 1.6+ acres	Framed by buildings with active ground-floor uses. The adjacent streetscapes with tree-lined streets, wide sidewalks, and active frontages with a retail focus. Designed to maximize sunlight and public access. May include retail, outdoor dining, and entertainment uses to generate lively pedestrian activity throughout the day and evening. Designed to accommodate community gatherings and events. Provides programming, such as farmers’ markets, food trucks, movies, and art exhibitions. Includes seating, site furnishings, and related elements (such as landmarks, public art, and other unique features) to encourage gathering and socializing. Encourage the incorporation of compatible active recreation amenities (bocce, swings, etc.)
Linear Park with Daylighted Creek	Linear space for community gathering, strolling, and access to nature; green connector between destinations. Rail-trail projects will be used for daily commuting.	Jolly Giant corridors currently undeveloped or suitable for daylighting. May also include railroad rights-of-way, unutilized City-owned public rights-of-way, and parcels dedicated as floodways.	ROW widths or 15 ft. min width from edge of riparian setback	Riparian habitat and natural resource protection with space for active and passive recreation. Supports trail-oriented activities, including walking, jogging, biking as well as daily commuting via trail . May incorporate smaller scale neighborhood park amenities, such as play areas, picnic areas, or exercise trails. Provides wildlife corridors and opportunities for non-motorized transportation.
Privately-Owned, Publicly-Accessible Open Space	On-site publicly-accessible open spaces under private ownership provided as part of new development.	South of 10 th Street and west of K Street; specific locations determined during project approval locations near housing, neighborhood commercial uses, major street corridors, and public paths are prioritized.	200 to 5,000 sq. ft.	Small plazas, landscaped areas and public art installations. Small-scale, open space available for civic purposes and commercial, intended as intimate spaces for seating or dining. Can also be used to create a formal space in front of a prominent building entrance. Encourage the development of publicly-accessible active recreation amenities (bocce, pickleball, basketball, tot lot, etc.) and areas designed play areas for young children.

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

POLICY CHAPTER 6: OPEN SPACE AND CONSERVATION

<p>Passive Open Space</p>	<p>Undeveloped lands left in a natural state for conservation or outdoor recreation.</p>	<p>Existing protected wetlands and riparian areas throughout Gateway Area, both public and private.</p>	<p>0.1 to 5.0 acres</p>	<p>Protect and manage unique or significant natural features, such as rivers and streams, wetlands and marshes, environmentally sensitive areas, and wildlife habitats. May allow for wildlife viewing, environmental interpretation and education, and nature photography.</p>
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-  Arcata City Limits
-  Gateway Area
-  Linear Park
-  New Community Square
-  Passive Open Space
-  Private Open Space

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 7: Conceptual Open Space Plan

Open Space and Conservation Policies

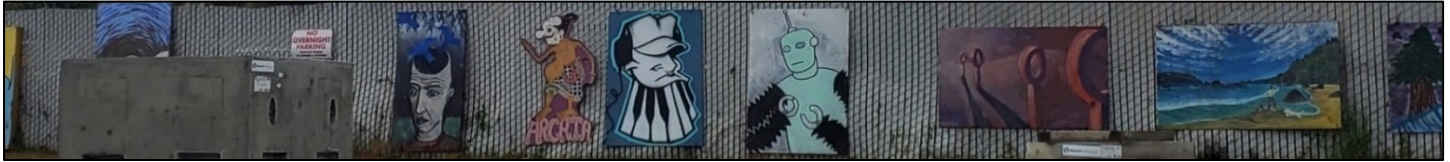
Objective: Establish an urban environment that enhances and complements stormwater management and the open space environment. open space system along streets and paths that enhance community interaction and compliment the urban environment. Promote public gathering, enjoyment, and active and passive use by a broad range of the community.

- GA-6a. Open Space Concept.** Provide for an integrated network for publicly accessible open space, including a new park site, consistent with the concept shown in Figure 7 and Table 8. The City shall actively encourage neighboring property owners to partner to develop Open Space over multiple sites.
- GA-6b. Diversity of Open Space Types.** Provide for a range of open space types, including urban plazas, pocket parks, linear parks adjacent to creeks, natural open spaces.
- GA-6c. Range of Activities.** Accommodate within open spaces a range of activities for all ages and abilities including sitting, walking, gathering, gardening, play and contemplation. Encourage and incentivize opportunities for active recreation (e.g., bocce, basketball, pickleball, etc.) within the Gateway Area boundary.
- GA-6d. Design Quality.** Ensure that accessible open spaces are visually inviting, safe and interesting. Encourage the development of universally designed park space, creating greater accessibility for the public than required under the Americans with Disabilities Act (ADA).
- GA-6e. Privately-owned Publicly Accessible Open Spaces.** Establish a series of privately-owned publicly accessible open spaces in the central area shown in Figure ~~X~~7. Ensure that these spaces are:
- Linked together by safe and convenient bike/pedestrian facilities;
 - Visible, accessible, and activated by ground floor uses including retail stores and restaurants; and
 - Coordinated so that a variety of spaces are provided (courtyards, tot lot, a sculpture garden).
 - Adequately maintained for public access and safety.
- GA-6f. Linear Park with Daylighted Creek.** Establish a linear park with passive recreational amenities adjacent to daylighted and restored segments of ~~the~~ Jolly Giant Creek.
- GA-6g. Bike/ped Connections.** Establish new off-street bike/ped connections to connect open spaces and activity centers in the Gateway area. Enhance connections between the Plan Area and parks and open space destinations near to the Plan Area, such as Arcata Plaza, Shay Park, and Arcata Marsh.
- GA-6h. Public Plaza in Southwest Industrial Area.** Establish a new public open space or plaza in the Barrel District that provides formal public space within the Barrel District that functions as a vibrant, pedestrian-scale open space serving both as a neighborhood gathering space and a regional draw. ~~is more than just a green area~~ Design a space of similar size and quality to, but ~~that is also~~ entirely distinctive from the existing Arcata Plaza.
- GA-6i. Enhanced Rail-Trail.** Could include new recreational amenities and stormwater management within and adjacent to the rail-trail.
- GA-6j. Vacant/Underutilized Land Development.** Allow for the development of existing vacant and underutilized properties with low natural resource value as a strategy to permanently protect high resource value open space and provide high-quality open space amenities for residents.
- GA-6k. Wetland Areas.** Maintain a no net loss standard but allow for passive recreational uses within and around wetland area in southwestern Gateway Area. Provide for bicycle and pedestrian connections to this area from other Plan Area locations. Enhance and restore wetland functions where feasible. Use mitigation and restoration together to consolidate scattered low-quality wetlands into larger higher quality wetland complexes through the Community Benefits Program.
- GA-6l. Jolly Giant Creek.** Require the restoration and enhancement of the Jolly Giant Creek north of 11th Street. Encourage the daylighting of creek segments south of 11th Street as part of new development projects. Consider

[additional opportunities for daylighting Jolly Giant Creek, especially along 10th Street, including bridges, road closures, and other alternatives to expose more open creek miles.](#)

GA-6m. Incentivize Privately-Owned Open Spaces as a Community Amenity. Utilize the community benefit program to incentivize the creation of new privately-owned, publicly-accessible open spaces in the Plan Area.

GA-6n. [Explore Tribal Partnerships in Restoration Project Planning.](#) [Explore opportunities for partnerships with the Wiyot Tribe and other Wiyot-Area Tribes when pursuing funding for habitat restoration within the Gateway Area, and seek guidance from appropriate Tribal representatives as part of the habitat restoration planning process.](#)



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Open Space Programs

Imp-GA-6.1. ~~Parkland Dedications and Fees.~~ ~~Require residential development projects to dedicate land and/or pay fees for publicly accessible open space within the Plan Area consistent with the Quimby Act, Land Use Code Section 9.86.030 (Park Land Dedication and Fees).~~

Imp-GA-6.2. Open Space Concept Diagram Revisions. As open space is provided, revise the conceptual open space diagram shown in Figure 7 to accurately reflect built conditions and ensure that subsequent open space provided is consistent with the Plan vision and to ensure diversity/non-redundancy of open space types.

Imp-GA-6.3. Public Open Space Acquisition Program. Seek to secure publicly accessible Open Space within the Gateway Area Boundary, both by easement and fee title, with an emphasis on acquiring lands for a publicly accessible open space in the Southwest Plan Area. Identify, develop, and prioritize parkland and Open Space projects that are eligible for Federal and State funds and continue to pursue all available options for funding new and improved parkland facilities.

Imp-GA-6.4. Open Space Habitat Map and Potential Project List. Develop a map and Potential Restoration Projects List of the Gateway Area. The Open Space Map should catalogue existing Open Space as either high- or low-quality resource value, with the intent of permanently protecting high resource value open space, while developing areas with lower-quality resource value. The map should build upon previous restoration efforts and existing creeks/culverts maps. Use this map to identify parcel-specific community amenity opportunities, as appropriate (e.g. creek daylighting)



Key to Revisions (Font Color/Highlighting)

Proposed changes recommended by each committee are shown in underline/strikeout text with color coded committee text as follows:

Transportation Safety Committee

Parks and Recreation Committee

Creeks and Wetlands Committee

Forest Management Committee

Historic Landmarks Committee

Energy Committee

Economic Development Committee

Public member/entity

Staff edits

7. MOBILITY

Overview

Setting the Tone for the Built Environment

In the simplest sense, mobility is the movement of people and goods in a multi-modal circulation system for passenger vehicles, delivery trucks, public transit vehicles, pedestrians, bicycles, and other non-motorized modes of travel, as well as the facilities for parking and storing all forms of vehicles. On a deeper level, mobility infrastructure is a public good and the foundation of the built environment. The dimensions, styles, and designs of streets and sidewalks set the tone for the Plan Area's architecture and building forms, determine the viability of commercial businesses, and fundamentally shape residents' quality-of-life. In other words, a safe, accessible, and efficient mobility network is crucial to the overall Plan vision.

Accommodating Planned Growth

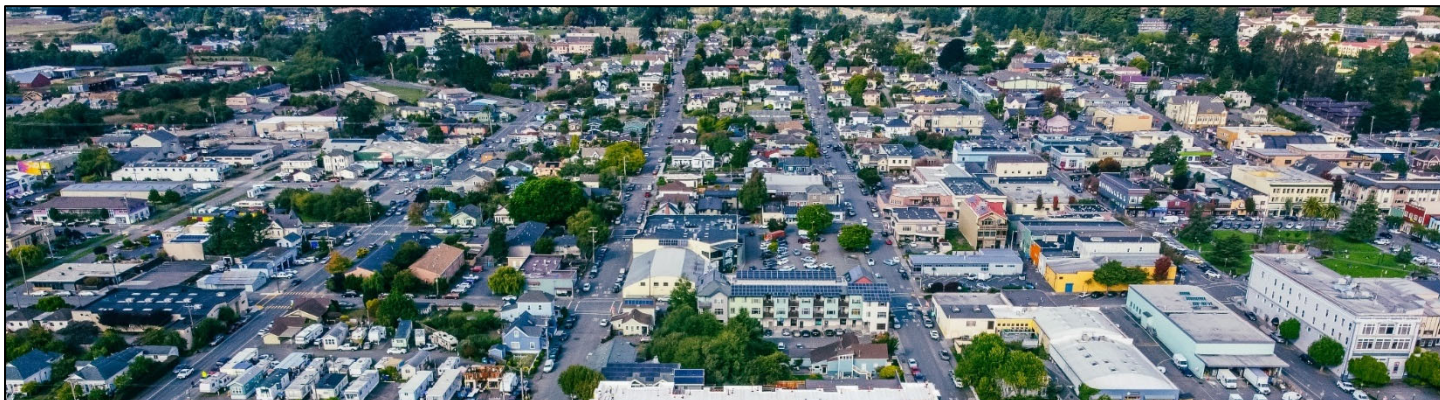
The Plan Area's mobility infrastructure must be sufficient to accommodate the travel volumes associated with planned growth outlined in the Policy sections above. Ultimately, the new circulation system must accommodate up to 3,500 new residential units as well as a substantial number of new commercial businesses, while still supporting a robust network of bicycle pedestrian and transit infrastructure to ensure all transportation modes remain comfortable, convenient, safe, and attractive to residents, workers, students, and visitors.

Options for a Car-free Lifestyle

While much of the Plan Area is currently motorized-vehicle-dependent, this section outlines strategies that seek to increase safe access to bicycling, walking, and transit. As stated repeatedly throughout this document, the Gateway Area is envisioned to be a sustainable neighborhood in which residents have the option to live car-free without sacrificing safety or convenience. To do this, transportation needs must be fully met via transit, pedestrian, and bicycle infrastructure that connects seamlessly to all other City destinations. This has the subsequent benefit of supporting the City's goals of reducing greenhouse gas emissions and vehicle miles traveled (VMT).

Level of Detail for Mobility Planning

Planning mobility infrastructure that connects the built environment, accommodates planned growth, and allows for a car-free lifestyle requires more detail than planning for the rest of the built environment. While the City establishes the required architectural standards for buildings, private developers ultimately design and construct each individual structure. Mobility infrastructure, on the other hand, must be envisioned, designed, constructed, and maintained entirely by the City. In addition, while individual buildings are typically designed and constructed one-at-a-time, transportation facilities must be addressed across large portions of the Plan Area, often spanning dozens of consecutive blocks. Accordingly, this Plan provides detailed concepts for the future mobility network to support the City's overall vision for the future of the Gateway Area.



Mobility Strategy

Upholding the Area Plan’s Vision Statement, the Plan Area will have a realigned roadway network that supports more efficient vehicular travel patterns while expanding from and connecting to bicycle and pedestrian facilities than currently exist. Figure 8 and Figure 9 outline a vision for the future circulation system that incorporates existing and new one-way couplets for vehicular traffic, expands bicycle and pedestrian facilities, and extends a new Class I trail through the Gateway Area. The concepts are reviewed in more detail in the *Mobility Concepts* section following the figures.

As shown in Figure 8, K Street is converted to a single-lane one-way northbound roadway. The conversion of K Street from a two-way to one-way roadway allows a reduction in the paved area dedicated to vehicular traffic, incorporating minimum six-foot sidewalks, a Class IV separated bikeway, and turn pockets at intersections, where determined necessary. L Street is constructed as a southbound companion to the new one-way K Street. The current Class I trail on the L Street alignment is retained but shifted to be adjacent to the roadway with a landscaped buffer separating the Class I trail from the roadway, and maintains bidirectional travel for bicyclists throughout the Gateway Area. The one-way couplet system will reduce conflict points at intersections by eliminating a direction of vehicular traffic entirely. The one-way couplet system will also provide acceptable vehicular operations, even as regional and local traffic volumes increase, without the need to widen existing intersections or install new intersection controls like traffic signals or roundabouts. One-way intersections with two-way and one-way streets will also benefit pedestrians and cyclists by shortening crossing distances, thus reducing exposure of vulnerable users to the impacts of vehicular traffic, from noise and odors to injuries and cardiovascular impacts.

A new Class I trail generally following the railroad right-of-way along the N Street alignment connects from Alliance Road at the north of the Plan Area to the Barrel District on the south end. Two trail connections from this new path to the west ends of 8th Street and 9th Street are also created, offering people bicycling and walking more direct access through the Gateway Area. A new Class I trail that follows the Q Street alignment south from 10th Street connects to the Barrel District and the Class I trail up to N Street, and another east/west Class I trail provides access from these two trails through the Barrel District to L Street. In addition, 8th Street and 9th Street are extended westward as an east/west couplet that terminates at N Street, expanding the circulation pattern that currently exists east of I Street. These conversions of two-way roads to single-lane one-way roads provide more space for bicycle and pedestrian facilities, allowing for pedestrian and bicycle infrastructure that fully meets the mobility needs of residents and that connects seamlessly to key destinations throughout the City.

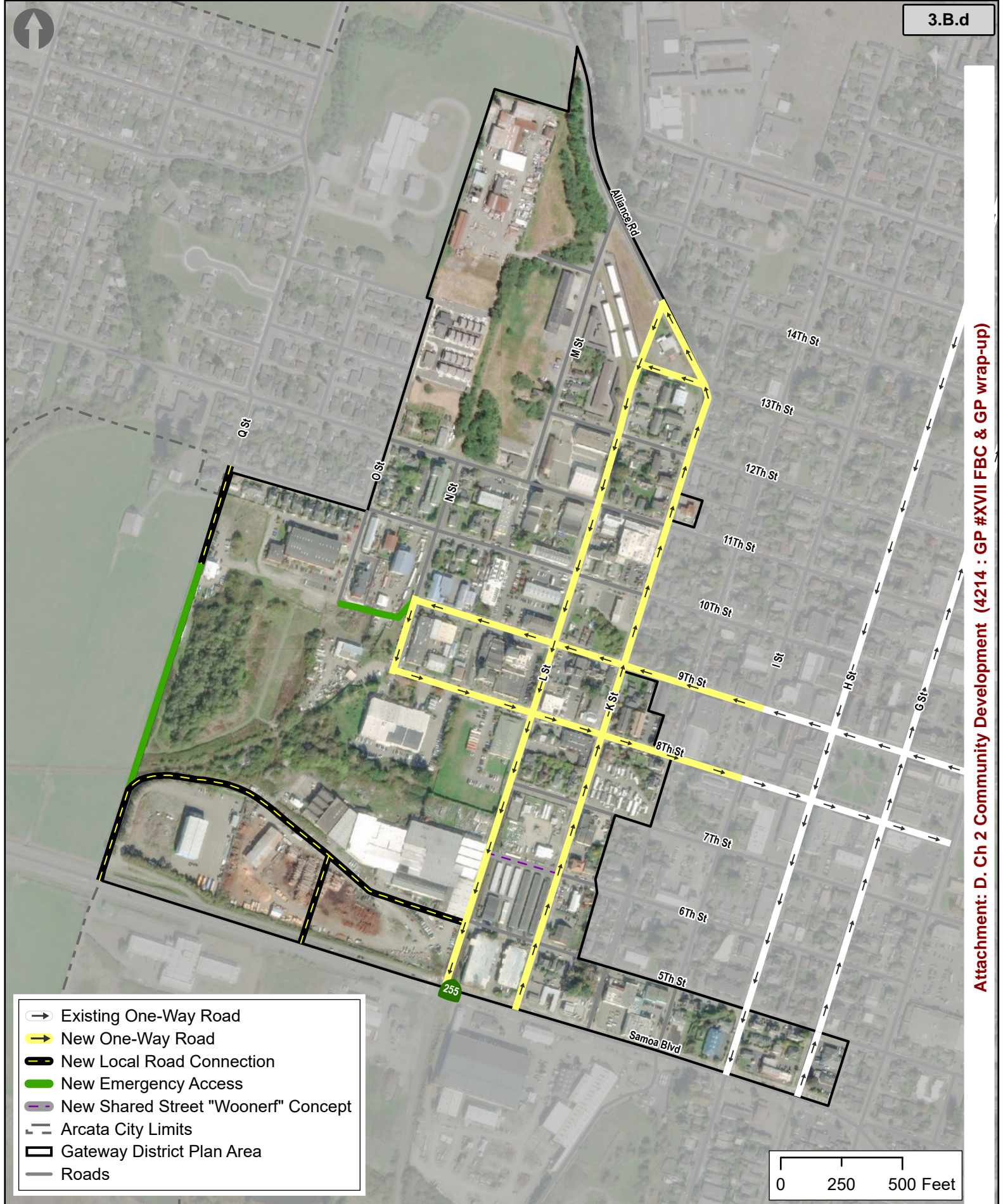


Key to Revisions (Font Color/Highlighting)

Proposed changes recommended by each committee are shown in underline/strikeout text with color coded committee text as follows:

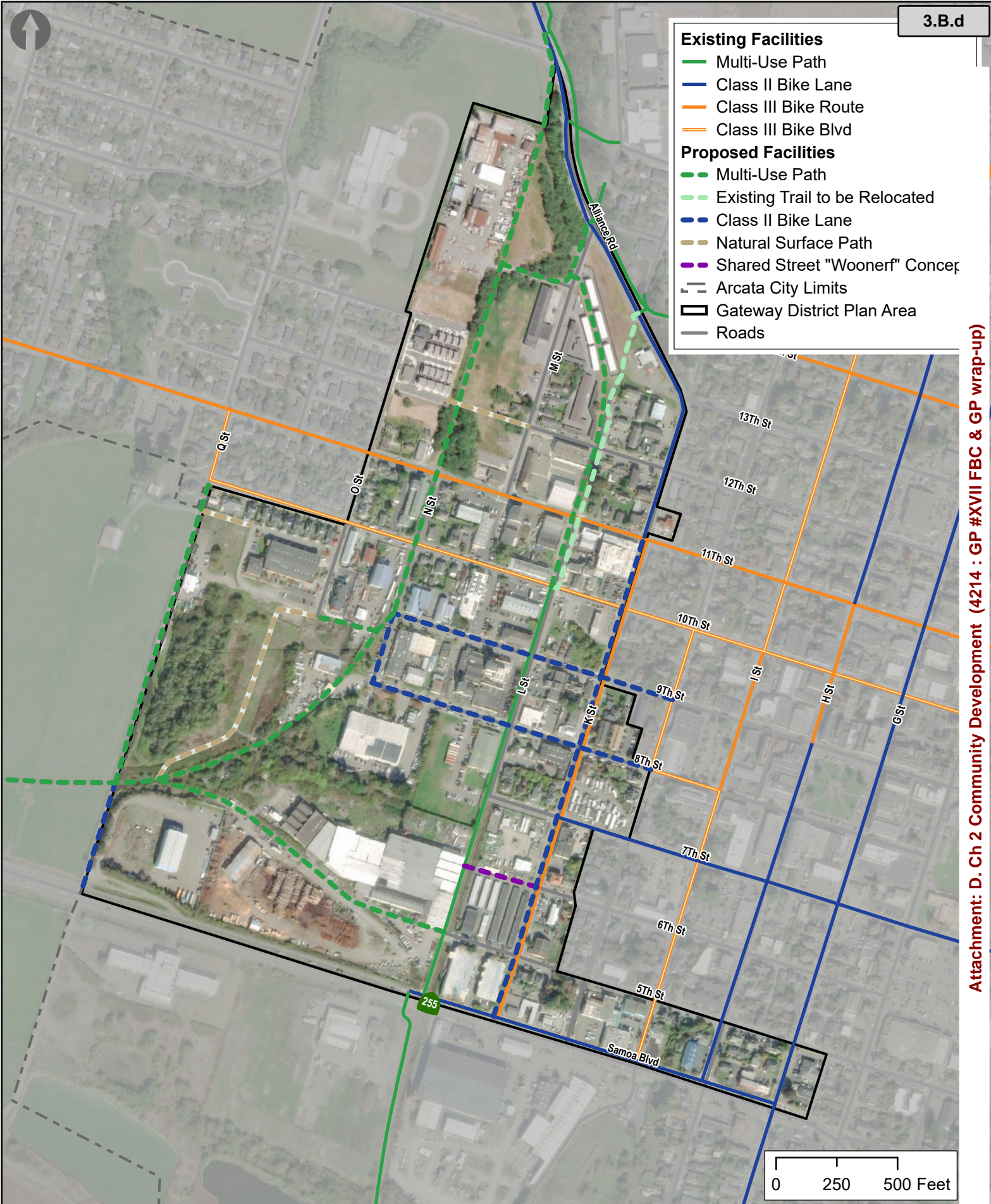
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Figure 8 Proposed Vehicular Circulation



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 9 Proposed Active Transportation Circulation

0 250 500 Feet

Mobility Policies

Objective: Prepare for a substantially realigned transportation network within the Gateway Area that significantly enhances bicycle and pedestrian facilities while also creating a more efficient vehicular circulation pattern with better north-south and east-west connections for all modes of travel. Seamlessly link the Gateway Area to key destinations throughout the City via non-motorized facilities to support opportunities for a car-free lifestyle.

GA-7a. Plan the Circulation System to Accommodate Planned Growth. In planning for improvements to the overall circulation system, design the system to accommodate the planned amount of growth outlined in other policies. Ensure the circulation system supports a functioning, safe, sustainable multi-modal network. Support increased demands for all efficient forms of mobility emphasizing alternative modes – vehicles, trucks, transit, bicycles, and pedestrians, bicycles, and other non-motorized or shared transit options, then vehicles, and trucks, in an effort to induce demand of multimodal transit alternatives and implement transportation demand management strategies, in keeping with Citywide Circulation Element policies (see also, GA-8a).

GA-7b. Design Mobility System per Plan Figures. Design and construct the mobility and circulation system of the Plan Area per Figure 8 and Figure 9 and the Cross Section and Intersection Design Concepts above, as well as in accordance with the Open Space, Streetscape, and Site Development sections of this Area Plan. In the engineering design stage of implementing the above cited Figures/Concepts, allow for deviations and alterations such as:

- a. Throughout the entire Plan Area, sidewalk widths may increase beyond six feet, especially on the north sides of east-west streets where expanded sidewalks on the sunny side of the street would allow welcomed outdoor seating, and at well-traveled pedestrian boulevards to ensure a clear path of travel. Adjusting sidewalk widths to will necessitate adjustments to the dimensions of other features, such as drive lanes, parking lanes, bike lanes, outdoor seating, street furniture, and the like. On-street parking lanes may need to be eliminated.
- b. Throughout the entire Plan Area, on-street parking angles may be adjusted as need to be either parallel, perpendicular, angled-in, or reversed angled-in. Adjusting parking angles may necessitate adjustments to the dimensions of other features, such as sidewalks, drive lanes, bike lanes, etc.
- c. Throughout the entire Plan Area, the presence of on-street parking may be eliminated in favor of adding or enhancing non-motorized facilities, such as sidewalks, bike lanes, landscaping, Class I trails, etc.
- d. Outside of City rights-of-way, the alignments and widths of Class I trails (i.e., separated shared use paths) may need to be adjusted based on environmental constraints, community needs, the availability of right-of-way, and other factors. Seek opportunities in public rights-of-way to daylight creeks using bridges when reconstructing bike/bed infrastructure.
- e. Throughout the entire Plan Area, Class II bicycle facilities (i.e., standard bike lanes) may be converted to Class IV bicycle facilities (i.e., protected bike lanes), which may necessitate adjustments to the dimensions of other features.
- f. Throughout the entire Plan Area, the widths, locations, styles, and details of various features may deviate from the cited map Figures at the time of final design based upon available traffic data, design context, and the latest guidelines provided by Caltrans, FHWA, AASHTO, NACTO, and other reliable sources. Features that are likely to require deviations include pavement markings, pavement color, pedestrian bump-outs, turn lanes, traffic control features, landscaping, and similar components.
- g. The junction of 13th Street, K Street, L Street, and Alliance Ave may require an alternate design depending upon right-of-way acquisition, available traffic data, design context, and the latest guidelines provided by Caltrans, Federal Highway Administration, American Association of State Highway and Transportation Officials, National Association of City Transportation Officials, and other reliable sources.

- h. New roadway connections where none currently exist (such as the far west end of 6th Street connecting K Street to the L Street right-of-way) may be designed and constructed as either new vehicular roadways, pedestrian-only thoroughfares, or bicycle/pedestrian facilities that allow restricted vehicular traffic. The City Engineer will determine which type of facility to design and install based on available traffic data, existing environmental constraints, community interests, right-of-way availability, and other engineering factors, and Plan principles.
- i. The trail within the Q Street right-of-way south of 10th Street may eventually need to be converted into a full vehicular roadway with a cross-section similar to other two-way roads proposed within the Plan Area.
- j. The final design of transportation facilities within the Barrel District may change substantially, but should follow the parameters outlined in Policies GA-7e and GA-7f.
- k. Emphasize Class IV bike lanes where greatest benefit, and not in conflict with other community values or amenities where warranted.
- l. Where available, pursue Opportunities for “green streets” infrastructure in streets/public right of ways, and provide for storm water features off-site (i.e., “storm water banks”). Consider opportunities to improve storm water drainage for the Jolly Giant Creek watershed.
- m. Throughout the Plan area, design sidewalks and street crossings for maximum accessibility. Accessibility features may include but are not limited to: widening sidewalks, requiring high-visibility/striped crosswalks, installing bulb-outs, pedestrian activated crossing signals, and aligning ramps with path-of-travel.
- n. Create separated walk/bike lanes in multi-use trails with explanatory signage on placement and how to safely pass, as deemed necessary based on increased use in future.
- o. Provide sidewalks or multi-modal trails on both sides of all streets in the Plan Area.

GA-7c. Balanced Transportation System. Create and maintain a balanced transportation system with choice of bus transit, bicycle, and pedestrian as well as private automobile modes. Reduce the percentage of trips that are made by automobile and provide the opportunity, incentives, and facilities to divert trips from automobiles to other modes. Provide negative incentives, such as parking meters, permit parking, time limited parking, carpool incentives, and other targeted parking measures that encourage alternative modes utilizing “induced demand” strategies.

GA-7d. Plan for Enhanced Transit Lines and Stops. As growth occurs in the Plan Area, work with relevant transit agencies, major employers, and area schools to plan for enhanced public transit and school bus lines and new transit stops ~~in order~~ to accommodate the new growth.

GA-7e. Consider Non-motorized Campus Layouts. For areas that have incomplete block patterns and/or are currently lacking in vehicular roadways (such as the Barrel District), consider providing limited to no new facilities for motorized vehicles. Instead, consider creating a campus layout with vehicular access on the perimeter and robust non-motorized facilities throughout the interior. Plan for the infrastructure that would be required for these areas to serve as key park and ride/transit hubs. Where new vehicular roadways are constructed within currently roadless areas (such as the Barrel District), provide for a wide right-of-way whose cross section includes ample on-street parking, narrow vehicle lanes, bike lanes, sidewalks in excess of six feet, street trees, and enhanced pedestrian crossings at least every 300 feet.

GA-7f. Barrel District Master Plan. For the Barrel District, require property owners to develop a Master Plan for a high-density walkable mixed-use residential campus with minimal vehicular infrastructure and overall site design that supports a pedestrian-friendly public realm. Require that the Master Plan includes plans for a circulation system that is generally consistent with Figure 8 and Figure 9 and in accordance with the Open Space, Streetscape, and Site Development sections of this Area Plan. Allow the Master Plan to relocate the proposed circulation facilities within the Barrel District from what is shown in this Plan as long as the ultimate design honors the basic theme

and overall design parameters consistent with the Policies herein. Require that new vehicular roadways provide for a wide right-of-way whose cross section includes ample on-street parking, narrow vehicle lanes, bike lanes, sidewalks in excess of six feet, street trees, and enhanced pedestrian crossings at least every 300 feet.

- GA-7g. Finish Incomplete Blocks with Active Transportation Infrastructure.** Where the urban grid pattern is interrupted or incomplete, evaluate opportunities to continue the circulation block patterns with new connections that consist of entirely non-vehicular active transportation facilities.
- GA-7h. Mobility Infrastructure that Supports Car-free Lifestyle.** Plan and implement the mobility and circulation infrastructure of the Plan Area to support a car-free lifestyle, [increase pedestrian safety](#), reduce greenhouse gas emissions, and minimize vehicle miles traveled, including:
- a. **Safe and Attractive Pedestrian Facilities.** Connect the Plan Area to the Downtown/Plaza core with safe and attractive pedestrian friendly walking routes that incorporate art and street lighting.
 - b. **Pedestrian-friendly Streetscapes.** [Ensure that streetscape design and improvements prioritize pedestrian circulation that promote walkability and support a car-free lifestyle and accessibility for all ambulatory modes.](#)
 - c. **Shorten Pedestrian Crossing Distances.** [Create additional and safer methods for crossings](#) Shorten distances for pedestrian crossings along K Street and 11th Street by [shortening distances for pedestrian crossings](#) to improve overall walkability in the Plan Area. Evaluate other roadways within the Plan Area that warrant shortened pedestrian crossings, [with an emphasis on areas with planned or developed alternative transportation infrastructure, such as 8th and 9th Streets.](#)
 - d. **Curb Extensions in All New Roadways.** In all newly created roadways, incorporate curb extensions (“bumpouts”) to increase pedestrian visibility and safety at crosswalks, calm traffic speeds, and provide space for rain gardens, tree planting, street furnishings, and other amenities.
 - e. **Widened Sidewalks.** Explore sidewalk widening strategies that include land dedication or easements to create unobstructed accessible pedestrian pathways.
 - f. **Intra-City Non-motorized Connectivity.** Reduce vehicle trips from other parts of the City by creating pedestrian and bicycle-friendly corridors that draw residents and visitors to enter the Plan Area via means other than motorized vehicles. Fulfill the potential of the existing and planned Class I trails by planning for expanded perpendicular connections that will draw bikes/peds from beyond the Plan Area.
 - g. **Ride Share.** [Support ride share in various modes \(car, bike, etc.\) through public and private infrastructure, ensuring complete systems designed to accommodate access to shared facilities. Improvements and programs should include public options, such as bike share racks or carpool parking, public-private partnerships, such as zip-car and Tandem Mobility bike share, and private facilities or programs, such as project-based car share.](#)
- GA-7i. No Net Loss of Class I Trail System.** [In general, retain the current total linear feet of Class I trails within the Plan Area, even if current facilities must be realigned or relocated to other routes within the Plan Area. For instance, if implementing the realigned roadway network shown in Figure 8 and Figure 9 impacts the existing Class I Rail-to-Trail facility within the L Street right-of-way, then design and construct a new Class I trail in another location within the Plan Area. In limited circumstances, the City shall retain the discretion to allow an applicant to demonstrate removal or relocation of Class I Trail sections would improve active transportation access and connectivity. Collaborate with the Great Redwood Trail Agency and other landowners and agencies to retain and expand the Class I trail and Class 4 bikeways throughout the Plan area, including along L Street.](#)
- GA-7j. Incentivize Active and Alternative Transportation as a Community Amenity.** Through the Gateway Area community benefit program, allow increased development intensity and simplified development processes for projects that provide on-site active and alternative transportation amenities, such as [car share/bike share](#), [free electric vehicle charging stations](#), employee showers, on-site [covered and secure indoor](#) bike parking, bus passes for residents [and/or employees](#), dedication of parcel frontage to transportation uses, [charging stations for e-](#)

bikes, shared parking, and related amenities that stimulate non-motorized and zero-carbon transportation options above and beyond current requirements of state law.

- GA-7k. Incentivize Dedication of Parcel Frontage as a Community Amenity.** Through the Gateway Area community benefit program, allow increased development intensity and simplified development processes for projects that dedicate parcel frontage for the creation of expanded right-of-way for the purposes of additional pedestrian facilities, off-street parking, open space, and/or other designated enhancements to the public realm. In locations identified as important pedestrian streetscapes, such as K, 8th, and 9th Streets, dedication may be a requirement to provide for the desired form and frontage.
- GA-7l. Parking Standards.** ~~Do not r~~ Disconnect parking minimums from land use and only require off-street parking as a development standard ~~in most only~~ in limited cases (e.g. hotels and other regional draws, employment centers). Discourage large volumes of off-street parking and instead support more valuable land uses and streetscapes that prioritizes human activity and movement. Encourage and incentivize clustered parking and un-bundling parking from rents, as well as the dedication of parcel frontage on block-long development projects that can be dedicated to additional on-street parking.
- GA-7m. Parking Lot Locations.** Disallow the placement of parking lots along street frontages in the interest of maintaining continuous building frontages along the primary commercial streets and improving walkability. Parking lots and structures must be located behind buildings, or otherwise located subordinate and obscured by design features.
- GA-7n. Minimize Vehicle Trips via Land Use.** Adopt and maintain zoning regulations that allow for a mix of land uses in order to reduce vehicle trips and the overall need for automobile use.

Mobility Programs

- Imp-GA-7.1. State and Federal Transportation Grants.** Actively track and pursue transportation infrastructure grants for the planning, design, permitting, and construction of the mobility improvements presented in this section. Identify, develop, and prioritize transportation projects that are eligible for Federal and State funds and continue to pursue all available options for funding new and improved circulation system facilities.
- Imp-GA-7.2. Circulation Projects in Capital Improvement Program.** Generate a list of circulation construction projects that will be required to implement this plan and then add those projects to the City's Capital Improvement Plan.
- Imp-GA-7.3. Fair Share Contribution for New Development.** Evaluate options and then adopt a program in which new development is responsible for constructing, dedicating, and/or paying a predetermined fair share contribution for any circulation system upgrades necessary to serve the development.
- Imp-GA-7.4. Master Plan for Barrel District.** For development projects within the Barrel District exceeding 10,000 sf, require property owners to develop a Master Plan per Policy GA-7f.



Key to Revisions (Font Color/Highlighting)

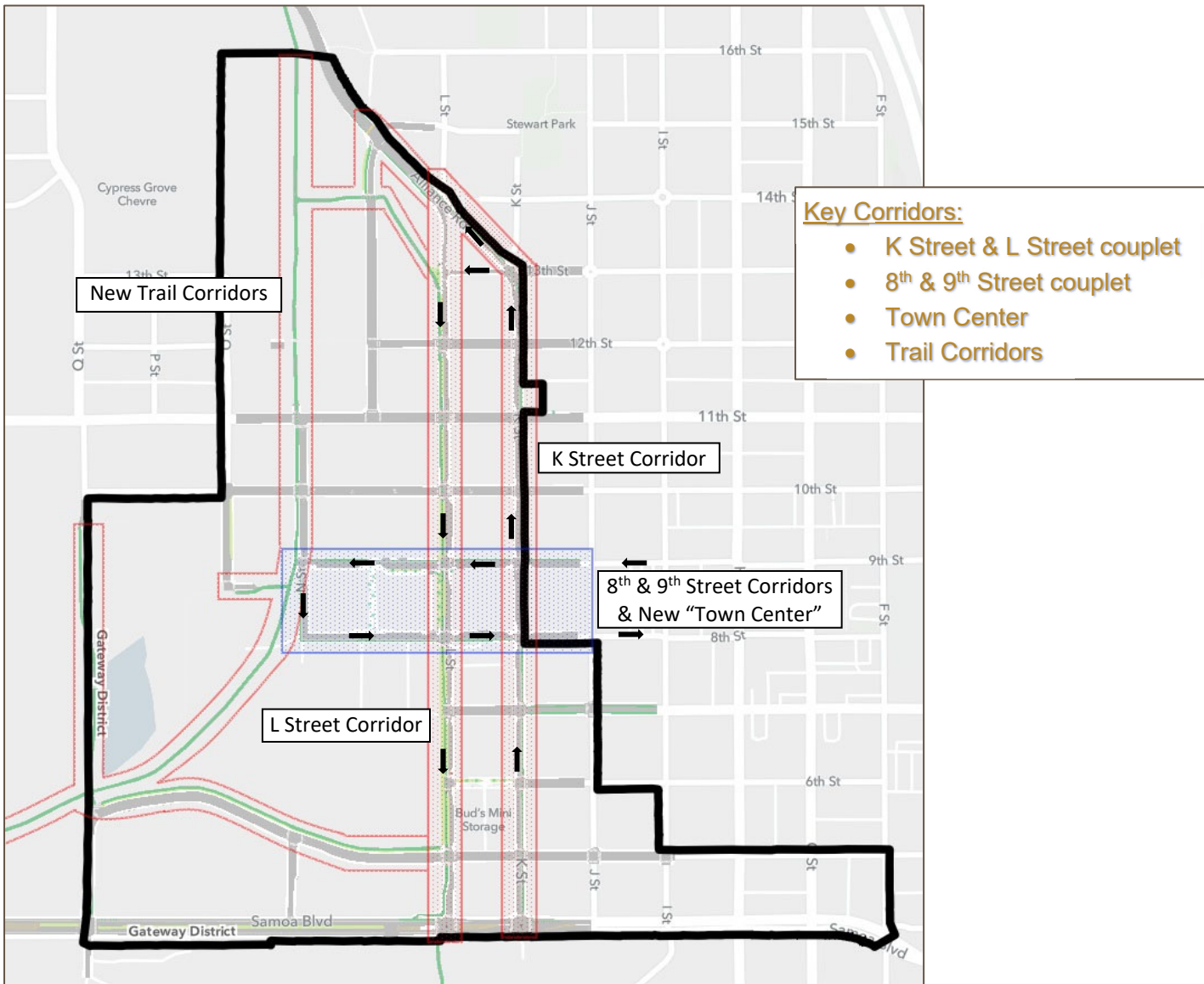
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Mobility Concepts

The next several pages illustrate details of the proposed Plan Area circulation system including strategies to address non-motorized connectivity, vehicular traffic, and parking, overviews of the proposed one-way street couplets, "typical" intersection treatments, and "typical" street features and cross sections. This concept proposes new parallel and connecting transportation facilities that have the potential to enhance corridor safety, enhance multimodal connectivity, reduce corridor congestion, and improve corridor reliability. These improvements are proposed to serve the overarching mobility goal of creating a "Complete Street" network that safely and efficiently provides access to all travel modes without increasing congestion to unacceptable levels.

Significant trail corridors and natural paths are proposed throughout the Gateway Area including Class I trails along the alignments of L Street, N Street, Q Street, the Barrel District in the south, and connecting up to the existing trail along Alliance Road. The image below provides a reference map of the corridors and areas of interest highlighted in greater detail in the following pages. Following the detailed illustrative pages is a Map Book which presents the design concepts for the majority of the Area, in Figures 10-a through 10-l.



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

COMPLETE STREET DESIGN – KEY ELEMENTS

Class I Shared-Use Path



Class I shared use paths are paved trails completely separated from the street. They allow two-way travel by people bicycling and walking and are often considered the most comfortable facilities for children and less experienced riders as there are few potential conflicts between bicyclists and vehicles.

Class IV Protected Bikeway



Class IV separated bikeways are on-street bicycle facilities that are physically separated from motor vehicle traffic by a vertical element or barrier. They can allow one- or two-way bicycle travel on one or both sides of the roadway. (Photo credit: Queensland DOT)

Class II Bike Lanes & Buffered Bike Lanes



Class II bicycle lanes are striped preferential lanes on the roadway for one-way bicycle travel. Some bicycle lanes include a striped buffer on one or both sides to increase separation from the traffic lane or from parked cars, where people may open doors into the bicycle lane.

Bicycle Conflict Markings



Green markings are used to highlight bicycle lanes or crossings at locations where potential conflicts with drivers are anticipated. They increase visibility of the bicycle facility and remind drivers to yield to bicyclists.

Bulb-outs and Curb Extensions



Curb extensions expand the sidewalk into the street at a crossing. Typically used where there is on-street parking, curb extensions improve visibility of pedestrians waiting to cross the street by moving them further into the roadway. They also improve safety by shortening the crossing, reducing the amount of time they are exposed to potential conflicts with drivers

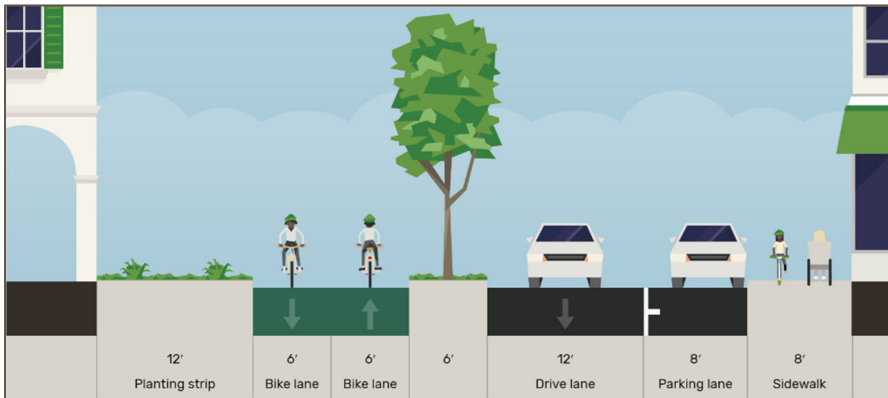
Bicycle Boulevards



A bicycle boulevard is a street which enhances bicycle safety by optimizing travel for bicyclists rather than automobiles. These roadways have low speed and low volume, traffic calming to discourage through or non-local vehicles, and prioritization of bicycles through intersections and along the roadway with signage and markings. (Photo Credit: City of Berkeley)

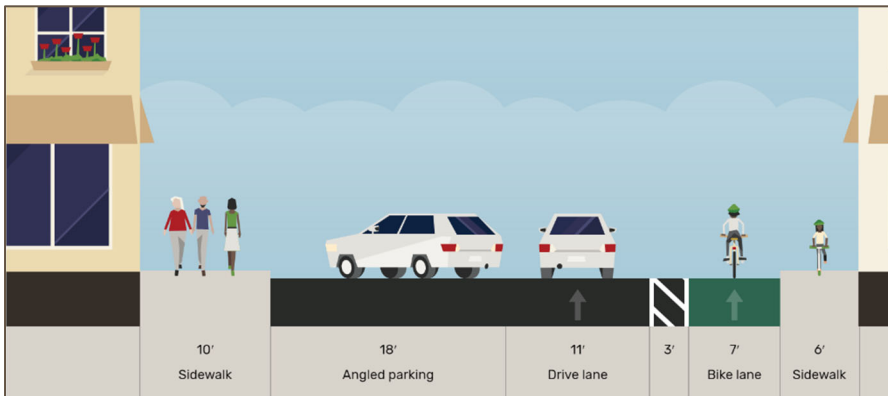
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ONE-WAY COUPLET SEGMENT CROSS-SECTIONS



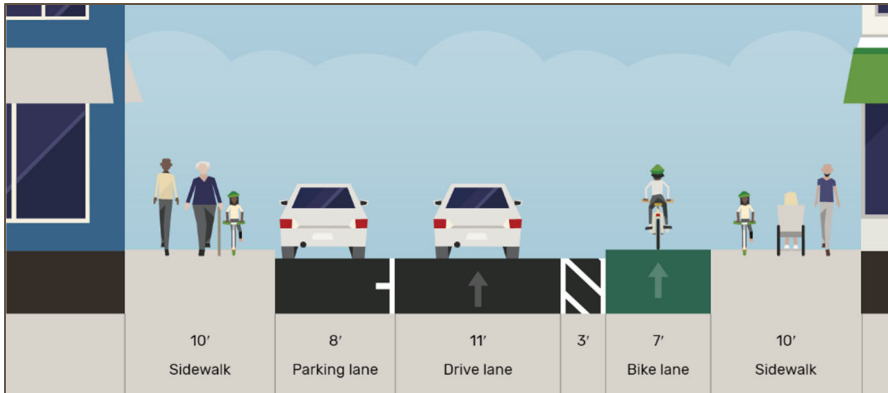
DESIGN A

- Class IV Protected 2-way Bikeway
- Bikeway separated by median, bollards, or planter boxes
- Sidewalks on one side
- Parallel parking on one side
- Single travel lane
- Typical for L Street



DESIGN B

- Buffered bike lane
- Sidewalks on both sides with varying width
- Angled parking on one side
- Single travel lane
- Typical for "Town Center" area of 8th Street and 9th Street



DESIGN C

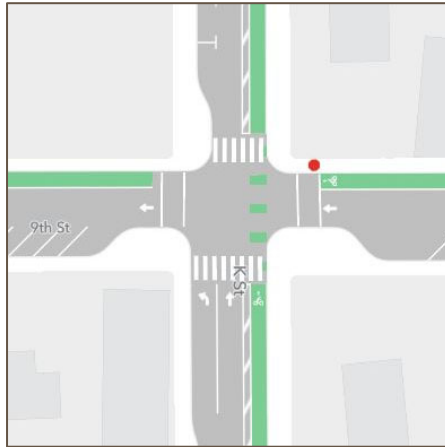
- Class IV Directional Bikeway
- Option to have bikeway level at sidewalk or roadway
- Buffer can also have bollards to enhance separation from travel lane
- Wide sidewalks
- Parallel Parking on one side
- Single travel lane
- Typical for K Street

The roadway cross-sections above are typical examples of the proposed circulation concepts for the one-way couplets of K Street, L Street, 8th Street, and 9th Street. Sidewalk widths may vary depending on space available. Other roadways in the Area Plan will retain their two-way configurations with parallel parking, sidewalks, and bicycle boulevards on some streets. Although on-street parking availability is a staple, incorporating multimodal access and connectivity throughout the area is priority.

Safe, comfortable, and convenient facilities should allow people of all ages and abilities to travel along or across the corridor to access destinations. Enhanced mobility along and across the corridor will support new and established residents and businesses. The corridors are also designed to support a healthy and diverse local economy by creating green and inviting places. In some cases, the width of the right-of-way can be enhanced through the dedication of private parcel frontages that can be used to supplement pedestrian facilities, off-street parking, open space, and/or other designated enhancements to the public realm.

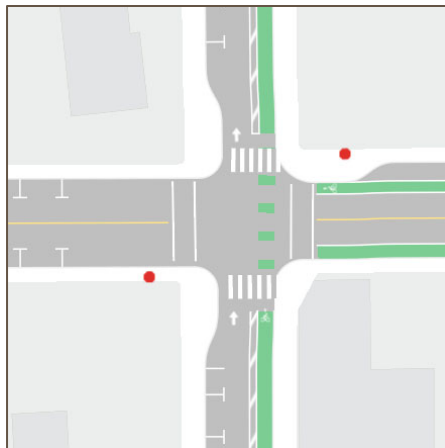
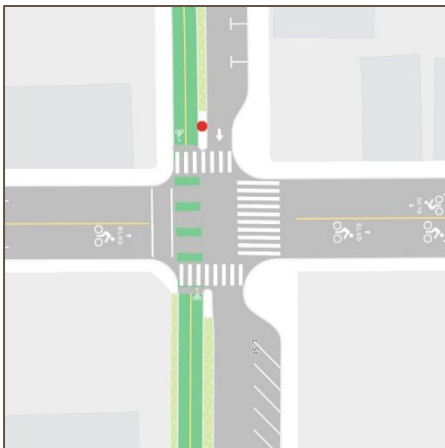
INTERSECTION DESIGN CONCEPTS

One-way Street at One-way Street



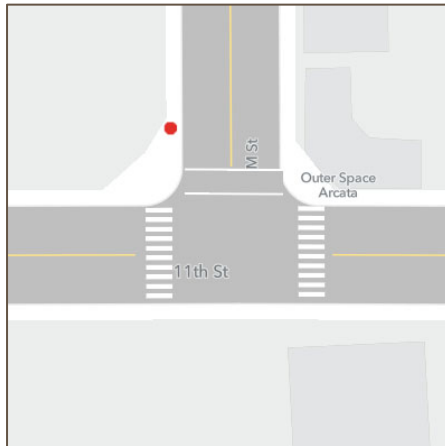
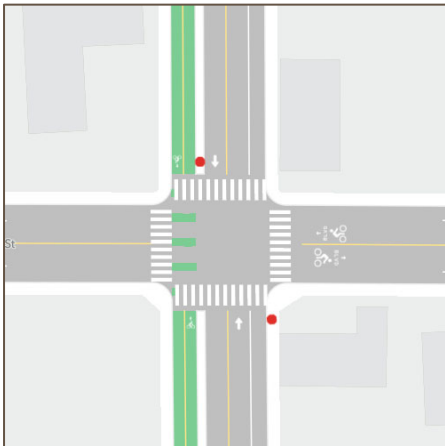
- Bulb-outs
- Shortest pedestrian crossings
- Intersection crossing markings for bicycles
- Enhanced visibility for both pedestrians and cyclists
- Slow vehicles through intersections
- Turn Lanes
- Stop Control on minor street

One-way Street at Two-way Street



- Bulb-outs, shorter pedestrian crossings
- Intersection crossing markings for bicycles along main route
- Separated facilities for cyclists and pedestrians
- Stop control as needed

Two-way Street at Two-way Street



- Intersection crossing markings for Bicycles
- Enhanced visibility for both pedestrians and cyclists
- Marked crossings at uncontrolled approaches
- Markings for Bicycle Boulevards
- Stop Control on minor approaches, except Bike Boulevards

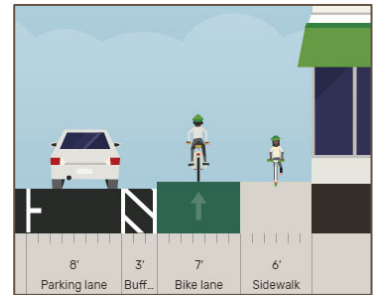
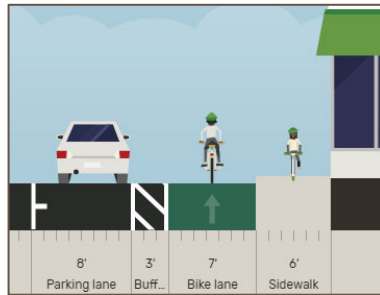
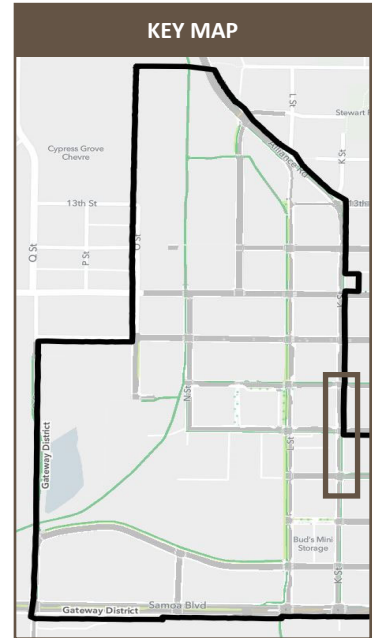
Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

K STREET ONE-WAY COUPLET NORTHBOUND



K Street serves as the main northbound roadway through the Gateway Area. A Class IV bikeway, adjacent to a sidewalk, provides safe and convenient access for all. A left turn lane is provided on K Street at higher-volume roadways such as 9th Street and 11th Street. Parallel parking provides convenient access to surrounding uses, while widened sidewalks enhance the pedestrian realm.

The bikeway along K Street can either be implemented as a buffered Class II bike lane or a directional Class IV bikeway. Class IV bikeways provide the option to have the bikeway level with either the roadway or the sidewalk. A variety of treatments can be used to separate the bikeway from the street, including bollards, raised or planted medians, or planter boxes.



Wide sidewalks and parallel parking balances the use of this space.



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K STREET & L STREET COUPLET – NORTH END JUNCTION



The implementation of the K Street and L Street couplets allows for more space for bicycle and pedestrian facilities while also improving traffic flow at intersections like 11th Street and K Street that would otherwise perform poorly with increased congestion in the future.

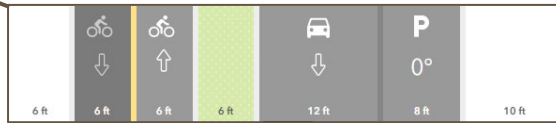
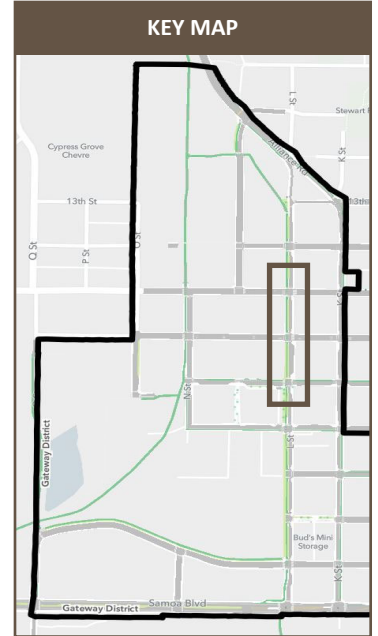
Alternative

As shown in the upper right corner, a left turn lane at the couplet junction could replace a new one-way road across 13th Street.

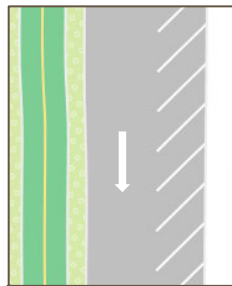
- K Street transitions from a one-way couplet back to a two-lane roadway (Alliance Road) south of 15th Street.
- L Street transitions from Alliance Road to the complementary southbound one-way roadway.
- Vehicles heading northbound will be able to turn around at 13th street, which connects to L Street southbound.
- Vehicles heading southbound will be able to turn around via 12th Street.
- The Class IV separated bikeway on L Street continues north as a Class I path and connects to M Street, which also provides a connection to the Class I path along Alliance Road to the north and the Class I path heading southwest to N Street.
- K Street provides a Class IV directional bikeway which continues further north along Alliance Road.
- A 12-foot wide sidewalk is also provided along Alliance Road as it transitions to L Street.

L STREET ONE-WAY COUPLET SOUTHBOUND

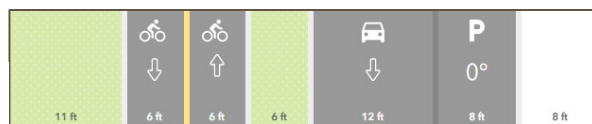
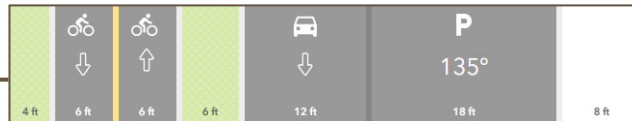
L Street serves as the primary route southbound through the Gateway Area. The existing Class I trail along the L Street alignment would be converted to a bidirectional Class IV bikeway on the west side of the roadway. This bikeway can be separated via various design elements such as a green-space median, potentially with trees, planter boxes, or other features. The Class IV bikeway would be designed to provide equivalent or increased levels of bicycle access and connectivity when compared to the existing Class I trail.



The corridor mainly consists of parallel parking on the east side, with only one block of diagonal parking. Diagonal parking increases the number of parking stalls at the expense of some traffic capacity where commercial growth is desired.



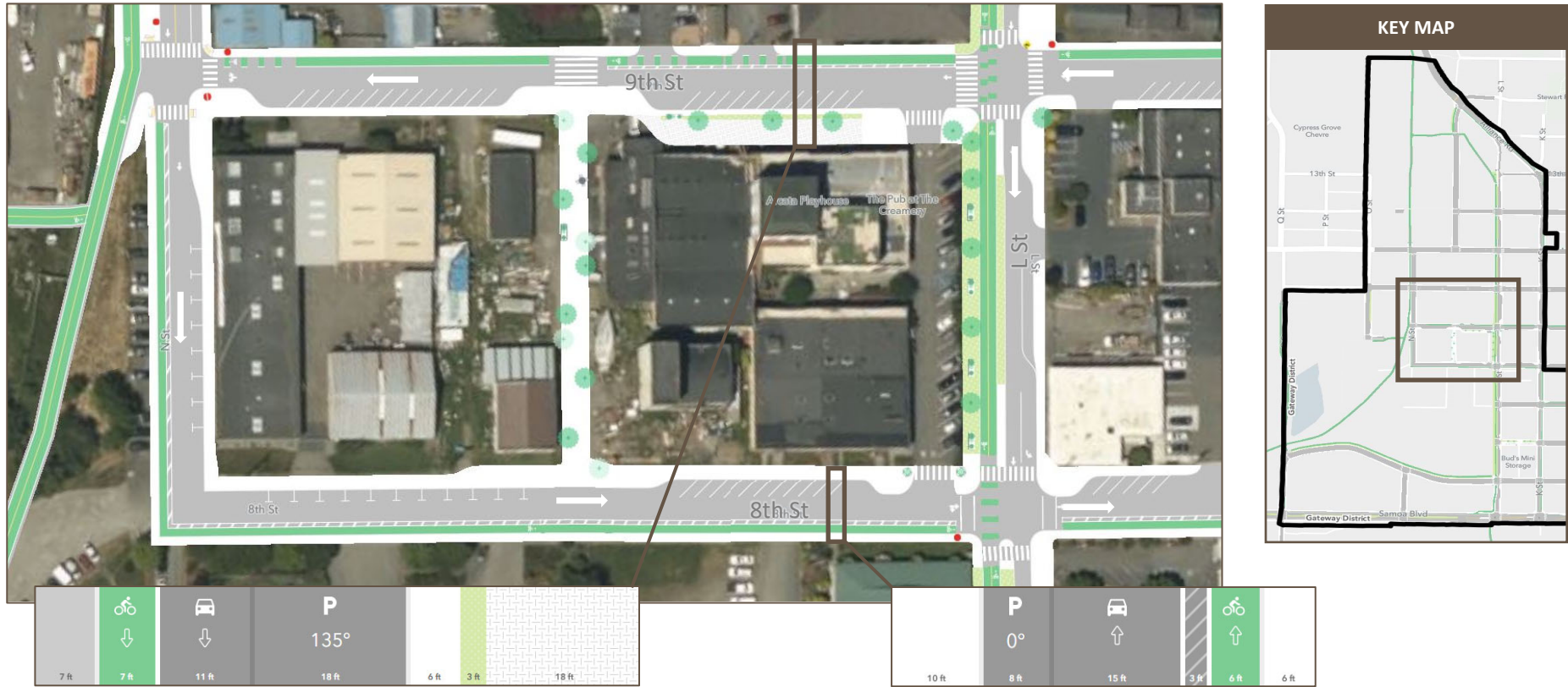
Alternative
Diagonal parking can either be implemented as pull-in or back-in parking. Back-in parking allows for vehicles to exist the parking stalls in a safer manner.



Similar to K Street, dedicated left turn lanes would be provided at intersections with higher volumes and truck routes such as 11th Street and 7th Street.

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

8TH & 9TH STREET ONE-WAY COUPLETS

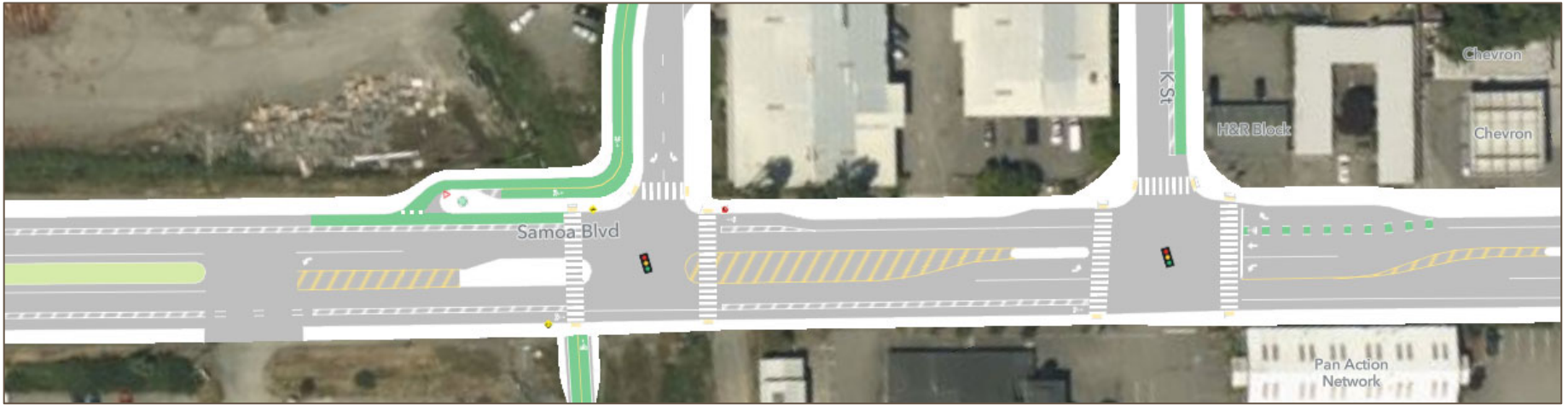


Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

The extension of 8th Street and 9th Street as one-way couplets west to N Street provides continuity with the existing one-way alignments in Arcata, logically connecting the Gateway District to the Arcata Plaza and existing downtown community destinations, homes, services, and shops. Widened sidewalk areas and a new M Street pedestrian walkway expand the public realm and provide opportunities for outdoor seating, retail display, and other creative space. As development occurs, particularly along south-facing frontages, consideration should be given to expanding shown sidewalk widths further into private rights-of-way. This would further expand semi-private space along the street that can be used for commercial purposes.

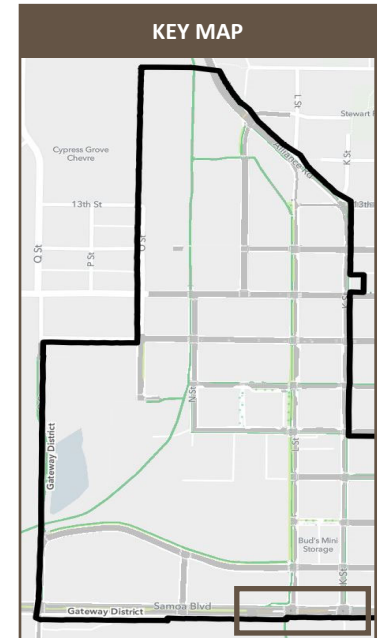
These two couplets, functioning in conjunction with the couplets of K Street and L Street, are designed to support a healthy and diverse local economy by creating green and inviting places. Similar to the Arcata Plaza, the intersection of the proposed one-way couplets of 8th, 9th, K, and L streets will create a Gateway District “town center” where traffic and people converge. Multimodal circulation around this core area is key to support new and established residents and businesses. The proposed buffered or separated bikeways on 8th Street and 9th Street will also connect the proposed Class I and Class IV bikeways along the L Street and N Street alignments. Parallel and diagonal parking supply is provided, with pedestrian realm enhancements.

SAMOA BOULEVARD



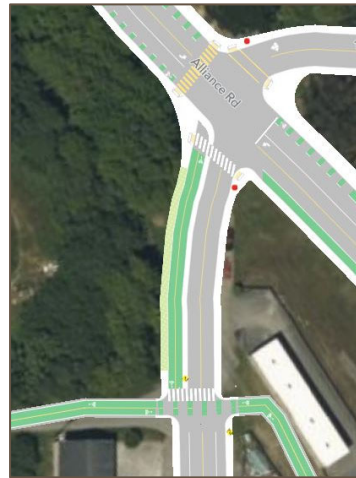
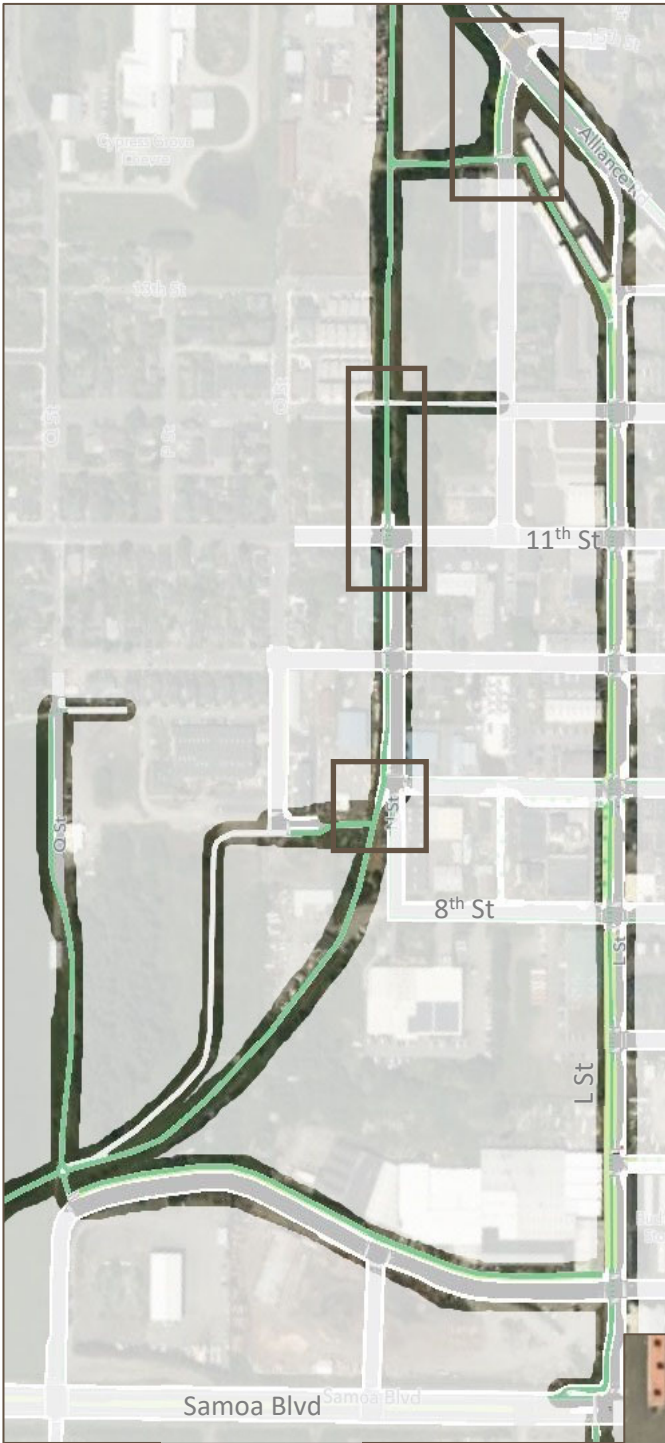
With the implementation of the K Street and L Street one-way couplets, intersection control is needed at the new connection of L Street at Samoa Boulevard. The new intersection is approximately 250 feet west of K Street and a traffic signal is recommended. The new traffic signal will need to be synchronized with the closely spaced K Street intersection, which is already operating in a coordinated signal system. Improvements along the corridor will need to be coordinated with and approved by Caltrans as Samoa Boulevard is a State facility (SR 255). The Class I multi-use path along L Street will connect across Samoa Boulevard to the existing Class I trail that continues south to the Arcata Marsh. The traffic signal at L Street should include either pedestrian/bicycle only phasing or a Leading Pedestrian Interval (LPI) to facilitate safe crossing of vulnerable road users.

Samoa Boulevard east of K Street is currently one lane in each direction with left turn lanes and Class II Bike Lanes. This Plan proposes to continue the cross-section west with a single travel lane in each direction, left turn lanes at intersections, and an improvement to Buffered Class II Bike Lanes west of K Street. Additionally, the Plan proposes to improve pedestrian crossings along Samoa Boulevard at the intersections of K Street, L Street, and the new westernmost access to the Barrel District by providing high-visibility crosswalks on all intersection approaches. The new mid-block access to the Barrell District is proposed as access-restricted (right-in, right-out only) to limit uncontrolled turning movements along Samoa Boulevard. The westernmost access to the Barrel District is proposed as a full-access intersection with stop control on the side streets. Class II Bike Lanes are proposed along the new roadway through the Barrel District which connect to Samoa Boulevard and the Class I trail north.



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

PROPOSED TRAILS



A new Class I trail is proposed along N Street along the prior rail line, connecting over to M Street and north to Alliance Road, and continuing south to connect to the Barrel District. This path will provide additional options for cyclists and pedestrians.

The Class I path will connect to N Street at 11th Street and continue south along N Street as a Class IV. The bikeways will have conflict markings across the intersections to enhance the visibility of crossing cyclists from crossing or turning traffic.

A Class I trail is also proposed along the Q Street alignment, south of 10th Street, and connecting to another new Class I trail through the Barrel District east to L Street, as well as connecting to the N Street trail. This system of trails aims provide safe and convenient continuous routes throughout the Gateway Area.

Natural paths are also proposed along the 12th Street alignment west of M Street, and in the southwest Open Space area connecting the O Street trail to the Barrel District.



At the intersection of N Street and 9th Street, priority will be given to cyclists and pedestrians by markings, signage, and stop control. This intersection will provide access for cyclists from 9th street via a Bike Lane. Cyclists that wish to continue south into town will have a Bike Lane available on N Street.

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)



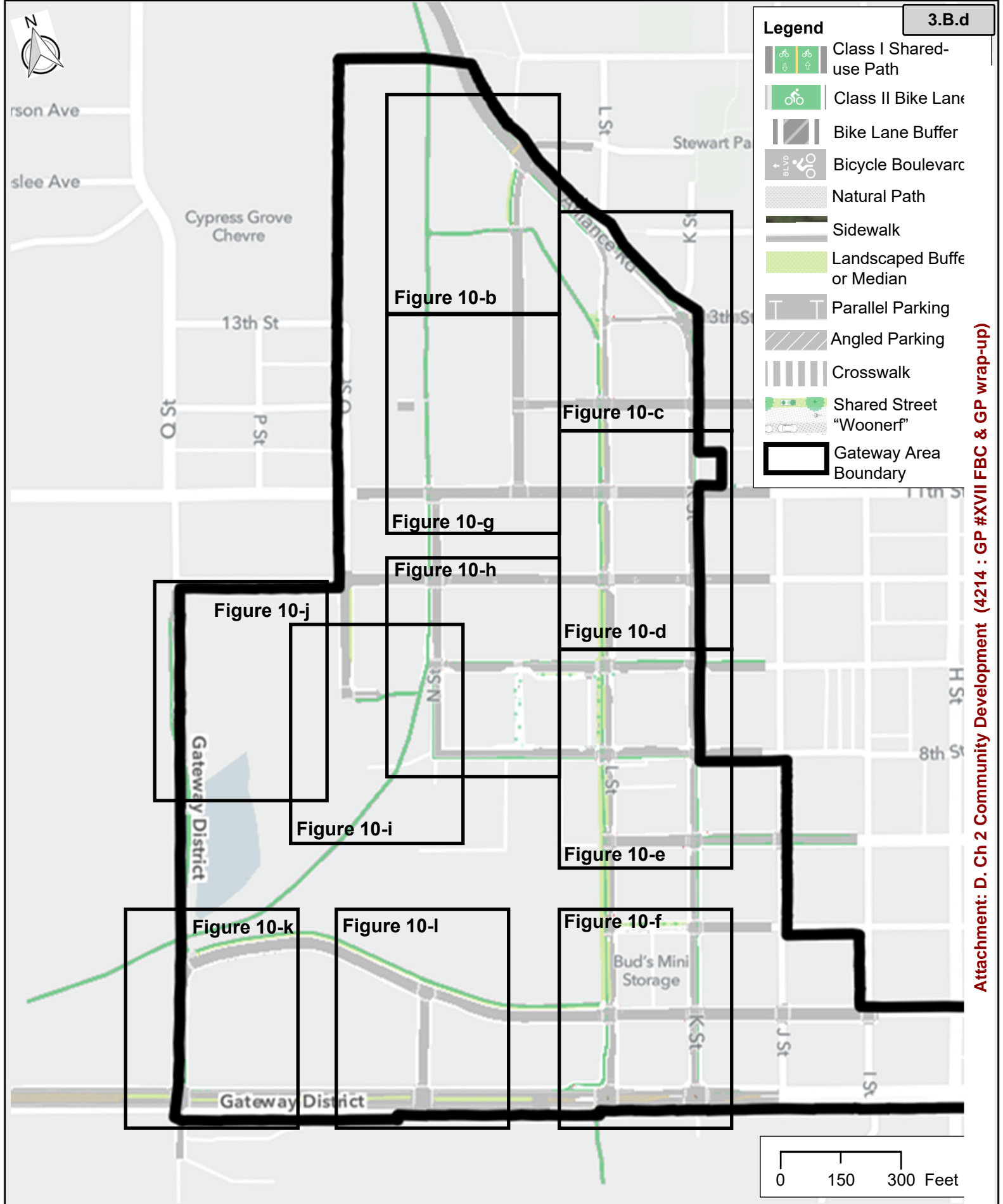
Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 10-b



Legend

- Class I Shared-use Path
- Class II Bike Lane
- Bike Lane Buffer
- Bicycle Boulevard
- Natural Path
- Sidewalk
- Landscaped Buffer or Median
- Parallel Parking
- Angled Parking
- Crosswalk
- Shared Street "Woonerf"
- Gateway Area Boundary



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 10-a Map Book Key Map

Source: If applicable, enter source information here.



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 10-c



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 10-d



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 10-e

Source: If applicable, enter source information here.



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 10-f



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 10-g



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 10-h

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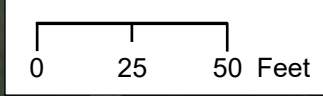


Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 10-i



Source: If applicable, enter source information here.



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 10-j



Source: If applicable, enter source information here.



Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 10-k

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Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 10-I



8. STREETSCAPE

Overview

The “streetscape” is the public spaces where people interact and includes the design of roadways, sidewalks, and building frontages facing the street. The streetscapes of the Gateway Area are an important component of the public realm because they heavily influence the way people feel about this place and how they interact with one another. In this way, public spaces designed for public interaction promote vitality and community use. This Plan incorporates attractive public spaces and public-oriented private spaces that encourage uses above and beyond mere transportation or conveyance. Streetscapes in the Gateway Area are places for neighbors to meet, for events to occur, the vantage point from which architecture and public art are enjoyed, where visitors most consistently experience the City, and where the Gateway community is truly made.

Streetscape Strategies

This section addresses streetscape design relating to sidewalks, street trees, street furniture, and lighting. Other aspects of streetscape design are addressed in Section 6 (Open Space), Section 7 (Mobility), and 9 (Site and Building Design).

Image 3 illustrates the components of a pedestrian-oriented streetscape design.

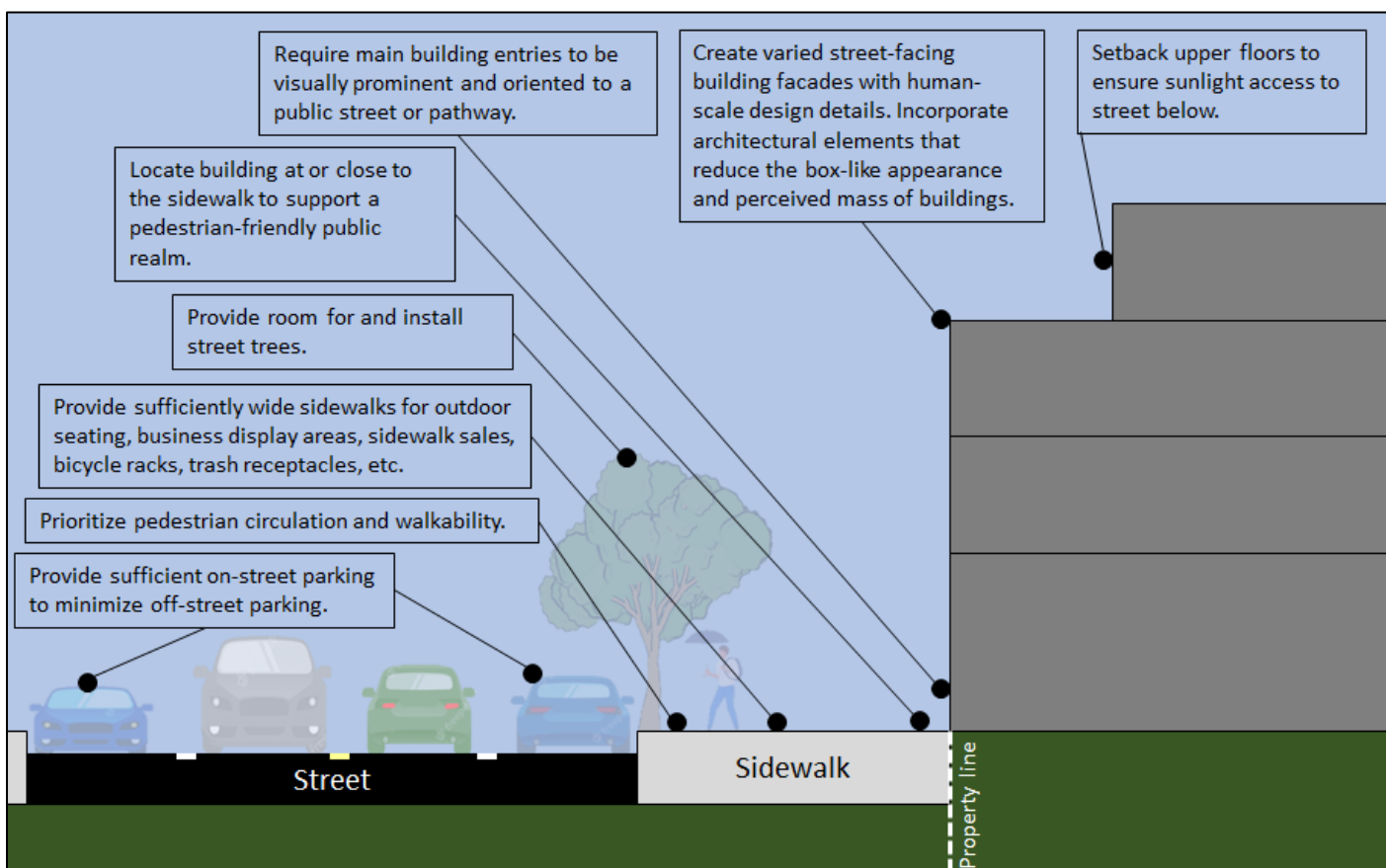


Image 3: Components of Streetscape Design

Streetscape Policies

Objective: Create high-quality streetscapes that are visually interesting and provide safe and comfortable places for people to walk and gather. Provide for a streetscape design that visually unites the Gateway Area and promotes a cohesive sense of place.

- GA-8a. Pedestrian Priority and Accessibility in Streetscape Design.** Ensure that streetscape design and improvements prioritize pedestrian circulation that promote walkability and support a car-free lifestyle and accessibility for all ambulatory modes and abilities. Require new development to consider the needs of a diverse range of abilities; for example, position obstructions to path-of-travel to the curb line or other less obtrusive locations wherever feasible.
- GA-8b. Improvements Required.** Require new development to provide streetscape improvements consistent with the Gateway Area Plan and Gateway Zoning Code.
- GA-8c. Sidewalk Zones – Core Area/Pedestrian Oriented Frontage.** Along pedestrian-oriented frontages, provide sufficiently wide sidewalks to accommodate outdoor seating, business display areas, and sidewalk sales. Except in unusual circumstances approved by the City Engineer, sidewalks shall be no less than 6 feet wide. Where viable, the City should strive for sidewalks eight feet or greater in width.
- GA-8d. Sidewalk Dining.** Encourage sidewalk dining for restaurants and cafes along commercial and mixed-use street frontages. Areas planned for sidewalk dining may require wider sidewalks including privately-owned public spaces. Consider accessibility for the visually impaired in structures that may create sidewalk obstructions.
- GA-8e. Street Trees Landscaping.** Install street trees and other landscaping consistent with the standard for the applicable street typology as identified in Chapter 7 (Mobility). Use planting techniques that extend the life and health of the trees.
- GA-8f. Street Tree and Landscaping Selection.** ~~Street tree species shall be consistent along each street. However, different species may be used at intersections to create special character.~~ Native plants may be required in areas adjacent to environmentally sensitive habitat. Elsewhere, the City encourages the use of native plants. Non-native invasive species shall not be approved by the City for use in landscaping. Landscaping plants as well as tree species and spacing shall be selected from a City tree master list species and planting guide, or in consultation with the City.
- GA-8g. Street Furniture.** Provide for consistent bicycle racks, seating, and trash receptacles integrated within the right-of-way and consistent and appropriate to the Plan Area character.
- GA-8h. Green Infrastructure.** Integrate green stormwater infrastructure into streets and public spaces to create attractive public areas while also capturing and treating runoff to meet water quality requirements. In this context “Green Infrastructure” is defined as the range of measures that use plant or soil systems, permeable pavement or other permeable surfaces or substrates, stormwater harvest and reuse, or landscaping to store, infiltrate, or evapotranspire stormwater and reduce flows to sewer systems or to surface waters.
- GA-8i. Utility Screening.** Minimize visual impacts from utility boxes, utility meters, backflow preventers, and other similar devices. Utility devices must be underground or located to the side or rear of buildings and screened from public view by landscaping. Exceptions may be granted through a discretionary review process.
- GA-8j. Lighting.** Provide adequate pedestrian-scaled street lighting to enhance public safety, encourage evening use of outdoor spaces, and enhance the visual quality of the public realm. Utilize “CPTED” (Crime Prevention through Environmental Design) guidance as applicable to ensure lighting achieves outlined goals of increasing pedestrian safety. Ensure all new lighting is compliant with “Dark Sky” policy guidance as outlined in the City’s General Plan.
- GA-8k. Bike Infrastructure.** Consider locations for public, outdoor bike lockers coordinated with public transit stops accessible in the public right-of-way.



Key to Revisions (Font Color/Highlighting)

Proposed changes recommended by each committee are shown in underline/strikeout text with color coded committee text as follows:

Transportation Safety Committee
 Parks and Recreation Committee
 Creeks and Wetlands Committee
 Forest Management Committee
 Historic Landmarks Committee
 Energy Committee
 Economic Development Committee
 Public member/entity
 Staff edits

9. DESIGN AND ARCHITECTURAL STANDARDS

Overview

The Gateway Plan Area design character promotes a mixture of building forms and types that reflect the evolving economic activity and conditions in Arcata. Many Plan Area properties are former industrial lands associated with lumber processing. Some of these properties have been redeveloped or adaptively reused, while others are in various stages of underutilization and disrepair. The eastern boundary of the Plan Area is three blocks from the Arcata Plaza, with the form and character of buildings in this area reflecting their proximity to Arcata's central core.

More specifically, aspects of the Gateway Plan Area existing design character include the following:

- **Block Pattern:** Arcata's grid block pattern extends into the eastern portion of the Plan Area. This block pattern is interrupted on western side of the Plan Area, particularly in areas south of 8th Street and north of 11th Street.
- **Site Layout.** Buildings are located on sites in a range of configurations, including structures built directly adjacent to property lines and sidewalks, building setback 10-15 feet from the sidewalk behind landscaping, and building located deep within properties with little relationship to the street. Some primary building entrances are oriented towards the street, while others are inward-facing.
- **Building Heights and Intensity:** Most buildings are one story, with some two-story buildings. The Creamery Building tower is the tallest structure in the Plan Area (over 50 feet). Some lots are either vacant or contain little habitable space. The floor area ratio (FAR) of individual properties is generally less than 1.0.
- **Building Character:** The overall character of buildings reflects the function for which the building was originally constructed. For many of the older non-residential buildings, this is a utilitarian light-industrial character. For buildings that have been adaptively reused, the original character remains though the use has changed. The Plan Area contains several distinctive landmark structures such as the Creamery Building and Portuguese Hall.
- **Parking Placement and Design:** On-site parking, where provided, can be found to the side or behind buildings, or in front of buildings as last option. Most on-site parking area is hardscape, and permeable wherever possible.
- **Facade Design.** Industrial and heavy-commercial buildings typically incorporate minimal architectural detailing. Older residential structure feature stylistic features typical of their period. Street-facing building frontage design are varied, ranging from residential porches, commercial storefronts, truck loading bays, to vinyl-clad blank walls.

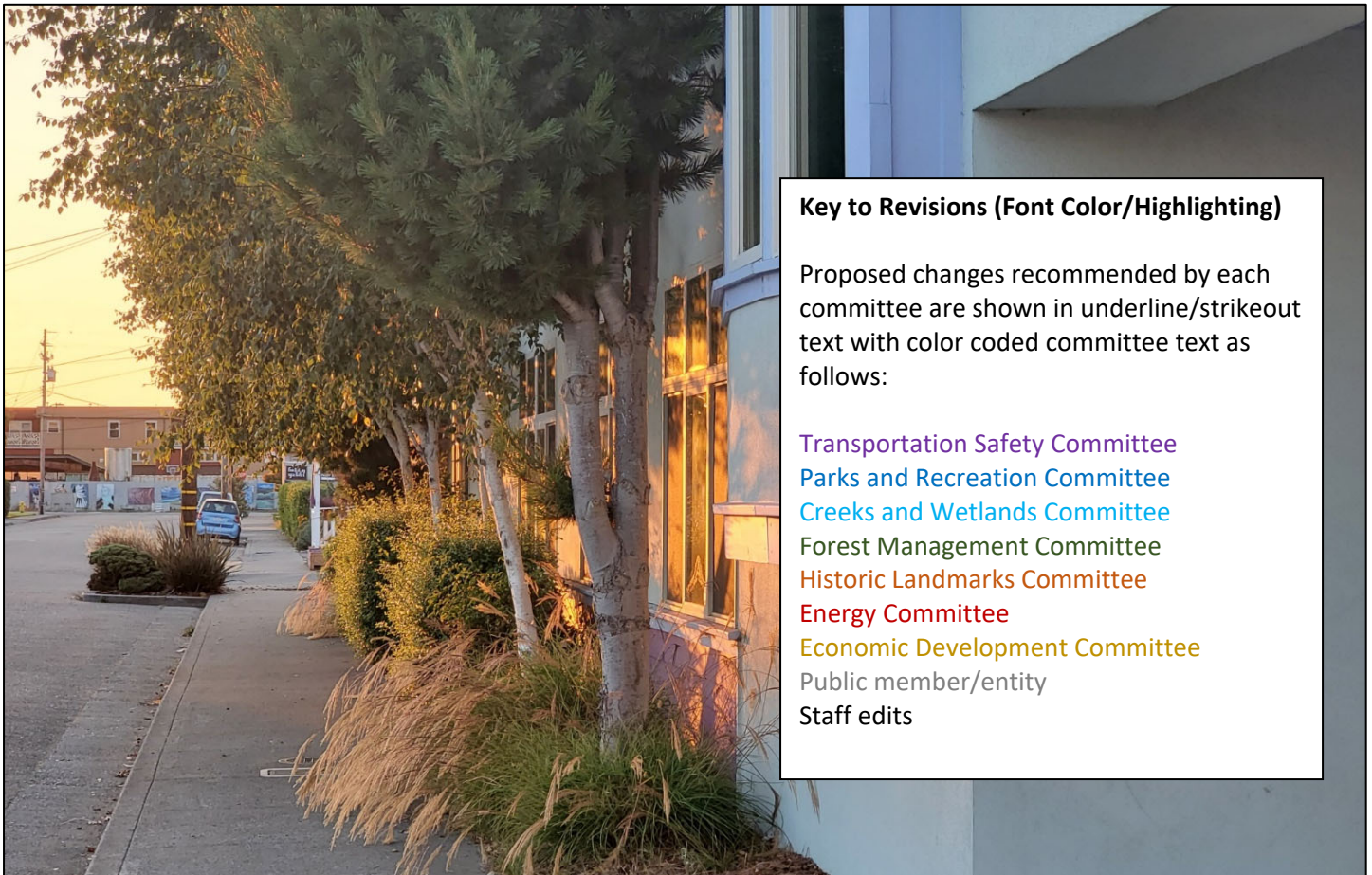


Regulatory Framework for Design Standards

The Gateway Zoning Code, the regulatory document that implements this Plan, identifies specific regulatory requirements in line with the framework established in this Chapter. The Gateway Area Plan will create a vibrant mixed-use neighborhood by requiring the design of new development to support an active and inviting public realm. New streets, where needed, will support a walkable neighborhood by connecting to and completing the existing grid system and block pattern. Buildings will be placed close to sidewalks, with spaces provided for outdoor dining, public assembly, courtyards, and landscaping as appropriate. Building entries will face the street with ground-floor frontages designed to generate activity and create visual interest. On-site parking, where provided, will be located behind buildings or within structures to minimize the visual prominence of parking facilities. Large buildings will be broken into smaller volumes with articulated building facades and human-scaled design details.

As described in the Land Use Chapter, the Gateway Area Plan does not identify required land uses in certain locations as a strategy to promote an active public realm. Instead, the Plan requires all buildings, whatever the use, to feature pedestrian-friendly and human-scale design. In addition, the Gateway Corridor (G-C) Land Use Designation (see Figure 4) includes special design requirements for ground-floor building frontages facing the street. These pedestrian-focused frontages require display windows for commercial use storefronts and enhanced design details with common entries for ground-floor residential uses.

The design concepts described in this chapter are implemented through the Gateway Zoning Code, which contains detailed building form and character standards. The Gateway Zoning Code also allows for streamlined by-right approval of development projects that conform to architectural standards and meet other requirements. With this approach, the City aims to fast-track the approval of new residential and mixed-use development that provides needed housing while also supporting a high-quality design environment.



Key to Revisions (Font Color/Highlighting)

Proposed changes recommended by each committee are shown in underline/strikeout text with color coded committee text as follows:

Transportation Safety Committee
 Parks and Recreation Committee
 Creeks and Wetlands Committee
 Forest Management Committee
 Historic Landmarks Committee
 Energy Committee
 Economic Development Committee
 Public member/entity
 Staff edits

Design Standards Policies

Objective: Ensure site design offers a mix of vibrant, mixed-use neighborhood structures with active and inviting public spaces. Provide for visual interest in walkable neighborhoods with street-level activity and public gathering places that support a car-free lifestyle.

Basic Principles

GA-9a. Enhanced Architectural Features and Exterior Designs as Community Amenities. Through the Gateway Area community benefit program, allow increased development intensity and simplified development processes for projects that provide enhanced architectural designs beyond those required.

GA-9b. Sustainable and Green Buildings as a Community Amenities. Through the Gateway Area community benefit program, allow increased development intensity and simplified development processes for projects that provide designated levels of sustainable and green building features.

GA-9c. Human-Scaled and Pedestrian-Friendly. Ensure that new development is human-scaled and supports a pedestrian-friendly public realm. [Focus on the “destination” aspect of the Gateway Area to celebrate the car-free nature by encouraging pedestrian-oriented streetscape amenities, such as parklets and outdoor dining.](#)

GA-9d. Design Diversity. Allow for and encourage complementary diversity in building forms and creativity in project design as long as projects support a pedestrian-friendly environment and other Plan goals.

GA-9e. Cohesive Identity. Ensure new development design includes recognizable design themes and a distinctive sense of place.

GA-9f. [Solar Shading.](#) [Ensure new development minimizes solar shading to surrounding properties.](#)

Block Length and Layout

GA-9g. Block Length. Where existing streets are extended or new streets established, aim to keep the length of new blocks to no more than 300 feet.

GA-9h. Block Layout. When establishing new streets and extending existing streets, connect to the existing block layout and complete grid system in a manner that also protects and enhances natural resources.



Building design to support a pedestrian-friendly public realm



Mid-block passageway integrated with public gathering place and outdoor dining.

- GA-9i. Mid-Block Passageways.** For blocks longer than 300 feet, provide for passageways mid-block through new development for convenient bike/ped through access. Passageway shall be an integral part of a project and be configured to provide safe access to rear parking lots, commercial storefronts, restaurants, and mid-block pathways.

Building Placement/Setbacks

- GA-9j. Building Placement.** Locate building at or close to the sidewalk to support a pedestrian-friendly public realm.
- GA-9k. Public to Private Realm Transitions.** Provide for sensitive transition from the public realm (sidewalk) to the private realm (residences).
- GA-9l. Use of Setback Areas.** Encourage outdoor dining, publicly-accessible courtyards, window-shopping areas, and other pedestrian-friendly uses in spaces between building walls and the sidewalks.
- GA-9m. Custom Standards Along Designated Streets.** Plan for custom use, development, and design standards along designated streets with the objective of generating a more welcoming and vibrant "gateway" into the City.



Outdoor dining between sidewalk and street-facing building wall.

Building Entries and Orientation

- GA-9n. Main Building Entrances.** Require main building entries to be visually prominent and oriented to a public street or pathway.
- GA-9o. Pedestrian Connections.** Required pedestrian walk paths to connect entrances directly to a public sidewalk.

Bulk and Massing

- GA-9p. Human-Scale Massing.** Provide for human-scale and pedestrian-friendly building massing where large buildings are broken into smaller volumes that fit into the surrounding neighborhood.
- GA-9q. Transitions to Lower Intensity Uses.** Require buildings to incorporate massing strategies to minimize impacts on adjacent single-family homes.



Entrances to individual units facing the street

Vehicle Access and Parking

- GA-9r. Pedestrian-Friendly Design.** Require parking location and design to not detract from pedestrian-friendly streetscapes, walkable neighborhoods, and active and inviting mixed-use districts. Locate parking facilities away from building frontages in the rear of the lot when feasible, unless determined infeasible in unique circumstances.
- GA-9s. Minimize Visual Dominance.** Screen parking facilities for limited visibility from the street frontage with landscaping, art, or other visually appealing methods.
- GA-9t. Car-Free Lifestyle.** Minimize vehicle parking requirements and maximize walk, bike, and transit infrastructure.



Human-scaled massing with buildings divided into smaller volumes

Facade and Roof Design

- GA-9u. Varied and interesting Facades.** Create street-facing building facades that are varied and interesting with human-scale design details.
- GA-9v. Articulation.** Incorporate architectural elements that reduce the box-like appearance and perceived mass of buildings.
- GA-9w. 360-Degree Design.** Provide for buildings designed as a unified whole with architectural integrity on all sides of the structure.
- GA-9x. Quality Materials.** Ensure quality materials that maintain their appearance over time.
- GA-9y. Design Details.** Promote design details and materials compatible with the existing neighborhood character.
- GA-9z. Incentivize Enhanced Architectural and Exterior Design as Community Amenities.** Through the Gateway Area community benefit program, allow increased development intensity and simplified development processes for projects that provide enhanced architectural and exterior designs that go beyond base standards.



Building facade and roof articulation and design details to create visual interest

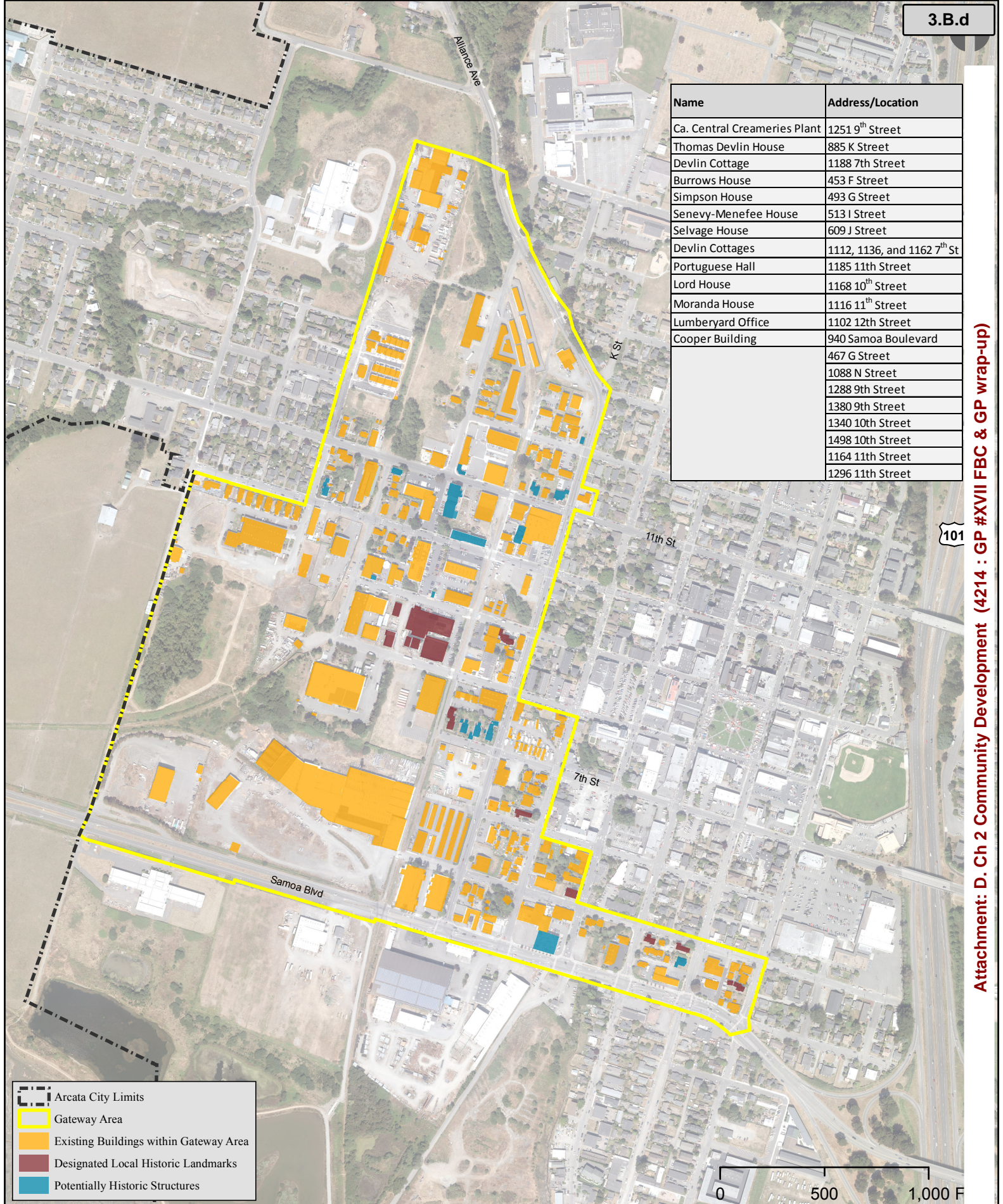
10. HISTORICAL RESOURCES

Overview

Table 9 and Figure 11 identify designated local historic landmarks and potentially historic structures in the Plan Area. These historic resources are subject to historic preservation policies in the General Plan Historic Preservation Element, preservation/permit requirements in Municipal Code Chapter 9.53, and design review requirements in Municipal Code Section 9.72.040. Figure 11 also shows the Central Neighborhood Conservation Area (NCA) that extends into the Gateway Plan Area. NCAs are areas identified in the General Plan and Land Use Code where additional requirements apply to ensure that new development is compatible with the existing neighborhood character.

Table 9: Historic Resources¹

NAME	ADDRESS/LOCATION	PERIOD OF SIGNIFICANCE
Designated Local Historic Landmarks		
Ca. Central Creameries Plant	1251 9 th Street	Craftsman
Thomas Devlin House	885 K Street	Victorian
Devlin Cottage	1188 7 th Street	Transitional
Burrows House	453 F Street	Settlement
Simpson House	493 G Street	Settlement
Senevy-Menefee House	513 I Street	Settlement
Selvage House	609 J Street	Settlement
Potentially Historic Structures²		
Devlin Cottages	1112, 1136, and 1162 7 th Street	Transitional
Portuguese Hall	1185 11 th Street	Craftsman
Lord House	1168 10 th Street	Victorian
Moranda House	1116 11 th Street	Craftsman
Lumberyard Office	1102 12 th Street	Victorian
Cooper Building	900-940 Samoa Boulevard	Early Modern
	467 G Street	Victorian/Transitional
	1088 N Street	Transitional
	1288 9th Street	Craftsman
	1380 9th Street	Craftsman
	1340 10th Street	Craftsman
	1498 10th Street	Victorian
	1164 11th Street	Transitional
	1296 11th Street	Craftsman
Rail Corridor	Various	N/A
Notes:		
1. Historic Resources in the Gateway Plan Area subject to the requirements of Municipal Code Chapter 9.53 are limited to the properties listed in this table.		
2. "Potentially Historic Structures" in the Gateway Plan Area subject to General Plan policies H-2d and H-2e and Municipal Code Chapter 9.53 and are limited to the Potentially Historic Structures identified in this table.		



Name	Address/Location
Ca. Central Creameries Plant	1251 9 th Street
Thomas Devlin House	885 K Street
Devlin Cottage	1188 7 th Street
Burrows House	453 F Street
Simpson House	493 G Street
Senevy-Menefee House	513 I Street
Selvage House	609 J Street
Devlin Cottages	1112, 1136, and 1162 7 th St
Portuguese Hall	1185 11 th Street
Lord House	1168 10 th Street
Moranda House	1116 11 th Street
Lumberyard Office	1102 12 th Street
Cooper Building	940 Samoa Boulevard
	467 G Street
	1088 N Street
	1288 9 th Street
	1380 9 th Street
	1340 10 th Street
	1498 10 th Street
	1164 11 th Street
	1296 11 th Street

- Arcata City Limits
- Gateway Area
- Existing Buildings within Gateway Area
- Designated Local Historic Landmarks
- Potentially Historic Structures

Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

Figure 11: Historic Resources in the Plan Area



Historic Resource Policies

Objective: Preserve historic resources in the Gateway Plan Area while allowing for new development consistent with the Plan vision.

- GA-10a. Local Historic Landmarks.** Encourage the preservation, rehabilitation, and adaptive reuse of designated local historic landmarks as identified in Table 9. Allow for additions and new buildings on properties containing designated historic landmarks when the addition or new building maintains the historic integrity of the property and its environment, in adherence with the Secretary of the Interior’s Standards for Rehabilitation of Historic Structures).
- GA-10b. Potentially Historic Structures.** Encourage the preservation, rehabilitation, and adaptive reuse of potentially historic structures as identified in Table 9. Allow for additions and new buildings on properties containing designated landmarks when the addition or new building protects the historic integrity of the property and its environment in adherence with the Secretary of the Interior’s Standards for Rehabilitation of Historic Structures.).
- GA-10c. Creamery Building.** Allow by-right approval of a development project on the Creamery Building property that meets the following requirement as determined by the Community Development Director: the project is eligible for by-right approval as specified in the Gateway Zoning Code.
- GA-10d. Design Review Required.** Except as allowed by Policy GA-10c (Creamery Building), continue to require Planning Commission Design Review consistent with Municipal Code Chapter 9.53 (Historic Resource Preservation) for the exterior modification, demolition, or relocation of a historic resource identified in Table 4. Such projects are not eligible for streamlined by-right approval. As noted in Table 9, “Noteworthy Structures” and “Buildings Constructed within the Period of Significance,” alterations to which require Design Review, are limited to the historic resources identified in in Table 9.
- GA-10e. Neighborhood Conservation Area.** If a project is eligible for streamlined by-right approval and is located in the portion of the Central Neighborhood Conservation Area that extends into the Gateway Plan Area as shown in Figure 11, require Design Review only for projects on properties that contain a historic resource as identified in in Table 9. This policy, and its implementing legislation, shall supersede policies elsewhere in the General Plan and Zoning Code.
- GA-10f. Historic Preservation.** In addition to existing Citywide preservation incentives, explore additional ways to incentivize the rehabilitation and preservation of significant and potentially significant buildings located within the Gateway Area. The purpose of these additional incentives is to ensure the added cost associated with restoring and preserving historic buildings is offset by the benefits of the incentives.

11. INFRASTRUCTURE AND SERVICES

Overview

Plan Area infrastructure other than circulation, parks and recreation, includes primarily City-managed water lighting, stormwater and wastewater systems. Circulation Infrastructure is covered in Section 7 (Mobility) and Section 8 (Streetscape), while parks and recreation areas are covered in Section 6 (Open Space). Utilities not provided by the City (such as solid waste/recycling, power, internet, and cable) are covered here minimally and only to the degree to which the City has authority or influence.

The City of Arcata is committed to providing adequate infrastructure and services to support the envisioned growth and to ensure a high quality of life in the Plan Area. This will require both upgrades to existing infrastructure as well as investments into new infrastructure. This section describes the infrastructure improvements that will be needed to fulfil the overall vision of this Plan. The Plan Area's overall infrastructure systems must be sufficient to accommodate the types and amount of planned growth, including up to 3,500 new residential units and new commercial businesses.

Infrastructure Policies

Objective: Provide sustainable infrastructure that serves the planned growth of the Gateway Area.

- GA-11a. Plan Utility Infrastructure to Accommodate Envisioned Growth.** In planning for improvements to the overall utility infrastructure, including wastewater and electrical demand, design the systems to accommodate the planned amount of growth outlined in other policies and consider the impacts of locating new infrastructure on potential future habitat restoration sites. Plan for future infrastructure needs due to conversion of uses (e.g., electrification). Infrastructure will be developed over time as needed, and the City's Capital Improvements Program (CIP) will reflect near term infrastructure needs through the annual update process.
- GA-11b. Add Utility Infrastructure System Improvements to City's Capital Improvement Plan.** Generate a list of utility infrastructure projects that will be required to implement this plan and then add those projects to the City's Capital Improvement Plan.
- GA-11c. Enhanced Infrastructure Finance District.** Work with the County to explore the formation of an EIFD to fund infrastructure and housing projects within the Plan Area.
- GA-11d. University Contributions to Infrastructure Costs.** Given that the City is planning for substantial residential growth in the Plan Area in part to accommodate the planned growth of Cal Poly Humboldt Humboldt State University, work with the University to explore ways in which the University can contribute to the infrastructure costs associated with the planned growth.
- GA-11e. Multi-Modal System Improvements Funding.** Identify, develop, and prioritize utility infrastructure projects that are eligible for Federal and State funds and continue to pursue all available options for funding new and improved circulation system facilities.
- GA-11f. Align Utility Infrastructure within Circulation System.** To the degree possible, locate new utility infrastructure within existing and planned circulation system rights-of-way.
- GA-11g. Green Water Conservation Infrastructure.** Continue to encourage the use of green infrastructure and practices that promotes efficient water and energy use such as rain barrels, green roofs, and retention structures. Encourage reduced water demand by requiring water-conserving design and equipment in new construction and

POLICY CHAPTER 11: INFRASTRUCTURE AND SERVICES

encouraging the retrofitting of existing development with water-conserving devices, [along with water capture and conservation practices](#). See GA-11k.

~~GA-11h. **Natural Drainage.** Encourage and maintain the use of natural stormwater drainage systems in a manner that preserves and enhances natural features while also allowing for maximum water reclamation and reuse.~~

GA-11i. Recreation Opportunities. Continue to consider recreational opportunities and aesthetics in the design of stormwater/retention, [treatment](#), and conveyance facilities.

GA-11j. Low Impact Design. [Development for Redevelopment](#). Encourage and incentivize opportunities to incorporate Low Impact Development in ~~both new construction and remodeling/renovation~~ [redevelopment and other development exempt from MS4 permit requirements](#) of existing structures and sites.

GA-11k. Incentivize Energy Efficiency and Electrification as Community Amenities. Through the Gateway Area community benefit program, allow increased development intensity and simplified development processes for projects that exceed Title 24 [and other State or local](#) energy efficiency requirements and all new construction to be electric only, with limited commercial cooking exceptions.

GA-11l. Utility Undergrounding. Existing above-ground and new utilities shall be placed underground as feasible as determined by the City Engineer. Recognizing undergrounding is expensive and may affect affordability of housing, aesthetics of aboveground utilities shall be considered in project development.

IMP-GA 11.1 Sewer Pre-Treatment. Explore sewer on-site pre-treatment for high-strength and high-volume land uses as a project-specific Community Amenity.

IMP-GA 11.2 Infrastructure Financing. Prepare an itemized budget with projected costs necessary for planned infrastructure improvements and services within the Gateway Area, and potential sources of funding and cost-share opportunities (grants, taxes, bonds, developer financing, etc.). Consider a range of financing opportunities, including establishing an Enhanced Infrastructure Financing District over the Gateway Area, collaboration with Cal Poly Humboldt, pursuing state and federal grants or low interest loans, and other sources of revenue.

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Attachment: D. Ch 2 Community Development (4214 : GP #XVII FBC & GP wrap-up)

IMPLEMENTATION

Implementation Programs and Phasing

Implementation of this Plan will achieve the community's vision and goals for the Gateway Area. Many of the policies presented in the Chapters above will be implemented through the Gateway Zoning Code and the overall General Plan. In addition, several specific Gateway Area implementation programs are also presented in various policy chapters above. Each specific Implementation Program includes identification of City departments responsible for implementation and the approximate timeframe of implementation. Timing will be influenced by the availability of funding and prioritization of resources. City will regularly review this Plan to evaluate progress in implementing its goals and policies. As the Gateway Area grows and develops, the City will also consider amendments to this Plan.

Table 10: Implementation Programs, Responsible Parties, and Timing

Implementation Program		Primary Responsibility Party	Timeframe
Imp-GA-1.1	Gateway Zoning Code	Community Development Dept.	2022 to 2023
Imp-GA-1.2	Relocation Assistance	Community Development Dept.	
Imp-GA-2.1	Create Community Benefits Program	Community Development Dept.	2022 to 2023
Imp-GA-2.2	Review of Community Benefits Program	Community Development Dept.	2022 to 2042
Imp-GA-3.1	Housing Monitoring	Community Development Dept.	2022 to 2042
Imp-GA-3.2	Resident Relocation Assistance	Community Development Dept.	2024
Imp-GA 3.3	Business Relocation Program	Community Development Dept.	
Imp-GA-6.1	Parkland Dedications and Fees	Environmental Services Dept.	2023
Imp-GA-6.2	Open Space Concept Diagram Revisions	Environmental Services Dept.	2022 to 2042
Imp-GA 6.3	Public Open Space Acquisition Program	Environmental Services Dept.	
Imp-GA 6.4	Open Space Habitat Map and Potential Project List	Environmental Services Dept.	
Imp-GA-7.1	State and Federal Transportation Grants	Building and Engineering Dept.	2022 to 2042
Imp-GA-7.2	Circulation Projects in Capital Improvement Program	Building and Engineering Dept.	2022 to 2042
Imp-GA-7.3	Fair Share Contribution for New Development	Building and Engineering Dept.	2025
Imp-GA-7.4	Master Plan for Barrel District	Building and Engineering Dept.	
Imp-GA-11.1	Sewer Pre-Treatment	Environmental Services Dept.	
Imp-GA-11.2	Infrastructure Financing	Building and Engineering Dept.	

IMPLEMENTATION

Key to Revisions (Font Color/Highlighting)

Proposed changes recommended by each committee are shown in underline/strikeout text with color coded committee text as follows:

Transportation Safety Committee

Parks and Recreation Committee

Creeks and Wetlands Committee

Forest Management Committee

Historic Landmarks Committee

Energy Committee

Economic Development Committee

Public member/entity

Staff edits

IMPLEMENTATION



The End

This document is designed to be viewed digitally or printed double-sided.

Chapter 3 – Housing and Human Services

Housing Element (2019 Sixth Cycle Element not included here)
Parks and Recreation Element

DRAFT

PARKS AND RECREATION ELEMENT

3.1 INTRODUCTION

The Parks and Recreation Element plays an important role in the Arcata General Plan. This Element expresses the degree to which the community values park and open space land and establishes a policy framework and priority for the provision of park and recreation services, park improvements and provides the framework required by state law for the dedication of land or payment of fees in lieu of dedication for park or recreational purposes as a condition of subdivision approval. In addition, the General Plan must identify existing and planned public facilities, including land for parks and recreation, which is critical for future dedications, acquisitions and park improvements which must be consistent with the General Plan.

In 1994, the City adopted a General Plan Parks and Recreation Element (Resolution 945-26) to serve as the central policy document to guide the provision of parks and recreation in the City. The Element considered a wide variety of recreation programs with policies addressing the community's desire to provide a wide spectrum of parks and recreation opportunities and facilities; encouraging cooperative agreements with other government entities and public/private partnerships to enhance the availability of recreation programs; and emphasized fiscal responsibility and the objective to minimize environmental impacts. This Element was maintained as a separately bound document in the Arcata General Plan, 2020.

In 2010, the City updated its Parks & Recreation Master Plan, originally compiled in 1979 and updated with the preparation of the Parks and Recreation Element in 1994. The Parks & Recreation Master Plan establishes a vision for a sustainable and interconnected parks and recreation system relying on local standards and guidelines and a comprehensive implementation strategy to addresses community needs. This Parks and Recreation Element updates the policies and program of the 1994 Element and incorporates the vision and objectives of the 2010 Parks & Recreation Master Plan.



The City of Arcata divides the responsibility for parks and recreation services between the Parks and Recreation Divisions of the Environmental Services Department. The Environmental Services Parks Division is responsible for the design, development, landscaping, and maintenance operations of the City's parks and facilities. The Environmental Services Recreation Division provides recreation services, facilities and programs geared to all ages and interests ranging from traditional and nontraditional athletics to arts, science, music, and seasonal community events that are intended to improve the quality of life in Arcata. Supported by the Environmental Services Department, the City Parks and Recreation

Committee holds regular public meetings and serves in an advisory capacity to the City Council on matters pertaining to public recreation, parklands, and related facilities.

Existing Parks and Recreation System Overview. The City of Arcata is a unique community, in terms of the parks, trails, and recreation opportunities it provides. The City owns and maintains more than 3,744 acres of parkland at 41 sites. The City's system for classifying parks is based on National Recreation and Park Association standards but defines park types and appropriate levels of service to reflect the unique characteristics of Arcata and to facilitate future parks planning for the City. Arcata's classification system includes:

- **Neighborhood Parks** which provide access to basic recreation opportunities for nearby residents;
- **Community Parks** which are larger parks that provide both active and passive recreation opportunities and that appeal to the entire community;
- **Special Use Areas** which are stand-alone recreation facilities not located within larger parks;
- **Greenways and Linear Parks** that are natural or built corridors that provide green buffers between communities or around the city; and
- **Natural Areas** that are undeveloped lands left in a natural state for conservation or outdoor recreation.

More than 97% of the City's parks and recreation acreage is provided as Natural Areas or undeveloped park reserves and a relatively small proportion of the park system is devoted developed parks. As shown in the table below, park development is not divided evenly among the existing park types. Most developed parkland (over 63.7%) is provided at two large community parks: Arcata Community Park and Redwood Park. Remaining acreage (approximately 32 acres) is distributed among 21 small parks, consisting of special use areas (19%), neighborhood parks (10.7%), and linear parks/greenways (6.6%). Figure PR-a illustrates the Arcata parks and playground amenities.

Developed Parks	#	Acres	Percent
Neighborhood Parks	12	9.5	10.7
Community Parks	2	56.6	63.7
Special Use Areas	6	16.9	19.0
Linear Parks/Greenways	3	5.9	6.6
SUBTOTAL	23	88.8	100%
Undeveloped Parks			
Natural Areas	15	3,625.4	
Undeveloped Sites	5	29.9	
SUBTOTAL	18	3,655.3	
TOTAL PARK SYSTEM	41	3,744.1	

In addition to City parks and recreation areas, there are a number of local schools in Arcata whose sport fields, sport courts, playgrounds, and gymnasiums are regularly used by the community, including: Arcata Elementary; Jacoby Creek, Pacific Union, and Sunny Brae. In addition, Cal Poly Humboldt is a major provider of recreational facilities and programs, primarily available to faculty, staff, and students, who also use City parks and recreation facilities. The Arcata Community Pool is managed by the Northern Humboldt Recreation and District Park and located adjacent to Arcata High School and serves surrounding communities as well, including Manila, Jacoby Creek, and the Warren Creek area. HealthSPORT is a private health club located in Arcata Community Park.

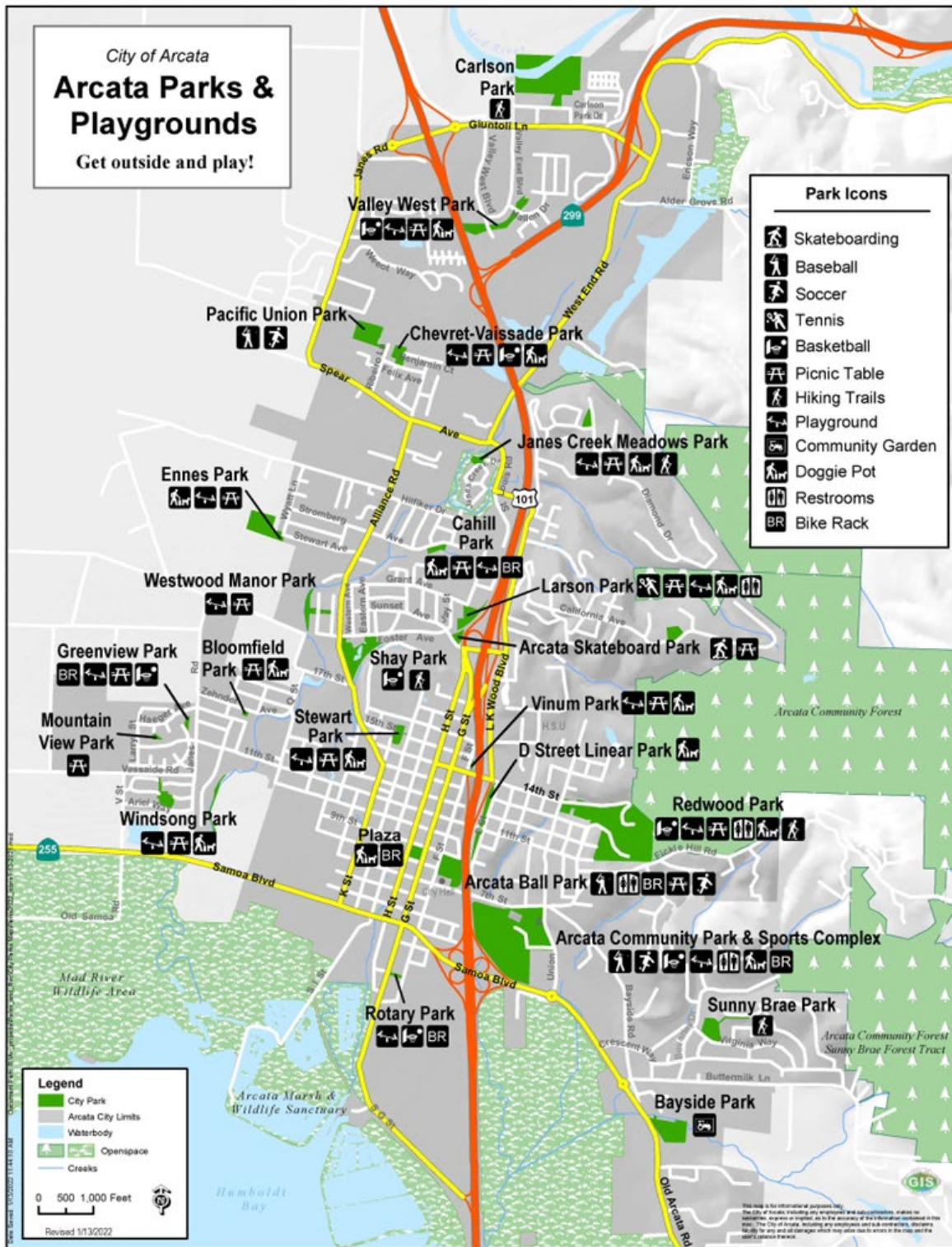


The City has a comprehensive system of trails within and connecting parks that provide a valuable recreation resource to the community in addition to regional linkages. The City has a mix of hard and soft surface multi-purpose trails that support activities, such as running, hiking, mountain biking and riding horses.

Parkland Dedication Standards. The State’s “Quimby Act” (Government Code Section 66477 of the Subdivision Map Act) allows cities to require residential subdividers to dedicate land, or to pay in-lieu fees, for park and recreational purposes if the general plan contains policies and standards for parks and recreational facilities and definitive regulations are in place specifying the proportion of a subdivision to be dedicated or the payment of in-lieu fees. The Quimby Act allows for minimum dedications of three acres per 1,000 residents and the dedications of up to five acres to match a jurisdiction’s actual parkland to population ratio. In spite of the small proportion of City parkland that is improved as community and neighborhood parks, Arcata maintains a very high parkland to population ratio, 5.15 acres per 1,000 residents. As a result, the City’s General Plan and Development Code require dedications of the maximum amount under the Quimby Act, five acres per 1,000 residents.



Figure PR-a Outdoor Recreation Map



Attachment: E. Ch 3 Housing and Human Services (4214 : GP #XVII FBC & GP wrap-up)

City Parkland Value

Natural Resources Values. The City of Arcata has a strong commitment to its parks and natural open space areas and the City's parkland protects natural resources, offers scenic views and contributes to the City's unique character, aesthetic appeal, and high quality of life. Arcata's parks, waterways, agricultural lands, wildlife sanctuary, and forests help establish community identity and a strong sense of place. The protection and preservation of the natural environment through park design is an important value in Arcata and environmental protection and sustainability are integral components of park design, forest and open space maintenance and management, operations and programming. In particular, the Arcata Marsh provides secondary treatment for the City's wastewater treatment plant and serves as an important natural and recreational area; and the Arcata Community and Jacoby Creek Forests serve many functions, including supporting recreation, education, watershed protection, wildlife habitat, and sustainable timber harvesting to support additional park acquisition, development and maintenance.

Community Values. Based on findings from the Parks & Recreation Master Plan Questionnaire, nearly all community members consider parks, recreation and open space to be important to Arcata's quality of life. Residents value a park system that emphasizes natural areas and trails and express a desire for additional multi-use trails, natural and multi-use parks that serve the whole community, and small parks in neighborhoods.

The people of Arcata also highly value art and culture. In 2021, the City Council adopted the City of Arcata Strategic Arts Plan 2022-2025, to articulate the community's values that expands and formalizes the City's support for Arcata's arts & artists and guides the City's engagement with community partners in the arts. In relation to the Parks & Recreation Element, the Strategic Arts Plan encourages community beautification and public art, which will no doubt involve city parkland. The Strategic Arts Plan also seeks to develop policies and support community organizations that provide services that improve the quality of life in Arcata and events that promote Arcata as a 'livable community', which closely matches the objectives of this Element.

The City and Cal Poly Humboldt are both experiencing demographic changes that will influence demand for parks and recreation services and facilities. The proportion of the County population that is 55 years and older is growing and expected to continue to increase into the future and as a result, more facilities and programs will likely be needed to serve a growing senior population in the City. The University is experiencing substantial change, which will increase the numbers of students and faculty that may increase the demand for active and passive City recreation activities.



Community Needs

The Parks and Recreation Master Plan included a comprehensive assessment of community needs. The need for parkland in Arcata considered the community's desire for diverse recreation experiences, including active and passive recreation opportunities, the need for open space, natural resource management and preservation, and opportunities to enjoy the outdoors. The needs analysis evaluated the existing parks level of service and geographical access to parks from within the City and then identified service gaps based on these factors, plus the preferences and demands by Arcata residents for recreation opportunities. Despite relatively good park coverage in Arcata and the wide range of park facilities, there are still a number of service gaps in the City. The Parks and Recreation Master Plan includes a series of



maps to characterize service areas for parks facilities and areas with identified needs and recommendations for actions to address these needs.

The City of Arcata has worked to incorporate accessible amenities and facilities into the City's park system. Although most playgrounds have surfacing that promotes universal accessibility, only a few have accessible amenities, such as picnic tables and benches, and many sites lack an accessible path of travel to link facilities. The

Parks and Recreation Master Plan Update includes recommendations to Update the City ADA Transition Plan for parks and recreation facilities and to incorporate universal design principals into all master planning efforts.

Guiding Principles and Goals.

- A. Provide residents with an accessible variety of recreation opportunities & park experiences that are equitably distributed throughout the City.
- B. Expand the range of park types and recreation programs to meet changing community needs & priorities.
- C. Design sustainably to maintain carbon neutrality, operate efficiently and minimize water/energy needs.
- D. Optimize natural resources & amenities for the enjoyment of residents and visitors.
- E. Make parks and recreation programs safe, accessible by multi modal transportation, usable by all ages, inclusive, and without barriers for those with limited mobility.
- F. Assess user needs & adapt as needed on an ongoing basis.

3.2 POLICIES

The Parks and Recreation Element includes the following policies:

- PR-1 Park Standards
- PR-2 Park Development and Maintenance
- PR-3 Coordinated Open Space, Park and Recreational Facilities Planning and Use

POLICY PR-1 PARK STANDARDS

Objective. Provide parklands to defined City Service Areas to meet unique community preferences and demands for recreation opportunities. Maintain park standards that facilitate future planning and help define appropriate levels of service for various types of parkland.

PR-1a **City of Arcata Parks Classification System.** The following is a summary of the City Parks Classification System, the complete system description is contained = in Appendix B of the Parks and Recreation Master Plan with the Park Design and Development Guidelines.

Neighborhood Parks. Located within walking and bicycling distance of most users, neighborhood parks are small parks that provide access to basic recreation opportunities for nearby residents. Designed primarily for non-supervised, non-organized recreation activities, neighborhood parks often include amenities such as playground equipment, outdoor basketball courts, picnic tables, pathways, and multi-use open grass areas. Neighborhood parks may include small-scale natural areas, such as riparian areas, and can contribute to environmental sustainability by incorporating sustainable design elements. These parks generally are one to five acres in size and

serve residents within a ½-mile radius. Smaller mini parks (of less than one acre) will have a smaller service area (less than ¼ mile).

Community Parks. Community parks are larger parks that provide both active and passive recreation opportunities that appeal to the entire community. These sites are generally designed around organized, active recreation, supported by facilities such as sport fields and outdoor courts. These parks also serve as community gathering spaces, offering a variety of facilities that can accommodate small or large groups, such as recreation centers, group picnic areas and shelters, rental space, festival space, amphitheaters, and large playgrounds. The optimum size of community parks is between 15-30 acres.

Special Use Areas. Special use areas include stand-alone recreation facilities not located within larger parks. These include single-purpose sites, such as community centers, aquatic centers, sports complexes, boat ramps, skate parks, outdoor theaters, urban plazas, and gardens. Special use areas promote a variety of personal, social, and economic benefits depending on facility type and location. In some cases, these facilities may attract new residents and businesses and support tourism.



Linear Parks/Greenways. Greenways and linear parks include natural or built corridors used to link features together and provide green buffers between communities or around the city. Greenways and linear parks typically support trail-oriented activities, including walking, jogging, biking, and skating, which play a major role in health and fitness. These parks may incorporate smaller scale neighborhood park amenities, such as play areas, picnic areas, or exercise trails. Trails between key destinations can help create more tightly-knit communities, improve children's access to schools, and provide opportunities for non-motorized transportation. Greenways and linear parks may be of various lengths and widths, and these corridors typically support facilities such as viewing areas, picnic tables, or trailheads.

Natural Areas. Natural areas are undeveloped lands left in a natural state for conservation or outdoor recreation. They protect and manage unique or significant natural features, such as rivers and streams, wetlands and marshes, steep hillsides, environmentally sensitive areas, and wildlife habitats. Some natural areas may have limited access due to resource conservation needs. Sites may provide a number of ecological benefits, such as shoreline protection, wildlife habitat, stormwater filtration, riparian corridor and wetland preservation, and erosion control. Where appropriate, natural areas may support passive, outdoor recreation, such as trail-related opportunities, bird and wildlife viewing, environmental interpretation and

education, and nature photography. These parks provide opportunities to promote conservation and stewardship, develop an appreciation for nature, and improve health and wellness by providing a physical and mental refuge from the built environment. The size and shape of the natural area will vary depending on the protected resource.

Undeveloped Sites. Undeveloped sites include land that has been acquired by the City for possible future park improvements. These sites are not managed as natural areas, although they do provide green space in the City. Their size varies depending on the purpose of their acquisition.

Beautification Areas. Beautification areas are landscaped areas, typically located along street rights-of-way and intersections, entry features, and parking lots. Beautification areas can incorporate flowers, shrubs, trees, native plants, or other ecologically appropriate landscaping to add green space to the urban environment, provide visual and economic benefits, and promote environmental health. Through the use of signage, fountains, or other features, these areas also may add to community identity. Unlike parkland, beautification areas have reduced recreational value.

PR-1b **Parkland Dedication.** For all residential subdivisions, require parkland dedications, or fees in lieu of dedication, according to the following standards:

- Land or fees shall be based on five (5) acres of parkland per 1,000 persons.
- Subdivisions having fifty-one or more lots shall automatically be reviewed by the Parks and Recreation Committee for possible dedications. Any other residential subdivision creating five or more lots, and that proposes to pay a fee in lieu of dedicating land, shall also be reviewed by the Parks and Recreation Committee for possible dedications.
- Land that is not suitable for recreation or wildlife habitat may not be counted toward fulfilling parkland dedication requirements.
- Parkland dedications should be consistent with the following criteria:
 - Average slope should not exceed 10 percent;
 - Access to the site shall be adequate for the purpose of the community or neighborhood park;
 - The site shall not have significant topographic or environmental constraints that would render the site unsuitable for development.
- Riparian areas, wetlands, and forests shall be accepted for dedication only if it has been determined that the recreational needs of the area will be adequately met by other sites. Where such lands are accepted, development of parklands should recognize existing wildlife and habitat values and minimize the disturbance of riparian, wetland, and other wildlife habitats.
- The City shall maintain a schedule for the use of land and fees acquired pursuant

to parkland dedication requirements, including mechanisms for tracking the receipt and expenditure of funds over a five-year period.

POLICY PR-2 PARK DEVELOPMENT AND MAINTENANCE

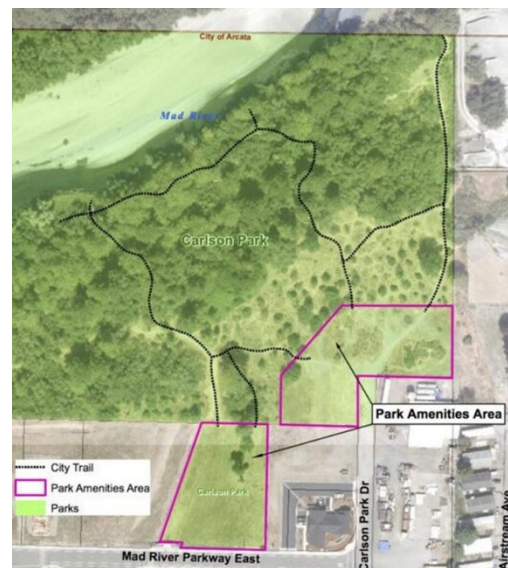
Objective. Encourage appropriate and sustainable new park design and development and existing park maintenance.

PR-2a **Natural resource protection.** Protect natural areas and wildlife habitat within parks and open space land to the greatest extent feasible. Integrate recreational use in natural areas, where not in conflict, through appropriately designed trails, trailheads, and interpretive resources to achieve the community's desire for access.

PR-2b **Park & Recreation Master Plan.** Use the Park & Recreation Master as a guide for the provision of parks and recreation in the City and implement the plan in a manner consistent with the Arcata General Plan. Periodically update the plan to reflect changes in the environment, economy, population, and land use and the unique needs of Arcata residents. Use Appendix B, Design and Development Guidelines for direction regarding the amenities and facilities to be provided for each park type.

PR-2c **Park system improvements.** Parks and recreation projects that provide desired levels of service and minimize energy and water use, sustain existing resources, improve maintenance efficiency, reduce life-cycle costs, and otherwise promote environmental sustainability should be given high priority. Park & Recreation Master Plan recommendations should be used as a guide for new park and trail development to meet community needs and as a guide to prioritize the renovation and rehabilitation of existing parks. In general, prioritize projects that:

- Enhance sustainability;
- Increase trail connectivity;
- Improve park access;
- Expand recreation opportunities;
- Serve future growth areas;
- Enhance natural areas;
- Support community values and strengthens the community;
- Utilize available funding; and
- Encourage participation and recognition of partners for matching funds and volunteer efforts.



PR-2d **Active use amenities.** Provide active use resources in new parks to expand recreation

opportunities in the City using the Park & Recreation Master Plan Appendix B, Design and Development Guidelines. Priority should be given to the redesign and integration of active use resources into the following existing neighborhood parks, where feasible:

- Bloomfield Park
- Cahill Park
- Mountain View Park
- Vinum Park
- Westwood Manor Park
- Windsong Park

PR-2e Trail improvements. Provide an interconnected, multi-purpose trail system with regional linkages that includes mountain bike, fitness, nature, and equestrian trails and that enhances individual mobility. Priority should be given to the following trail projects identified in the Parks & Recreation Master Plan:

- Arcata Rail with Trail Corridor integrated with the Great Redwood Trail
- Annie & Mary Rail-Trail
- Hammond Trail
- Aldergrove Open Space/Arcata Community Forest trail connection
- Route 255 Connection
- Baylands Trail
- Arcata Ridge Trail – enhancements to existing trail segments connecting the Arcata Community Forest Tract and the Sunny Brae Forest
- Jacoby Creek Forest Trails – Conduct planning with community partners and adjacent property owners to identify feasible public access opportunities for the future

PR-2f Mobility and Accessibility; Design new, and enhance existing, parks and recreation facilities to be inclusive for all community members, including people with cognitive and physical disabilities; multi-lingual/multi-cultural users; groups hosting events; teams; and seniors.

PR-2g Maintenance best practices. Use best management practices to ensure that parks are maintained at the desired levels of service. Prepare a Maintenance Management Plan and Natural Resource/Open Space Management Plan that reflects each facility's use, assigned maintenance tier, and level of service and that incorporates the anticipated lifecycle of facilities and projects and an appropriate reinvestment and replacement schedule.

PR-2h Art in parks. Integrate arts and cultural activities with City parkland through the implementation of the City of Arcata General Plan and the Strategic Arts Plan.

POLICY PR-3 COORDINATED OPEN SPACE, PARK AND RECREATIONAL FACILITIES PLANNING AND USE

Objective. Undertake effective ongoing coordination between local and regional parks and natural resources agencies, community groups, and the private sector that enhances environmental sustainability and the availability of park and open space for Arcata residents.

- PR-3a **Coordinated recreational use of Community Forests and Marsh & Wildlife Refuge.** Work with local and regional partners to protect and enhance the roles of the:
- Arcata Marsh & Wildlife Refuge as an important regional natural and recreational area and as an essential component of the City's wastewater treatment plant; and
 - Arcata Community and Jacoby Creek Forests as a source of watershed protection, wildlife habitat, and sustainable timber harvesting to support City park acquisition, development, and maintenance.
- PR-3b **Coordinated recreational use with schools and State/Federal Agencies.** Coordinate closely with community organizations, schools, Cal Poly Humboldt, the private sector, and local, state, and federal agencies in developing creative solutions for meeting recreation program and facility needs. Consider developing a formal partnership with the schools to maintain and program sports fields at school sites, in particular a formal partnership with Cal Poly Humboldt to address existing and potential increases in service demands for City parks and recreation facilities from planned increases in student enrollment, faculty, and other staff.
- PR-3c **Use and management of indoor and athletic facilities, special recreation facilities and gathering areas.** Work with community groups, sports organizations, and other coalitions to define mutually compatible recreational facility needs and mechanisms for the operation, maintenance, and enhancement of these facilities.

3.3 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURES	RESPONSIBLE PARTY	TIME FRAME
PR -1a	Design and Development Guidelines Use the Appendix B, Park Design and Development Guidelines as the standard for park and recreation improvements, facilities, and amenities. (Park & Recreation Master Plan Implementation C)	Environmental Services Department	Ongoing

#	IMPLEMENTATION MEASURES	RESPONSIBLE PARTY	TIME FRAME
PR -1b	<p>Parkland Dedication Program and Schedule</p> <p>Continue to implement fees / dedications pursuant to Quimby Act (California Government Code § 66477, as amended), including (1) the preparation and maintenance of a five-year schedule specifying how, when, and where it will use the land or fees, or both, to develop park or recreational facilities to serve the residents of subdivisions; a scheduled re-evaluation of the formula for fees in lieu of land based on current persons per dwelling unit and fair market value of land; and (2) a procedure to ensure that accounting and use of parkland fees is consistent with 66477(a)(6). (1994 Parks and Rec Element IV-A6, IV-A7)</p>	Environmental Services Department	Every five years starting in Year 2
PR -1c	<p>Level of Service Standards-Parkland to Population</p> <p>Update the Park & Recreation Master Plan Community Needs, Level of Service Standards-Parkland to Population.</p>	Environmental Services Department	Every five years starting in Year 2
PR -2a	<p>Park Funding</p> <p>Continue to the implement the strategic funding options outlined in the Park & Recreation Master Plan, Implementation A through C (Park & Rec. Master Plan Implementation A, B, and C) and continue to use a portion of net forest revenues for park development, maintenance, and acquisition. (1994 Parks and Rec Element IV-A2)</p>	Environmental Services Department	Ongoing
PR -2b	<p>Park Development and Maintenance</p> <p>Continue to the implement Park & Recreation Master Plan recommendations relating to park development and maintenance, including those for Master Planning and Design, universal design upgrades, Natural Resource Management, and Maintenance Practices (Park & Rec. 5. Recommendations)</p>	Environmental Services Department	Ongoing
PR-2c	<p>Maintenance Management Plan and Natural Resource/Open Space Management Plan</p> <p>Prepare a Maintenance Management Plan and Natural Resource/Open Space Management Plan that reflects each facility's use, assigned maintenance tier, and level of service and that incorporates the anticipated lifecycle of facilities and projects and an appropriate reinvestment and replacement schedule.</p>	Environmental Services Department	Ongoing starting in year 2

#	IMPLEMENTATION MEASURES	RESPONSIBLE PARTY	TIME FRAME
PR -3a	<p>Coordinated Open Space, Park and Recreational Facilities Planning and Use</p> <p>Update the Park & Recreation Master Plan every ten years. Use periodic updates to identify opportunities for enhanced partnerships with agencies and organizations to enhance City parks and recreation facilities and activities. Coordinate updates and regional trails planning with Humboldt County, HCAOG, Great Redwood Trail Agency, Redwood Community Action Agency, Redwood Coast Mountain Bike Association and the Humboldt Trails Council.</p>	Environmental Services Department	Every ten years starting in Year 2

Appendices

Appendix A - Parks & Facility Inventory

Appendix B - Design Standards by Classification



Appendix A:
Park and Facility Inventory

Table A-1 Arcata Park and Facility Inventory: City Parks and Facilities

PARK NAME	TOTAL ACREAGE	ATHLETIC FIELDS				OUTDOOR ATHLETIC FACILITIES			INDOOR FACILITIES			OTHER PARK AMENITIES							
		BASEBALL FIELD	FOOTBALL FIELD	SOCCER FIELD	SOFTBALL FIELD	BASKETBALL COURT	SKATE PARK	TENNIS COURT	COMMUNITY CENTER	GYMNASIUM	POOL	OFF-STREET PARKING	TRAILS	COMMUNITY GARDEN	BOCCE	GROUP PICNIC AREA	PICNIC TABLE	PLAYGROUND	RESTROOMS
NEIGHBORHOOD PARKS																			
Bloomfield Park	0.23															X	1		
Cahill Park	0.73															X	1		
Chevret-Vaissade Park	1.47					1										X	1		
Ennes Park	0.47															X	1		
Greenview Park	0.36					1										X	1		
Janes Creek Meadows Park	0.82												X			X	1		
Mountain View Park	0.26															X	1		
Rotary Park	0.28					1										X	1		
Stewart Park	1.29															X	1		
Vinum Park	0.09															X	1		
Westwood Manor Park	1.72															X	1		
Windsong Park	1.74															X	1		
<i>Subtotal</i>	9.46	0	0	0	0	3	0	0	0	0	0	0	1	0	0	0	12	12	0
COMMUNITY PARKS																			
Arcata Community Park	30.35			3	2	1			1	1		1	X			X	1	1	Commercial Kitchen, Lighted softball field
Redwood Park	26.21					1			2			1	X		2	X	1	1	Redwood Lodge, Redwood Lounge, Boy Scout Hut, stage area
<i>Subtotal</i>	56.56	0	0	3	2	2	0	0	3	1	0	2	2	0	0	2	2	2	
SPECIAL USE AREAS																			
Arcata Ball Park	4.15	1							1			1						1	Concession stand, Judo Hut, maintenance building, lighted ballfield
Arcata Plaza	1.01																		
Arcata Skate Park	0.65							1										1	Skate park
Bayside Park	4.72											1		1				1	Greenhouse, tool shed, market stand
Larson Park	2.18											1	X		2	1	X	1	Gazebo, tennis courts are lighted
Pacific Union Park	4.15			2	1							1						1	Maintenance shop
<i>Subtotal</i>	16.86	1	0	2	1	0	1	3	1	0	0	4	1	1	2	1	1	1	4
LINEAR PARKS/GREENWAYS																			
D Street Linear Park	1.23								1			1	X						Neighborhood Center, commercial Kitchen, Sports flooring
Janes Creek Linear Park	1.04												X						
Valley West Park	3.59																X	1	
<i>Subtotal</i>	5.86	0	0	0	0	0	0	0	1	0	0	1	2	0	0	0	1	1	0
NATURAL AREAS																			
11th & M Street Wetland	0.45																		
Aldergrove Marsh	23.02												X						
Arcata Baylands	570.72												future						
Arcata Community Forest: Community Park Tract	610.29											1	X						
Arcata Community Forest: Jacoby Creek and Sunny Brae Tracts ^A	2,069.70												X						
Arcata Marsh & Wildlife Sanctuary	226.38								1			1	X						Interpretive Center
Butcher Slough/Jolly Giant Creek Restoration Area	11.58																		
California Avenue Open Space	0.94																		
Janes Creek Open Space	2.55																		
Janes Creek Meadows Open Space	9.74												X						
Jolly Giant Creek Open Space	0.07																		
McDaniel Slough	88.40												X						
Sellers Pond Open Space	7.34																		
Sunny Brae Park	2.93												X						
Woodland Heights Park	0.94																		
Zehndner Avenue Open Space	0.35																		
<i>Subtotal</i>	3,625.40	0	0	0	0	0	0	0	1	0	0	2	7	0	0	0	0	0	0
UNDEVELOPED SITES																			
California Park	1.31																		
Ennes Park Expansion	4.12																		
Carlson Park ^B	19.80												planned			planned	planned		River access
Shay Park	4.66												X						
<i>Subtotal</i>	29.89	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0
TOTAL	3,744.03	1	0	5	3	5	1	3	6	1	0	9	14	1	2	3	16	16	6

^A This number represents the total deeded acreage for both forests. This accounts for 797 acres within the City Limits, and 1272.7 acres outside the City Limits. These forests are counted as two separate sites.
^B Carlson Park is not yet City-owned. However, this site is anticipated to be acquired before the end of the planning process.

Attachment: E. Ch 3 Housing and Human Services (4214 : GP #XVII FBC & GP wrap-up)

Table A-2: Arcata Park and Facility Inventory: Schools

PARK NAME	TOTAL ACREAGE ⁴	SCHOOLS that help meet City recreation needs	DEDICATED ATHLETIC FIELDS				OUTDOOR ATHLETIC FACILITIES				INDOOR FACILITIES		OTHER PARK AMENITIES				
			BASEBALL FIELD	FOOTBALL FIELD	SOCCER FIELD	SOFTBALL FIELD	BASKETBALL COURT	SKATE PARK/AREA	TENNIS COURT	TRACK	GYMNASIUM	POOL	DISC GOLF	PICNIC SHELTER	PICNIC AREA	PLAYGROUND	RESTROOMS ¹
ARCATA SCHOOLS																	
Arcata High (9-12 Grade)	28.88		1		1	1	1		3		1						
Arcata Elementary (K-5 Grade)	11.33	√			1		1									1	
Jacoby Creek Charter School (K-8 Grade)	10.34	√	1				2			1	1		1			2	
Pacific Union Elementary (K-8 Grade)	11.10	√					1				1					1	
Sunny Brae Middle (6-8 Grade)	10.19	√			1		1				1					1	
<i>Subtotal</i>	71.84		2	0	3	1	6	0	3	1	4	0	1	0	0	5	0
PRIVATE/CHARTER SCHOOLS																	
Arcata Christian	1.68										1						
Blue Heron Learning Center	0.20		1														
Arcata School District Charter	7.82	√			2		3									2	
St. Mary's Catholic School/Laurel Tree Learning Center	13.67				1		2				1						
Trillium Charter	0.58															1	
Union Street Charter / Equinox Center	0.93															1	
<i>Subtotal</i>	24.89		1	0	3	0	5	0	0	0	2	0	0	0	0	4	0
COLLEGES/UNIVERSITIES																	
Humboldt State University ²	134.51			1	1	1	1			1	2	1	1		Y		1
<i>Subtotal</i>	134.51		0	1	1	1	1	0	0	1	2	1	1	0	1	0	1
OTHER SCHOOLS³																	
Arcata Children's Center	0.36																
Juell's Nursery School	0.81																
Mad River Montessori Preschool	1.32																
Mistwood Center for Education	0.37																
Northcoast Children Services	0.32															2	
Northcoast Preparatory & Performing Arts	1.80																
<i>Subtotal</i>	4.99		0	0	0	0	0	0	0	0	0	0	0	0	0	2	0
TOTAL	236.24		3	1	7	2	12	0	3	2	8	1	2	0	1	11	1

¹ This category includes outdoor restrooms only.

² Other schools are noted for information only. Their facilities will not be counted in the needs assessment.

³ HSU also operates the Humboldt Bay Aquatic Center, located in Eureka.

⁴ Total acres includes building foot prints and open space.

Table A-3: Arcata Park and Facility Inventory: Other Providers

PARK NAME	OWNERSHIP	TOTAL ACREAGE	ATHLETIC FIELDS				OUTDOOR ATHLETIC FACILITIES			INDOOR FACILITIES			OTHER PARK AMENITIES					
			BASEBALL FIELD	FOOTBALL FIELD	SOCCER FIELD	SOFTBALL FIELD	BASKETBALL COURT	SKATE PARK	TENNIS COURT	COMMUNITY CENTER	GYMNASIUM	POOL	COMMUNITY GARDEN	PICNIC SHELTER	PICNIC AREA	PLAYGROUND	RESTROOMS	Trails in linear feet/miles
SPECIAL USE AREAS																		
Arcata Pool N.H.P.R.D.	Public / Contract	0.00										1					Y	
Arcata Little League	Non-Profit		2															
Baywood Golf Course	Private Country Club	200.00								1 clubhouse		1					Y	
HealthSPORT ¹	Private	0.00										1					Y	
<i>Subtotal</i>		200.00	2	0	0	0	0	0	0	1	1	3	0	0	0	0	3	N/A
LINEAR PARK/GREENWAY																		
Hammond Coastal Trail	Humboldt County																	5.5
<i>Subtotal</i>		0.00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5.5
NATURAL AREAS																		
BLM - Madel South	BLM	154.00																1 mile
Friends of the Dunes	Non-Profit	61.00														Y		1 mile
HBMWD Pump Station Parks	District	24.00												1	4		2	Disc Golf Course
Humboldt Bay Wildlife Refuge	USFWS	770.75													Y		1	Y
Jacoby Creek Land Trust	Land Trust	63.00																
Lamphere Christian Dunes	Fish and Wildlife	38.00																
Mad River Slough and Wildlife Area	California	294.78																Y
Mad River Beach and Coastal Areas	Humboldt County																	
<i>Subtotal</i>		1,405.53	0	0	0	0	0	0	0	0	0	0	0	1	6	1	2	N/A
TOTAL		1,605.53	2	0	0	0	0	0	0	1	1	3	0	1	6	1	5	N/A

¹ HealthSPORT also has weight rooms, cardio areas, two studios and on-site childcare.

Table A-4: Arcata Park and Facility Inventory: Trails

TRAIL NAME	EXISTING MILEAGE	PROPOSED MILEAGE*	TOTAL MILEAGE	HARD SURFACED	SOFT SURFACED	SIGNAGE	TRAILHEADS WITH PARKING
Aldergrove Marsh	0.7		0.7		Yes		Yes
Arcata Baylands		0.9	0.9		Yes	Yes	Yes
Arcata Community Forest Trails	10.9	0.2	11.1		Yes	Yes	Yes
Arcata Community Park		0.5	0.5	Yes	--		Yes
Arcata Marsh and Wildlife Sanctuary	4.4		4.4		Yes	Yes	Yes
Humboldt Bay Trail	0.0		0.0		Yes	Yes	Yes
Janes Creek Meadows	0.7		0.7		Yes		
McDaniels Slough	0.9		0.9		Yes	Yes	Yes
Redwood Park	0.5		0.5		Yes		Yes
Shay Park	0.3		0.3		Yes		
Sunny Brae Park		0.2	0.2		Yes		
United Indian Health Services	0.5		0.5	--	Yes	Yes	Yes
<i>TOTAL</i>	18.9	1.8	20.7	1	12	6	9

* Proposed trail acreage is an estimate based on 2009 anticipated routes, which have been further updated

Table A-5: Arcata Park and Facility Inventory: Trails by Type

TRAIL NAME	EXISTING MILEAGE	PROPOSED MILEAGE*	TOTAL MILEAGE	HARD SURFACED	SOFT SURFACED	SIGNAGE	TRAILHEAD WITH PARKING	NOTES
PEDESTRIAN AND BICYCLE TRAILS								
Arcata Community Forest Trails	0.7		0.7		Y	Yes	Yes	
Arcata Marsh and Wildlife Sanctuary	4.4		4.4		Y	Yes	Yes	
Redwood Park	0.2		0.2		Y		Yes	
<i>Subtotal</i>	5.3	0.0	5.3		3	2	3	
PEDESTRIAN, BIKE and EQUESTRIAN TRAILS								
Arcata Community Forest Trails	7.7		7.7					
<i>Subtotal</i>	7.7	0.0	7.7					
PEDESTRIAN ONLY TRAILS								
Aldergrove Marsh	0.7		0.7		Yes		Yes	
Arcata Baylands		0.9	0.9		Yes			
Arcata Community Forest Trails	2.5	0.2	2.7		Yes			
Arcata Community Park		0.5	0.5	Yes				
Janes Creek Meadows	0.7		0.7		Yes			
McDaniels Slough	0.9		0.9		Yes			
Redwood Park	0.3		0.3		Yes			
Shay Park	0.3		0.3		Yes			
Sunny Brae Park		0.2	0.2		Yes			
United Indian Health Services	0.5		0.5		Yes	Yes	Yes	Not City Owned or Maintained
<i>Subtotal</i>	5.9	1.8	7.7	1	9	1	2	
<i>TOTAL</i>	18.9	1.8	20.7	1	12	3	5	

* Proposed trail acreage is an estimate based on 2009 anticipated routes, which have been further updated in 2010.



Appendix B:
Design and Development
Guidelines

CLASSIFICATION	DEFINITION	BENEFITS	SIZE AND SERVICE	EXAMPLES	MINIMUM RESOURCES	ADDITIONAL RESOURCES	CONFLICTING RESOURCES
All Parks	All parks in Arcata must meet minimum guidelines for design and development. Minimum resources for all parks are highlighted here.	<ul style="list-style-type: none"> • Provide open space • Enhance City identity • Provide opportunities for outdoor recreation 	<ul style="list-style-type: none"> • Size should be appropriate for park type and use 		<ul style="list-style-type: none"> • Park identification sign • ADA-compliant pathway system • Any required safety features • Site-appropriate landscaping (trees, shrubs, native flora and fauna) • Appropriate site furnishings (bike rack, benches, trash/recycle receptacles, etc) 	<ul style="list-style-type: none"> • Safety lighting • Additional beautification and landscaping at appropriate sites (floral plantings, hanging baskets, lawns, gardens, etc.) • Active-use facilities and natural areas should be separated, but natural areas may be incorporated into all park types • Public art 	<ul style="list-style-type: none"> • Facilities and/or landscaping that does not appropriately reflect the park environment, character, or intended use
Neighborhood Parks	Located within walking and bicycling distance of most users, neighborhood parks are small parks that provide access to basic recreation opportunities for nearby residents. Designed primarily for non-supervised, non-organized recreation activities, neighborhood parks often include amenities such as playground equipment, outdoor basketball courts, picnic tables, pathways, and multi-use open grass areas. Neighborhood parks may include small scale natural areas, such as riparian areas, and can contribute to environmental sustainability by incorporating sustainable design elements.	<ul style="list-style-type: none"> • Provide close-to-home opportunities for physical activity • Support family and small group gatherings • Enhance neighborhood identity • Preserve open space within neighborhoods • Promote health and wellness • Provide opportunities for outdoor recreation 	<ul style="list-style-type: none"> • Typically 1-5 acres • Serves residents located within walking and biking distance • Larger parks may serve residents within a ½ mile • Small parks (less than 1 acre) may serve residents within a ¼ mile or less 	<ul style="list-style-type: none"> • Bloomfield Park • Cahill Park • Chevret-Vaissade Park • Ennes Park • Greenview Park • Janes Creek Meadows Park • Mountain View Park • Rotary Park • Stewart Park • Vinum Park • Westwood Manor Park • Windsong Park 	<ul style="list-style-type: none"> • Children’s play area* • Picnic tables and benches • Perimeter path or sidewalks • Open turf area/ multi-use field • At least one additional, active recreation resource (see next column)* • On-street parking 	<ul style="list-style-type: none"> • Sports courts (basketball, tennis, wall ball, handball, racquetball, and/or volleyball courts) • Other small-scale active recreation resources (skate spot, horseshoe pits, par course, shuffleboard lane, etc.) • Table shelter, shade structure or gazebo • Informal sports fields (baseball, soccer, softball, multi-purpose) • Off-street parking 	<ul style="list-style-type: none"> • Destination facilities or resources with citywide draw • Game-quality sports fields (baseball, football, soccer, softball) • Sports complexes • Community or recreation centers • Large-group facilities • Spray grounds or swimming pools (indoor or outdoor) • Permanent restrooms

*Neighborhood parks should include play area(s) to serve children ages 2-12, along with an additional active recreation resource, where feasible. The presence and design of these resources may be influenced by site constraints.

CLASSIFICATION	DEFINITION	BENEFITS	SIZE AND SERVICE	EXAMPLES	MINIMUM RESOURCES	ADDITIONAL RESOURCES	CONFLICTING RESOURCES
Community Parks	Community parks are larger parks that provide both active and passive recreation opportunities that appeal to the entire community. These sites generally include organized, active recreation, supported by facilities such as sport fields and outdoor courts. These parks also serve as community gathering spaces, offering a variety of facilities that can accommodate small or large groups, such as recreation centers, group picnic areas and shelters, rental space, festival space, amphitheaters, and large playgrounds. Since community parks generally attract a large number of people from a wide geographic area, support facilities are required, such as off-street parking and restrooms. Community parks may also include significant natural areas and trails.	<ul style="list-style-type: none"> • Provide a variety of accessible recreation opportunities for all ages • Serve as community gathering places, providing social opportunities that bring diverse groups of people together and build stronger families and communities • Contribute to community identity • Serve recreation needs of individual, families, small and large groups • Provide close-to-home opportunities for physical activity • Promote lifelong fitness, health and wellness • Connect residents to nature and relieve stress from urban living • Preserve open space • Provide opportunities for outdoor recreation 	<ul style="list-style-type: none"> • Optimum size 15-30 acres • May draw residents from the entire community • Provides access from a collector or arterial street • Should be located to incorporate bus and transit access • Supports bicycle and pedestrian access 	<ul style="list-style-type: none"> • Arcata Community Park • Redwood Park 	<ul style="list-style-type: none"> • Children’s play area • Picnic tables and benches • Picnic shelter, group picnic area, shade structure or gazebo • Sport courts (basketball tennis, wall ball, and/or volleyball courts) • Open turf area • Multi-purpose trails and pathways • At least one additional, active recreation resource (see next column) • Off-street parking • Restrooms (permanent; sufficient to support large-group facilities; additional portables may be brought in for special events) 	<ul style="list-style-type: none"> • Active recreation resources (bocce, handball/racquetball court, croquet court, disc golf course, fitness stations/ par course, tennis backboard, horseshoe pit, shuffleboard lanes, mini skate park, etc.) • Sports fields (baseball, football, rugby, soccer, softball, multi-purpose) • Special use trails and pathways (e.g., looped path, fitness or jogging trails, mountain biking, equestrian, nature or interpretive trails) • Community garden • Off-leash dog area • Outdoor stage • Upgraded utility service to support special events • Other facilities with community-wide draw (amphitheater/concert venue, arboretum, botanical garden, aquatic center, sports tournament complex; indoor nature center; multi-purpose recreation or community center) • Natural areas • Maintenance facilities 	<ul style="list-style-type: none"> • Any development that damages protected natural areas or resources

CLASSIFICATION	DEFINITION	BENEFITS	SIZE AND SERVICE	EXAMPLES	MINIMUM RESOURCES	ADDITIONAL RESOURCES	CONFLICTING RESOURCES
Special Use Areas	Special use areas include stand-alone recreation facilities not located within larger parks. These include single-purpose sites, such as community centers, aquatic centers, sports complexes, boat ramps, skate parks, outdoor theaters, urban plazas, and gardens. Outdoor areas may incorporate native plantings or small natural areas.	<ul style="list-style-type: none"> Promote a variety of personal, social, and economic benefits depending on facility type and location Provide local, citywide and/or regional opportunities for recreation, social and cultural activities Serve recreation needs of targeted user groups May attract new residents and businesses and support tourism Contribute to community identity 	<ul style="list-style-type: none"> Ideal size is determined by use The size of the service area is determined by the type of facilities and opportunities offered The type of access required also depends on the use, but should include where appropriate pedestrian, bicycle, boat, public and private transit 	<ul style="list-style-type: none"> Arcata Ball Park Arcata Plaza Arcata Skate Park Bayside Park Larson Park Pacific Union Park 	<ul style="list-style-type: none"> Features and facilities to support a specialized recreation opportunity 	<ul style="list-style-type: none"> Specialized active recreation facilities (bocce courts, skate park, tennis center, climbing wall, gymnasium) Sport fields or tournament complexes Children’s play area Multi-purpose community or recreation center, nature center Motorized or non-motorized boat launch Commercial ventures or features; concessions; farm market Historical or interpretive facilities Community garden, botanical garden, arboretum, greenhouse, farm extension Off-leash dog area Large-group areas, reunion venues, wedding space Infrastructure to support large community events; festival space; stage/amphitheatre Memorials, flag poles, or benches Natural areas Maintenance facilities Multi-use trails Parking (on-street or off-street as needed to support use) Restrooms (permanent or portable) 	<ul style="list-style-type: none"> Any resource, amenity, or facility that conflicts with the intended purpose of the site

CLASSIFICATION	DEFINITION	BENEFITS	SIZE AND SERVICE	EXAMPLES	MINIMUM RESOURCES	ADDITIONAL RESOURCES	CONFLICTING RESOURCES
Linear Parks/Greenways	Greenways and linear parks include natural or built corridors used to link features together and provide green buffers between communities or around the city. Greenways and linear parks typically support trail-oriented activities, including walking, jogging, biking, skating, etc. These parks may incorporate smaller scale neighborhood park amenities, such as play areas, picnic areas, or exercise trails. Linear parks may include abandoned railroad lines, active transportation or utility rights-of-way, wildlife corridors, or elongated natural areas defined by drainage features or topographical changes. Greenways and linear parks may be of various lengths and widths, and these corridors typically support facilities such as viewing areas, picnic tables, or trailheads.	<ul style="list-style-type: none"> Enhance connectivity, creating more tightly-knit communities Improve children’s access to schools Provide opportunities for active, non-motorized transportation, promoting health and fitness Increase opportunities to experience nature Connect residents to nature and relieves stress from urban living Contribute to community identity and quality of life May protect valuable natural corridors May contribute to the environmental health of the community, including protecting the tree canopy and improving water and air quality Provide opportunities for nature-based recreation and environmental education 	<ul style="list-style-type: none"> The size, shape, and service area will vary depending on its function and use Trail corridors should be located to support or incorporate pedestrian, bicycle, automotive, bus and transit access 	<ul style="list-style-type: none"> Janes Creek Linear Park D Street Linear Park Valley West Park 	<ul style="list-style-type: none"> Green space (landscaped or natural) 	<ul style="list-style-type: none"> Picnic tables Small group picnic areas and shelters Multi-purpose or special use trails Trailhead, trail kiosk, or entry Interpretive and directional signage Viewpoints, viewing blinds, or boardwalks Par course Small play area Small active use amenity Parking at trailhead (on-street or off-street) Restroom (portable or permanent) at trailhead 	<ul style="list-style-type: none"> Active-use facilities and natural areas should be separated

CLASSIFICATION	DEFINITION	BENEFITS	SIZE AND SERVICE	EXAMPLES	MINIMUM RESOURCES	ADDITIONAL RESOURCES	CONFLICTING RESOURCES
Natural Areas	Natural areas are undeveloped lands left in a natural state for conservation or outdoor recreation. These parks are designed to protect and manage unique or significant natural features, such as rivers and streams, wetlands and marshes, steep hillsides, environmentally sensitive areas, and wildlife habitats. Some natural areas may have limited access due to resource conservation needs. Where appropriate, natural areas may support passive, outdoor recreation, such as trail-related opportunities, bird and wildlife viewing, environmental interpretation and education, and nature photography. The size and shape of the natural area will vary depending on the resource it protects.	<ul style="list-style-type: none"> • Protect valuable natural resources and open space • Contribute to the environmental health of the community, by providing a number of ecological benefits, such as shoreline protection, wildlife habitat, stormwater filtration, riparian corridor and wetland preservation, and erosion control • Contribute to community identity and quality of life • Improve the aesthetic quality and beauty of Arcata • Provide opportunities for nature-based recreation and environmental education • Promote conservation and stewardship • Enhance community and individual appreciation for nature • Improve health and wellness by providing a physical and mental refuge from the built environment 	<ul style="list-style-type: none"> • The size, shape, and service area will vary depending on its function and use 	<ul style="list-style-type: none"> • I1th & M Street Wetland • Aldergrove Marsh • Arcata Baylands • Arcata Community Forest • Arcata Marsh & Wildlife Sanctuary • Butcher Slough/Jolly Giant Creek Restoration Area • Jacoby Creek and Sunny Brae Forests A • Janes Creek Open Space • Janes Creek Meadows Open Space • Jolly Giant Creek Open Space • McDaniel Slough • Sellers Pond Open Space • Sunny Brae Park • Woodland Heights Park • Zehndner Avenue Open Space 	<ul style="list-style-type: none"> • Natural open space 	<ul style="list-style-type: none"> • Trail or pathway system • Trailhead, trail kiosk, or entry • Interpretive and directional signage • Viewpoints, viewing blinds, boardwalks, or viewing piers • Interpretive center, educational facilities or classrooms (indoor or outdoor) • Preservation areas (with no public access) • Picnic tables • Shelter or gazebo • Open grass areas • Nature-themed recreation elements or active recreation elements appropriate for natural areas (e.g., playground, ropes course, par course, Frisbee golf, canopy tours, mountain biking trails, dirt BMX course, etc.) • Dog play area (fenced or fenceless) • Parking (on-street or off-street) • Restrooms (portable or permanent) 	<ul style="list-style-type: none"> • Sports facilities (fields, paved courts, etc.) • Any resource and level of development that conflicts with the intended purpose of the site • Any development that damages protected natural resources

Attachment: E. Ch 3 Housing and Human Services (4214 : GP #XVII FBC & GP wrap-up)

Chapter 4 – Environmental Quality and Management

Open Space Element

Resource Conservation Element

Air Quality Element

OPEN SPACE ELEMENT

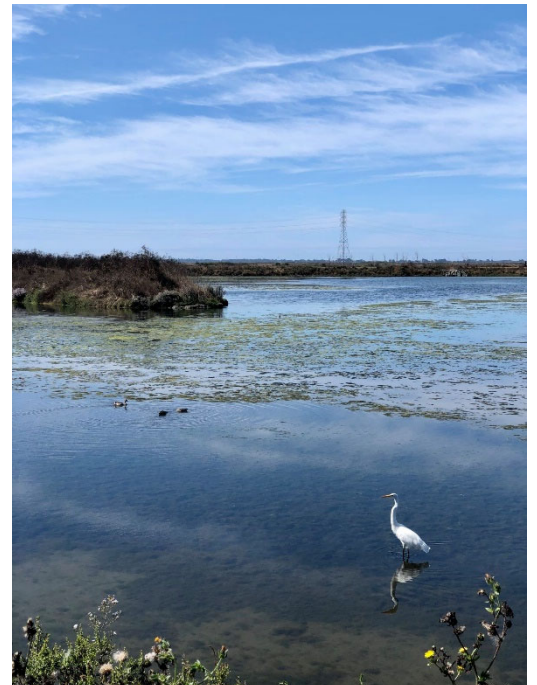
4.1 INTRODUCTION

Overview of Arcata's Open Space Resources. Arcata's open spaces take many forms and serve a variety of functions. Open space ~~areas represent a significant and desirable component of~~ ~~contributes to~~ the community's character and ~~maintain~~ ~~provide~~ natural, recreational, and visual resources for ~~future~~ community use and enjoyment. Open space is valuable for both passive and active uses. Designating or otherwise protecting lands as open space provides for: protection of natural habitats and species; managed production of natural resources such as agricultural and forest products; recreational uses; coastal access; scenic, aesthetic resources; and avoidance of development on such areas as steep slopes, faults, and flood zones ~~that are potentially hazardous to the community~~. Open space is also valuable for preserving scenic views and other aesthetic considerations. ~~The overall system is also~~ ~~Open Sspace networks are~~ enhanced when ~~open space lands are linked by~~ natural biological corridors, greenways, easements, parks and other ~~types of~~ connections are linked. Open space categories are described in more detail below, and mapped on Figure OS-a ~~located in the map pocket at the end of this chapter~~.

Open Space for natural resourcethe maintenance and enhancement of natural resources protects plant and ~~animal~~ wildlife habitat, especially in areas where rare, endangered, or threatened species exist. Arcata's creek watersheds and watercourses, McDaniel, Gannon, Butcher and Mad River sloughs, Arcata and Aldergrove Marshes, wetlands, baylands and tidelands, and ecological and scientific study areas, may also be included in this category.

Open space for the managed production of resources includes forest lands on the west slopes of Fickle Hill, agricultural lands in the Arcata Bottom and Bayside areas, aquaculture areas in Arcata Bay, and the aggregate deposits along the Mad River. These areas are important for their production of ~~food~~ agricultural, ~~wood~~ forest, and mineral ~~resources~~ products, as well as for recharge of groundwater basins. Open space management also enhances surface water supplies for Arcata Bay, McDaniel Slough, Arcata Marsh, the Mad River, and the multiple streams that flow through the area, maintaining water quality and other ecological functions.

Open space for outdoor recreation includes both active and passive recreation uses in City parks and recreation



areasballfields, such as Redwood Park and the Community Center; cultural and special use areas such as the Arcata Ball field, Skate park, and Plaza; neighborhood parks such as Sunny Brae and Stewart Parks; and natural areas such as the Arcata Marsh and Shay Park.

Open space for public health and safety includes earthquake fault zones, liquefaction areas, steeply sloped hillsides, and 100-year floodplains. Vegetation in open spaces provide habitat, sequester carbon and reduce air pollution, wetlands filter contaminants. Linked open space can accommodate trails, which are part of alternative transportation routes that aid in reducing greenhouse gases, by promoting cycling and walking.

Open space lands address public health by providing accessible and low-cost public recreation opportunities to all, regardless of income, age or ethnicity. Active recreation facilities have beneficial effects on reducing obesity and illness in youth and adults; and common areas accessible to all improve equity for the community. Open spaces support community gardens, where food production increases access to fresh local produce for farm to table connections.

Open spaces are important environmental sustainability elements considerations for the City. Natural systems managed and even expanded as open space, provide active public stewardship opportunities for aquatic and terrestrial biodiversity. This significantly contributes to watershed health and implements environmental, ecological and conservation policies.



Open space offers economic benefits as well. City investments in parks, ballfields, sport courts recreation equipment, benches, picnic tables and landscaping make the City's private lands more desirable for investment, help attract and expand local businesses, and can increase tourism.

In many cases, Open space can provide multiple benefits—, such as Agricultural lands in the Arcata Bottom and forested lands on the western slopes of Fickle Hill are important for their natural and resource production capabilities. These lands also retain natural drainage systems, sustain natural habitats, enhance the community's viewshed and provide an aesthetic resource. Designating these and other open spaces in the General Plan documents their value to the community. It also allows City decision-makers to identify open space as an

important resource when considering proposed changes in community form.

Within the city and surrounding Planning Area, natural resource lands that are part of ~~the a~~ broader open space system include: ocean beach; sand dunes; back-dune woodlands; Arcata Bay; Mad River; Mad River Slough; and buffer ~~corridors strips~~ along the Mad River, and Jacoby, Jolly Giant, Janes, Sunset, Grotzman, Beith, and Campbell Creeks.

The Open Space Element is closely linked with ~~the General Plan~~ Resource Conservation and Management, Public Safety, Health, and Parks and Recreation Elements ~~of the General Plan~~. These linked elements contain policies for the protection, management, enjoyment, and access to and appropriate use of identified open space areas. The Open Space Element identifies natural and productive resource areas, parks, coastal access, outdoor recreation, and hazard areas that should be considered for their open space values.

Guiding Principles and Goals.

- A. Protect open space lands with native biotic resources as a natural legacy for future generations.
- B. Protect and manage public trust lands to sustain plant and animal species and ecosystem health.
- C. Recognize that the value of natural resources lands of all sizes and shapes are significantly enhanced when linked together in an open space system.
- D. Designate as open space, resource lands capable of producing agricultural, forest, mineral, and aquaculture products; and manage those lands for sustained production as well as habitat, hydrological, mineral, recreational, and aesthetic values.
- E. Preserve sufficient lands, for both active and passive recreational activities, and coastal access to serve the present and future needs of the community.
- F. Protect lands that, due to instability, flooding, high fire hazards, or seismic risks, pose potential ~~risk to~~ human health and well-being risks.
- G. Provide additional entryways to the Community Forest to promote greater accessibility from Arcata's adjacent neighborhoods.
- H. Recognize open space as an important feature that impacts individual and community health. Manage open space access equitability and with accessibility considerations to ensure underserved areas are included.

4.2 POLICIES

-The Open Space Element includes the following policies:

- OS-1 Overall Open Space System
- OS-2 Natural Resource Protection and Enhancement
- OS-3 Open Space for Managed Production of Resources
- OS-4 Open Space for Outdoor Recreation/Coastal Access
- OS-5 Open Space in Health and Safety Hazard Areas

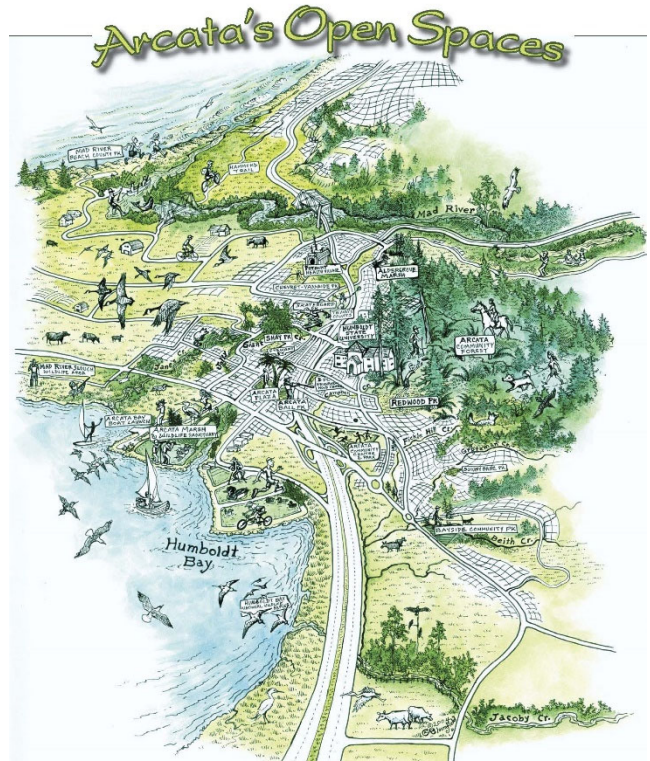
POLICY OS-1 OVERALL OPEN SPACE SYSTEM

Objective. Designate, maintain, and enhance the quality, and increase the amount of permanently protected open space in the Arcata Planning Area, including: natural resource areas; resource production areas; outdoor recreation areas; and areas subject to health and safety hazards. These areas are to be protected, linked together ~~in a network~~ wherever practical for habitat connectivity~~accessibility~~, managed for resource production, and maintained for enjoyment by City residents and visitors.

- OS-1a **Designation of open space lands with native biotic resources and ecosystems.** The native biotic resources of the forested western slopes of Fickle Hill, river and creek riparian zones, the Arcata and Aldergrove Marshes, and Arcata Bay tidelands and sloughs are unique ecosystems that have important habitat values in addition to their other open space values. These areas as designated on Map OS-a shall be protected as open space for their resource values.
- OS-1b **Open Space Plan Map.** The areas designated as open space are shown on Figure OS-a. Generally, these lands ~~are have designated as A-E, NR, or PF Agriculture-Exclusive (A-E), Natural Resource (NR), or Public Facility (PF) on the land use map designations.~~ Other lands, where identified open space resources have been preserved through easements or other means, are also subject to this element's policies.
- OS-1c **Relationship to Resource Conservation and Management and Public Safety Elements.** ~~This element identifies~~ hazard areas ~~that~~ shall be maintained as open space for the benefit of the community. The policies of this element and Public Safety Element policies ~~found in the Public Safety Element~~ provide common direction for ~~the hazard area~~ designation and avoidance ~~of hazard areas~~. The natural open space features of these areas, such as vegetation, shall be retained, except where they contribute to instability or increased hazards.

OS-1d **Linkages between open space areas.** ~~Linkage of open~~ Open space ~~lands~~ links, especially ~~along in~~ biological corridors and greenways, is important for ~~animal~~ wildlife migration, non-motorized vehicle transportation, and community recreation, and shall be encouraged. Trails along levees or adjacent to railroad tracks and street rights-of-way can serve as links to parks, open space, and natural areas. Easements shall also be considered as a lower cost alternative to preserving links between open space. The City shall preserve or create corridors in underserved areas, such as the Valley West neighborhood, to prepare for future linkages.

OS-1e **Appropriate uses and development limitations within open space lands.** Certain open space areas contain wetlands and other critical habitat, and must be preserved in a natural condition and enhanced. Other areas can accommodate managed activities such as mining and timber harvesting, subject to sustainable yield policies RC-6 and RC-8 in the Resource Conservation & Management Element, while other areas shall be designated for interpretive and recreational use. Each designated open space area ~~of the~~ City shall be evaluated by the appropriate City advisory board (e.g., Creeks & Wetlands & Creeks, Forest Management Committee) to determine ~~the~~ resources present, ~~the~~ acceptable use levels ~~of use~~, and appropriate preservation ~~management~~. ~~The management of, and development in, open~~ Open space areas management and use are ~~subject to~~ guided by applicable ~~policies of the~~ Resource Conservation and Management and Land Use Elements policies.



OS-1f **Designation of lands with scenic, aesthetic, historic, and cultural value.** The City has scenic routes, including ~~State Routes~~ Highway 101 and State Route 255 - Samoa Boulevard; vistas, including the forested slopes of Fickle Hill, ~~and~~ the Arcata Bottoms; and areas of historic and cultural value, such as the Plaza. The open and natural characteristics of these areas shall be maintained. Policies for retaining scenic vistas and landscape features are included in the Community Design and Arcata Gateway Area Plan Elements ~~of the~~ General Plan.

OS-1g **Public and private ownership and management of open space.** Open space resource areas are owned and managed by the City, state agencies, tribal governments, land trusts, corporations, and private individuals. The City shall set the standard for

responsible resource land stewardship through its management of the Community Forest, marshes, parks, and other resource lands, and encourage other public and private entities, entrusted with the ownership and management of similar resource areas, to consider natural resource values to the community in all long-term use decisions.

- OS-1h **Greenbelts.** “Greenbelt” is a general term that refers to natural, undeveloped, and/or agricultural lands that surround urban areas. Preserving greenbelts of agricultural and other open space lands is an effective method of defining urban development limits. The City shall encourage the County to preserve agricultural designations in the City’s Planning Area. The City also supports greenbelt preservation through land and conservation easement acquisition.

These measures will help preserve visual and associative links to nature, and reinforce the distinction between the City and adjacent communities.

- OS-1i Additional open space area identification and Acquisition of open space areas. There are ~~several privately held~~ land parcels, including forested property on the west slopes of Fickle Hill, and lands around Arcata Bay which would contribute significantly to the City’s and regional open space systems. The City shall continue to pursue, or coordinate with land trusts, for acquisition ~~of these parcels~~, from willing sellers, ~~for their~~ to expand the amount of open space and also for conservation easements to preserve open space values of private parcels values. Joint land acquisition funding ~~for land acquisition~~ will be coordinated with County, regional and state agencies. Community Forest acquisitions and acquisitions to benefit sea level rise adaptation will continue to be a priority. Other acquisition priorities shall be based on capital, operation and maintenance costs, accessibility, resource preservation, ability to complete or enhance an existing open space linkage system and unique environmental features. Lands acquired outside of the City Limits should be annexed pursuant to the Growth Management Element.

POLICY OS-2 NATURAL RESOURCES PROTECTION AND ENHANCEMENT

Objective. Designate, maintain, and enhance natural resource areas, including sensitive habitat areas, necessary to sustain plant and animal life and native biological diversity.

- OS-2a **Open space plan map designations for natural resource protection.** Publicly held lands containing creeks, sloughs, wetlands, other open water, ~~marsh~~, sensitive habitat, forests, and other important natural resources are designated on the Open Space Map (OS-a). While this map is comprehensive, it is not exhaustive. Additional natural resource lands suitable for protections may be identified through site specific studies and or develop in new areas due to changing environmental conditions.

- OS-2b **Development limitations and management for maintenance of biotic resources and diversity, including aquatic resources and sensitive habitats.** Creeks, ~~marshes,~~ and wetlands are significant components of Arcata's natural open space system. The City shall restore and maintain this system for the benefit of tribal members, residents, visitors, fish, and wildlife.

The Arcata Bay and tidelands represent an important natural edge and open space feature of the City. Buildings, landform alterations, or access routes in this area shall be of a design and scale that preserves open space and natural characteristics and maintains public views to the Bay.

Local creeks which flow openly through the developed portion of the community shall have biological corridors and greenways established, and shall be maintained as visual assets to any developments which adjoin them. Daylighting of channelized or culverted creeks is encouraged where feasible, with a special emphasis on Jolly Giant Creek.

The natural features of the Mad River corridor, Arcata's creeks and adjacent areas, marshes, and other wetland areas, shall be retained.

Unique vegetation and wildlife areas shall remain in a natural condition. Such areas include sand dunes and backdune woodlands, eel grass areas, salt marshes, and special habitats (tern and osprey nesting areas, cormorant, night heron and egret rookeries, and harbor seal resting areas and egret roost). The policies of this element shall also call for protection for habitat of species that become threatened in the future.

POLICY OS-3 OPEN SPACE FOR MANAGED PRODUCTION OF RESOURCES

Objective. Designate and conserve resource areas, including forest and agricultural lands, fisheries and aquaculture, groundwater, and mineral resources, for their sustainable, long-term productive capabilities.

- OS-3a **Designations for forest, agriculture, fisheries, aquaculture, groundwater, and mineral resource uses and management.** All publicly held lands, and some privately held lands adjacent to the Community Forest and east of Aldergrove Industrial Park, actively managed for production of forest, fisheries, aquaculture and mineral resources are designated as Natural Resource [NR] on the



General Plan Land Use Element map. Productive agricultural lands are designated on the Open Space Map.

- OS-3b **Development limitations and management for productive resource areas.** The policies of the Resource Conservation and Management Element shall be followed for the development and management of productive resource lands. This includes policies for: Natural Biological Diversity; Streams Management; Wetlands Management; Baylands and Tidelands; Agricultural and Forest Resources; and Water, Energy, Soils and Mineral Resources. The City shall require that [open space resource management](#) ~~of open space resource~~ be consistent with these and other applicable General Plan policies.

POLICY OS-4 OPEN SPACE FOR OUTDOOR RECREATION AND COASTAL ACCESS

Objective. Designate and secure public access to a sufficient supply of land and water areas with recreation resource value, including parks, forests, coastal areas, baylands, and stream corridors, to meet the outdoor recreation needs of Arcata residents and visitors.

- OS-4a **Designations for park lands and outdoor recreation areas.** All publicly held park lands and outdoor recreation areas are designated as Public Facility [PF] on the General Plan Land Use Element map. The natural resources present on these lands are also subject to the applicable policies of the Resource Conservation & Management Element. Recreation areas are mapped on Figure OS-a.



- OS-4b **Coastal access policy.** The City shall maintain coastal access corridors to Arcata Bay and other public use areas and public trust lands within the coastal zone.

Coastal access routes include:

1. Access from Samoa Boulevard to Arcata Bay via South "I" and "G" Streets.
2. Access to Mad River Beach via Mad River Road.
3. Access to Manila Dunes via Samoa Boulevard.

- OS-4c **Relationship to the Parks and Recreation Element.** This element contains policies for management of open space lands designated for outdoor recreation. The Parks and Recreation Element contains goals and policy direction for: providing a range of recreation opportunities; sharing facilities; park and recreation program efficiency; environmental compatibility; and user safety.

POLICY OS-5 OPEN SPACE IN HEALTH AND SAFETY HAZARD AREAS

Objective. Designate health and safety hazard areas such as seismic fault and liquefaction zones, unstable soils or slopes, floodplains, areas susceptible to wildland fire, and watershed/reservoir safety zones for risks such as dam and tsunami inundation. Provide appropriate protections, or restrictions, to minimize unnecessary exposure of people and property to health and safety hazards.

OS-5a **Designation of open space for public safety.** Designated open space for public safety is shown on Figure OS-a. Setbacks for seismic faults and liquefaction zones, unstable soils or steep slopes, mapped Flood Hazard Zone A, areas susceptible to wildland fire, and watershed/reservoir safety zones, shall be established as part of the development review process. Where severe safety considerations exist (e.g., within the Alquist-Priolo Zone), open space easements shall be granted to the City to protect people and property from health and safety hazards.

Open space ~~land~~ areas, with slopes 15% or greater shall retain their natural landform features; excavation shall be restricted, according to the City's adopted grading ordinance, and removal of vegetation shall be limited to selected thinning of timber stands and removal of hazard trees.

Open Space areas that are flood-prone may be used for agricultural and recreational purposes but shall be kept free from urban development. A flood plain overlay zone shall be applied to all Natural Resource [NR] and Agricultural [AE] areas subject to inundation according to the Flood Insurance Rate Map (Flood Hazard Boundary Map) developed by the Federal Emergency Management Agency (FEMA) or the Federal Insurance Administration.

OS-5b **Development limitations and management for health and safety hazard areas.** ~~The policies of the~~ Public Safety Element policies shall be followed for all development activity in areas with known or suspected safety hazards. In particular, seismic hazards, other geologic hazards, and flood hazards policy topics are applicable. Areas of high fire risk shall also be subject to defensible space, non-flammable materials and suppression measures as applicable.

4.3 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURES	RESPONSIBLE PARTY	TIME FRAME
OS -1	<p>Open Space Preservation Preserve existing and acquire additional open space lands identified in this element through the following measures:</p> <ul style="list-style-type: none"> • Purchase of open space lands in fee. • Secure easements by negotiated agreement. 	<p><u>Environmental Services/</u> Community Development Dept./</p>	<p>Year <u>±Ongoing</u></p>

	<ul style="list-style-type: none"> Maintain open space designations on City and County planning maps. An appointed Open Space Advisory Committee will be responsible for implementing the Open Space Element and making relevant recommendations to City Council, including possible funding through bond measure approval. The advisory body is to participate in the next County General Plan update and promote incorporation of the City's open space goals, policies, and implementation measures in the County community plans covering the Arcata Planning Area. 	Planning Commission / <u>City Council</u>	
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RESOURCE CONSERVATION & MANAGEMENT ELEMENT

4.4 INTRODUCTION

Overview of Arcata's Natural Resources. Collectively, Arcata's natural resources constitute are a significant component of the community. The forested hillsides, including the community Arcata Community Forest and Jacoby Creek forests, the Arcata Bottom, baylands, tidelands, creeks, sloughs and wetlands are features of the defining natural ecosystem features. ~~which is~~ They are as much a part of the community as homes, businesses, and schools. Goals and policies for conserving, enhancing, and managing the City's natural systems and features are critical ingredients of the General Plan.

Arcatans have demonstrated that natural resource conservation and management are civic responsibilities, ~~which that~~ can be met by emphasizing resource enhancement rather than resource depletion. By taking an ecosystem management approach, the City can evaluate natural resource interrelationships, and plan to maintain regional biodiversity when making resource conservation and management decisions.

Biodiversity - "The variety of organisms considered at all levels, from genetic variants belonging to the same species through arrays of species to arrays of genera, families, and still higher taxonomic levels; includes the variety of ecosystems, which comprise both the communities of organisms within particular habitats, and the physical conditions under which they live."

Edward O. Wilson
The Diversity of Life, 1992

Overview of Arcata's watercourses, wetlands, baylands and tidelands. Arcata's nine named creeks and their associated sloughs provide: flood control, freshwater habitat, riparian habitat, marine habitat, scenic enjoyment, water quality, educational opportunities, public safety, fish and wildlife habitat (e.g., fish spawning and migration, wildlife nesting, roosting, and foraging areas), scenic enjoyment, open space, recreation, educational opportunities, marine habitat, and groundwater recharge. These creeks also have tributaries with similar features and functions. Arcata's creeks and sloughs, including areas with tidal action, are illustrated in Figure RC-a, on the following page.

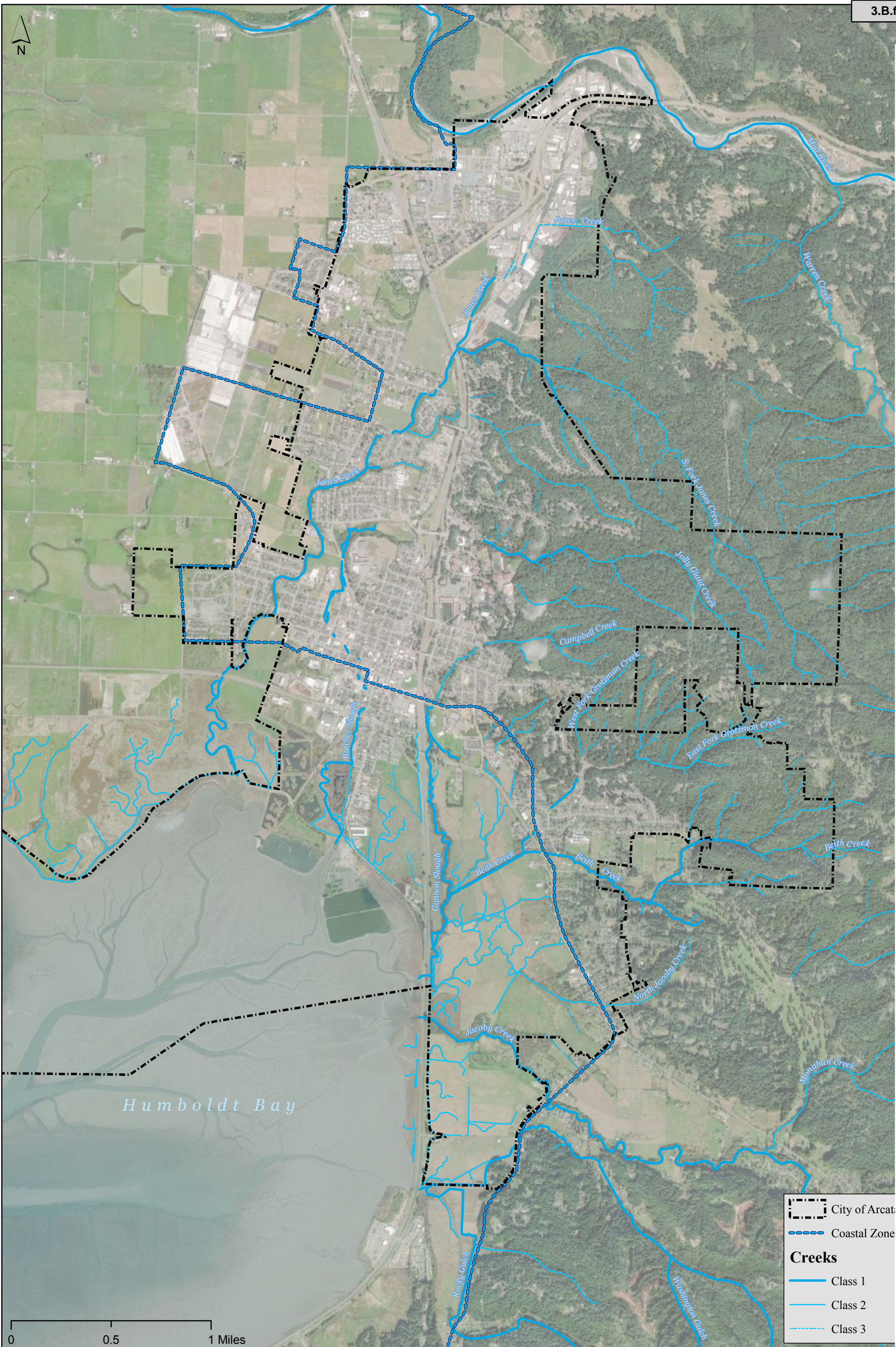
The City ~~s has an~~ adopted a Creeks and Wetland Management Plan (CMP) which in 1991 that which contains policies for: creek zone and flood hazard management; erosion and sedimentation control; vegetation and wildlife protection; water quality protection; recreation; and public awareness. ~~The~~ CMP-Creeks and Wetland Management Plan was adopted to address land uses that have significantly altered



Arcata's creeks from their original condition, and to provide guidance for management of creeks that flow through Arcata in order to provide the fullest realization of the creeks' beneficial uses. This guidance applies both to new and modified development along creeks, as well as existing development and practices in the creek zones, and includes both policies and implementation measures which allow for measurement of progress towards identified goals.

Figure RC-a

~~(Figure update in progress)~~

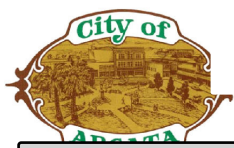


Attachment: F. Ch 4 Environment (4214 : GP #XVII FBC & GP wrap-up)

Figure X.

City of Arcata Protected Watercourses

Draft Figure RC-a
Created by Planwest Partners
Map Date: 6/24/2022



~~These~~ Those past alterations have resulted from the present use of stream courses being used as for stormwater conveyance drains, and past land use practices that which produced large amounts of sediment, that contributed contributing to creek degradation. Alterations also included installation of structures such as tide gates that, which prevent or severely limit access for anadromous fish passage to all creeks except for but Jacoby, Janes and Jolly Giant creeks.

The Resource Conservation & Management Element contains overall goals and policies for creek management, which are supported by the Creeks Management Plan policies and implementation measures. - The Creeks Management Plan provides policy direction for new and modified development along creeks, and for existing activities in creek zones, in order to fully realize the creek's beneficial uses.

~~-The City has implemented many dozens of stream restoration projects in the last thirty years to address habitat degradation and fish passage limitations, although many restoration opportunities remain. Additionally, the implementation of the City's Municipal Separate Storm System (MS4) Stormwater Permit has helped to reduce With the exception of Except for Jacoby Creek, the riparian forests have been completely removed from at least half of each stream channel. Ppollutants from a variety of sources, including petroleum products from urban runoff and suspended sediments from soil erosion, from entering the City's creeks and riparian areas.} have degraded the appearance and the biological integrity of the creeks.~~

Jolly Giant Creek is one of Arcata's urban creeks. The headwaters of Jolly Giant Creek start is in the Arcata Community Forest, and the creek flows through the urban downtown before becoming tidally influenced near Samoa Boulevard where its name changes to Butcher Slough. Butcher Slough flows into Humboldt Bay at the Arcata Marsh. Jolly Giant Creek was dammed in the 1930s to provide drinking water to the City. In 1964, the City transitioned to using the Mad River for its source of drinking water, but the dam remained in place. As the City was became urbanized, sections of Jolly Giant Creek were realigned and routed through culverts and paved over for road crossings and other forms of urban development. Restoration efforts in Jolly Giant Creek started in the 1980s and continue to the current day, with the goals of enhancing fish and wildlife habitat and reducing flooding by daylighting culverted sections of the creek, realigning sections of the creek channel, removing barriers to fish passage migration, planting native riparian and wetland vegetation, and establishing City protection zones/easements along the creek corridor.

~~Through out~~ Throughout the Jolly Giant Creek watershed, the City has conducted invasive plant removal projects throughout the Jolly Giant Creek watershed, (including reed canary grass and spartina removal) projects, extensive riparian enhancements, fish passage and culvert improvements, and tide gate replacements.

Similar to Jolly Giant Creek, Janes Creek's headwaters is start in the Arcata Community Forest and adjacent to the Samuels Conservation Easement, and the creek flows through the urban neighborhoods of Arcata, west of Jolly Giant Creek, before becoming tidally influenced near Samoa Boulevard where its name changes to McDaniel Slough. McDaniel Slough flows into

Humboldt Bay through the Arcata Marsh. The City has undertaken many restoration and enhancement projects in the Janes Creek watershed, including watershed-wide reed canary grass removal, installation of in-stream structures for fish habitat restoration, planting native riparian and wetland plants, channel realignment, culvert replacement to improve fish passage, and tide gate removal. The McDaniel Slough Project has been the City's largest project in the Janes Creek watershed; this project ~~which~~ opened tide gates on McDaniel Slough in 2013, ~~restoring and restored~~ tidal action to 212 acres of former tidelands. In 2021, the City completed a fish passage improvement project along Janes Creek under Alliance Road. ...

~~The Resource Conservation and Management Element contains overall goals and policies for creek management, which are supported by CMP Creek and Wetland Management Plan policies and implementation measures. The CMP Creek and Wetland Management Plan provides policy direction for new and modified development along creeks, and for existing activities in creek zones, in order to fully realize the creek's beneficial uses.~~

The Mad River's westernmost reach ~~of the Mad River~~ forms the northern boundary of Arcata's Planning Area. The river originates at the northern edge of the Yolla-Bolly wilderness area, in Trinity County, approximately 100 miles southeast of its outlet to the Pacific Ocean. The river's ~~its~~ associated riparian corridor forms the northern portion of the City's perimeter greenbelt, and is a natural buffer between Arcata and the community of McKinleyville, to the north. The Mad River also serves as ~~is~~ the source of drinking water for the City of Arcata.

Wetlands provide flood protection, groundwater recharge, water quality treatment, food production, and fish and wildlife habitat, which are valued by the community. Wetlands are highly productive, complex ecosystems, seasonally or permanently saturated with water, and support specially adapted vegetation. Wetlands are often found in transitional zones, or ecotones, between uplands and open water habitats. Arcata's marshes may be among the best examples of local wetlands and are reflective of pre-European conditions.

The Arcata Marsh and Wildlife Sanctuary encompasses 317 acres of diverse marshland, and is also home to the City of Arcata's innovative wastewater treatment facility. The ~~S~~sanctuary includes freshwater marshes, salt marsh, tidal sloughs, grassy uplands, mudflats, brackish marsh, approximately five miles of walking and biking paths and an Interpretive Center. By integrating conventional wastewater treatment with the natural processes of constructed wetlands, Arcata has succeeded in turning wastewater into a resource. –A portion of the Arcata Marsh and Wildlife Sanctuary is shown in the photo on the following page. The City implemented the McDaniel Slough Restoration Project that removed tide gates, deepened historic slough channels, and removed failing or obsolete levees to restore the former tidelands between Humboldt Bay and Samoa Boulevard.

The Aldergrove Mmarsh was a log pond that has ~~now~~ since been reconstructed and significantly enhanced as a ten-~~acre~~ freshwater marsh, as part of the Aldergrove Industrial Park development. In 2021, the City removed invasive aquatic vegetation, including reed canary grass, in approximately 0.6 acres to restore the open water habitat and restore biodiversity in

the marsh. ~~A plan view portion of the 170 acre Arcata Marsh and Wildlife Sanctuary (AMWS) is shown in the photo on the following page.~~

Arcata Bay is part of Humboldt Bay, which is fourteen miles in length, from north to south; covers more than 17,000 acres; and is the second largest coastal estuary in California. A significant portion of the northerly waters of Arcata Bay are either owned or held in trust by the City, are within its City limit, and represent a significant natural, visual, aquacultural, tribal, and recreational resource for the community. The tidelands adjacent to the Bay include salt marshes and sloughs, excepted where they had been diked for use /reclaimed and used as pastureland.



Overview of agricultural resources. Agricultural lands represent an important natural resource within the City. Arcata's agricultural lands are currently used primarily for flowers, silage and hay production, food production, and livestock grazing. The Ferndale, Russ, and Loleta soil series are Arcata's most productive agricultural soils.

The agricultural lands in and around Arcata produce crops of raspberries, strawberries, lilies, daffodils, potatoes, corn, artichokes, hay (forage for cattle), and a number of other shallow rooted crops. There is community support for the continuation of dairy, beef, vegetable, fodder, and flower production in the City and the Planning Area, and recognition that protection of agricultural values, as well as open space and recreational values, is important.

Arcata's agricultural lands include ~~farmed diked former tidelands~~ ~~wetlands~~. ~~Most of the farmed wetland areas around Humboldt Bay are former tidelands,~~ once owned by the State, ~~that~~ ~~which~~ private parties acquired ~~from the State~~ under the California Swamp and Overflowed Lands Act. These lands, ~~now used primarily for grazing,~~ were diked ~~and drained~~ ~~(reclaimed)~~ ~~for agricultural use between about 1890 and 1910.~~ ~~around the turn of the century.~~



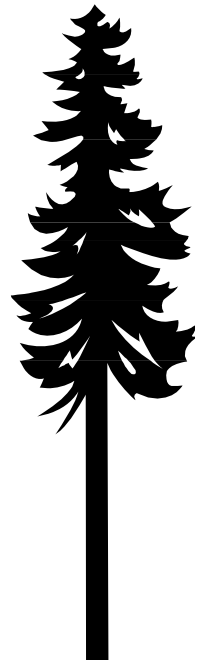
These ~~diked former tidelands~~ ~~areas~~ are below ten feet in elevation, have relatively impermeable soils, and retain run-off for long periods of time. ~~While the State conveyed the fee title interest in these former tidelands, they are still subject to an easement under the Public Trust Doctrine, for the benefit and enjoyment of the people of this state. Much of this Public Trust land bordering Arcata Bay can provide important wildlife habitat and recreational opportunities.~~

These ~~farmed wetlands~~ ~~areas~~ are no longer salt ~~and~~ brackish wetlands, but now function as freshwater wetlands, with meandering year-round creek and slough channels. Arcata's diked former tideland areas typically include the less productive types of Loleta and Bayside soils and are generally used for pasture.

Soil classifications are based on the most recent surveys. In the event that an updated soil survey is completed in the future, the classifications and associated mapping shall be changed accordingly.

Overview of forest resources. The eastern portion of Arcata is located on forested slopes of Fickle Hill Ridge. These ~~se~~ slopes contain mostly second-growth conifer stands. These forested lands are both publicly- and privately-held. The City of Arcata owns ~~three separate tracts of forestland that comprise approximately 2,445 acres~~ ~~two separate tracts of forest land that comprise approximately 1,125 acres.~~ ~~Together,~~ the publicly-owned Arcata Community Forest, ~~which that~~ includes the ~~and~~ Jacoby Creek ~~Unit, Sunny Brae Unit, and the Arcata Forest Unit,~~ ~~forests~~ constitutes a significant ecological, recreational, economic, and educational resource for the citizens of Arcata and the surrounding region.

The City ~~adopted the~~ ~~updated the~~ ~~Arcata Community Forest Management Plan 2020 Update in 2022.~~ The 2022 Forest Management Plan replaces the 1994 Arcata Community Forest & Jacoby Creek Forest Management Plan, ~~by~~ incorporating, updating, and revising much of the 1994 document. The 2022 Forest Management Plan update reflects updated information, recent research, and State Forest Practice Rule changes, thus providing management direction with a higher degree of



environmental protection. The goal of the 2022 Forest Management Plan is to provide a management flexible and adaptive management program that provides for protection and use of forest resources,; addresses local and regional issues and concerns,; and fulfills legislative requirements. The updated plan is fundamentally designed to restore and transitionmove a relatively even-aged forest to a more structurally complex forest. The long-termultimate goal is to develop late-seral or old-growth forest characteristics in the Arcata Community Forest. Tangible outcomes of this forest management approach include:

- Fostering and accelerating the transition to an old-growth forest stand structure through selective thinning that promotes light in the forest understory and stimulates recruitment of new tree age classes.
- Obtaining support from the community for management that includes timber harvests in close proximity to residential areas and recreational use areas.
- Protecting and enhancing biological diversity and rare species, including maintenance of northern spotted owl (*Strix occidentalis caurina*) nesting pairs.
- Contributing to the local economy by providing a source of wood products and jobs in the woods.
- Providing an opportunity for residents to be involved in forest planning, as well as on-the-ground activities, with volunteer work days that amount to at least 5,000 volunteer hours per year.
- Providing opportunities for non-motorized recreation and contributing to the local tourism economy.
- Testing different silvicultural practices and “no-cut” watercourse protection zones to protect and enhance water quality, as well as providing a network of connectivity between of older seral forest habitat for species that require those conditions.
- Maintaining a climate resilient landscape within the city forestlands.

The 2022 Forest Management Plan provides direction and guidance for the managed uses of forest resources and non-timber resources with an emphasis on fish and wildlife habitat, recreation, watershed protection, demonstration and education, research, and timber management.~~to provide guidance for integrated multi-resource management activities and to establish standards and guidelines for the Arcata Community Forest and Jacoby Creek Forest.~~ The Resource Conservation & Management Elements contains overall goals and policies for forest management, which are derived from the Forest Management Plan. ~~The Forest Management Plan includes goals, policies, detailed management direction, monitoring and evaluation techniques for the City-owned forests.~~ ~~The forest management plan goals are listed below.~~

Forest Management Goals

The Arcata Community Forest Goals are:

- Maintain the health of the forest system, specifically, maintain the integrity of the watershed, wildlife, fisheries and plant resources, their relationships, and the process through which they interact with their environment.
- Produce marketable forest products and income to the City in perpetuity, balancing timber harvest and growth.
- Enhance carbon sequestration and climate resiliency
- Provide opportunities for education and research
- Foster productive relationships with adjacent landowners
- The Community Forest shall also be managed to provide forest recreational opportunities for the Community.

Wildlife Habitat Diversity and Resiliency Overview

The City applies certain practices from the CDFW's Natural Community Conservation Planning (NCCP) approach to biological diversity planning.¹ This includes a city-wide ecosystems approach for protecting native plants, animals, and their habitats, while allowing compatible and appropriate economic activity. One of the key conservation practices for maintaining natural habitat diversity is invasive exotic non-native species control. This includes control of Pampas grass (*Cortaderia jubata*), Himalayan blackberry (*Rubus discolor*), Scotch broom (*Cytisus scoparius*), blue gum eucalyptus (*Eucalyptus globulus*), English ivy (*Hedera helix*), English holly (*Ilex aquifolium*), reed canary grass (*Phalaris arundinacea*) and cotoneaster (*Cotoneaster franchetii*), which that displace native plant species. These non-native species reduce natural diversity, biological integrity, and aesthetics.

The City's wildlife habitat planning accounts for current habitats, wildlife health, projected habitat changes due to climate change, wildlife conservation, furthering responsible development, and addressing growing human population needs. The City recognizes the importance of habitat connectivity and potential habitat threats from development pressures, habitat fragmentation and edge effects. The Resource Conservation & Management Element's focus on wildlife is consistent with the State Wildlife Action Plan, California Essential Habitat Connectivity Project, Regional Advance Mitigation Plans, the Federal Migratory Bird Act, and conservation plans developed by state and regional entities.

Energy Resources Overview

The City of Arcata is part of the Redwood Coast Energy Authority (RCEA) a joint powers authority (JPA) formed in 2003 representing all seven of the County's cities, the Humboldt Bay Municipal Water District, and Humboldt County. As a JPA, RCEA is the regional energy authority, and is governed by a board of representatives from each jurisdiction. RCEA's mission statement is:

¹ <https://wildlife.ca.gov/Conservation/Planning/NCCP>

The Redwood Coast Energy Authority's purpose is to develop and implement sustainable energy initiatives that reduce energy demand, increase energy efficiency, and advance the use of clean, efficient, and renewable resources available in the region.

RCEA implements energy strategies through a Comprehensive Action Plan for Energy. This action plan is maintained by the RCEA Board. The City of Arcata also implements energy conservation through policies and implementation measures. The Energy component of the Conservation Element promotes self-sufficiency, independence, and local energy management and supports diversity and creativity in energy resource development, conservation, and efficiency. This can reduce energy demands, stimulate the economy, and help meet greenhouse gas emission reduction targets. [Summarized and adapted from Humboldt County General Plan, Chapter 12.]

RCEA adopted the Humboldt County Comprehensive Action Plan for Energy (CAPE) in 2012, which is RCEA's primary guiding document. Expanding on the strategies outlined in the CAPE, RCEA initiated RePower Humboldt, a community-wide effort to define a vision and Strategic Plan for achieving energy independence and energy security in Humboldt County.

The City adopted a Community Greenhouse Gas Reduction Plan in 2006, and City Council priorities continue to emphasize energy conservation for City facilities, shifting towards a Green Fleet, and promoting alternative transportation options.

Climate Action Planning Overview

The City has participated in preparing the Humboldt Regional Climate Action Plan (CAP), a collaborative County and Cities regional approach to address climate change challenges. This regional approach enables improved county-wide coordination to maximize greenhouse gas (GHG) emissions reduction measures' effectiveness and improve the potential for future grant funding. The primary CAP goal is to reduce greenhouse gas emissions from local sources. There is scientific consensus that significant human-caused greenhouse gas emissions reductions are needed by the mid-21st century to prevent the most catastrophic effects of climate change.

Guiding Principles and Goals.

- A. Protect, maintain and enhance natural ecosystem processes and functions in the region, in order to maintain their natural ecological diversity and tribal connection.
- B. Restore and maintain the physical and biological integrity of Arcata's streams.
- C. ~~To protect~~ protect, restore, enhance, and maintain riparian habitat on those lands subject to wetlands and streamside protection zone.
- D. Recognize and protect wetlands as highly productive complex ecosystems that provide vital habitat and pollution filtration functions~~cleansing systems.~~
- E. Restore and maintain the physical and biological integrity of publicly owned former tidelands (~~farmed wetlands~~) subject to the Public Trust easement, ~~to~~ resulting in a diversity of tidal, freshwater, and riparian habitats.

- F. Protect and enhance prime agricultural lands for their food production, resource values, and aesthetic values.
- G. Manage a sustainable production of both public and private forest products ~~on both public and private timberlands~~.
- H. Manage water resources at the watershed level, to maintain high groundwater and surface water quality.
- I. Manage surface water and groundwater resources to provide water quality and quantity adequate to support natural ecosystem processes and functions.
- J. Conserve soil resources as the foundation of resource production, and minimize erosion and other soil depleting processes.
- K. Promote energy conservation, and development and use of alternative, non-polluting, renewable energy sources for community power, with an all-electric emphasis, in both the public and private sectors.
- L. Maintain an active relationship with adjacent communities, tribal entities, and government agencies to encourage cooperative management of natural resources and ecosystems in Arcata's Planning Area.
- M. Conserve natural resources through reduced materials consumption and recycling (see integrated waste management policies in the Public Facilities & Infrastructure Element).
- N. ~~Establish an Agricultural Advisory Committee~~ Support the Wetlands & Creeks Committee and Forest Management Committee in providing recommendations to the City Council to continue to enhance the City's natural resources and -to help maintain a compatible relationship between agricultural and non-agricultural activities and uses.

4.5 POLICIES

POLICY RC-1 NATURAL BIOLOGICAL DIVERSITY/ ECOSYSTEM FUNCTION

Objective. Set an overarching policy that emphasizes the ~~overall~~ value of biological diversity, and ~~the fact that all the optimal function of~~ natural resources ~~are optimized when they function~~ as part of a healthy ecosystem.

The following policies are included in the Resource Conservation and Management Element:

- RC-1 Natural Biological Diversity/Ecosystem Function
- RC-2 Streams Conservation & Management
- RC-3 Wetlands Management
- RC-4 Open Waters of Arcata Bay and Tidelands
- RC-5 Agricultural Resources Management
- RC-6 Forest Resources Management
- RC-7 Water ~~R~~esources Management
- RC-8 Energy Resources Management
- RC-9 Soils and Mineral Resources Management

RC-1a **Maintain ~~B~~iological and ecological integrity.** Maintaining ecological balance, system function, biological integrity, and natural diversity is the primary focus of the Resource Conservation ~~&and~~ Management Element. Protecting the ecological functions of natural habitats, and natural drainage and infiltration processes, will enhance natural ecosystems in the Planning Area. Ecological ~~system~~-functions ~~elements~~ and processes are maintained through the following measures:

1. The structure and composition of ecological systems within the City shall contain the same native plant and animal species, in the same relative abundances and proportions, ~~that which~~ are found in the least-disturbed natural ecosystems in the Planning Area.
2. The ecological functions performed by ecological systems in the City shall resemble the functions of the least-disturbed natural ecosystems in the Planning Area.
3. Ecological systems and natural processes ~~are~~shall not be disrupted by ~~exotic~~non-native organisms to a significant degree.
4. Ecological systems and natural processes ~~are~~shall not ~~to~~ be disrupted by land use activities to a significant degree (e.g., a culvert or other drainage device that restricts flow or blocks fish passage).

An "adaptive management" approach shall be ~~utilized~~ used to maintain ecological and biological integrity, including monitoring the status of ecological systems in the City and adjusting the City's implementation of this Plan, in order to more closely approximate the conditions provided in the Planning Area's least-disturbed natural ecosystems.

RC-1b **Non-native plant ~~and animal~~ species.** Some non-native plant species, such as pampas grass (*Cortaderia jubata*), ~~H~~Himalayan blackberry (*Rubus discolor*), Scotch broom (*Cytisus scoparius*), blue gum eucalyptus (*Eucalyptus globulus*), English ivy (*Hedera helix*), English holly (*Ilex aquifolium*), ~~and~~ cotoneaster (*Cotoneaster franchetii*), ~~and~~ reed canary grass (*Phalaris arundinacea*) are invasive ~~exotics that can and do and~~ displace native species. The presence of these non-native species reduces the area's natural biodiversity, biological integrity and aesthetics. Only native plant species, or species demonstrated to be non-invasive, shall be used in public landscapes, ~~and~~ native plant species are to shall be strongly encouraged in private landscapes. The City shall provide public information ~~that explains why on~~ invasive plant species, ~~are a problem. The City shall also~~ maintain a program that recommends effective but non-toxic eradication measures for invasive plant species, and eradicates non-native plant species on public lands where they are displacing native species.

RC-1c **~~Habitat value protection~~Protection of Environmentally Sensitive Habitat Areas.** Environmentally sensitive habitat areas (ESHA) shall be protected against any significant disruption of their habitat values, and only uses dependent on and compatible with maintaining those resources shall be allowed within ESHAs. Proposed development in areas adjacent to ESHAs shall be sited and designed to prevent impacts ~~which that~~ would significantly degrade such areas, and must be compatible with the continuance of such habitat areas.

RC-1d **~~Sensitive habitat definition~~Identification of Environmentally Sensitive Habitat Areas.** The City declares the following to be ESHAs within the Planning Area:

1. Rivers, creeks, sloughs, and associated riparian habitats: Mad River; Jacoby Creek; Beith Creek; Fickle Hill Creek; Grotzman Creek; Campbell Creek; Jolly Giant Creek; Janes Creek; Gannon Slough; Butcher Slough; and McDaniel Slough.
2. Wetlands, estuaries, and associated riparian habitats: Arcata Bay; Mad River Slough; Liscom Slough; Butcher Slough; the Aldergrove marshes and ponds; and the Arcata Marsh and Wildlife Sanctuary.
3. Other ~~unique~~important habitat areas: waterbird rookeries; shorebird concentration sites; habitat for all rare, threatened, or endangered species on federal or state lists; and vegetated dunes.
4. Public Trust lands such as ~~grazed or =~~ farmed wetlands (i.e., diked and drained /reclaimed former tidelands that are grazed).

RC-1e **Threshold of City review for sensitive habitat effects.** Development on parcels designated Natural Resource [NR] on the Land Use Plan Map, or within 250 feet of

such a designation, or development potentially affecting a sensitive habitat area, shall be required to be in conformance with applicable habitat protection policies of this Element. All proposed development plans, including grading and drainage plans, submitted as part of a planning entitlement application for these areas, shall show the precise locations of all sensitive habitat areas on the site plan.

- RC-1f **Sensitive habitat buffer requirements.** A setback shall be required separating all permitted development from adjacent sensitive habitat areas ~~shall be required~~. The purpose of such setbacks shall be to prevent any degradation of the ecological functions provided by the habitat area as a result of the development. The following shall apply to such setbacks:
1. The minimum width of setbacks ~~for~~ from development to streams and wetlands shall be as provided in policies RC-2 and RC-3, respectively.
 2. The minimum width of setbacks from development to all other types of sensitive habitat ~~setbacks~~ shall be 100 feet, unless the designated setback would eliminate all reasonable use of the property.
 3. A definition and map of sensitive habitat will be maintained by the City.

- RC-1g **Sensitive habitat information required in development application review.** Where there is a question regarding the boundary, buffer requirements, location, or current status of an ESHA identified pursuant to General Plan policies, the public or private applicant shall provide the City with the following:
1. Base map delineating topographic lines, adjacent roads, and location of dikes, levees, culverts, flood control channels, and tide gates, as applicable.
 2. Vegetation map, including identification of species that may indicate the existence or nonexistence of a sensitive environmental habitat area.
 3. Soils map delineating hydric and non-hydric soils.
 4. Census of animal species indicating the existence, or non-existence, of an environmentally sensitive habitat area.

This information shall be provided to the California Department of Fish and Wildlife (CDFW) Game, U.S. Fish and Wildlife Service, NOAA Fisheries (also known as the National Marine Fisheries Service), and other affected agencies for review and comment. Any comments and recommendations provided by ~~the Department~~ these state and federal agencies shall be immediately sent to the applicant for ~~his or her~~ their review response. The decision concerning the boundary, location, or current status of the environmentally sensitive habitat area in question shall be based on the substantial evidence in the record and supported by written findings.

- RC-1h **Habitat integration for ecological integrity and development of a protected habitat corridor system.** An ecological connection network plan for linking native habitats in the Planning Area, and all of the environmentally sensitive habitat areas identified in

this Plan, shall be prepared. The network shall incorporate all existing large areas (or "nodes") of habitat for fish and wildlife species (such as marshes and forests) and "linkages" or "corridors" of natural habitat (such as stream zones and sloughs) for migration and species movement. The plan will link large "nodes" of natural habitat together with the "linkage" connections as a functioning ecological network. Nodes and linkages shall include a "core" of natural ecosystem elements and shall provide a protected "buffer" along the outer margins of the core habitat ~~which that~~ shall function to protect the ecological ~~values~~ services in the "core" habitat.

- RC-1i **Use of biocides and other compounds with biological consequences.** Pesticides, herbicides and insecticides (biocides); hormones and antibiotics (growth promoters); and hydrocarbon-based compounds, used both commercially and individually, can accumulate to toxic levels in biological organisms, including humans. Certain of these substances, even at low levels, can affect reproductive health.

The City shall maintain and make available a current list of alternative, environmentally-safe products for controlling unwanted vegetation and pests, growing crops and enhancing production of animal products. The use of substances and compounds ~~which that~~ can accumulate to toxic levels is restricted by the City (Pesticide Ordinance), ~~and the City shall develop~~ a program for fostering the reduction of pesticides in private use ~~shall be developed and implemented~~.

POLICY RC-2 STREAMS CONSERVATION & MANAGEMENT

Objective. Enhance, maintain, and restore the biological integrity of entire ~~steamecourses~~ stream courses (headwaters to mouth), and their associated riparian habitats, as natural features in the City's landscape.

- RC-2a **Designation of protected streams.** The provisions of this policy shall apply to those streams shown on the Protected Watercourse Map (Figure RC-a). These watercourses and their associated riparian areas serve as habitat for fish and wildlife, provide space for the flow of stormwater runoff and flood waters, and furnish open space and recreational areas for city residents.
- RC-2b **Environmental Buffer Area (EBA).** A streamside protection area is hereby established along both sides of the streams identified on the City Watercourse Map. The purpose of the EBA is to remain in a natural state in order to protect streams' ecosystems and their associated riparian habitat areas. The EBA shall include:
1. In areas where existing development, as defined in the Land Use Zoning Code, is adjacent to the stream, the EBA shall be not less than 25 feet outward on both sides of the stream, measured from the top of bank.
 2. In all other locations within the City, the EBA shall be not less than 100 feet outward on both sides of the stream, measured from the top of bank.

3. In locations within the City having significant areas of riparian vegetation exceeding 100 feet in width measured from the top of bank, the EBA shall be expanded to encompass all of the riparian vegetation, except in no case shall the EBA exceed 250 feet in width from the top of bank on either side of the stream.

EBAs outside of the City shall follow the policies in the Humboldt County Framework Plan, regarding Streamside Management Areas.

RC-2c **Allowable uses and activities in Environmental Buffer Areas.** The following compatible land uses and activities may be permitted in EBAs, subject to all other policies in this Element, including those requiring avoidance of impacts and other mitigation requirements:

1. Outside the Coastal Zone:
 - a. agricultural operations compatible with maintenance of riparian resources;
 - b. fencing along property boundaries and along EBA setback boundaries to prevent bank erosion and degradation of natural riparian vegetation by livestock;
 - c. maintenance of existing roads, driveways, and structures;
 - d. construction of public road crossings;
 - e. forest management practices as permitted by the State of California or Arcata's Forest Management Plan;
 - f. construction and maintenance of ~~foot~~ trails for public access;
 - g. construction and maintenance of utility lines;
 - h. resource restoration projects;
 - i. emergency or preventive removal of sediment and vegetation for flood control purposes (only when authorized by the City of Arcata).
2. In the Coastal Zone:
 - a. all uses and activities listed in (1) above;
 - b. public coastal access improvements;
 - c. boat launching facilities.
3. If the provisions herein would result in any legal parcel, not on Public Trust lands, created prior to the date of this plan, being made unusable in its entirety for any purpose allowed by the land-use plan, exceptions to the foregoing may be made to allow a reasonable economic use of the parcel, subject to approval of a conditional use permit. Any land use, construction, grading, or removal of vegetation ~~which~~that is not listed above shall be prohibited.

RC-2d **The Wetland and Stream Protection Combining (:WSP) Zone.** The :WSP zone of the ~~Land Use and Development Zoning~~ Code shall be applied to all streamside protection areas. [The WSP zone should be a land use designation under the NR district, e.g., NR-WSP, ~~NR-AG, NR-TPZ.~~]

- RC-2e **Review and approval of projects affecting streamside protection areas.** Applications for development on any parcel ~~which~~that is located partially or wholly within an SPA shall be subject to the requirements of Policy RC-1 and RC-2.
- RC-2f **Conservation easement.** Dedication of a conservation easement, or equivalent deed restriction, encompassing the area within the EBA shall be required as a condition of approval of any discretionary planning permit, including design review, when any portion of the project site falls within an EBA. Such easements may be conveyed to the City of Arcata, or to another governmental agency ~~which~~that shall manage the easement to protect the EBA's functions, or to ~~a mutually agreeable~~an appropriate non-profit entity.
- RC-2g **Maintenance of streams as natural drainage systems.** Arcata's creeks carry a significant amount of the City's stormwater runoff. Drainage controls shall be enforced through implementation of the Drainage Master Plan, to protect water quality, and to minimize erosion, sedimentation and flood impacts to City creeks. A comprehensive stream maintenance program shall be prepared to augment stormwater utility rehabilitation projects designed to ~~improve flow capacity, minimize channel erosion, and enhance riparian habitat.~~ maintain or improve flow capacity, trap sediment and other pollutants that impair water quality, minimize channel erosion, prevent new sources of pollutants from entering the stream, and enhance instream and riparian habitat.
- RC-2h **Restoration of degraded creek resources.** Portions of Janes, Jolly Giant, Campbell, and Grotzman Creeks are culverted or covered, causing degradation of creek resources. ~~Streams such as Janes Creek have~~ Tide gates on creek systems which can ~~be~~ barriers that prevent anadromous salmonids from accessing critical habitat. Furthermore, recreational use ~~can~~has degraded ~~d~~ riparian vegetation along upland reaches of certain creeks (e.g., Jolly Giant, Campbell, and Jacoby Creeks) within Redwood Park and the Community Forest. Lack of vegetation along creek courses can cause erosion, resulting in water quality and air quality ~~borne~~ impacts. Restoration activities for improving degraded stream resources shall include:
1. Uncovering of creek courses in public rights-of-way, as part of public works improvement projects.
 2. Encouraging landowners to restore degraded EBA and stream resources, including native riparian vegetation establishment and ~~exotic~~invasive species removal, as part of a new development or renovation.
 3. Controlling uses that are damaging to upland reaches of creeks in the Community Forest and Redwood Park.
 4. Removing or modifying barriers such as tide gates that prevent migrating anadromous salmonids which are federally listed endangered species from reaching their critical habitat.
 5. Exclusionary fencing to keep livestock out of the EBA.

5-6. Identifying and addressing sources of pollutants that adversely impact water quality, if applicable.

~~The Streams Management Plan shall be implemented to provide guidance for rehabilitation and management of creeks that flow through Arcata. The SMP addresses new and modified development along creeks, and existing activities in creek zones. Stream rehabilitation projects shall be designed to maintain or improve flow capacity, trap sediments and other pollutants which decrease water quality, minimize channel erosion, prevent new sources of pollutants from entering the stream, and enhance instream and riparian habitat.~~

~~[Policies RC-2b, c, f, & h revised by Ordinance No. 1377, September 2008]~~

POLICY RC-3 WETLANDS MANAGEMENT

Objective. To protect existing wetlands areas and their functional capacities and ~~values~~services, maintain a standard of “no net loss” ~~in~~ of wetland area and ~~value~~services, restore degraded wetland areas, enhance wetlands functions, and create additional wetland areas to replace historical losses.

- RC-3a **Requirement for wetland delineation and study.** All proposed development applications shall include a site plan that shows the precise location of any wetlands that exist on the subject property. Any application for development on a parcel where wetlands may be present shall include a wetland reconnaissance or delineation report as follows:
1. The wetland reconnaissance or ~~wetlands~~ delineation ~~and~~ report shall be based upon field investigations and shall be prepared by a professional or technical expert qualified in wetlands ~~biology or plant ecology science~~.
 2. For the purposes of this plan, wetlands shall include ~~e~~Coastal ~~z~~Zone lands where one or more of the following three ~~characteristics~~wetland indicators are present or non-~~e~~Coastal ~~z~~Zone lands where two or more of the following three ~~characteristics~~wetland indicators are present:
 - a. source of water (surface or subsurface) which is present for sufficient periods to promote hydric soils formation or growth of hydrophytic plant species;
 - b. hydric soils; or
 - c. hydrophytic plants.
 3. Where a wetland reconnaissance indicates the probable existence of wetlands, ~~marsh reeds~~ a detailed wetland delineation shall be required, including a map with the best available contour information showing where each of the three ~~factors~~wetland indicators are present, why the boundary was established with data sheets to back it up, and the precise boundaries of any areas ~~which~~that are determined to be wetlands.

4. If wetlands of any size are found to exist on the property, an analysis of the potential functional or habitat ~~values~~services of the wetlands shall be ~~provided~~required.

RC-3b **Filling of wetlands.** The following shall apply:

1. Filling of wetlands shall be prohibited in the Coastal Zone, unless it can be demonstrated that:
 - a. the wetland restrictions, if imposed, would render a parcel, not subject to the Public Trust, unusable for any use permitted by the land use plan;
 - b. there is no feasible, environmentally ~~superior~~less damaging alternative to wetland fill for development of a permitted use; and
 - c. the fill is the least amount necessary to allow development of permitted uses.
2. Filling of wetlands outside the Coastal Zone may be permitted only when the following has been demonstrated by the project proponent:
 - a. the fill is the least amount necessary to allow a reasonable and harmonious configuration of development on the parcel;
 - b. the wetlands proposed to be filled are small and isolated, and have limited functional ~~values~~services when compared to larger, contiguous wetland areas.
3. Filling of wetlands shall only be authorized if appropriate mitigation, resulting in “no net loss” in the area and ~~values~~services of wetlands, is provided. Mitigation may consist of creating and maintaining a new wetland of equal or greater functional capacity and ~~values~~services than the wetland proposed to be filled, restoration of previously degraded wetlands, or enhancement of existing wetland areas.

RC-3c **Designation of Environmental Buffer Areas ~~(EBA) for Wetlands.~~** An ~~EBA~~Environmental Buffer Area shall be established to separate all permitted development from adjacent existing wetlands ~~which~~that are to be preserved in a natural state, and from new wetland areas ~~which~~that are created as ~~a~~mitigation of wetland infill. The ~~Environmental Buffer Area~~EBA's purpose is to remain in a natural state in order to protect wetland ecosystems and their associated habitat areas from destruction or degradation. The extent of the ~~Environmental Buffer Area~~EBA shall be established based upon analyses and recommendations contained in a site-specific wetland delineation ~~study, but~~study but shall include the wetland area and a setback area ~~which~~that shall generally range from a 50-foot minimum to a 100-foot maximum width. Specific findings, based on evidence provided for City review, shall be required for setbacks less than 100 feet in width.

RC-3d **Allowable uses and activities in Environmental Buffer Areas for Wetlands.** The following compatible land uses and activities may be permitted in ~~Environmental Buffer Area~~EBAs for wetlands, subject to all other policies in this Element, including those requiring avoidance of impacts and other mitigation requirements:

1. Resource restoration or enhancement projects.
2. Farming, consistent with policy RC-3I.
3. Outdoor recreation activities, such as bird watching, hiking, boating, horseback riding, and similar activities.
4. Education, scientific research, and use of nature trails.
5. Drainage ditches when compatible with wetland function.
6. Minor modification of existing, serviceable structures.
7. Fencing to prevent livestock from degrading wetlands and riparian vegetation.

Any use, construction, grading, or removal of vegetation ~~which~~that is not listed above shall be prohibited.

RC-3e **Wetland and Stream Protection Combining (:WSP) Zone.** The :WSP zone of the City’s ~~Land Use Zoning~~ Code shall be applied to all Wetland Protection Areas.

RC-3f **Review and approval of projects affecting Environmental Buffer Areas for wetlands.** Applications for development on any parcel ~~which~~that is located partially or wholly within an ~~EBA~~Environmental Buffer Area for wetlands shall be subject to the requirements of Policy RC-1 and RC-3.

RC-3g **Conservation easements.** Dedication of a conservation easement, or equivalent deed restriction, encompassing the area within the ~~Environmental Buffer Area~~EBA for wetlands shall be required as a condition of approval of any discretionary action, including design review, when any portion of the project site falls within an ~~Environmental Buffer Area~~EBA. Such easements may be conveyed to the City of Arcata, or another governmental agency, or a City-approved non-profit entity ~~which~~that shall manage the easement to protect the ~~Environmental Buffer Area~~EBA’s functions.

RC-3h **Designation of wetland protection zones.** The :WSP Zone shall be applied to wetlands, wetland setbacks, wetland buffer areas and modified wetland buffer areas, as defined in the City’s ~~Land Use Zoning~~ Code, at the time of development review and approval.

A wetlands map, maintained by the City, will show the general location of wetlands, riparian corridors, and uplands within the City limits and urban services zone. All proposed development within or adjacent to the areas identified on the map as wetlands or riparian corridors shall comply with City Wetlands Development Standards and shall include the following in the development proposal:

1. A wetland delineation.
2. A mitigation plan for impacted areas.
3. Setback areas from delineated wetlands.
4. Easements for onsite delineated wetlands.

5. Permitted and protected uses and activities within delineated wetland areas.
6. Fencing to prevent livestock from degrading wetlands and riparian vegetation.

A Wetlands Buffer Area shall be required to protect the areas shown as wetlands on the Wetlands Map. All proposed development within the buffer areas shall comply with the Wetlands -Buffer Area Development Standards of the Coastal Coastal Zoning Ordinance Land Use and Development Guide.

RC-3i **Management of Arcata Marsh for wetlands values services as well as wastewater treatment.** The Arcata Marsh and Wildlife Sanctuary serves a variety of purposes and functions, including providing wetland habitat for a variety of species, wastewater treatment, and recreational use. These purposes shall be balanced for the benefit of all users.



RC-3j **Minimum mitigation requirements for wetland impacts.** Diking or filling of a wetland that is otherwise in accordance with the policies of this General Plan, shall, at a minimum, require the following mitigation measures, monitoring program, and funding.

1. A detailed restoration plan, monitoring program, and funding source for each site shall be required as part of the project application. The restoration plan shall include provisions for restoration to equal or greater wetland biological productivity and contingencies for mitigation as appropriate. The monitoring program shall include reporting requirements that document mitigation success and contingency plans as determined necessary by staff. Dedication of the land to a public agency, purchase, or other stewardship method which that permanently restricts the use of the site to habitat and open space purposes, shall be required. The site shall be ~~dedicated, purchased, or other stewardship agreed upon,~~ and mitigation funding shall be provided, prior to any permitted diking or filling.
2. Areas adequate to maintain functional capacity shall be opened to tidal action, or other sources of surface water shall be provided. This provision shall apply to diked or filled areas which that themselves are not environmentally sensitive habitat areas, but would become so if, as part of a restoration program, they are opened to tidal action or provided with other sources of surface water. All of the provisions for restoration, purchase (if necessary), and dedication described under part 1 shall apply to any program or activity performed pursuant to this policy.
3. Mitigation shall, to the maximum extent feasible, be of the same type as the wetland to be filled (e.g., freshwater marsh for freshwater marsh, and saltwater marsh for saltwater marsh, etc.).

4. Where no suitable private or public restoration or enhancement sites are available, or where a wetlands mitigation bank in Arcata's Planning Area has been established that provides suitable replacement area, an in-lieu fee may be required to be paid. The fees shall be paid to an appropriate public agency for use in the restoration or enhancement of an area of equivalent productive value or surface area, or to the entity managing the wetlands mitigation bank.

RC-3k **Wetland functional capacity maintenance requirement.** Diking, filling, or dredging of a wetland or estuary shall maintain or enhance the functional capacity of these resources. Functional capacity means the ability of the wetland or estuary to be physically and biologically self-sustaining and to maintain natural species diversity. In order to establish that the functional capacity is being maintained, all of the following must be demonstrated:

1. Presently-occurring plant and animal populations in the ecosystem will not be altered in a manner that would impair the long-term stability of the ecosystem (i.e., natural species diversity, abundance and composition are essentially unchanged as the result of the project).
2. A species that is rare or endangered will not be significantly adversely affected.
3. Consumptive uses (e.g., fishing, aquaculture and hunting) or non-consumptive functions (e.g., water quality improvement and research opportunity) values of the wetland or estuary ecosystem will not be significantly reduced.

RC-3l **Uses allowed in diked and /drained reclaimed former tidelands.** Allowable uses and development in grazed or farmed wetlands are limited to uses compatible with the Public Trust doctrine. These uses are specified in Land Use Element Policy LU-6 and are summarized below.

1. Agricultural operations limited to accessory structures, apiaries, field and truck crops, livestock raising, greenhouses (provided they are not located on slab foundations and crops are grown in the existing soil on site), and orchards.
2. Farm-related structures, including barns, sheds, and farmer-occupied housing, necessary for the performance of agricultural operations. Such structures may be located on an existing grazed or farmed wetland parcel only if no alternative upland location is available for such purpose and the structures are sited and designed to minimize adverse environmental effects on Public Trust resources and uses. No more than one primary and one secondary residential unit shall be allowed per parcel.
3. Restoration projects.
4. Nature study, aquaculture, and similar resource-dependent activities compatible with Public Trust resources and uses.
5. Incidental public service purposes whichthat may temporarily impact the resources of the area (such as burying cables or pipes).

Expanding farming operations into non-farmed wetlands, by diking or otherwise altering the functional capacity of the wetland is not permitted. Farm-related structures (including barns, sheds, and farm-owner occupied housing) necessary for the continuance of the existing operation of the farmed wetlands may be located on an existing farmed wetland parcel, only if no alternative upland location is viable for such purpose and the structures are sited and designed to minimize the adverse environmental effects on the farmed wetland. Clustering and other construction techniques to minimize both the land area covered by such structures and the amount of fill necessary to protect such structures will be required.

[Policies RC 3a, c, d, f, & g revised by Ordinance No. 1377, September 2008]

POLICY RC-4 OPEN WATERS OF ARCATA BAY & TIDELANDS

Objective. Maintain existing Bay wetlands and tide lands, protect them from urban and agricultural encroachments, or degradation, and manage the open waters of Arcata Bay for their wildlife, fisheries, ~~navigation~~ and ecological ~~values~~ services, and as well as navigation, recreation, and tourism uses.

RC-4a Protection of open waters and tideland areas of Arcata Bay.

The ~~tidal and water~~ open water areas and tidelands of Arcata Bay constitute a fragile Public Trust resource and access shall be controlled to avoid resource degradation, while maintaining the public's right to navigation. Tidal marshes shall be enhanced and maintained, especially in the areas of McDaniel, Gannon, and Butcher's Sloughs, to protect wetland ~~values~~ services.

RC-4b Access to Arcata Bay. The following bicycle and pedestrian routes are designated as Public Access Corridors, and ~~are to~~ shall be properly signed and identified as approved Bay access points.

1. "I" Street from Samoa Boulevard, south through the Arcata Marsh and Wildlife Sanctuary to the boat launching facility on Arcata Bay.
2. South "G" Street south of "H" Street, to Highway 101.
3. ~~Highway 101~~ Humboldt Bay Trail from Samoa Boulevard (Highway 255), south to Bayside Cutoff and beyond.
4. Samoa Boulevard from Highway 101 west to Mad River Slough.

A system of foot trails and interpretive sites shall be established along the Arcata Bay shore westward to the City limit, subject to the following guidelines.

5. All planning and development in the area that is both s south of Samoa Boulevard and west of State Route Highway 101, and ~~which that~~ is identified as tidelands, former tidelands, wetlands or riparian corridor on the adopted Wetlands Map shall

- be reviewed by the ~~Creeks &~~ Wetlands ~~and~~ Creeks Committee, and coordinated with California Department of Fish and ~~Game~~ Wildlife.
6. Development in the area bounded by Butcher's Slough and Gannon Slough ~~should~~ shall occur in conjunction with management of the USFWS National Wildlife Refuge, ~~and the~~ Arcata Marsh and Wildlife Sanctuary and the Jacoby Creek Gannon Slough Wildlife Area.
 7. Motorized vehicles shall be restricted to paved roads and parking lots.
 8. Pedestrians shall be restricted to designated trails and facilities.
 9. Valid scientific and educational studies of wetlands and tidelands are encouraged with a City Nature Area Entrance Permit.

RC-4c **Coastal-dependent and ~~P~~ublic ~~T~~rust uses of Arcata's tidelands.** Tidelands of Arcata Bay support a variety of wildlife, as well as human activities. The following provisions shall be made for managing tideland areas.

1. ~~New development shall not restrict access to the shoreline. Access to coastal areas shall be required for new development.~~
2. Tidelands and open water areas of Arcata Bay shall be designated Natural Resource-Public Trust ~~L~~ands [NR-~~PTL~~], and identified as passive use recreational areas.
3. The Arcata Marsh and Wildlife Sanctuary shall be designated as Natural Resource [NR], and the recreational component of the project identified as a passive use recreational area.
4. The continued use of the tidelands for scientific and educational studies is encouraged.
5. The Arcata Marsh and Wildlife Sanctuary (~~AMWS~~) shall be maintained and any new facilities shall be built consistent with the AMWS-Arcata Marsh and Wildlife Sanctuary plan adopted by the City Council.
6. The South "I" Street boat launch shall be enhanced or relocated ~~and maintained~~ to accommodate small watercraft and windsurfing.
7. The placement of interpretative sites along the Arcata Bay shore, including Nature and Wildlife Centers, shall be coordinated with other agencies, and serve as an educational focal point for Arcata's natural resource areas.
8. Access on the levee from the Arcata Marsh and Wildlife Sanctuary ~~AMWS~~ westward to the City limit ~~will~~ shall be provided for passive recreation and nature observation.

RC-4d **Diking, dredging, filling, and shoreline structures.** Diking, dredging, or filling, ~~or dredging~~ of Bay waters, wetlands, and estuaries ~~shall be~~ discouraged and only permitted where it has been demonstrated that the Public Trust resources and values are being protected, and mitigation measures have been provided, which minimize adverse environmental effects, for the following limited uses:

1. Incidental public service purposes including, but not limited to, burying cables and pipes, and maintaining existing dikes and public facilities.

2. Maintaining a channel adequate to serve the boat ramp at current levels of use.
3. Resource restoration purposes.
4. Nature study, aquaculture, or similar Public Trust resource dependent activities.
5. Agriculture as currently practiced within existing ~~farmed wetlands~~diked former tidelands but not including the expansion thereof.

In order to protect existing development, shoreline structures (such as dikes or tide gates) that may alter the natural shoreline, ~~may~~shall be permitted only when they do not ~~adversely effect~~affect any federally ~~list~~ed ~~endangered or threatened~~ species; and no other feasible, less environmentally-damaging alternative is available, and only when the structures are not located within a wetland, unless the wetland will be the primary beneficiary of the structure.

The ~~disposal~~placement of dredge ~~spoils material~~ on existing wetlands shall not be permitted unless such ~~disposal~~placement is necessary for either a Public Trust resource restoration project, or for the maintenance of existing agricultural operations in ~~farmed wetlands~~diked former tidelands. ~~Fill~~Wetland fill shall ~~will~~ be allowed for aquaculture projects if it can be shown that it is necessary for the project, is required to be located within the wetland, and there is no other feasible, less environmentally ~~damaging~~, alternative.

RC-4e **Aquaculture use of coastal wetlands ~~and~~/tidelands.** To protect aquaculture activities in Arcata Bay, the City shall:

1. Ensure that its wastewater discharge does not ~~aggravate~~exacerbate existing problems with coliform ~~bacteria loading problems~~levels in Arcata Bay.
2. Take measures to reduce coliform loading of perennial streams within its jurisdiction, as part of a stream maintenance program. These measures shall include controlling identified sources of coliform loading such as septic tank leachate and runoff from agricultural operations.

Aquaculture shall not adversely impact natural ecological processes nor native wildlife or fisheries or their habitat in the Bay. No new aquaculture uses shall be permitted unless it can be demonstrated that adequate precautions will be taken to prevent new adverse impacts to natural ecological processes. The City shall continue its management of:

1. Integrated wetland enhancement and wastewater treatment.
2. The tidelands, for commercial and native oyster harvesting.

RC-4f **Management of bayfront and marsh areas for coastal access, recreation, and tourism.** Tidelands and open water areas of Arcata Bay shall be designated Natural Resource-Public Trust ~~land~~ [NR-PT~~land~~] and protected from uncontrolled access. The following guidelines shall be used when permitting access to these areas:

1. Motorized vehicles shall be restricted to paved roads and parking lots.
2. Pedestrians shall be restricted to designated trails and facilities.
3. Valid scientific and educational studies of the wetlands and tidelands shall be encouraged.

New development shall not restrict public access to the shoreline. Public access to the shoreline shall be required of new development. Where consistent with the Humboldt Bay National Wildlife Refuge's Management Plan, controlled public access to the Refuge's Jacoby Creek Unit shall be developed along Arcata Bay from the AMWS to the City's westward limit.

POLICY RC-5 AGRICULTURAL RESOURCES MANAGEMENT

Objective. Protect and enhance agricultural uses on prime agricultural lands within the City, and encourage more productive agricultural use of agriculturally suitable lands.

RC-5a **Promotion of and participation in agricultural production within the City.** Diverse and intensive agricultural production and increased participation in agricultural production, shall be promoted, in order to maintain the value of agricultural lands, improve the economic base, and increase employment and food production. The City does not, however, advocate more intensive agricultural uses and practices that would have adverse environmental impacts. Agricultural operations, such as Community Supported Agriculture (CSA) are strongly encouraged.

~~RC-5b **Agricultural Advisory Committee.** The City shall appoint an Agricultural Advisory Committee to advise on agricultural issues and programs. The responsibilities of the committee shall include, but are not limited to:~~

- ~~1. Development of a Community and Farm Protection Ordinance, as well as conflict resolution protocol.~~
- ~~2. Development of programs (educational, leasing, and purchase) that will encourage responsible productive uses of agricultural lands.~~
- ~~3. Identification of lands for preservation and/or acquisition programs.~~
- ~~4. Maintain a database of resources available to farmers, such as Williamson Act advantages, conservation easements, organic farming practices, and marketing strategies.~~

RC-~~5c~~-5b **Community and farm protection.** Maintaining a compatible relationship between agricultural and residential uses ~~will~~shall be based on:

1. Recognizing the rights of owners of productive agricultural land to make agricultural use of their land.

2. Identifying and minimizing potential conflicts between agricultural operations and adjacent residential, commercial, and community facility uses.
3. ~~A Community and Farm Protection Ordinance shall provide a foundation for minimizing conflicts, educating the community, and a protocol for mediating unresolved disputes. Once adopted, the ordinance shall be mailed to all owners of agricultural and adjacent lands and disclosed to affected property owners at the time of parcel transfer.~~

RC-~~5d~~-5c **Permanent protection for agricultural lands.** Protection of agricultural resources shall be secured through the purchase of conservation easements, development rights, and outright acquisition. The City shall work in conjunction with other entities such as land trusts, whenever possible, to preserve agricultural buffers and maintain and enhance agricultural uses on prime agricultural soils.

POLICY RC-6 FOREST RESOURCES MANAGEMENT

Objective. Protect and enhance private and public forest lands (e.g., the Arcata Community and Jacoby Creek Forest Tracts) to maintain the resiliency and integrity of the ecosystem while ~~providing protection~~ timber production, recreation, ~~and~~ habitat values, and opportunities for education and research.

RC-6a **Management of Arcata Community Forest** ~~(Not applicable in Coastal Zone)~~. The City's ~~2022 F~~forest ~~M~~management plan includes the following policies:

1. **Recreation and aesthetics resource management** - The community forest will emphasize dispersed, day-use opportunities. Recreational use shall not be allowed to impact other resources such as fish, wildlife, or watershed.
2. **Timber resource management** - To ensure the sustainable and long-term production of forest products, the rate of harvesting must not exceed the rate of production. Long-term productivity refers to the continuing ability of the forest to produce timber while retaining the associated values of watershed, wildlife, soils, recreation and aesthetics. This is dependent upon the use of management practices that do not allow for the deterioration or impairment of soil productivity ~~or the alteration of the natural landscape beyond its ability to recover~~. For planning purposes, long term means that exceeding fifty years.
3. **Watershed resource management** - Water quality, soil, riparian, and aquatic biological productivity shall be maintained and enhanced through the application of City forest management standards and the implementation of watershed improvement projects.
4. **Wildlife resource management** - Wildlife habitat is managed to promote species diversity and to ensure that populations of indigenous species are maintained. This can best be achieved through the maintenance and enhancement of habitat values.

Habitat values ~~which~~that lead to species diversity include the following elements: breeding, foraging, watering, rearing, hiding and thermal cover.

5. **Vegetation and botanical resources** - Maintain the native ~~component~~biodiversity of species found in ~~at~~the redwood forest habitat, both by controlling exotics and managing for a species mix that would ~~be found~~ naturally occur in ~~at~~the redwood forest habitat.

RC-6b **Management of Jacoby Creek Forest** ~~(Not applicable in Coastal Zone)~~. The management policies for the Jacoby Creek Forest are the same as those for the Arcata Community Forest, listed above, except that the Jacoby Creek Forest is not open to recreational use.

RC-6c **Allocation of forest fund revenues** ~~(Not applicable in Coastal Zone)~~. ~~At least twenty percent of net forest fund~~ Forest fund revenues, derived from timber ~~cutting~~harvest and carbon projects shall be deposited into a special revenue account within the City to be utilized for forest management purposes. Excess net forest fund revenues, when available, may be ~~shall be~~ directed towards park acquisition, maintenance, and development. This can include acquisition of stream corridors, and riparian and greenbelt areas. These areas contribute to the diversity of parks and, in the case of linear parks along stream corridors, provide passive recreation areas compatible with the environment. The acquisition of open space shall be emphasized as an appropriate use for the remaining revenues.

RC-6d **Management practices for private timberlands** ~~(Not applicable in Coastal Zone)~~. The management of private timberlands shall be encouraged to use current principles of sustainable forestry for all aspects of forest use and function: recreation; timber production; biodiversity; air and water quality; and carbon storage. Timber owners are encouraged to apply for conservation easements, certified forestry, or compensation for carbon storage.

RC-6e **Timber harvest plans** ~~(Not applicable in Coastal Zone)~~. The City, in cooperation with California Department of Forestry, shall request review of all Timber Harvest Plans (THP) within the Planning Area. The City shall review THPs for measures that protect water quality, control erosion and flooding, and preserve the City viewshed. The city shall recommend that THPs ~~that~~which do not include these measures not be approved.

RC-6f **Urban conversions** ~~(Not applicable in Coastal Zone)~~. The sustainable management of timber resources, and related uses, shall be encouraged, so that the long-term economic return from productive timber production will provide sufficient incentives to prevent urban conversions. Urban conversions are discouraged within the Urban Services Boundary.

RC-6g **Setbacks** ~~(Not applicable in Coastal Zone)~~. Development adjacent to the Community Forest boundary shall be setback at least 150 feet, unless this would make the use of the

parcel infeasible for its designated purpose. However, larger setbacks may be required to prevent exposure to potential hazards and to maintain forest integrity.

RC-6h **Monitoring** ~~(Not applicable in Coastal Zone)~~. Monitoring of forest practices, to ensure consistency with adopted management and harvest plans, shall be carried out as an implementation measure of this Element. The general objectives of the monitoring will be to:

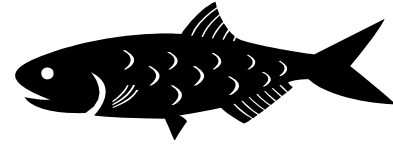
1. Determine the effectiveness of management practices at multiple scales (i.e., individual sites to watersheds).
2. Validate ecosystem functions and processes have been maintained as predicted.

POLICY RC-7 WATER RESOURCES MANAGEMENT

Objective. Manage Arcata's water resources from a watershed perspective, to maintain surface water and ~~subsurface~~ ground water quality and quantity. Runoff will be managed for the benefit of aquatic habitats, native and non-invasive vegetation, and soil conservation, and to recharge groundwater.

C-7a **Protection of surface waters from point and nonpoint pollution sources.** The use of natural stormwater drainage systems, which preserve and enhance natural features, shall include the following:

1. Efforts to acquire land or obtain easements for drainage and other public uses of floodplains, where desirable to maintain stream courses in a natural state, shall be supported.
2. Recreational opportunities and aesthetics shall be considered in the design of stormwater ~~detention/~~retention, detention, treatment, and conveyance facilities.
3. Sound soil conservation practices shall be required, and impacts of proposed developments, with regard to water quality and effects on watersheds, wetlands and drainage courses, shall be carefully examined.
4. The quality of runoff from urban and suburban development shall be improved through use of appropriate and feasible ~~mitigation measures~~Best Management Practices (BMPs) including, but not limited to, bioretention basins, artificial wetlands, grassy swales, ~~infiltration/sedimentation basins, riparian setbacks~~, oil/grit separators, ~~and other best management practices (BMPs)~~, with an emphasis on a Low Impact Development approach to stormwater management.
5. New development shall be required to ~~mitigate~~minimize to the maximum extent feasible increases in stormwater peak flows and/or volume, to the extent feasible. ~~Mitigation~~Stormwater management measures ~~should~~shall take into consideration potential adverse impacts on the Mad River, Arcata Bay, and adjoining lands in the City and Planning Area.



6. New development projects ~~designs~~ shall be designed to minimize drainage concentrations, maximize permeable surfaces (such as unpaved parking areas) and maintain, to the extent feasible, natural site drainage conditions.
7. New development projects that may adversely affect the quantity and quality of surface stormwater runoff shall be required to allocate land necessary for detaining post-project flows and/or for incorporating measures to ~~mitigate~~ minimize water quality impacts ~~related to~~ from urban runoff. To the maximum extent feasible, new development shall not produce a net increase in peak stormwater runoff.
- ~~7-8.~~ All development shall comply with the City's post construction stormwater management program (under the MS4 General Permit) which ~~may~~ includes stormwater management measures for site design, source control, runoff reduction, stormwater treatment, ~~or~~ and baseline hydromodification controls, as applicable based on project type and size.

RC-7b **Protection of groundwater sources.** Septic systems and onsite disposal of toxic substances are the leading causes of groundwater contamination. Septic systems within the Urban Services Boundary shall not be permitted, and incidents of onsite toxics disposal shall be referred to the appropriate county and state agencies.

RC-7c **Watershed and urban runoff management.** To protect structures, critical facilities, existing habitat values and water quality, flooding shall be managed on a watershed basis, using a combination of biotechnical solutions, flood protection practices, and Drainage Master Plan's management practices.

RC-7d **Water quality monitoring.** Water quality and quantity shall be monitored on a regular basis to ensure that City policies are being adhered to.

POLICY RC-8 ENERGY RESOURCES MANAGEMENT

Objective. Reduce ~~the net emissions of greenhouse gases from Arcata; reduce other negative impacts of energy production and use through conservation and efficiency practices, including risks from nuclear power, air emissions, fuel spills, and wildlife and habitat destruction;~~ reduce energy costs to the city and its residents, and increase the percent of energy purchases from renewable sources ~~within our region;~~ increase ~~the city's and nation's~~ energy security and reduce our vulnerability to ~~changes in energy availability outages and increased~~ price; increase public awareness of energy issues and encourage an energy conservation ethic;~~;~~ ~~monitor the cost and effectiveness of Arcata's actions so we and others can learn from them; and implement Arcata's~~ Advisory Proposition B.

Advisory Proposition B
Approved by Arcata Voters April 8, 1980

~~"In accordance with America's renewed determination to be energy self-reliant, be it resolved that the citizens of Arcata and their City government are committed to the enactment of conscientious energy conservation measures and the accelerated development and active promotion of safe and economical alternative renewable energy sources for our community.~~

~~— Be it further resolved that the City government of Arcata support complete independence from nuclear power including the permanent closure of the Humboldt Bay nuclear power plant and its replacement by safe, clean and efficient generating sources more compatible with the resources and health and safety of the Northcoast, such as conservation, solar power and generation from wood waste."~~

The City will actively participate in Redwood Coast Energy Authority's primary guiding document the Humboldt County Comprehensive Action Plan for Energy (CAPE) and RePower Humboldt, a community-wide effort to define a vision and Strategic Plan for achieving energy independence and energy security in Humboldt County

RC-8a **Encouragement of appropriate energy alternatives.** ~~In making~~The City will participate in Redwood Coast Energy Authority's Community Choice Energy (CCE) program, supporting renewable energy purchases over non-renewable sources. ~~'the City shall consider how suppliers meet the objectives of this policy. The City shall choose suppliers that provide good tradeoffs among these objectives, giving due consideration to investment in energy conservation as an alternative use of energy funds.~~
~~In addition, the City shall attempt to purchase at least 10% of its electrical energy (in energy units, not cost) from renewable sources within Humboldt County by the year 2020. The City shall take measures to encourage the availability to, and use by, residents of energy suppliers that best meet the objectives of this policy. The City shall convert City vehicle fleets to a mix of fuels that best meets the objectives of this policy.~~

RC-8b **Encouragement of energy efficiency and conservation.** The City shall ~~coordinate with energy suppliers and agencies~~ disseminate [Redwood Coast Energy Authority information](#) to educate residents, property owners, and business operators about the need for and benefits of conserving energy. ~~The City shall maintain and distribute current~~ [This includes](#) information about building insulation; energy efficient appliances, lighting, and heating; other conservation measures and materials; and home power alternatives.

The City shall continuously seek and implement cost-effective steps to reduce City energy use. ~~The City shall attempt to reduce the City's total consumption of purchased energy by at least 20% (in energy units, not cost) by the year 2010.~~

The City shall adopt [national "Energy Star Program"](#) ~~the goals of the national "Energy Star Program"~~ (or its successor programs) for all City construction projects and all construction projects assisted by grants for which the City is an applicant. These goals include achieving a minimum of 15% greater energy efficiency than would a building designed with existing Title 24 standards.

Explore and, if appropriate, adopt energy efficiency standards for existing residential and commercial buildings upon substantial remodel. Consider requiring energy efficiency inspections, disclosure, and retrofits at change of ownership based on cost-effective and commercially available energy efficiency measures.

RC-8c **Promotion of energy efficiency in transportation.** ~~The City shall give strong consideration to energy conservation and the goals of this policy in all transportation and traffic management decisions. It is City policy to r~~Reduce ~~the need for~~ motor vehicle trips within the city and between the city and other destinations, and ~~to~~ reduce per-trip energy consumption; this policy applies to trips by residents, non-residents, and city staff. Such measures as bike and pedestrian paths, public transportation, [shared parking and traffic management](#), and ~~encouraging use of~~ alternative-fueled vehicles shall be used to make these reductions.

RC-8d **Restoration for Greenhouse Gases Absorption.** Foster and restore forests and other terrestrial ecosystems that offer significant carbon mitigation potential, [consistent with the Humboldt County Climate Action Plan](#).
[\[Policies RC 8b & d revised by Ordinance No. 1377, September 2008\]](#)

RC-8e **City Electrification Ordinance.** [Prepare and pass an 'all electric' Ordinance that phases out natural gas infrastructure and use.](#)

POLICY RC-9 SOILS AND MINERAL RESOURCES

Objective. Conserve and manage soil and mineral resources.

RC-9a **Erosion control measures on slopes and other areas of instability.** General Plan Public Safety Element Policy PS-3 - Other Geologic Hazards ~~in the General Plan Public Safety Element~~ includes provisions-protections for ~~protecting~~ steep and unstable slopes, and to minimize ing erosion, ~~and~~ sedimentation and landslides. This policy shall be followed as a safety precaution and also to conserve soil resources.

RC-9b **Protection of productive soils and soils with limitations.** Local soils range from productive soil types capable of supporting agriculture and forestry, to those susceptible to shrink-swell and erosion. Clay soils are the most susceptible to shrink-swell, caused by fluctuations in moisture content. According to available soils information, the Bayside series is the only soil type in the Arcata area with identified clay content. Building construction on this soil type shall include measures to avoid damage from shrink-swell.

Certain areas of the City have high liquefaction potential during seismic events. Policy PS-2 - Seismic Hazards, in the General Plan Public Safety Element, addresses mitigation of liquefaction hazards. This policy shall be followed as a safety precaution, and also to manage related soil limitations. Policy RC-5, relating to agricultural soils, shall also be followed to conserve productive soils. The continued research, identification, and protection of productive soils by the Natural Resource Conservation Service and educational institutions shall be encouraged.

RC-9c **Management of mineral resource (gravel) extraction, processing and transport (gravel).** Areas along the Mad River, within and upstream of the City’s Sphere of Influence, are currently used for aggregate (i.e., gravel) resource extraction. The City shall encourage Humboldt County to limit the quantity of aggregate extracted to an amount that does not exceed the ~~that is~~ mean annual recruitment, and shall request that Policy RC-1 and RC-2 be applied ~~to~~ protect natural biological diversity and ecosystem functions along the river. ~~The City shall also request that the County not approve or renew permits for commercial mineral resource extraction in A-E designated lands inef~~ the City’s Planning Area. ~~Mineral resource extraction~~ operations shall not result in additional soil runoff, and shall be consistent with the City’s seismic safety policies (see Policy PS-2 in Public Safety Element).

Attachment: F. Ch 4 Environment (4214 : GP #XVII FBC & GP wrap-up)

4.6 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
RC -1	Creeks & Wetlands Management Plan Regularly u Update the City Creeks Management Plan, <u>at least every five years,</u> to <u>include wetlands and continue to</u> implement current provisions for maintaining biological integrity of entire watercourses. The Creeks & Wetlands Management Plan will also include updated provisions for education and restoration programs for degraded creeks.	Environmental Services Dept./Wetlands & Creeks Advisory Committee	Year 1 then Every 5 years

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
RC -2	Community Forest Management Plan Update the Community Forest Management Plan, at least every ten years, to implement current provisions for managing recreation, aesthetic, timber, watershed, wildlife, and vegetation resources. The Management Plan will also include updated provisions for allocation of forest fund revenues and urban conversions, as well as setbacks from the Community Forest boundary and a monitoring program for forest practices.	Environmental Services Dept./Forest Management Committee	Year 5 then Every 10 years
RC -3	Energy Efficiency and Conservation Program Conduct a continuous program to identify and purchase appropriate energy supplies, implement and evaluate energy conservation measures, provide energy education and public information, and promote energy efficiency in transportation. Establish a funding mechanism to assure that a significant portion of the savings are used to fund energy programs and as a reward for savings.	Environmental Services Dept./ Energy Task Force Committee	Year 1 then every 5 years Ongoing
RC -4	Non-native Plant and Animal Species Removal Program The City shall <u>continue to</u> provide public information that explains why invasive <u>plant</u> species are a <u>problematic</u> . The City shall maintain a program that recommends effective but non-toxic eradication measures, <u>for invasive species</u> and eradicates non-native <u>plant</u> species on public lands where they are displacing native species.	Environmental Services Dept./ Agricultural Advisory Committee	Year 1 then every 5 years Ongoing
RC -5	SPAs, WSP Combining Zone, Resource Setbacks and Energy Conservation Measures (PLUC Amendment) Revise the PLUC (formerly LUDG) regulations to be consistent with the General Plan policies for SPAs, the WSP combining Zone, natural resource (forest) area setbacks and energy conservation techniques.	Environmental Services Dept./ Planning Commission	Year 1 then every 5 years
RC -65	Surface Water Quality Ordinance Prepare and adopt a water quality ordinance using water quality standards established in the Drainage Master Plan. The ordinance shall address the physical, biological, and chemical parameters of water quality, include monitoring provided through the MOU with HSU, and shall be updated at least every five years. Implement the City's Municipal Separate Storm Sewer System (MS4) requirements for post-construction activities- including Ordinance No. 1463 that sets forth standards for discharge into the stormwater drainage facilities for the City of Arcata, and establishes a stormwater pollution control program in compliance with the Clean Water Act.	Environmental Services Department	Year 1 then every 5 years Ongoing
RC -76	Jacoby Creek Gannon Slough Wildlife Area Wetlands Management Plan Prepare a long-term management plan for the Jacoby Creek Gannon Slough Wildlife Area Wetlands Management Plan that includes habitat mapping of all known wetland areas, guidelines for wetlands management, setbacks, and restoration goals and objectives, and review and approval requirements for wetland alterations.	Environmental Services Dept./ Wetlands & Creeks Advisory Committee	Year 2
RC -87	Sensitive Habitat Mapping Using the sensitive habitat definition from Policy RC-1d, prepare and regularly update a map of sensitive habitat in the City.	Environmental Services Dept.	Year 1 Ongoing
RC -98	Pesticide Ordinance Regularly update the City's Pesticide Ordinance.	Environmental Services Dept.	Every 5 years

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
RC-10	Create Agricultural Advisory Committee This City shall appoint a committee to be an impartial forum for addressing agricultural issues between property owners and agricultural operators. The committee will also be responsible for preparing the agricultural operations ordinance, researching incentives for continued agricultural operations, and advising the Planning Commission on any proposed development that would affect agricultural productivity.	City Council	Year 1
RC-11	Participate in Humboldt Bay Management Plan The City shall designate a representative to attend meetings, review documents, and represent the City's interest during the preparation of the Humboldt Bay Management Plan.	City Council appoints a representative	Year 1
RC-12	Community and Farm Protection Ordinance The Agricultural Advisory Committee shall develop and maintain a Community and Farm Protection Ordinance, which shall provide a foundation for minimizing conflicts, educating the community, and a protocol for mediating unresolved disputes.	Agricultural Advisory Committee	Year 1
RC - 139	Biocides and Other Compounds Programs to Promote Alternatives to Pesticides The City shall implement a program to foster the reduction in private use of pesticides by the public. This shall include maintaining and making available a current list of alternative, environmentally-safe products for controlling to control unwanted vegetation and pests, growing crops and enhancing production of animal products. The use of substances and compounds which that can accumulate to toxic levels is restricted by the City (Pesticide Ordinance).	Environmental Services Dept.	Year 1 Ongoing
RC-10	Electrification Ordinance and Program Prepare and adopt an electrification ordinance for new construction. Create an electrification program for phase out of natural gas including short-term, mid-term, and long-term actions, including educational and promotional materials.	Environmental Services Dept.	

Attachment: F. Ch 4 Environment (4214 : GP #XVII FBC & GP wrap-up)

AIR QUALITY ELEMENT

Draft 6/16/23

4.7 INTRODUCTION

The community values clean air. To promote clean air, the City, in partnership with State and Federal responsible agencies establishes goals and policies to retain and improve the region's air quality. The City emphasizes land use practices, permitting, and programs that reduce introduction of airborne pollutants. While the Air Quality Element is a mandatory General Plan Element pursuant to state law, the City of Arcata is committed to maintaining the quality of life that we all benefit from by implementing proactive air quality policy and standards.

Guiding Principles and Goals.

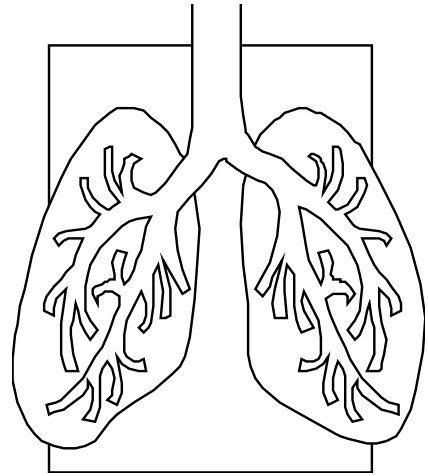
- A. Reduce motor vehicle related air pollution.
- B. Participate in regional efforts to improve air quality.
- C. Educate the community about the effects of air pollution and how ~~it~~they can be reduced.
- D. Reduce emissions from wood-burning stoves and fireplaces.
- E. Reduce emissions from forest management and burning.
- F. Reduce emissions from industrial sources.
- G. Reduce emissions from residential open waste burning.

Overview of factors contributing to air pollution. One of the best ways to control air pollution is to develop transportation infrastructure and land use goals and policies that ~~complement~~complement, and work in harmony towards, air pollution control objectives. Activities affecting aAir quality in the City of Arcata ~~is~~are regulated by the North Coast Unified Air Quality Management District (District). The District's primary responsibility is to achieve and maintain federal and state air quality standards. Humboldt County is located in the District North Coast Air Basin (Basin), which includes the District oversees, includes Del Norte, Humboldt, Mendocino, and Trinity Counties, as well as the northern and western portion of Sonoma County. The District oversees Humboldt, Del Norte, and Trinity Counties. The District's region currently meets all federal standards, but is classified as non-attainment (exceeds maximum limits) for California Ambient Air Quality Standards for airborne particles that are ten microns in diameter and smaller (PM-10) for exceedances of the 24-hour standard.

Federal and state ambient air quality standards also include ozone, carbon monoxide, nitrogen dioxide, sulfur dioxide, and lead. Of these pollutants, motor vehicles are a major contributor of carbon monoxide, nitrogen dioxides, and ozone. While engine and fuel improvements have significantly reduced these emissions from motor vehicles, measures to reduce vehicle travel

can further improve air quality from these pollutants.

Particulate matter includes a wide range of solid or liquid particles including smoke, dust, aerosols, and metallic oxides. ~~Two s~~Significant sources of PM-10 in the Basin ~~include include~~ transportation (road dust, mobile sources), woodstoves, open burning, and a minor contribution from permitted sources (District2022). Stationary sources include Humboldt Sawmill Company (Scotia), the PG&E Humboldt Bay Generating Station (Eureka), and the DG Fairhaven Power Plant (Fairhaven). ~~motor vehicle exhaust with its associated secondary reactions in the atmosphere related to exhaust gases, and wood-burning stoves/fireplaces.~~ PM-10 emissions associated with motor vehicles include vehicle exhaust and tire and brake wear. ~~However, most particulate releases from motor vehicles are a result of road dust suspension. For example, road dust comprises 77% (580 tons/year) of vehicle-related PM-10 releases in the Arcata/Eureka area.~~ ~~Because road dust sources cannot are more difficult to controlbe controlled~~, reductions in vehicle use ~~and miles traveled~~ are needed to significantly reduce PM-10 emissions caused by suspended road dust.



Wood-burning stoves, fireplaces, and residential open waste burning are also a source of PM-10 emissions. Research on human health effects of PM-10 shows a correlation between elevated PM-10 concentrations and aggravation of chronic illnesses and elevated mortality rates. Fine particulate matter can affect health more than larger particles because it can bypass respiratory filtration systems and lodge deep in the lungs¹.

Overview of Arcata's air quality. Air quality is affected by both emissions and meteorological conditions. Arcata air quality is influenced by its coastal location and relatively stable temperatures are throughout the year. Temperatures average 50 degrees Fahrenheit, with a yearly average range of 40-60 degrees Fahrenheit. Prevailing winds are from the northwest in summer and southwest in the winter. During winter months moderate temperatures, frequent fog, and moderate to heavy precipitation cause inversions which impact air quality.

Arcata is within the northwestern most air district in the State, the District, which encompasses 7,100-753 square miles including the counties of Humboldt, Del Norte, and Trinity, and serves a population of ~~nearly 170,000~~ approximately 184,000. The District presently meets all federal and state air quality standards, except for the state standard for particulate matter of ten microns and smaller (PM-10). The table below shows the federal and state PM-10 standards.

NOTE: Yellow highlighted text to be updated.

TABLE AQ-1 AMBIENT AIR QUALITY STANDARDS FOR PM-10 EMISSIONS

AVERAGING TIME	FEDERAL STANDARD	CALIFORNIA STANDARD
Annual Arithmetic Mean	50 ug/m ³ None	30-20 ug/m ³
24 Hour Average	150 ug/m ³	50 ug/m ³

Source: [North Coast Unified Air Quality Management District Particulate Matter \(PM10\) Attainment Plan, Draft Report, 1995](#) California Air Resources Board, [Ambient Air Quality Standards, 2016](#). ug/m³ = micrograms per cubic meter.

The District began measuring North Coast PM-10 concentrations in 1985. Of the total suspended particulates measured, PM-10 comprise approximately 60% of particulate matter. Table AQ-2 shows local PM-10 measurements.

TABLE AQ-2 PM-10 MEASUREMENTS IN THE ARCATA/EUREKA AREA

MONITORING LOCATION	MAX. 24 HOUR VALUE	ANNUAL AVERAGE
Arcata (1990)*	43.0	11.8
Eureka (1985)	75.0	32.7
Eureka (1990)	83.0	24.4
Eureka (1996)	87.3	15.9

Source: [Summary of District Air Monitoring Data, June 1997](#).

All values are in ug/m³ = micrograms per cubic meter.

* The 1990 measurement in Arcata was part of a special purpose study performed by the state.

[Humboldt County is classified non-attainment, while Del Norte and Trinity are classified as attainment. Between 2014 and 2019, the California PM-10 standard was only exceeded 12 times. Nine exceedances were due to wildfire events and three were attributed to woodstove burning \(District 2022\). Table AQ-3 shows general overall sources of pollution by major category, and the percent contribution of each source to the various pollutants. While the values shown in Table AQ-2 do not indicate that Arcata or Eureka always exceed state standards, the District as a whole has a non-attainment classification and all communities within the district contribute to that status.](#)

[Primary sources of PM-10 contributors in the Arcata/Eureka area include residential fuel combustion \(24%\); industrial wood and paper manufacturing \(19%\); paved road dust \(16%\); construction and demolition \(14%\); and unpaved road dust \(7%\). During periods of high PM-10 releases, wood-burning fireplaces account for approximately 50%, automobiles 31%, pulp mills 14%, and other sources 5%. Table AQ-3 shows general overall sources of pollution by major category, and the percent contribution of each source to the various pollutants.](#)

[The last district study conducted by the District was the Chemical Mass Balance Study of Composition of Particulate Matter, in conducted in 1992. That study did find found that diesel emissions constituted a fairly large component of PM-10. Diesel emissions have been declared](#)

a toxic emission by the State, and the State Air Resources Board is instituting a diesel engine replacement/retrofitting program. [California adopted new regulations that will phase out diesel trucks by 2036.](#)

~~Measurements indicate that the Eureka area has the greatest measured PM-10 concentrations in the Humboldt Bay area of the District. While the Eureka area air quality is improving, several days during the winter months still exceed state PM-10 standards.~~

District-wide, the number of days which exceed standards have decreased from about 24% in 1985 to [about 8% in 1993; 3% in 1994; 2% in 1995; 3% in 1996; and 2% in 1997, and less than 1% in 2021.](#) This represents a 92% decrease over the past twelve years. The months with highest PM-10 concentrations are December, January and February due to meteorological conditions² and increased use of wood burning stoves and fireplaces.



~~Although air quality is improving, air quality is only measured on 1/6 of the days in the year. The general criterion for non-attainment is one exceedance of the standard during a calendar year. Generally, the District must record no exceedances for three consecutive years to be considered in attainment for pollutants.~~

Significance criteria for air contaminants. The District publishes ~~significant~~ [Significance Thresholds for emission rates for](#) stationary sources of air contaminants (Regulation I, Rule ~~130110~~). Emissions are considered significant (defined in terms of tons emitted per year) if a new or modified stationary source exceeds the values shown in Table AQ-4. There are no established significance criteria for mobile sources of emissions, but large projects (such as residential subdivisions and shopping centers) can be compared with stationary source criteria to identify the cumulative impacts of many mobile sources such as motor vehicles.

North Coast Unified Air Quality Management District Particulate Matter (PM-10) Attainment Plan. As required by the California Clean Air Act, the District adopted a ~~Particulate Matter (PM10) Attainment Plan (Plan)~~ in 1995 to identify major PM-10 sources and develop and implement control measures to meet state ambient air quality standards. The District's ~~attainment plan~~ [Plan](#) established goals to reduce PM-10 emissions and ~~eliminate~~ [reduce the number of days in which standards are exceeded](#) PM-10 levels to meet the [California Ambient Air Quality Standards](#). Exceptions are made for uncontrollable events such as wildfires, structure fires, and unusually high winds. The plan includes three areas of recommended control strategies to meet these goals: transportation, land use, and burning. The table below identifies the categories of measures included in each control strategy. The draft PM-10 attainment plan developed by the North Coast Unified Air Quality Management District was adopted May 11, 1995. [Since that time, Del Norte and Trinity have reduced PM-10 to attainment status. Humboldt is still designated non-attainment.](#)

TABLE AQ-3 EMISSIONS SOURCES AND CONTRIBUTION TO VARIOUS POLLUTANTS

SOURCE TYPE	EXAMPLES OF EMISSION CONTRIBUTORS				
Industrial	pulp mills, sawmills, power plants, other heavy industry				
Commercial	gas stations, restaurants, dry cleaners, body shops, etc.				
Residential	home heating, residential open waste burning, solvent/ paint use, lawn equipment etc.				
Mobile	cars, planes, trains, road dust and other transportation sources				
Agriculture and Forestry	forest management burning, field burning, herbicide use, etc.				
POLLUTANTS	INDUSTRIAL	COMMERCIAL	RESIDENTIAL	MOBILE	AGRICULTURAL
Nitrogen Dioxide	17.2%	1.0%	3.0%	78.8%	0%
Carbon Monoxide	7.1%	2.0%	4.1%	46.9%	39.8%
Sulfur Dioxide	59.0%	1.0%	1.0%	39.0%	0%
PM-10	13.1%	7.1%	6.1%	58.6%	15.2%
Total Organic Gases	7.1%	47.5%	6.1%	30.3%	9.1%

TABLE AQ-4 SIGNIFICANT EMISSION RATES FOR STATIONARY SOURCES (TONS/YEAR)

CONTAMINANTS	TONS PER YEAR
Carbon Monoxide	100
Nitrogen Oxides	40
Sulfur Dioxides	40
Particulate Matter (PM-2.5)	2510
Particulate Matter (PM-10)	15
Ozone	40
Various Other Contaminants [1]	0.0004 to 10

Source: Regulation I of the District, Rule [130110-Definitions\(2015\)](#)

[1] Other contaminants include lead, asbestos, beryllium, mercury, vinyl chloride, fluorides, sulfuric acid mist, hydrogen sulfide, and reduced sulfur compounds. Contact District for detailed information on emission rates and significance criteria.

TABLE AQ-5 District CONTROL MEASURES

CATEGORY	CONTROL MEASURES
Transportation	Public transit, rideshare programs, park and ride lots, vehicle buy back and smoking vehicle programs, traffic flow improvements, bike routes.
Land Use	Pedestrian and transit oriented development, walkable communities, integration of land use and transportation planning.

Burning	Residential open waste burning restrictions, conventional fireplace replacements, improved woodstoves, new development requirements, woodstove curtailments on high smoke days, education, and home weatherization.
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The control measures described above are included in the District's PM-10 Attainment Plan and provide additional measures to reduce air pollution emissions. The District has existing control measures for commercial, non-residential burning, industry, forestry and agricultural burning, and construction. These measures are not included in the attainment plan because emissions reductions resulting from them are already reflected in the air quality monitoring. This element's objectives and policies include many of the District's [PM-10 Attainment Plan's](#) control measures, particularly for transportation and land use planning.

4.8 POLICIES

The Air Quality Element includes the following policies:

- AQ-1 Reduce Point and Area Sources of Air Pollutants
- AQ-2 Reduce Mobile Sources of Air Pollutants
- AQ-3 Regional Air Quality Standards, Monitoring, and Education
- AQ-4 Odor

POLICY AQ-1 POINT AND AREA SOURCES OF AIR POLLUTANTS

Objective. Improve air quality by reducing emissions from stationary point sources of air pollution (e.g., equipment at commercial and industrial facilities), and stationary area sources (e.g., wood-burning fireplaces and gas-powered lawn mowers), which cumulatively emit large quantities of emissions.

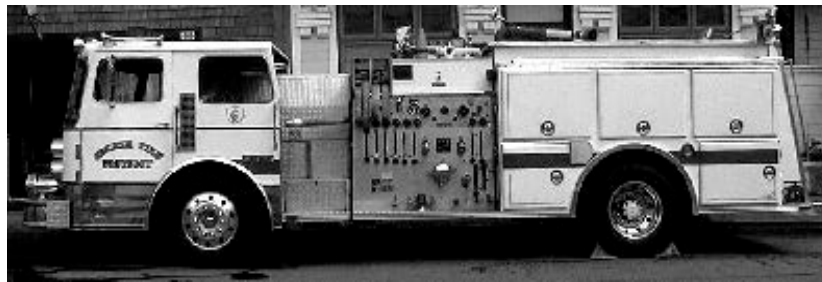
AQ-1a Reduce emissions from stationary point sources: commercial and industrial.

Coordinate with energy providers to develop incentive programs encouraging the use of less polluting, energy efficient designs and equipment in commercial and manufacturing uses. Encourage commercial and industrial uses to self-enforce emissions reductions by maintaining and repairing equipment, correcting leaks, installing control devices, and minimizing accidental releases. Coordinate with the District to establish buffer zones between point sources and the public, particularly sensitive receptors such as schools, hospitals, and convalescent facilities.

AQ-1b Reduce emissions from stationary area sources: residential, commercial, and industrial. Limit wood-burning fireplace installations in new construction to low -

emitting, State and EPA certified fireplace inserts or woodstoves, pellet stoves, or natural gas fireplaces. New construction retrofits must comply with energy efficient construction codes to reduce energy consumption including high-efficiency windows, water heaters, and furnaces. [Prohibit Woodburning and Gas hearths and Fireplaces in New Residential Development.](#)

AQ-1c **Coordination between [the District](#) and Arcata Fire Protection District.** [The City will encourage](#) Arcata Fire Protection District officials ~~shall to~~ coordinate with the District to develop procedures for identifying, monitoring, and informing the public of high pollutant incidents related to fires and accidental or intentional releases of toxic or unknown materials. Coordination should encompass current air quality levels, meteorological conditions (stagnant air), prevailing wind directions, location of nearby sensitive receptors, potentially affected land uses, and types of potential toxic materials. Coordination and required permits are particularly important during the planning and implementation of controlled burns.



AQ-1d **Review of development projects for emissions reductions.** Evaluate new construction plans to reduce point and area sources of pollution. Consult with the District during the environmental review process to ensure that:

1. Air quality impacts of development projects are assessed using analytical methods and significance criteria for emission rates approved by the District.
2. Air quality mitigation is feasible, workable, monitorable, and cost effective.
3. Impacts of projects that may be individually insignificant, but cumulatively significant are minimized or mitigated.
4. Innovative measures are incorporated into the project design to reduce air quality impacts.
5. [The City shall require air filtrations systems at new sensitive receptor buildings to be designed and constructed with air filters rated at a minimum efficiency reporting value \(MERV\) 13 or higher.](#)

Encourage the District to enforce these measures and their related policies.

POLICY AQ-2 MOBILE SOURCES OF AIR POLLUTANTS

Objective. Improve air quality by reducing emissions from transportation sources, particularly motor vehicles, and other mobile sources. Reduce vehicle miles of travel and encourage shifts to alternative modes of travel.

AQ-2a Implement land use measures to reduce vehicle trips, miles traveled, and air pollutant emissions. Implement or encourage the land use and development measures which reduce motor vehicle travel as outlined in the Transportation Element. These measures are also effective in reducing mobile sources of air pollutants.

AQ-2b Implement transportation measures to reduce vehicle trips, miles traveled, and air pollutant emissions. Implement or encourage the following measures to reduce vehicle miles traveled and provide alternatives to the single occupant motor vehicle, as outlined in the Transportation Element.

1. Provide as direct and safe a travel route as possible for all travel modes.
2. Implement and support public education programs explaining the negative impacts of single occupant vehicle use, and encourage the development of employer-based measures to reduce employee automobile travel.
3. Require A&MRTS and encourage other fleet operators to convert vehicles to run on less polluting alternative fuels at the earliest feasible time (See Policy RC-8a).



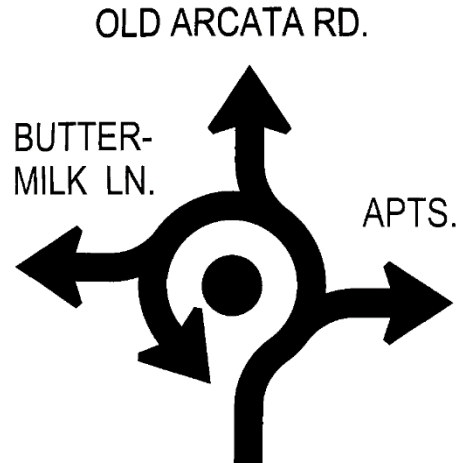
AQ-2c Reduce or minimize the creation of “hot spots” or localized places of concentrated automobile emissions. Implement or encourage the following measures to reduce hot spots, which occur where groups of vehicles are required to idle (e.g., at congested intersections, driveways and drive-through facilities).

1. Minimize the delay and congestion at unsignalized and signalized intersections to reduce emissions from idling vehicles. Attempt to achieve this through reducing automobile travel, minor capacity improvements, or fine-tuning of intersection operations. Discourage major capacity improvements at intersections, minimize new signalized intersections, or any other improvement which discourages walking, bicycling, or transit use.
2. Minimize or restrict land uses with drive-through facilities located in areas of concentrated traffic or near congested intersections.
3. Construction of projects with large parking lots or high volume driveways shall identify traffic impacts and provide evidence that project design will optimize internal circulation and minimize delay. Ensure that mitigation measures

balance the needs of automobiles, pedestrians, bicyclists, and transit riders.

AQ-2d **Design Arcata's highest traveled arterials to minimize stopping.** Recognize that automobiles are most efficient and less polluting at constant, moderate speeds between 25 and 35 miles per hour. Minimize idling delay, excessive congestion, and excessive speeds with the following measures:

1. Encourage Caltrans to coordinate traffic signals on Samoa Boulevard to maximize progression.
2. Eliminate traffic bottlenecks with traffic flow improvements (such as re-allocating turning lanes, or converting all-way stop control to roundabouts or two-way stop control), without impacting the safety of pedestrians, bicyclists, or transit facilities.
3. Review access plans for commercial driveways to ensure designs minimize idling vehicles and concentrations of traffic. For larger projects require multiple driveways rather than single driveways and consider turn restrictions where delays to existing driveways could be significant.
4. Encourage and support law enforcement's efforts to expeditiously manage traffic incidents.



AQ-2e **Recognize that poor air quality is caused by the combination of high pollutant emissions and meteorological conditions that do not allow for dispersal of pollutants.** The City shall coordinate a joint effort with the District to minimize the impact of high pollutant incidents and notify the public about meteorological conditions that contribute to poor air quality. The joint effort shall include employing the following measures:

1. Implement added air pollution control measures during predictable meteorological events of stagnant air. Inform the public of high pollutant incidents and encourage measures which minimize impacts, such as limiting use of wood-burning fireplaces, gas powered equipment, and avoiding non-essential vehicle travel.
2. Promote and encourage employer-based [and project-based](#) Transportation Demand Measures (such as subsidized bus fare, flexible work hours, [indoor bike storage](#), and incentives to carpool) to reduce automobile travel, particularly during periods of poor air quality.
3. Support and encourage local industrial and commercial efforts to reduce emissions and particulate pollution from industrial plants and trucks, particularly during periods of poor air quality.
4. Require traffic and construction site dust control measures at construction

projects. Require measures which reduce emissions from construction activity and maximize efficiency of traffic flow during inversion conditions.

AQ-2f Enforce air quality control measures and monitoring at construction sites.

Construction emissions shall be controlled because, although they are temporary in nature, they can often be the greatest air quality impact of a project. Require the following dust and erosion control measures for construction activities ~~when necessary:~~

- ~~1. Water all active construction areas twice per day and use erosion control measures to prevent water runoff containing silt and debris from entering the storm drain system.~~
- ~~2. Cover trucks hauling soil, sand, and other loose material.~~
- ~~3. Pave, water, or apply non-toxic soil stabilizers on unpaved access roads and parking areas.~~
- ~~4. Sweep paved access roads and parking areas daily.~~
- ~~5. Sweep streets daily if visible material is carried onto adjacent public streets.~~

~~For larger construction sites (four acres or greater) require the following measures when necessary in addition to those above:~~

- ~~6. Hydroseed or apply non-toxic soil stabilizers to inactive construction areas.~~
- ~~7. Enclose, cover, water, or apply non-toxic soil binders to open materials stockpiles.~~
- ~~8. Limit traffic speeds to 15 mph on unpaved access roads.~~
- ~~9. Install erosion control measures to prevent silt runoff onto public roadways.~~
- ~~10. Replant vegetation in disturbed areas within 30 days after project completion.~~

~~For construction sites near sensitive receptors, require the following measures when necessary, in addition to those above:~~

- ~~11. Install wheel washers for exiting trucks, or wash all equipment leaving site.~~
- ~~12. Install wind breaks, or plant trees/vegetation at windward sides of construction areas, or avoid removing existing vegetation which acts as a windbreak.~~
- ~~13. Suspend excavation and grading activity when winds exceed 25 mph.~~
- ~~14. Limit area subject to excavation, grading, and other construction activities at any one time.~~

AQ-2g Enforce air quality control measures and monitoring for agricultural operations. Air emissions from agricultural operations, including field burning, airborne soils, and over-spray from herbicide applications, shall be controlled and monitored through air quality standards, as well as adherence to the Land Use Code.

POLICY AQ-3 REGIONAL AIR QUALITY STANDARDS, MONITORING, AND EDUCATION

Objective. Participate in regional efforts to improve and monitor air quality and meet air quality goals, coordinate transportation and land use development planning with the North Coast Unified Air Quality Management District, and educate the public.

AQ-3a Air quality standards and monitoring. Identify potential emission sources of airborne toxins from mobile and stationary sources. This may be in coordination with the California Air Resource Board and the District, as appropriate. Enforce rigid high standards to restrict fumes, smoke, dust, or other environmental pollutants from stationary sources of pollution.

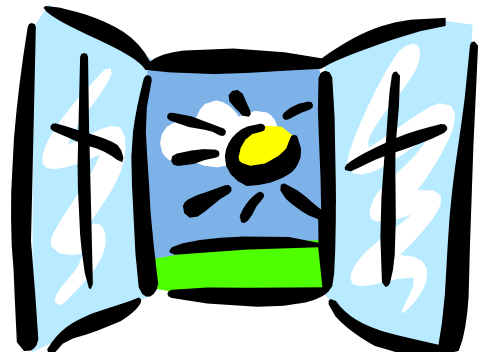
AQ-3b Develop and distribute material to educate the public on air quality issues. Work with Humboldt State University, the California Air Resources Board, and the District to develop educational material regarding air quality, impact of air quality on people, plants and animals, and what citizens can do to improve air quality. The City will make this information available.

AQ-3c Cooperation in enforcement activities and programs. Cooperate with the District in implementing and enforcing the district's rules and programs. Consider joint implementation of programs between the City and the district such as:

1. A voluntary wood-burning-devices dryness certification program.
2. Free cordwood moisture checks.
3. Brochures on wood burning.
4. Conversion of conventional wood burning devices to EPA certified devices.
5. Use of district non-compliance funds for low-cost replacements.

Develop ~~stricter land ordinances~~ [use ordinances and](#) guidelines, ~~and development agreements~~ for new residential development to limit wood burning devices. Use ~~District techniques strategies~~ to identify improper wood burning device use, improperly dried fuel, and faulty equipment, and provide education to violators or take enforcement action.

AQ-3d Indoor air pollution. Factors such as sealed building interiors, inadequate ventilation, non-openable windows, and use of building materials that release toxic substances contribute to indoor air pollution. To maximize indoor air quality, the installation of openable windows and adequate ventilation systems, the use of pollution-reducing houseplants, as well as the selection of non-toxic building materials and interior finishes, is encouraged in all new buildings and in the retrofitting of existing buildings. ~~The City shall maintain a list of non-toxic building materials and interior finishes, provide available information about building techniques and designs that reduce or eliminate indoor air pollution, and encourage a good~~



~~faith effort by private industry to use those materials and techniques.~~

[AQ-3e Greenhouse gas reduction. Many of the chemicals of concern for air quality are also greenhouse gases. The City shall work locally and regionally to develop plans and programs to reduce or reverse greenhouse gas emissions to meet State and Federal greenhouse gas reduction goals.](#)

POLICY AQ-4 ODOR

Objective. Minimize public exposure to noxious odors from industrial, manufacturing, processing, and food and beverage production operations.

AQ-4a **Odor controls.** Identify potential sources of noxious odors and regulate those sources to avoid adverse affects on adjacent sensitive receptors. Noxious odors are defined as foul smelling airborne emissions that are sufficiently concentrated to cause physical discomfort to those inhabiting adjacent areas. Regulations imposed to reduce effects of these odors shall include limiting hours for odor emissions, periodic monitoring, and filtering to reduce concentrations.

4.9 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
AQ - 1	Air quality education and air emissions reduction programs Continue monitoring local air quality and setting high standards for air quality. The City, including the Fire Department, shall work with the District to establish an air quality monitoring station in Arcata.	District	Ongoing
AQ - 2	Funding sources for wood-burning appliance retrofits for low income and elderly Research and apply for grant funding for qualifying low-income and elderly households to retrofit wood-burning appliances that have high emission rates.	Community Development Dept.	Ongoing
<u>AQ-3</u>	<u>Educational materials</u> <u>The City shall maintain a list of non-toxic building materials and interior finishes, provide available information about building techniques and designs that reduce or eliminate indoor air pollution, and encourage a good-faith effort by private industry to use those materials and techniques.</u>	<u>Community Development Dept</u>	<u>Ongoing</u>
<u>AQ-4</u>	<u>Climate Action Plan</u> <u>Adopt a Climate Action Plan and collaborate regionally to implement the plan.</u>		<u>Year 1 and ongoing</u>

<p>AQ-5</p>	<p>Implement Construction Screening and Reduction of TACs Emissions The City shall require applicants for development projects that would be located within 1,000 feet of an existing sensitive receptor to identify the construction schedule and, for projects that would require more than 12 months of construction, implement the following minimization measures to reduce potential TACs emissions. Measures to reduce risk from construction emission may include, but are not limited to: – During construction, use construction equipment rated as US EPA Tier 4 Interim for equipment of 50 horsepower or more. – During construction, use construction equipment fitted with Level 3 Diesel Particulate Filters for all equipment of 50 horsepower or more.</p>	<p>Community Development and Engineering Departments</p>	<p>Year 1</p>
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Endnotes

1. For further information on the health effects of PM-10 see the California Air Resources Board publication "Facts about Air Pollution and Health" (ARB Publications Department); the EPA document [National Air Quality and Emissions Trends Report](#) (EPA Office of Air Quality Planning and Standards, Research Triangle Park, NC); the Air Resources Board December 1982 publication [California Ambient Air Quality Standards for Particulate Matter \(PM-10\)](#); Federal Register Vol. 62, No. 138, 1997 for information on PM-2.5; and Health & Environment Digest Vol. 10, No. 4 "Airborne Particulates: A Deadly Public Health Concern."

Attachment: F. Ch 4 Environment (4214 : GP #XVII FBC & GP wrap-up)

HEALTH ELEMENT

2.10 INTRODUCTION

Health Element Organization. The City’s role in supporting health in Arcata is aiding in the creation of physical and social environments that afford its residents the greatest possible opportunities for wellbeing. This charge includes the provision of safe and affordable housing and transportation, as well as opportunities for economic prosperity, physical activity, recreation, education, and community connection.

While this Health Element was developed for the General Plan 2045 the City of Arcata’s recognition of its responsibility to positively affect health and wellbeing in its jurisdiction is not new. Decades of City policy codify the City’s continuous commitment to acting in service of community health. For example, the 1976 *Physical Design Recommendations for Achieving Community Goals Social Goals* report establishes objectives such as: provide for a healthful environment, provide for personal safety, provide for a socially diverse community, promote a sense of community membership and social worth, and more—goals that are reflected and updated in the policy priorities of this Element.

Many of the City’s responsibilities that support health are also detailed in other General Plan Elements (including Housing, Infrastructure and Public Facilities, Transportation Open Space, and Parks and Recreation), and language related to health is integrated throughout them. However, to prioritize Arcatans’ wellbeing, this Element is dedicated to health-specific goals and policies.

This Element also focuses on health equity. Disparities in health and wellbeing outcomes correlate with lower socioeconomic status, race and skin color, and disability status. Some residents face disproportionate barriers to health and wellbeing due to these factors, which should not be determinants of health. This Element includes policies to reduce inequitable health outcomes.

Overview of health and wellbeing in Arcata. Health is defined by the World Health Organization as a state of complete physical, mental and social wellbeing rather than merely the absence of disease or infirmity. Health is shaped by the cultural, social, political, economic, ecological, and physical settings in which we live, work, and play. These “social determinants of health” dictate daily experiences, physical and emotional wellbeing, how long we live, and our ability to change the quality and course of our lives.

“Health equity” refers to efforts to ensure that all people have full and equal access to opportunities that enable them to lead healthy lives while respecting differences in identity. These differences include, but are not limited to, culture, language, race, gender, sexuality, economic status, citizenship, ability, age, and religion.¹

Arcata is home to Mad River Hospital, one of ~~four~~ ~~the two~~ hospitals in Humboldt County. The community is also served by Open Door Clinic, United Indian Health Services (UIHS),

¹ <https://www.ci.richmond.ca.us/ArchiveCenter/ViewFile/Item/6999>

approximately 8 dental providers,² and mental health care providers offering a range of therapy and counseling services. Furthermore, there are many businesses and organizations in Arcata that support physical health and community gathering—including gyms, yoga studios, bike shops, and running clubs.

Some neighborhoods in Arcata have a disproportionate concentration of barriers to healthy living. The Valley West neighborhood specifically is disconnected from many of Arcata’s assets in terms of healthy living. Compared to those nearer to Arcata’s downtown, Valley West residents have decreased access to safe open spaces and healthy food options.

The City of Arcata envisions a community that is a safe and healthy place to live, work, play, and learn. This Health Element contains policies intended to ensure that Arcata’s economy, education, health services, built environment, and social context all support the physical, mental, and emotional wellbeing of all its residents.

Barriers to health and wellbeing in Arcata. As in many rural areas across the country, Humboldt County residents face barriers to accessing affordable medical, dental, and mental health care services. One of the primary obstacles to increasing the availability of these services is Arcata’s lack of affordable housing, which impacts the security and mental health of Arcata residents as well as the community’s ability to retain health professionals. The city’s housing shortage also affects the mental and physical health of its residents; when a household spends a disproportionate amount of its monthly income on housing costs, it impacts their ability to purchase medications, healthy foods, and other items important to health and wellbeing.

Among other consequences, Arcata’s lack of accessible care providers prevents many residents from accessing necessary medical and dental services (much less seeking preventative care, such as cancer screenings). Similarly, the lack of mental health care resources means that some Arcatans with diagnosed mental health disorders are unable to get necessary medication due to the inaccessibility of local psychiatrists able to write prescriptions.³

As there is specific difficulty in finding a primary care provider, dentist, or mental healthcare professional that has availability for new patients, access to care is an issue that disproportionately affects students and other Arcata residents recent to the area.

In addition to the challenges faced in seeking healthcare, many Arcata residents struggle to afford it. Census data shows that the number of Arcatans living below the poverty line is far higher than the state average. Beyond the difficulty poverty presents in accessing health services, economic insecurity inherently results in mental and physical stress. Poverty and low-income status are associated with various adverse health outcomes, including shorter life expectancy and higher death rates.⁴

² <https://findadentist.ada.org/search-results?address=Arcata,California&distance=5>

³ Racial Equity Working Group (2023)

⁴ <https://www.aafp.org/about/policies/all/poverty-health.html>

The limited transportation options between Arcata, Eureka, and McKinleyville also make it difficult for Arcata residents who do not own reliable personal vehicles to get to appointments in neighboring communities or outside the area.⁵ As in many rural areas nationwide, Humboldt County residents frequently have to travel hundreds of miles for medical or dental appointments. This contributes travel expenses to the cost burden faced by those seeking care.

Another barrier to health in Arcata faced by those with lower incomes and limited transportation options is access to healthy foods. Healthy foods are usually more expensive, and it is difficult for residents that do not live near downtown to access places that sell them.

Low-income status also makes it difficult to access stable and safe housing, especially in Arcata's high-cost, high-demand rental market. Living unsheltered poses difficulties to maintaining a healthy diet, securing/showing up for medical appointments, and properly storing medication. For those with conditions that require daily management and attention—such as diabetes—this can make living unhoused immediately life-threatening.⁶

Racism and discrimination present another barrier to health within the Arcata community. The negative health impacts of racism present on individual levels—such as stress and housing discrimination—as well as structural levels, meaning differential access to goods, services, and opportunities.⁷

Assets to health and wellbeing in Arcata. Health and wellbeing in Arcata are benefitted by the city's unique natural resources and the community's commitment to protecting them. Open space lands support public health by providing accessible and low-cost public recreation opportunities to all, regardless of income. Furthermore, they allow for community gardens which can increase access to fresh local produce.

Arcata's small-town nature and the time and energy that its residents put into building community also support health. Loneliness and social isolation have been linked to increased risks of premature death, dementia, heart disease, and stroke, as well as higher rates of depression, anxiety, and suicide.⁸ The wide variety of community classes, recreation activities, events, and other programming offered by the City and other local organizations expands and deepens community connections in Arcata by bringing together people of shared interests. These connections benefit residents of all ages, but especially Arcata's seniors; the CDC has found that many adults aged 50 and older are socially isolated or lonely in ways that put their health at risk.⁹

Furthermore, Arcatans' health and wellbeing is supported by Cal Poly Humboldt and the educational, social, and cultural benefits that the university brings to the community. Lifelong learning has been linked to cognition and memory, social connection, and self-

⁵ Racial Equity Working Group (2023)

⁶ Open Door Community Health Centers

⁷ [What is Health Equity? | Health Equity | CDC](#)

⁸ <https://www.cdc.gov/aging/publications/features/lonely-older-adults.html>

⁹ <https://www.cdc.gov/aging/publications/features/lonely-older-adults.html>

esteem.¹⁰ The opportunities for learning that the university gives to the community—through events, community classes, and the perspectives of its students, faculty, and staff—set Arcata apart from other towns of its size.

Finally, the nonprofit organizations and community groups working to make Arcata a more equitable place to reside—decreasing violence and discrimination related to race, class, gender identity/expression, and sexual orientation and increasing support and opportunities for all Arcatans—are a benefit to the health of the community.

Guiding Principles and Goals.

- A. Ensure all residents can enjoy physical, social, and mental health and wellbeing, regardless of their race, age, gender, sexual orientation, social position, economic position, culture, background, or any other factor of identity.
- B. Understand that a community’s socio-economic, built, and natural environments influence every resident’s health. Recognize both the integrated nature of these systems¹¹ and the City’s responsibility in ensuring that its actions promote physical, social, and mental health and wellbeing.
- C. Partner with and promote local agencies, nonprofits, and community-based organizations that are already working to support the health and wellbeing of Arcata residents.
- D. Eliminate the disproportionate and unjust barriers to health that some Arcata residents face based on neighborhood and/or factors of identity.
- E. Support healthy eating and active living programs and facilities to enable them.
- F. Increase opportunities for community engagement.

2.2 POLICIES

The Health Element contains the following policies areas.

- H-1 Community Health
- H-2 Healthy Behaviors
- H-3 Community Culture
- H-4 Healthy Government

POLICY H-1 COMMUNITY HEALTH

Objective. Increase accessibility of medical and mental healthcare for all Arcatans. Prioritize groups experiencing differential health outcomes.

- H-1a **Determinants of health.** Work with the community and stakeholders in medical, mental, and dental healthcare fields to identify determinants in the Arcata community that impact health and wellbeing, paying specific attention to factors

¹⁰ <https://extension.usu.edu/mentalhealth/articles/the-benefits-of-being-a-lifelong-learner>

¹¹ Racial Equity Working Group (2023)

- affecting specific groups or demographics disproportionately. Facilitate access to financial and professional resources to document and utilize necessary epidemiological and health care data.
- H-1b **Access to services.** Work with stakeholders in medical, mental, and dental healthcare fields to identify barriers to healthcare access and affordability. Explore ways in which the City can aid in reducing those barriers. Work with community partners to aid individuals and families in accessing health resources.
- H-1c **Health equity.** Strive for health equity by identifying and reducing barriers to health faced disproportionately by vulnerable groups within Arcata, including those with lower socioeconomic status as well as children, the elderly, persons with disabilities, and Limited English Proficiency (LEP) individuals.
- H-1d **Support unhoused community members.** Increase City coordination with/support for organizations that provide resources and health services those living unsheltered in Arcata. Work with partners to aid unhoused individuals and families in accessing health-supporting resources such as insulin and medication.
- H-1e **Retain existing healthcare institutions and resources located in Arcata.** City policies should strongly support retention of existing healthcare institutions, facilities, and services located in Arcata, including Mad River Hospital, Open Door Clinic, United Indian Healthcare, and other more specialized medical practices.
- H-1f **Land use regulation to support health care.** Amend land use regulations to support provision and accessibility of healthcare services, including locations of healthcare providers in multiple-use and commercial land use classifications.

POLICY H-2 HEALTHY BEHAVIORS

Healthy Behaviors

Objective. Develop and support systems conducive to physical, mental, and social wellbeing. Design an environment in which the healthiest choice is the easiest choice.

- H-2a **Daily physical activity.** Ensure that residents of all neighborhoods, ages, incomes, and abilities have access to opportunities for daily physical activity. Develop convenient access to safe and attractive outdoor spaces and places throughout the city. Make access to spaces, events, and programs that support physical activity affordable and convenient.
- H-2b **Opportunities for healthy eating.** Ensure-Support City and community programs that improve opportunities for nutritional health, food security, and healthy eating among residents of all races, neighborhoods, ages, genders, incomes, and abilities

- ~~have opportunities for healthy eating.~~
- H-2c **Opportunities for community connection.** Create opportunities to strengthen the sense of community in Arcata through connection, relationships, and trust. This includes expanding offerings of community classes and events, as well as increasing their accessibility and affordability. Engage Arcata residents to identify actions the City can take to help define, share, and support community expressions of Arcata culture.
- H-2d **Commercial tobacco.** Reduce the use of commercial tobacco. Commercial tobacco use is linked to asthma, cancer, COPD, diabetes, tooth loss, heart disease, stroke, and birth outcomes. The City shall prioritize the need of non-smokers to breath smoke-free air in public places.
- H-2e **Alcohol misuse.** Reduce the misuse of alcohol and the prevalence of alcohol-related accidents and injuries, paying specific attention to groups that may be at higher risk for alcohol misuse. Alcohol misuse is linked to injuries, violence, unsafe sexual behavior, adverse birth outcomes, blood pressure, heart disease, stroke, liver disease, cancer, and decreased emotional wellbeing. The City shall consider the impact of land use [and transportation](#) policies, [including public transit service access, availability, and frequency near alcohol serving establishments](#), and programs on alcohol misuse.
- H-2f **Substance use disorders.** Humboldt County's rate of drug overdose deaths is higher than the State and National rates. The City shall work with partners to reduce the prevalence of substance use and overdose deaths in Arcata, increase support for those struggling with substance use, and decrease stigmas. Feeling stigmatized can reduce the willingness of individuals struggling with substance use disorders to seek treatment and increase the social isolation of individuals using drugs.¹²

POLICY H-3 COMMUNITY CULTURE

Objective. Encourage a healthy, cohesive community culture. The strength of relationships and the sense of shared identity between members of a community are linked to emotional wellbeing, safety, and higher levels of investment in health-promoting community resources. This section outlines desired community cohesion outcomes and addresses how the City can support and contribute to a strong, diverse, and collective Arcata identity.

- H-3a **Healthy and equitable schools.** Work with schools to improve and or sustain programs that support mental, physical, emotional, and social wellbeing. Childcare and school environments influence the foods children eat and drink outside the

¹² <https://nida.nih.gov/nidamed-medical-health-professionals/health-professions-education/words-matter-terms-to-use-avoid-when-talking-about-addiction>

home, rates of physical activity, and social and emotional development. Educational attainment is also linked with lifespan. Increase the ways in which Arcata's schools contribute to students' health and wellbeing through:

1. Integrating physical activity into students' daily educational experience through both programming and the design of school sites and structures, and linkages with City facilities.
2. Integrating the arts into students' daily educational experience.
3. Supporting walking and biking to school.
4. Ensuring students have access to healthy foods on school sites.
5. Leveraging schools to remove mental health and safety barriers to physical activity among youth.
6. Encouraging schools to ensure that classrooms and curriculums integrate and dialogue with historically excluded knowledge sources, empower and support students of all races, ethnicities, and cultures, and represent/appreciate all cultures respectfully.

H-3b **Healthy workplaces.** The City shall seek to support resources to increase opportunities for healthy behaviors on workplace sites. Support walking and biking to work. Increase opportunities for racial equity training in workplaces. Increase opportunities for cross-agency/industry professional development training, which provide Arcata's professionals continuing educational opportunities in leadership and facilitate community connections within/between workplaces.

H-3c **Support Arcata arts & artists.** Support artists, arts organizations, and arts endeavors as a means for building and healing community in Arcata. Arcata's artists lead the way in welcoming and celebrating diversity, supporting local businesses, caring for community health & wellness, bringing residents into community with one another, fostering pride of place, igniting community conversations, and much more.

H-3d **Community consciousness of racial equity.** Encourage community dialogue around historic and current systems of advantage and exclusion. Cultivate a collective consciousness of equity and inclusion. Work to make Arcata a Welcoming, safe, and racially equitable community by working to erase racial disparities for all people of color in all aspects of community life including housing, health, education.

POLICY H-4 HEALTHY GOVERNMENT PRACTICES & PROTOCOLS

Objective. Engage and collaborate across City departments, partner agencies, local nonprofits, and the community to develop and achieve a collective vision for a healthy, equitable community. Elected officials and municipal staff play central roles in creating and implementing the policies, programs, and services in a long-range plan. This section addresses practices and protocols that the City of Arcata will implement to ensure the promotion of health and equity throughout its regular activities.

H-4a **Healthy and Equitable City operations.** Ensure City-wide knowledge of, and commitment to, core principles of diversity, equity, and inclusion, and continue to support the equity arcata network as a key partner in achieving the goals of this policy. Integrate trauma-informed practices into City operations and services. Utilizing trauma-informed practices involves considering the following six principles when interacting to provide programs, services, and activities:

1. Safety
2. Trustworthiness and transparency
3. Peer support
4. Collaboration and mutuality
5. Empowerment, voice, and choice
6. Factors such as race, culture, and gender identity

H-4b **Transparent and responsive government.** Increase government transparency and build trust among Arcatans of all ages, races, identities, and income levels. Demonstrate the City's credibility, reliability, openness, and community orientation through establishing a coordinated set of proactive channels for community-wide engagement between City departments, stakeholders, and the public. Respond promptly, reliably, and openly to criticism of City operation and administration from members of the public. Continue this engagement on an ongoing basis regardless of specific plans or projects underway. Evaluate implementation of plans, projects, and programs to ensure that established policies are being carried out. Monitor how implementation is serving different groups within the community ~~in comparison to each other,~~ and respond to community critiques.

H-4c **Health in all policies.** Identify and address existent inequities in City policy that are resulting in disparities, increase equity at the systems and structural levels. Build relationships with community partners to identify ways in which policies can take health outcomes into consideration. Implement procedures that allow the City to prioritize community health in all programs and practices. Educate the community and other stakeholders about Arcata's overall health goals and actions.

H-4d **Collaborative community planning.** Involve and collaborate with residents and community organizations in the development of plans, projects, and programs that affect their health. Establish a coordinated set of proactive, ongoing, and inclusive ways that City departments communicate with the public to discuss health-related issues and values. Make specific efforts to engage Arcata's young people in long-range community planning.

H-4e **Health data.** Establish metrics and develop/adopt tools for measuring community health. Partner with a network of organizations and agencies, including the County

- Departments of Public Health and Health & Human Services, to begin collecting community health data on an ongoing basis.
- H-4f **Funding for health.** Identify, pursue, and dedicate funding streams that support actions to improve community health outcomes. Incorporate health considerations into City budget and project funding decisions.
- H-4g **Youth engagement.** Build capacity among Arcata’s youth to advocate for healthy changes in their community.
- H-4h **Senior Engagement.** Support capacity and initiatives by Arcata’s senior citizens and residents to advocate for changes in Arcata’s infrastructure, programs, and City operations to help seniors remain healthy and active.

H-4i **Engage with Arcatans of multiple abilities.** Support capacity among Arcatans of differing abilities and disabilities to represent their interests and concerns to the City with regard to City infrastructure, programs, and operations that can improve possibilities for people of different abilities to pursue healthy, active, mobile lives in Arcata.

2.12 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
H-1	<p>Access to services</p> <ol style="list-style-type: none"> 1. Identify opportunities to cross-promote local health resources on the City website and in City facilities. 2. Identify opportunities for Arcata City Council and staff participation in coalitions and stakeholder groups addressing health within Arcata and the North Coast Region. <u>3.</u> Pursue partnerships to create centers equitably distributed throughout the City that provide resources to the community and makes residents feel connected, welcome, supported, and cared for. <u>3-4.</u> <u>The City shall work with existing healthcare institutions and medical practices in Arcata to retain services and improve access to them.</u> 	City Manager’s Office and Community Development Department	Ongoing

Attachment: F. Ch 4 Environment (4214 : GP #XVII FBC & GP wrap-up)

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
H-1	<p>Health equity</p> <ol style="list-style-type: none"> 1. Support the development of a Health Equity working group within Equity Arcata to coordinate with medical facilities and healthcare organizations. Encourage facilities to provide equity, diversity, inclusion, bias, and/or cultural competency training to all doctors, nurses, and support staff on an annual basis. 2. Encourage community partners that work frequently with Limited English Proficiency populations to collaborate with medical facilities and healthcare organizations to provide translated resources on healthcare, insurance, and mental health/dental/medical care. 2-3. <u>Routinely engage people knowledgeable about needs of people of varied abilities to ensure that City facilities, programs, and practices serve people of varied abilities and disabilities. Strive for Universal Design in all City facilities.</u> 	City Manager’s Office, Equity Arcata	Ongoing
H-2	<p>Opportunities for healthy eating</p> <ol style="list-style-type: none"> 1. Collaborate with food system partners, such as the Humboldt Food Policy Council, the North Coast Growers’ Association, Cal Poly Humboldt, the California Center for Rural Policy (CCRP), Arcata House Partnership, and the Humboldt County Department of Health & Human Services to strategize approaches the City can consider to help grow a healthy food system in Arcata. 2. Support local agricultural projects such as community gardens and farmers’ markets in areas of Arcata beyond downtown. Ensure zoning allows a range of urban agriculture activities in all districts with relevant operating standards. 3. Building off past efforts by the Food Policy Council and California Center for Rural Policy, collaborate with local partners to develop a community food assessment to analyze challenges and opportunities related to Arcata’s local food system. The assessment should pay particular attention to which neighborhoods and/or groups within Arcata may be lacking access to healthy food at higher rates. 3-4. <u>Work with existing commercial businesses to improve access to healthy food.</u> 	Community Development & Environmental Services	Ongoing

Attachment: F. Ch 4 Environment (4214 : GP #XVII FBC & GP wrap-up)

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
H-4	<p>Support Arcata arts & artists</p> <ol style="list-style-type: none"> 1. Implement the Arcata Strategic Arts Plan (ASAP) with intermittent re-evaluations of priority projects. 2. Identify and pursue sources of funding that could be used to support arts organizations and endeavors in Arcata. 3. Explore funding sources and possible locations for the development of artist housing/live-work spaces in Arcata. 4. Prioritize support of intersectional arts and equity projects/collaborations in Arcata area schools. 	City Manager's Office and Community Development	Ongoing
H-5	<p>Equitable City operations</p> <ol style="list-style-type: none"> 1. Provide equity, diversity, inclusion, bias, and/or cultural competency training to City staff and elected/appointed officials on a regular basis. 2. Continue supporting the Equity Arcata network. Alter job descriptions to include involvement with Equity Arcata in key City staff positions that integrate well with the collective's working groups. 3. Scope and initiate an inclusive, community-driven process to update the Draft Solution Set addressing current racial inequities in Arcata, and assessing progress made since its initial drafting. 	City Manager's Office - Personnel	Ongoing

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
H-6	<p>Community Engagement Protocol</p> <p>Building on the work that the Community Development Department undertook with the Institute for Local Government through the BOOST Program over the course of 2020, establish a Community Engagement Plan that outlines how the City engages community-based organizations, other partners and stakeholders, and the public on proposed plans, projects, and programs. Update the Plan and “Community Landscape” list periodically to ensure its continued relevancy.</p> <p>Ensure the following concepts are integrated into the Plan:</p> <ol style="list-style-type: none"> 1. Utilize the “Community Landscape” list in outreaching to identified stakeholders and community groups as appropriate. 2. Prioritize outreach to neighborhoods and populations facing disproportionate barriers to healthy living. Prioritize outreach to groups historically excluded from City and community planning processes, such as Black, Indigenous, Hispanic, Asian, LEP, and disabled residents. 3. Continue to pursue opportunities for partnership with Arcata schools (specifically high schools) to present on long-range planning projects and solicit student feedback. 3.4. <u>Respond promptly and openly to critiques of City operations and decisions, including those made through administrative channels, and especially to those engaging with City committees, the Planning Commission, and City Council.</u> 	Community Development Department	Every 10 years starting in year 2
H-7	<p>Health data</p> <p>As part of the City’s Annual General Plan Progress Report submitted to the State Office of Planning and Research, include applicable information from departments, residents, and other stakeholders about how improvements to support health, <u>or changes in programs, policies and services that threaten it</u>, have affected residents. Give specific attention to discrepancies faced by specific neighborhoods and groups.</p>	Community Development Department	Ongoing
H-8	<p>Health in all policies</p> <p>Develop criteria for health and equity project review. Integrate health and equity review into decision-making processes for policy updates and capital projects. Update these review criteria periodically to ensure continued relevancy.</p>	Community Development Department, City Manager’s Office	Every 10 years starting in year 2

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
H-9	<p>Discrimination-free, accessible housing</p> <p>Collaborate with agency partners to establish financial and other incentives for landlords and property managers that go through City-endorsed anti-discrimination trainings such as the Humboldt Tenant Landlord Coalition “Living in Community” Training.¹³</p>	Community Development Department	Ongoing

Key to following Implementation Measures

Measure will be integrated into:

Public Safety
Parks and Recreation
Circulation

PS-X	<p>Exposure to community violence and discrimination</p> <ol style="list-style-type: none"> Increase awareness around the Arcata Police Department’s Juvenile Diversion Program, the primary objectives of which are to reduce the number of youths in the Criminal Justice System and reduce dropout/expulsion rates in Northern Humboldt area school districts. Make implicit bias and cultural humility training available to Public Safety Committee members. Work with the County and other local social service agencies to reduce stress and provide services and support to groups found to be at higher risk of being victims or perpetrators of violence. Explore partnerships with programs, social service agencies, and community health advocates that work to deter, intervene in, and mediate violence in the Humboldt County community. Familiarize City staff that frequently interface with the public with these resources. 	Arcata Police Department	Ongoing
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¹³ Racial Equity Working Group (2023)

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
PS-X	<p>Community policing</p> <ol style="list-style-type: none"> 1. Continually publicize the training that APD officers complete on an ongoing basis related to racial equity, implicit bias, cultural humility, and interacting with individuals who have mental illnesses, intellectual disabilities, and substance use disorders. 2. Increase support for programs such as the Mobile Intervention and Services Teams (MIST), which allows Humboldt County Department of Health & Human Services (DHHS) Behavioral Health Branch staff to work with the APD in responding to incidents that involve people with mental/behavioral health issues and/or substance use disorders. 3. Increase support for programs such as the City of Arcata's Community Ambassador Program, which has a stated mission of welcoming all community members to Arcata businesses and inviting and guiding them to retail, restaurant, entertainment, recreational, and supportive options and services all while enhancing the beautification and safety of Arcata. 4. Raise awareness around the APD's internship program, available to Cal Poly Humboldt and College of the Redwoods students, which allows participants to collaborate on developing best practices in policing and to share information on student expectations of law enforcement in Arcata. 5. Raise awareness around the APD's ride-a-long program, available to any member of the community, which allows residents to see how APD delivers public safety services, meet APD staff, and ask questions. 6. Incentivize City staff and APD officers to engage with local cultural learning opportunities and equity trainings outside of those mandated in their line of duty. 	Arcata Police Department	Ongoing

Attachment: F. Ch 4 Environment (4214 : GP #XVII FBC & GP wrap-up)

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
C-X	<p>Safe, convenient, connected, and multi-modal transportation</p> <p>Ensure current versions of the Regional Transportation Plan, Humboldt Regional Bicycle Plan, ADA Transition Plan and Local Road Safety Plan:</p> <ol style="list-style-type: none"> 1. Focus pedestrian and bicycle transportation infrastructure improvements near primary areas of activity, including schools, town centers, commercial clusters, and workplace districts. 2. Ensure public health and equity considerations are included as part of transportation network improvement decisions, <u>including accessibility and mobility considerations for people of diverse abilities and disabilities.</u> 3. Plan capital improvements that will ensure sidewalks and other pedestrian routes are continuous, creating a cohesive network. Continue to assess existing sidewalks and bike lanes (or existent lacks thereof) throughout the City and identify those most in need of construction or improvement as part of yearly CIP planning. 4. Identify opportunities for the creation of a non-vehicular network of paved and/or unpaved trails, paths, or other travel-ways through blocks or open spaces to reduce travel distances and create safer routes for pedestrians and bicyclists. 	Engineering Department, Transportation Safety Committee	Ongoing
C-X	<p>Increase transportation accessibility</p> <ol style="list-style-type: none"> 1. Plan capital improvements that will remove physical barriers to and/or build facilities for walking and biking along transportation routes—especially those with higher volumes of pedestrians and bicyclists, routes known to have concentrations of people without vehicles, and along routes that provide access to major destinations including local schools. 2. Require new projects to build transportation network improvements for pedestrians, bicyclists, and wheelchairs on sites and along adjacent streets as part of all new developments/improvements (and especially those in areas where there are clusters of activity and/or existing transit stops). 	Engineering Department, Transportation Safety Committee	Ongoing

Attachment: F. Ch 4 Environment (4214 : GP #XVII FBC & GP wrap-up)

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
C-X	<p>Targeted improvements to existing transportation network</p> <ol style="list-style-type: none"> 1. Increase street lighting around Arcata to increase safety of walking/biking during early mornings and evenings, especially those with higher volumes of pedestrians and bicyclists, routes known to have concentrations of people without vehicles, school zones, and along routes that provide access to major destinations¹⁴ 2. When designing new or improving existing streets, implement complete streets policy to incorporate pedestrian and bicycle safety improvements. <u>Ensure that facilities and design promote and support mobility by people whose mobility is impaired, who move slowly, or who need mobility assistance.</u> Use cost-effective street improvements such as striping, cones, and temporary features to test improvements throughout the City in a cost-effective manner and address needs temporarily when funding does not permit a more permanent solution. 3. When designing new or improving existing streets, use traffic-calming techniques (such as narrowing traffic lanes or adding flashing beacons/radar signs) to improve street safety and access (especially in residential neighborhoods, school zones and areas with high pedestrian traffic). 3.4. <u>Consider developing City-operated traffic lights with appropriate signals for people with impaired vision, hearing, or mobility.</u> 	Engineering Department, Transportation Safety Committee	Ongoing
PR-X	<p>Increase use of public spaces</p> <p>In collaboration with community organizations, establish a calendar of City-sponsored programs and events taking place in public spaces, including offerings that encourage physical activity, educational opportunities, arts programming, and community outreach.</p>	Recreation Department Community Development Department	Ongoing
PR-X	<p>Natural features and built facilities</p> <p>Prioritize improvements to areas with comparatively less open space/public facilities access as part of the ongoing established public space maintenance schedule</p>	Parks and Recreation Department	Ongoing

¹⁴ Racial Equity Working Group (2023)

Health language proposed for other Elements:

Land Use

Land use and zoning that promote community health and wellbeing. Patterns of land use and development can either promote or deter physical, mental, and social wellbeing in different ways. Support patterns of land use and development that encourage physical, mental, and social wellbeing, as well as shape activity centers, districts, corridors, and neighborhoods to support walking, biking, and transit.

Transportation

Transportation network that promotes wellbeing. Create a transportation network that promotes physical, social, and mental wellbeing. Transportation networks consist of the routes that connect places and shape how people move around their communities. Prioritize the design of transportation infrastructure projects to support convenient and safe active transportation and affordable public transit. Recognize [the systemic barriers that many Arcata residents, specifically persons with disabilities, families with children, and lower-income Arcatans, face accessing that both active transportation and public transit is inaccessible for many Arcata residents, specifically persons with disabilities, families with children, and lower-income Arcatans.](#)

Increase transportation accessibility. Plan a transportation network that is accessible to persons with disabilities and Arcatans of all ages and income levels. Prioritize public transit in planning for Arcata's transportation network.

Parks & Recreation

Create a public space network that promotes physical, mental, and social wellbeing. Design an accessible public space network that promotes physical and mental health, contributes to a sense of community, improves air quality, and supports storm water management (see Open Space and Parks and Recreation Elements for more information).

Public spaces promote community connection, engagement with Arcata's natural environment, and physical activity. Research has also found that residents who live adjacent to green space have lower levels of illness and disease than people of similar income levels who do not, and that that social cohesion is partially dependent on residents' ability to connect through public spaces and facilities within them. Rates of physical activity, in turn, are linked to a range of health outcomes including emotional wellbeing, risk of mental disorders, body weight, blood pressure, heart disease, respiratory illness, and social interaction.

Ensure a diversity of public spaces. Plan for a variety of types of open spaces—including plazas,

parks, playgrounds, and amphitheaters/stages—that accommodate a variety of types of activities (formal, informal, structured, and unstructured) and promote physical activity and social interaction. Consider public arts, both visual (murals, sculptures, etc.) and opportunities for public performing arts (theater, music, etc.), in planning for new public spaces and in activating existing public spaces. Prioritize areas lacking in existing parks, facilities, and/or recreational opportunities in the development of new public spaces.

Resource Conservation & Management

“Greening” efforts. Prioritize “greening” efforts in Arcata’s urban spaces to help dampen ambient noise, cool pavement surfaces, and maintain high air quality. These may include the addition of street trees, community gardens, and green spaces.

Public Safety

Exposure to community violence and discrimination. Continue to ensure that the organizations and individuals aiding those who have experienced violence in the community are well-versed in how best to support the victims of hate crimes, whether they be racially motivated, or crimes committed with bias against an individual’s gender identity/expression or sexual orientation. All City departments share responsibility in providing consideration to groups facing exposure to violence at higher rates in Arcata as well as discrepancies in crime by neighborhood.

Community safety. Improve residents’ safety and perceptions of community safety through reducing crime and violence. Integrate Crime Prevention Through Environmental Design (CPTED) elements in street, open space, and development projects. Prioritize the needs of neighborhoods with higher crime rates.

Equitable policing. Continue to ensure that community members feel safe, protected, represented, and respected when interacting with law enforcement—regardless of race, gender identity/expression, or sexual orientation, and continue to invest in creating opportunities for mental and behavioral health experts to collaborate with the Department in responding to incidents involving mental illness/behavioral health and/or substance addiction. Increase awareness of the Arcata Police Department as a resource for residents who have experienced instances of hate or discrimination in the community.

Chapter 5 – Design and Historic Preservation

Design Element

Historic Preservation Element

DESIGN ELEMENT

Draft 05.23.23

Preface

~~The Design and Historic Preservation Design Elements are is not mandated a required element under the state California Pplanning law Guidelines. They are it is included in the Arcata General Plan because the issue due to the importance of community appearance, form, function and livability is important to Arcata. In addition to addressing community wide design features and criteria, the Design Element also is important for protecting on of City scenic and visual qualities of the City and those in the coastal zone as required by the California Coastal Act of 1976.~~

~~The Design Element is intended to guide form and functions of the city's future built environment while at the same time respecting the unique relationship to the natural environment. Design considers both preserving character defining neighborhoods, commercial areas, public spaces and buildings; and for continual adaptation and improvement of the built environment.~~

5.1 INTRODUCTION

~~The Design Element is intended to guide form and functions of the City's future built environment while at the same time respecting the unique relationship to the natural environment. Design considers both preserving character defining buildings and public spaces; and for continual adaptation and improvement of the built environment.~~

~~—The Design Element is not a required element under California Planning Guidelines. It is included in the Arcata General Plan due to the importance of community appearance, form, function and livability to Arcata. In addition to addressing community-wide design features and criteria, the Design Element is important for protecting City scenic and visual qualities and those in the coastal zone as required by the California Coastal Act of 1976.~~

~~The Design Element is intended to guide form and functions of the cityCity's future built environment while at the same time respecting the unique relationship to the natural environment. Design considers both preserving character defining buildings and public spaces; and for continual adaptation and improvement of the built environment.~~

~~**Overview of Arcata's Design Character.** The cityCity of Arcata's is located in a physical and cultural setting of on the California North Coast has great natural beauty is one of its most valuable assets. A sense of physical-Physical and visual separations from other communities is,~~

~~provided by extensive open space lands which surround the city. These include Arcata Bay, marsh landses, tidelands, and agricultural lands to the south; the agricultural lands of the Arcata Bottoms to the west; the Mad River and its agricultural floodplain to the north; and forested hills to the east. For residents and visitors alike, the aesthetic encounter with the landscape of the North Coast is presented along the State Route 101 corridor and several other principal roadways. From the south, the State Route 101 parallels and provides scenic views~~ Views to the marshes and open waters of Arcata Bay to the west and agricultural land with a forested coastal foothill backdrop to the east. This separation provides a distinct City image which is visible from many different vantage points. The City's built environment has evolved for more than 100 years and represents several architectural themes and forms. The sense of scale and "fit" of ~~the Arcata townscape~~ complements ~~this the~~ scenic rural coastal environment.

The ~~overall urban form of the city~~ City's consists of a dense and compact downtown urban core centered around the ~~and~~ Plaza ~~and are downtown, with a series of~~ surrounded by varied and dispersed residential ~~many smaller commercial centers and distinct city~~ neighborhoods extending into the hills and bordering agricultural areas. These mixed-use areas and neighborhoods provide ~~residents with~~ a range of housing, working and recreational environments and a direct n immediate relationship to natural areas. ~~Several interspersed~~ There are still some agricultural areas accentuate the juxtaposition of town and country within City limits. Much of ~~the Arcata's~~ character ~~of Arcata~~ is derived from the architectural styles of its buildings, particularly ~~of~~ older historical residences and commercial buildings near the City's center. ~~For the most part, buildings are small in scale and only a few buildings are three stories or more in height.~~

The Element plans for continually integrating public spaces for informal and organized community gatherings, with art and other visual amenities, where residents and visitors can engage in both active and passive outdoor activities, and where community events can occur. These public spaces are defined in part by the City's history, diversity, distinctive physical qualities, uniqueness and desire for equitable accessible gathering venues.

There is an established grid of transportation corridors throughout the City. Development along these corridors must accommodate multi-modal transportation while maintaining neighborhood connections. There can be pressures to widened roadways for increased motorized vehicle capacity, which can adversely affect other transportation forms, notably pedestrian and bicycle travel. Future corridor design features should include pedestrian and cycling routes that increase mobility and create viable multi-modal corridors. There are also neighborhood development opportunities adjacent to the corridors, to improve the overall circulation system and extend sustainable streetscape practices.

In addition to the Design Element update, a Gateway Area Plan Element, specific to the part of the City west of downtown, is added to the General Plan. ~~The Gateway Area intends to fulfill Arcata's community aspiration of providing high quality of life and equitable housing/ employment options to everyone, regardless of skin color, economic standing, or national origin.~~ The Plan has a substantial design component, promoting a distinctive and attractive

sense of place built through creative placemaking. There is also a connected series of green spaces, trails, protected greenbelts, and daylighted creeks planned to provide pockets of active and passive recreational opportunities, complementing the Gateway Area's urban environment. The Plan Element includes a form-based code for development that ~~also preserves greenbelts and open space around the City perimeter~~ adheres to objective design standards.

Guiding Principles and Goals.

- A. Promote orderly and harmonious development of the City.
- B. Assure ~~that~~ new development is designed to preserve important natural features, ~~built environment assets~~ historic resources, and scenic resources.
- C. Promote building designs ~~that are~~ well suited to their functions and sites.
- D. Prevent excessive and unsightly grading ~~of hillsides~~ associated with development.
- E. Create both natural and built visual environments ~~which are~~ of high aesthetic quality and variety.
- F. Achieve maximum benefit from natural environmental settings.
- G. Assure ~~that~~ new buildings ~~are~~ designed to fit appropriately with the existing neighborhood or area context.
- H. Promote design that maintains land value stability ~~of land values~~ and encourages desirability of City investment in the City.
- I. Incorporate "green building" and "universal design" concepts and features into new and renovated structures.

5.2 POLICIES

The Design Element contains the following policies:

- D-1 Overall Community Character
- D-2 Downtown Arcata Design
- D-3 Scenic Routes, Resources, and Landscape Features
- D-4 Subdivision Design
- D-5 Residential Design
- D-6 Design of Commercial and Industrial Development
- D-7 Landscape Design

POLICY D-1 OVERALL COMMUNITY DESIGN CHARACTER

Objective. Maintain ~~a~~ community ~~with~~ diversity and quality in the built environment; with ~~small-scale~~ structures ~~that are~~ and other physical features harmonious with their neighborhood context; and with a ~~sharp~~ physical and visual distinction transition between ~~the~~ urban area and the surrounding open space lands.

D-1a **Maintain small scale of Building Characteristics.** Buildings shall be design~~ed to maintain the small scale~~ shall to consider established City characteristics and planned character features, including and shall offset the neighborhood impact of increased heights and new uses in some areas -of the community. This shall be accomplished by:

1. ~~This may be accomplished by~~ breaking ~~designing the massing of~~ larger developments through setbacks into several smaller buildings rather than constructing a single large, monolithic building. _____
2. ~~This shall be accomplished by~~ Avoiding large, unbroken expanses of wall and roof planes.
3. ~~This shall be accomplished by~~ includes providing articulation in building mass, surfaces, rooflines, wall planes, and facades, and including distinctive architectural features and ornamentation.

D-1b **Emphasize Arcata Plaza area as at the main the central community focal point ~~(Not applicable in Coastal Zone)~~.** Buildings fronting on streets around facing the Arcata Plaza shall be multi-story.

Architectural and other design elements shall emphasize the importance of the Arcata Plaza as a the community's main focal point the City's center for commerce, entertainment, and special events. Designs shall promote pedestrian access and continuity of retail space at the street level back of sidewalk. Parking should be accommodated off-site to the extent practicable. NOTE added as GPA by Resolution 090-028



D-1c **Promote quality and diversity of design compatible with neighborhood context.** Site and building design shall be harmonious with the consider neighborhood context, including existing structures, especially when increased structure heights and coverages are proposed. Within new subdivisions development, diversity in building appearance structure facades and materials, rather than repetitive designs is encouraged.

D-1d **Preserve natural landforms and landscape features.** Site designs shall have the minimum disturbance necessary to natural conditions such as existing contours and vegetation native landscape, and shall preserve, to the maximum extent practicable, any unusual natural features.

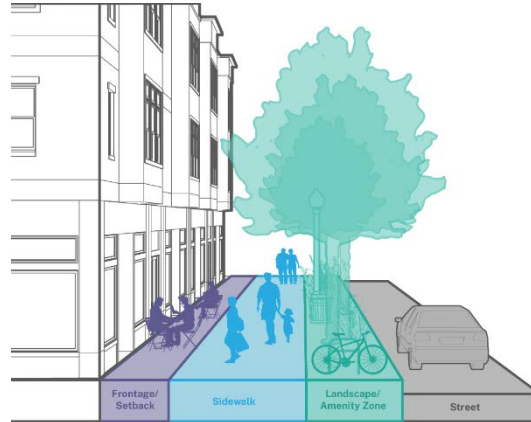
D-1e **Promote energy efficiency and solar access.** Site and building design shall emphasize energy efficiency, and solar orientation and minimize consider shading of adjacent structures to the extent feasible, balancing development needs with solar access.

- D-1f **Create buffers between incompatible land uses.** At boundaries between different land-use designations, and where different ~~and incompatible~~ land-uses are adjacent, buffer areas shall be incorporated into site design ~~for new development~~. Buffers may consist of additional setbacks, landscaping, and visual and noise barriers such as fences or walls.
- D-1g **Provide for bicycles, pedestrians, and transit in design.** Design ~~of commercial, industrial, and multi-family housing~~ shall incorporate provisions for bicycle and pedestrian circulation, and ~~bus~~ transit. Facilities should be located and designed so that these alternative travel modes are fully interconnected.
- D-1h **City edges.** ~~The d~~Development patterns ~~shall be designed shall to be managed to retain the sharply demarcated~~ have distinctive physical and visual separations ~~of Arcata from the urbanized areas of Eureka and McKinleyville adjacent communities~~. This shall be accomplished by:
1. Restricting development in surrounding open space lands to very low density (minimum parcel size from twenty to sixty acres).
 2. Requiring that construction in hillside areas shall be sited so that it does not ~~intrude~~ protrude above the ridgeline.
 3. Retaining existing vegetation, providing landscape screening, and shielding exterior lighting to minimize visible impacts ~~of any development~~ on prominent sites or in open space areas, especially on hillside ~~sites~~ visible from State Route 101.
- D-1i **Renewable green building.** Site and building design shall incorporate green building concepts including maximizing recycled materials use ~~of recycled materials and recycling~~, energy efficiency, solar access, insulation, energy efficiency, use of toxic-free materials, natural lighting, native landscaping, permeable surfaces around structures, and minimizing construction waste generation disposal.

POLICY D-2 DOWNTOWN (CENTRAL – COMMERCIAL) DESIGN

Objective. Maintain and continue to develop a built environment which accentuates the Plaza and surrounding downtown ~~lands designated in the Commercial Central use category~~ as the commercial and cultural hub of the city ~~City~~, maintains the ~~consider~~ historical flavor of ~~building and site~~ design features, integrates bicycle and pedestrian facilities, and which incorporates appropriate trees and other "softscape landscape" elements.

D-2a **Arcata Plaza Design of Arcata Plaza (Not applicable in Coastal Zone).** The basic historical pattern of Plaza design of the Plaza shall be retained, including the symmetrical pathways arrangement of pathways, the open lawn expanses of lawn, and the central as a community focal point of the McKinley statue for the community. ~~Tracked Changes adopted by Resolution 189-43. See: P:\Arcata Services\Arcata SIRP 2021\3-Tasks\3-GeneralPlanAmendments\A-PastVersionsOf GP\Past GP Amendments~~



D-2b **Streetscape design.** Future changes to public street rights-of-way in the downtown shall focus on improving include amenities such as non-obstructive street furniture, access and safety for pedestrians, and bicycles, and reasonable and safe while maintaining vehicle access including the possibility of "car free" streets. The following design features should be considered in future improvement projects:

1. Increase the width of sidewalks.
2. Demarcate pedestrian crosswalks with pavement marking or special paving materials or colors.
3. Provide or improve bike lanes, with an emphasis on protected bike lanes, where appropriate.
4. Incorporate street trees in appropriate locations.
5. Use special paving materials or patterns for sidewalks at key locations or intersections.
6. Provide landscape screening between parking lots and the street, with preference for parking associated with new buildings behind, or away from street frontage.
7. Provide shielded and directional street and parking lot lighting ~~that is~~ adequate for safety but that is not overly bright.
- ~~8. Establish a uniform lighting fixture and post (or pole) design for streetlights;~~
- ~~8.9. Establish a uniform design for various items of "street furniture," such as benches, trash receptacles, water fountains, etc. Consider the possibility of "car free" streets.~~
9. Strongly encourage undergrounding of utilities to eliminate poles and overhead wires.
- ~~10. Require undergrounding of utilities and elimination of poles and overhead wires.~~

D-2c **Alleys.** The existing alleys in the downtown shall be retained and should be improved as multi-functional accessways. Businesses are encouraged to use alleys for secondary entries. Enhancements should emphasize amenities and safety for pedestrians, such as

improved surfacing, lighting, landscaping, and enclosures for garbage and recycling receptacles where space permits.

D-2d **Street trees.** The City shall ~~encourage the installation of~~ maintain a street trees ~~within the downtown area. The City shall develop a comprehensive street tree planting and maintenance program which includes~~ with the following components:

1. Identification of streets where trees may be installed.
2. Siting ~~S~~standards, ~~for the location of street trees; generally, locations will~~ either be in wells located between on-street parallel parking areas, in cutouts within the sidewalk where the sidewalk is of sufficient width (7 feet or more), or in containers where the preceding locations are not workable.
- ~~3. Standards for spacing between street trees (usually 20 to 35 feet on center).~~
4. ~~Specification of a list of acceptable tree species and the appropriate streets and locations for each species.~~
- ~~5. Standards for size of trees and specifications for their installation (e.g. size of well, staking, materials).~~
- ~~6. Identification of responsibilities, procedures, and standards for tree maintenance. Where space is insufficient for street trees, trees may be incorporated into the landscape design on private property adjacent to the street property line.~~

D-2e **Design criteria for new structures and additions.** The height, scale, and mass (volume) of new buildings and additions to existing buildings shall ~~be compatible~~ take into account ~~with~~ other buildings in the immediate vicinity. Each building shall have an entry from the sidewalk to the street-level floor. Building elevations shall be articulated: long, continuous, unbroken wall and roof planes should be avoided. The visual organization and proportions of building elevations — including the size, spacing and shape of window and door openings — should be consistent with neighboring buildings. Architectural detailing and ornamentation, such as cornices, eaves, recessed or covered entryways, and awnings, are encouraged. Design review applications shall include depiction of buildings on adjoining lots, either in elevation drawings or photographs.

D-2f ~~Design criteria for vacant lots on Arcata Plaza. In addition to the criteria in D-2e, the following criteria shall also apply to development on vacant parcels with frontage on streets surrounding the Plaza:~~

- ~~1. All buildings shall have a minimum height of two stories to create a sense of enclosure for the City's central open space and focal point.~~
- ~~2. All floors of buildings should be parallel to and at the street parcel line.~~
- ~~3. Any building located at a corner or intersection shall incorporate architectural features at the ground floor which emphasize pedestrian circulation, such as building cut-offs, walk-through arcades, pedestrian spaces, or similar elements.~~
- ~~4. Parking is encouraged to be provided off-site; if any on-site parking is provided, it shall be accessed from the rear.~~

~~5. Loading docks shall not be required.~~

D-2g **Design criteria for remodeling existing facades (storefronts).** ~~In remodeling facades,~~ ~~the~~ Distinguishing original qualities or character of a building, structure, or site and its environment shall not be destroyed. The removal or alteration of any historic material or distinctive architectural features shall be avoided whenever possible. Deteriorated architectural features shall be repaired rather than replaced whenever practicable. Storefronts shall be designed to fit inside the original opening and not extend beyond it. Contemporary design for alterations and additions to existing structures shall not be prohibited when such alterations and additions do not destroy significant historical or architectural character of the property. The size and proportions of any additions shall be compatible with the original building.

D-2h **Site design, including parking areas.** The placement of new buildings and building additions on a site should be compatible with site layout on neighboring properties. In general, buildings should be sited immediately adjacent to the sidewalk and any onsite parking should be placed to the rear of the building. Parking areas shall be the minimum necessary and be separated from the street and sidewalk by a landscape buffer ~~of at least six feet~~. Trees that reach a mature height of at least twenty feet should be included in the interior of all parking lots with more than two parking spaces. Pedestrian spaces, circulation areas, or gathering areas are encouraged as design elements.

~~D-2i Design of signs. Permanent signs in the Central Commercial area shall be oriented to pedestrians rather than automobile traffic, and shall conform to the following criteria:~~

- ~~1. Signs shall be placed so that they do not obscure other building elements such as windows, cornices, or decorative details.~~
- ~~2. Size, materials, style, and color shall complement the building facade and shall be compatible with the surrounding area.~~
- ~~3. Copy shall be limited to icons, logos, business identification, and hours of operation (rather than advertising copy).~~
- ~~4. Flush-mounted signs with copy in a specifically designated horizontal band is the preferred type; monument and hanging (cantilevered) signs may be allowed, provided that the latter has a vertical clearance of at least 7.5 feet and an encroachment permit is obtained.~~
- ~~— Pole signs shall be prohibited, except for public traffic, directional and safety signs.~~
- ~~6. When lighted, signs shall be designed to minimize glare and with the minimum amount of illumination necessary to make the sign legible; neon signs with distinctive designs are acceptable. Plastic faced internally lit signs should be discouraged.~~
- ~~7. At the time of any future alterations of an existing sign, the sign shall be required to be modified to conform in its entirety to these policies. Alterations shall mean any change to the structure, area, or height of a sign, but shall not mean a change in copy.~~

~~D-2j2i~~ **Incorporation of amenity features in new development.** Any new development shall incorporate an appropriate combination of project enhancements in lieu thereof. Potential enhancements include, but are not limited to, the following:

special paving materials in parking lots	special architectural features
public art, including sculpture and murals	flower beds
outdoor spaces for public use	window boxes
street trees or street furniture	courtyards
fountains or other water features	awnings
secondary pedestrian access from alleys	planted wall trellises
balconies or decks on upper floors	recessed entryways
sidewalk and/or entry mosaics or decorative tile	

POLICY D-3 SCENIC ROUTES, RESOURCES, AND LANDSCAPE FEATURES

Objective. Identify and protect scenic routes, resources, and landscape features. Retain natural features, coastal scenic resources, and scenic vistas as important aesthetic components of the built environment and visual and associative links to nature. Minimize impairment and obstructions of scenic views to the minimum necessary to allow ~~reasonable~~ development.

~~D-3a~~ **Designation of coastal scenic highways.** ~~The following are designated coastal scenic highways routes are hereby designated:~~

- ~~1. 7th Street and Bayside Road, from 7th Street overcrossing to Crescent Drive~~
- ~~2. Bayside Cutoff, from State Route 101 to Old Arcata Road~~
- ~~3. Old Arcata Road, from Bayside Cutoff to Crescent Drive~~
- ~~4. Samoa Blvd. (State Route 255), from Crescent Drive to Manilathe western City limits at Slaughterhouse Road~~
- ~~5. Janes Road, from 11th Street to Foster Avenue~~
- ~~6. State Route 101, from the southerly City boundary to the Seventh Street overpass Mad River~~
- ~~7. South "I" Street, from Samoa Blvd. south to the Arcata Marsh and Wildlife Sanctuary~~
- ~~8. South "G" Street, from "H" Street to State Route 101~~
- ~~9. All public roads west of the City in the Arcata Bottom~~

~~D-3b~~ **Designation of non-coastal scenic highways (Not applicable in Coastal Zone).** ~~The following non-coastal scenic highways are hereby designated:~~

- ~~1. Fickle Hill Road~~
- ~~2. Jacoby Creek Road~~
- ~~3. Golf Course Road~~
- ~~4. L. K. Wood Blvd. from the St. Louis Road Overcrossing to 14th Street~~
~~added as GPA by Resolution 090-028~~

D-3c **Design policy for projects affecting scenic ~~highways~~ routes and State Routes.** The following standards shall apply to any development which affects scenic ~~highways~~ routes, as well as views from State Routes 101, 255, and 299:

1. Billboards or other off-premises ~~freeway-oriented~~ signs are prohibited.
2. Landscape planting along State Route 101 shall not interrupt scenic views to the bay or eastward across agricultural lands.
3. New development or redevelopment in the ~~industrial~~ area of South "G" Street shall provide dense landscape screens along all perimeter lot lines visible from State Route 101.
4. The City shall work jointly with the County of Humboldt, Caltrans, and the Coastal Commission to enhance scenic views along scenic highways, particularly State Route 101 and 255 corridors.

D-3d **City Scenic entryways.** The appearance of the following additional entryways, ~~while not scenic resources~~, should be enhanced with appropriate landscaping and entry signs or structures:

1. Samoa Blvd. (State Route 255) between Jackson Ranch Road and "K" Street
2. State Route 101 between Bayside cutoff and Samoa Blvd.
3. State Route 101 between the Mad River and Giuntoli Lane
4. State Route 299 from North Bank Road to Giuntoli Lane
5. Old Arcata Road from Bayside Cutoff to Jacoby Creek Road

These public improvements may include uniform landscaping, pedestrian enhancements, and directional signing.

D-3e **Arcata Bay—Open waters, shoreline, and tidal marshes.** Proposed land uses and development shall not significantly alter the natural appearance or landforms of the waters, shoreline, and tidal marshes of Arcata Bay, which are designated in the natural resource land-use category. Where these resources are visually degraded, developments shall be required to restore or enhance their appearance. Development within the area bounded by Samoa Blvd., Butcher's Slough and Gannon Slough shall include local native plant landscaping, screenings and other measures to ensure compatibility with scenic coastal resources and with the educational, recreational, wildlife and other uses of the Humboldt Bay National Wildlife Refuge and the Arcata Marsh and Wildlife Sanctuary.

D-3f **Bay and ocean views.** Views of Arcata Bay and the Pacific Ocean from vantage points along public streets in hillside areas of Arcata shall ~~not be blocked by development be preserved to the extent practicable, balancing development rights in these areas~~. Any impairment or partial obstruction of these ocean views from new development shall be the minimum necessary ~~to for~~ allow ~~able reasonable~~ development.

- D-3g **Wooded hillsides.** Views of wooded hillsides forming the City's eastern edge from vantage points along public streets west of the State Route 101 should not be blocked by development: to the extent practicable, balancing development rights in these areas. Any impairment or partial obstruction of these ocean views from new development shall be the minimum necessary for allowable development. The City shall encourage Cal Poly Humboldt to avoid blocking views in its new development.
- D-3h **Farmlands and open countryside.** Views of farmlands and open countryside — in the Arcata Bottom, along the State Route 101 south of Samoa Boulevard, north of Giuntoli Lane, and along State Route 255 west of the ~~city~~City, should be protected to the extent practicable, balancing development rights in these areas. Any impairment or partial obstruction of these views from new development shall be the minimum necessary for allowable development. New development should be sited and designed to minimize any impairment of such views.
- D-3i **~~Preservation of hedgerows (windrows, or rows of trees).~~** ~~Preservation of the following windrows, hedgerows, or groves of trees shall be encouraged:~~
- ~~1. The cypress windrows separating Arcata Heights from the Sunset neighborhood.~~
 - ~~2. The trees along the north and western edges (Alliance Road) of the Sunset neighborhood.~~
 - ~~3. The trees on the small knoll adjacent to State Route 101 south of Spear Avenue.~~
 - ~~4. The windrow of trees adjacent to the west side of the State Route 101 between 7th and 14th Streets.~~
 - ~~5. Windrows in the Arcata Bottom.~~
 - ~~6. The row of trees along the westerly side of State Route 101 at the State Route 299 interchange.~~
 - ~~7. The groves of eucalyptus and other trees which line Bayside Road between Union Street and Crescent Way.~~
 - ~~8. The elm trees on "J" Street between 14th and 16th Streets.~~
 - ~~9. Redwood Grove at 7th and Union, and Bayside Road to be left intact.~~
- D-3j **Streamside riparian areas.** Creeks or drainage channels and any associated riparian vegetation shall be retained in a natural state and incorporated into site design as a visual asset to development which that adjoin them. Design codes should encourage "daylighting" streams on City and private property, and restoration of riparian ecology and function.-

POLICY D-4 SUBDIVISION DESIGN

Objective. Achieve subdivision design which that accommodates orderly growth; assures ensures proper development of land and access to lots; promotes open space retention;

~~insures~~ adequate circulation, utilities, and services; preserves existing landforms; and retains significant vegetation.

D-4a **Design of roadways ~~and~~, subdivision, and redevelopment site improvements.** New subdivisions and infill or redevelopment site design shall provide orderly arrangement of complete streets that minimize impacts on natural landforms and habitats and that create connectivity between existing neighborhoods and recreational areas using pedestrian oriented development and principles of Crime Prevention Through Environmental Design. The following criteria shall be required and or encouraged as appropriate: comply with the following criteria:

1. Proposed street alignments including sidewalks and bikeways shall conform to the relevant Transportation and Road Safety Plans, wherever possible. Unless it is demonstrated to be infeasible, all new lots shall have frontage on a public street or improved alley, or publicly accessible courtyard or open space. ~~Where direct access is infeasible, the number of lots or units to be served by a common access driveway easement shall be based on accessway design meeting all, but not limited to, these requirements: public safety access, ingress, egress, parking, utilities, drainage, and environmental issues.~~
- ~~2. The Proposed street alignments arrangement of proposed streets shall conform to the Transportation Plan, where possible. When not shown on the Street Plancity standards, the design Alignments shall provide for the have appropriate connections to existing streets. Cul-de-sacs should be avoided where possible.~~
- ~~3. The subdivision design may utilize narrow or "skinny" streets and various traffic calming approaches; alleys are encouraged for access to garages.~~
- ~~2.4.~~ Gated subdivision streets shall be prohibited in order to maintain a sense of community.
- ~~5. Use of natural drainage techniques in subdivision design is encouraged. In general, curbing and gutters are appropriate for street drainage, safety and delineation and protection of pavement edge. Where curbing is not required, some other type of edge definition and stabilization shall be furnished.~~
- ~~3. 6.~~ Natural drainage patterns shall be preserved to minimize potential slippage and flooding. Building site grading shall be contoured to direct water away from structures.
- ~~7. No lighting shall be allowed that results in illumination above the tree canopy.~~
- ~~8. Sidewalks and bikeways shall be provided; sidewalks may be furnished on one side of the street only. In planned developments, additional sidewalks and bikeways may be located away from the street.~~
- ~~3.49.~~ Appropriate landscaping and illumination shall be applied to enhance safety and provide attractive screening and distinctions between spaces; illumination of streets shall be unobtrusive and the lowest intensity compatible with safety, complying with night sky standards wherever possible.

~~10. Landscaping shall be provided as part of subdivision design; plantings shall include street trees or an equivalent number of trees planted on individual lots adjacent to the street frontage.~~

~~11. Where subdivisions abut a major street or railroad, or include more than one land-use or housing type, landscape buffers shall be included at appropriate locations to create a visual screen and minimize any adverse impacts.~~

D-4b **Lot patterns.** Lot boundaries should be regular in shape and lots should either have direct access to a public street or to an access easement which connects to a public street. Clustering of lots with common open space areas and/or common parking lots is encouraged. Condominium or other common multiple-unit tenancy designs may be approved with alternative vehicular access requirements, within fire safety considerations.

D-4c **Grading and hillside subdivisions.** In hillside areas, subdivision design and grading shall minimize disturbance to natural landforms, ~~not destroy visual quality, nor create and mitigate~~ conditions that ~~could~~ increase the risk of landslides, flooding, or erosion. Designs shall comply with the following criteria:

~~1. Street layout should in general be parallel to topographic contours to reduce the extent of cut and fill slopes.~~

~~2. Long, continuous, or unbroken manufactured slopes should be avoided.~~

~~3. Graded slopes should be contoured by varying slope increments and undulating banks vertically and horizontally; building pad elevations should be varied to avoid the appearance of monotonous, flat, level pads.~~

~~4. Generally, slopes greater than 2:1 (horizontal: vertical) should be avoided.~~

~~5. Permitted densities shall be adjusted in designated hillside areas such that the steeper the slope, the less the allowed density; in such areas, at least fifty percent of each new lot shall be retained as ungraded "natural area."~~

~~6. Along ridgelines, new development or lighting which would extend above the tree canopy shall be prohibited.~~

~~7. Parallel on-street parking should not be provided on steep street segments; in such areas, alternative designs for on-street parking shall be required.~~

D-4d **Retention of natural features.** ~~Design of subdivisions~~ Subdivisions shall be designed shall be based upon an analysis of the ~~considering the~~ natural conditions and features of the site. To the maximum extent practicable, development shall be located to preserve natural features and avoid areas of environmental sensitivity, including but not limited to: wetlands, significant or mature locally native trees or stands of such trees, creekside riparian habitat areas, floodplain areas, and areas with slopes greater than thirty percent. Residential structures, including decks and balconies, shall not extend into significant natural areas.

D-4e **Open space and parks.** New residential subdivisions shall either be required to dedicate land for public parks or stormwater detention facilities, provide commonly-owned and maintained park or open space lands for use by residents of the subdivision, pay a fee in-lieu thereof, or some combination of the foregoing.

POLICY D-5 RESIDENTIAL DESIGN

Objective. Create residential living environments which meet the needs of residents, are aesthetically pleasing, provide for personal safety and privacy, promote social interaction, maintain continuity with the community's past, and provide for leisure needs. Blend residential design objectives with neighborhood conservation area objectives expressed in the Historical Preservation Element.

D-5a **Multi-family housing design.** Within each neighborhood where multi-family is allowed by the Land-Use Element, multi-unit housing designs should comply with the following criteria:



1. Citywide objective design standards shall be developed to ensure fulfillment of the following design policies for all new multifamily projects:

1. Buildings should maintain the scale and ~~character of other residential structures in the immediate vicinity and avoid abrupt changes in height and bulk between structures~~ massing allowed in the City's zoning ordinances.
2. Buildings should be grouped compactly to provide more usable open space.
3. Building elevations should be articulated and long, continuous wall and roof planes should be avoided. Architectural features such as bay windows, balconies, porches, and similar elements are encouraged.
4. Features should be incorporated into site and architectural designs which provide maximum exposure to sunlight and protection from rainstorms and other adverse climatic conditions (such as covered entryways).
5. Site and building design shall incorporate features to mitigate noise from nearby noise sources (see Noise Element).
6. Sufficient useable outdoor open space should be provided to accommodate the recreation and leisure needs of the residents, of the development, and individual households.
7. Individual units should be designed to be readily distinguishable from one another from the exterior.
8. Parking should be designed to protect the privacy of residents and prevent intrusion of noise and lights from vehicles.
9. Parking lots shall be landscaped with trees that reach a mature height of at least twenty feet and shall be visually screened from the street by solid walls, fences, or a

planted landscape buffer of at least six feet in width. Site design should incorporate safety features that maintain visibility and provide security lighting.

10. Service and storage areas, such as for recycling and garbage, shall be screened by fencing or walls; appropriate landscape planting and setbacks from adjacent properties shall be provided.

D-5b **Single-family residential design.** The intent in single-family residential design shall be to allow maximum flexibility consistent with ~~maintaining existing community objective design standards~~ character. ~~Maintaining character means~~ Design standards shall provide architectural style ~~consistency,~~ standards for structures proportional to the site and surroundings, and ~~harmony with the~~ that emphasize or contrast design of adjacent buildings. Good design respects its neighbors, is properly sited for privacy and solar access, minimizes disturbance of natural site conditions, and is landscaped to complement the streetscape, including street trees where appropriate.

Design of single-family houses on existing lots in hillside areas shall ~~adhere to the following additional criteria:~~ minimize visual impact on the terrain and blend with the natural features. Development on ridgelines shall be avoided unless there is no alternative developable area. In light of the policies that encourage addition of accessory dwelling units on "single family" zoned lots, strive for accessory dwelling unit design that: complements surrounding development; retains privacy; access to direct sunlight; and a sense of appropriate scale.

- ~~1. Disturbance of existing landforms (cut and fill) shall be minimized.~~
- ~~2. Terrain adaptive architectural designs which fit into the contour of the hillside are encouraged, including:~~
 - ~~a. Reduced footprint design with multi-level structures;~~
 - ~~b. Multiple "Step-up" or "step-down" structures;~~
 - ~~c. Stilt houses, with enclosure of area underneath the building.~~
- ~~3. Significant trees and drainageways should not be disturbed.~~
- ~~4. Development should be confined to portions of a site which are less steeply sloped.~~
- ~~5. Garages should be located under or over the structure depending on whether the lot is uphill or downhill from the street.~~
- ~~6. The scale and character of buildings should be matched with the terrain, scale, and character of the surrounding neighborhood.~~

POLICY D-6 **DESIGN OF COMMERCIAL AND INDUSTRIAL DESIGN DEVELOPMENT**

Objective. Create functional commercial centers and industrial areas ~~which are functional,~~ suited to ~~the needs of the particular businesses~~ staff and customer needs, responsive to site conditions, and compatible with the neighborhood context.

D-6a **Design of commercial development.** The following criteria shall apply to design of new commercial development, except for the Plaza area, ~~which is addressed in Policy D-2~~:

1. To encourage pedestrian access buildings should be sited so that entries and front facades face the public sidewalk; loading docks and vehicular entrances shall be located to the side or rear.
2. When located adjacent or near to residential areas, buildings should respect the residential character by avoiding long, uninterrupted expanses of wall and roof planes, and by incorporating architectural features such as covered entries or porches, cupolas, towers, arbors or pergolas, etc. which add variety and interest to larger buildings.
3. Pedestrians and bikes should be accommodated through appropriate location of walkways and rain-sheltered entrances to buildings.
4. Drive-through service windows for restaurants shall be restricted to the Valley West area.
5. Where commercial development abuts residential or other non-commercial uses, appropriate visual and noise buffers shall be included in the site design, such as increased setbacks, [noise attenuation features](#), or landscaped screening.
6. Service and storage areas shall be screened by fencing and appropriate landscape plantings.
7. The siting and design of buildings shall promote energy-efficiency and [consider](#) solar access, ~~and shall minimize impacts on other nearby uses~~ [balancing development needs with solar access](#).
8. ~~Parking areas are encouraged to be provided~~ [If parking is necessary, the minimal necessary parking should be located](#) to the rear or side of buildings and include trees that reach a mature height of at least twenty feet whenever feasible.
9. Noise impacts shall be minimized through acoustical features in building design and on site sound attenuation.



D-6b Design of Mixed Use Development. In addition to the criteria stated in Policy D-6a, the following criteria shall also apply to new mixed-use development:

1. [Ensure the design provides for internal compatibility and privacy between the residential and non-residential uses on the site.](#)
2. [Ensure that new development is human-scaled and supports a pedestrian-friendly public realm. Buildings should be located at or close to the sidewalk.](#)
3. [Provide for sensitive transition from the public realm \(sidewalk\) to the private realm \(residences\).](#)

4. Encourage outdoor dining, publicly-accessible courtyards, window-shopping areas, and other pedestrian-friendly uses in spaces between building walls and the sidewalks.
5. Encourage ~~Provide for~~ convenient pedestrian access from the public street into the nonresidential portions of the project, through such means as courtyards, plazas, walkways, and street furniture.
6. Require main building entries to be visually prominent and oriented to a public street or pathway, ~~or publicly accessible courtyard.~~
7. Locate parking facilities away from building frontages in the rear of the lot.
8. Encourage non-residential uses to be located above the first floor or behind ground-floor commercial, except in limited applications.
9. Ensure that the residential units are of a residential character, ~~and that appropriate privacy between residential units and other uses on the site is provided.~~

D-6b6c Design of industrial development. In addition to the criteria stated in Policy D-6a, the following criteria shall also apply to new industrial development:

1. Building facades that include regional materials (e.g., locally produced wood, stone and recycled products) are encouraged. ~~Prefabricated and manufactured materials may be used only where they are compatible with surrounding development.~~
2. Outdoor storage areas, including storage of trucks and equipment, shall be screened from view from adjacent streets and parcels by appropriate fencing and landscaping.

D-6c6d Design of institutional development. Since institutional uses—such as churches, schools, government facilities, and others—are frequently located within residential areas, their design shall be reviewed for compatibility with the adjacent residential neighborhood. In addition, the City requests that ~~HSU~~ Cal Poly Humboldt, school districts, and other institutional entities adhere to the applicable City design ~~following~~ criteria, which ~~shall~~ apply to design of any facilities within Arcata:

1. Long, uninterrupted expanses of wall and roof planes should be avoided and architectural features ~~which add interest and variation~~, such as porches, cupolas, towers, arbors or pergolas, etc., should be incorporated.
2. Appropriate buffers and screening should be provided between institutional uses and adjacent residential uses to minimize noise and visual impacts, including increased setbacks, fencing, and landscaping.
3. The massing of buildings and the visual organization of facades, including the proportion of window and door openings to total wall surface, exterior materials and colors, and architectural detailing and ornamentation, should be designed to harmonize with any adjacent residential uses.
4. ~~Appropriate setbacks and landscaped buffers should be provided to minimize noise and visual impacts.~~

D-6e **Design of signs.** Signs shall be scaled, sized, and located appropriate to the uses on site and readily viewable from the public right of way without causing distractions through animation, flashing, or glare, or other off-site impacts that affect public safety and enjoyment.

The following criteria shall apply to all signs, except those located within the Central-Commercial Area use category:

1. Signs shall be placed so that they do not obscure other building elements such as windows, cornices, or decorative details.
2. Size, materials, style and color shall complement the building facade and shall be compatible with the surrounding area.
3. Copy shall be limited to icons, logos, services identification, and hours of operation (rather than advertising copy) except as required by law or ordinance.
4. Flush-mounted signs and monument signs are the preferred types; no more than one free-standing sign may be allowed per business street frontage (including alleys), and freestanding signs shall not exceed fifteen feet in height.
5. Pole signs are discouraged; when permitted, such signs shall not exceed fifteen feet in height.
6. Signs shall not be of a height or design which is intended to be read from the State Routes 101 or 299, except for in the commercial visitor serving district.
7. When lighted, signs shall be designed to minimize glare and with the minimum amount of illumination necessary to make the sign legible; neon signs are acceptable.
8. All off-premises (or off site) business identification and advertising signs are prohibited.
9. The planned sign program is encouraged whenever there are multiple businesses on a site; no more than three signs may be allowed per parcel except through approval of a planned sign program permit. The planned sign program shall require harmony in shape, placement, materials, and other appropriate design elements for all signs on the site.
10. At the time of any future alterations of a lawfully permitted existing sign, except for a change in copy, the entire sign shall be modified as necessary to conform to these policies.

POLICY D-7 LANDSCAPE DESIGN

Objective. Promote landscape designs which are appropriate for the climate zone and emphasizing native plants, that may be complemented by non-invasive, non-native species specific site conditions, when they integrate harmoniously with the scale and architecture of buildings on the site, and improve the overall aesthetic appearance of the city and its neighborhoods, and serve to protect the general safety and welfare.

D-7a Landscape plans required. A landscape plan drawn to scale shall be required for all new development subject to City discretionary review by the City. The plan shall identify existing and proposed trees, shrubs, groundcovers, and other landscape and hardscape elements. Native species are encouraged for in all new landscaping.

- D-7b **Planting area required.** A minimum landscape planting area shall be required in all commercial, industrial, and multi-family residential land-use zones; ~~these shall be contained in the zoning ordinance~~ and expressed as a percent of site area.
~~[Revised by Ordinance No. 1377, September 2008]~~
- D-7c **Parking lot landscaping.** All parking areas, other than single family residential, containing two or more parking spaces shall be landscaped. Landscaping shall include a minimum of a six-foot wide screen or buffer between the parking area and any public street. Trees that reach a mature height of at least twenty feet shall be provided within the parking area at a ratio of one or more trees to each five parking spaces.
- D-7d **Site design criteria.** Landscaping shall be an integral part of site development, connecting site design elements, enhancing the site identity, and creating a pleasing appearance. Landscape designs ~~shall conform to the following criteria:~~ should incorporate existing natural vegetation where appropriate; provide for erosion control, help to manage stormwater onsite for absorption and percolation, and for privacy, and beauty.
- ~~1. Existing natural site vegetation should be incorporated, to the extent appropriate.~~
 - ~~2. Coverage by impervious surfaces should be minimized in order to reduce runoff.~~
 - ~~3. Cut and fill slopes shall be landscaped for erosion prevention.~~
 - ~~4. Landscaping should include trees adjacent to the public street right of way, where appropriate.~~
 - ~~5. Consideration should be given to native plant species and to those non-invasive exotics which have demonstrated adaptability to local climate.~~
 - ~~6. Where a development borders a major street, railroad, or different land-use type, a landscape buffer shall be provided to create a visual screen, promote privacy, and to shield the development from any adverse external effects, and to shield neighboring properties from any adverse effects of the development. The buffer may include fencing, berms, plantings, or a combination thereof. The appearance of fences from public streets should be softened with plant materials.~~
 - ~~7. The design for a particular site should harmonize with the surrounding landscape, including the landscape design of adjacent lots.~~
 - ~~8. Site design should incorporate safety features such as maintaining visibility and providing security lighting.~~
- D-7e **Upgrade of non-conforming landscape.** When improvements are made to structures on sites where landscaping is non-conforming, landscaping should be required to be upgraded if feasible.
- D-7f **Maintenance of required landscaping.** All required landscape plantings shall be properly maintained to assure survival; ~~any non-surviving~~ with plants shall be replacements as needed for first five years.

5.3 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURE	RESPONSIBLE PARTY	TIME FRAME
LU-1	Objective Design-Review Standards, Sign-Regulations, Language Standards (LUC-Zoning Amendment) Prepare a <u>PLUC-zoning</u> amendment (formerly LUDG) to implement Design Element policies, including <u>objective design standards for residential projects standards for review, sign regulations, landscape standards</u> , and applicability of design review (specify when design review and approval is required).	Community Development Dept./ Planning Commission	Year <u>2-5</u> ±
D-1	Design-Review Commission/Project-Design-Review Review and approve the design of private and public projects.	Comm. Devel. Dept./ Design-Review Comm.	Ongoing
D-2	Downtown-Street-Tree-Program Prepare a street tree program consistent with the Design Element for review and recommendation by the Design-Review Commission and approval by the City Council. Financial assistance may be provided by the Arcata Community Development Agency.	Public Works Department/ Downtown-Business Organizations	Year 1
D-3	Design Criteria and Brochure Prepare design review brochures or handouts which explain and illustrate design criteria, standards, and procedures, <u>explaining differentiation between design guidance and objective design standards.</u>	Community Development Dept./Design Review Comm.	Year <u>3-6</u> ±
D-4	Design-Awards-Program Recognize and award projects that demonstrate the highest quality design and context.	Comm. Devel. Dept./Design-Review Comm.	Ongoing
D-5	Valley West-Samea Blvd. Public Improvements Program The City of Arcata shall work to obtain funding to develop a specific public improvements program to improve the appearance of <u>Samea Blvd-Valley East and Valley West, and Giuntoli Boulevards.</u> from State Route 101 to "I" Street. These improvements may include uniform landscaping, pedestrian enhancements, and directional <u>signingsignage.</u>	Public Works Department Environmental Services and Engineering Departments	Year 2
D-6	Design-Review-MOU Develop Memorandum of Understanding with other governmental agencies for design review	City/County/State agencies/Special Districts	Year 2

Attachment: G. Ch 5 Design Historic (4214 : GP #XVII FBC & GP wrap-up)

3/20/23 Draft ~~HISTORICAL~~ PRESERVATION ELEMENT

5.4 INTRODUCTION

No City can hope to understand its present or forecast its future if it fails to recognize its past. By tracing and preserving its past, a City can gain a clear sense of the process by which it achieved its present form and substance. Arcata's rich and varied historical and cultural resources include buildings, structures, objects, sites, landscapes, districts, archaeological sites, and traditional cultural properties that possess historical, scientific, architectural, aesthetic, cultural, or ethnic significance.

Although not always easily distinguishable, these resources, with their inherent ability to evoke the past, represent important aspects of the history of Arcata and the region. They include evidence from the time before and during European contact with Native Americans of this area, examples from the periods of development in and around the City's core, early transportation routes and the spread of development outward, through both world wars and beyond. The identification, evaluation, registration, and protection of these resources, and thereby the preservation of Arcata's past for its current and future residents, are the essential components of Arcata's historical preservation program. This Element includes policies intended to guide the preservation of Arcata's past, with more specific regulatory guidance provided in the City's Zoning Ordinance.

Historical and cultural resources include elements from the built environment such as buildings, structures, objects, and districts; landscape features, including significant trees and plantings, hardscape, fountains, lighting, sculptures, signs and other natural or designed features; interior elements and fixtures designated in conjunction with a property; significant archaeological sites; and traditional cultural properties.

Arcata History.

~~—————Arcata's Historical and Cultural Resources. For centuries—~~Since time immemorial, before the arrival of European-American settlers in 1850, Arcata and the Humboldt Bay region were the home of the Wiyot. An Algonquian-speaking people, the Wiyot resided in settlements lived along the lower Mad River and Eel Rivers as well as, other local streams, and along Humboldt Bay and on the hills and ridges or lands in between. Their way of life was shaped by the bountiful remarkable surroundings of forested hills, bountiful streams and rivers, the Pacific Coast and protected marshes and sloughs of Humboldt Bay and the Pacific and Bay shores, which generously provided for generations of subsistence for both their survival and cultural needs.

Humboldt Bay was located by European-Americans using overland routes for the first time in December of 1849. The discovery of gold in the Trinity and Klamath River regions soon resulted in large numbers of settlers coming to the area. The displacement, disease, violence, and cultural disintegration accompanying colonial-white settlement brought great changes almost

~~total annihilation~~ to the Wiyot peoples.

The City of Arcata recognizes and respects the indigenous Wiyot people are the ancestral inhabitants of all lands within the City of Arcata and that the lands on which we located are the unceded ancestral lands of the Wiyot tribe. Known in the Wiyot language as Goudi'ni, (goo-deet-NEE") meaning "over in the woods" or "among the redwoods", the Wiyot are the traditional stewards of these unceded lands. Past governmental policy encouraged the removal of the Wiyot and other Indigenous peoples from their ancestral land base or prevented their use of ancestral lands. These actions combined with intolerance have led to the displacement of the Wiyot community and their inability to engage in many traditional cultural practices. The City of Arcata acknowledges the Wiyot Tribe, their elders both past and present, as well as future generations. This Acknowledgment seeks to aid in dismantling the legacy narratives of settler colonialism.

Today, the Wiyots people are federally recognized in the Wiyot Tribe Reservation, Bear River Band of the Rhonerville Rancheria and the Blue Lake Rancheria, for the most part, associated with three Humboldt Bay area rancherias. They are These tribes are actively involved in various tribal economic projects and in the revitalization of cultural traditions such as language, basket weaving, ceremonies, and reclaiming ancestral lands. It is the City's goal to support and encourage Tribal efforts to revitalize cultural traditions and invest in the cultural and physical health and well-being of Tribal members as a key component of the cultural and physical health of the Arcata community.

Arcata, ~~first called known as Union~~ by the European Settles until 1860, was settled in the spring of 1850 as became a supply center for the interior gold mining districts (the original name of "Union" derived from the Union Mining Company). The townsite on the well-watered coastal at terraces at the foot of Fickle Hill and close to the northern shore of Humboldt Bay was selected by the Union Company and subdivided into blocks and lots. A wharf was soon constructed into Arcata Bay with a horse-drawn railway connecting to the Plaza, where merchandising establishments supplied both the miners and growing numbers of residents.



The importance of gold, however, was soon eclipsed by lumbering. It was timber resources — particularly the vast, virgin forests of giant redwoods which covered the nearby ridges and valleys along California's north coast — that sustained the development of Arcata through the

19th century and into the mid-twentieth century. ~~By 1930 Arcata's population had reached 1,700 and was growing.~~ A public water system and fire department came along in 1884, followed by the *Arcata Union* newspaper in 1886, electricity in 1895, railroad connections with San Francisco in 1914, the establishment of Humboldt State Normal School (now ~~Humboldt State University~~ Cal Poly Humboldt) in 1914, and the Redwood Highway in ~~1925~~ 1925 [Susie Van Kirk: *Touring Arcata's Architecture*, 1988].

Many fine examples of both residential and commercial ~~buildings structures~~ from Arcata's early history survive today. An inventory of Arcata's historical built environment in 1979 [Susie Van Kirk: *Reflections of Arcata's History: eighty years of architecture*, 1979] identified four early periods of residential building styles: settlement (1850-1885) Victorian (1885-1900), Transitional (1900-1910) and Craftsman (1910-1930), in addition to the modern period (1930-present). These identified styles have been translated to the four styles protected and celebrated under current City preservation policy, with the "Modern" style used to encompass not only Craftsman styles but International, Streamline Moderne, Art Deco, and Ranch styles, among others, in order to continue to capture post-1930 styles throughout the planning period of this document (2020-2045).

The Arcata Plaza itself, with the statue of McKinley (1906) at its center, dates from the town's beginnings and has always functioned as a centralized community space, a common town design feature for the era, and recalls the "greens" of New England or the town squares of the south. Although none of the original 1850s buildings around the Plaza remain, a variety of classical revival and false-front buildings from the turn-of-the century survive around its perimeter. The Plaza remains the city's commercial hub to the present day. Many of the commercial buildings have been restored, best exemplified by the Jacoby Building (1857), ~~which pioneered modern-day historical preservation endeavors in the city with its restoration in 1977.~~

~~An inventory of Arcata's historical structures and sites in 1979~~ [Susie Van Kirk: *Reflections of Arcata's History: eighty years of architecture*, 1979] identified four early periods of residential building styles: ~~settlement (1850-1885) Victorian (1885-1900), Transitional (1900-1910) and Craftsman (1910-1930), in addition to the modern period (1930-present).~~ The City's first historic preservation ordinance [Ordinance No. 935] was adopted by the City Council in 1980 with the intent of recognizing the value of Arcata's many historic resources and neighborhoods and protecting them from blight, neglect and demolition through education and adaptive reuse. Since that time, 9585 buildings structures or sites have been formally designated by ordinance as local historic landmarks. The City's Historic Landmarks and Neighborhood Conservation Areas, and Specific Plan Districts are shown on Figures HP-a and HP-b.

Guiding Principles and Goals.



Attachment: G. Ch 5 Design Historic (4214 : GP #XVII FBC & GP wrap-up)

- A. Promote preservation of buildings, structures and sites that are representative of the various periods of the city's social and physical development, including Settlement, Victorian, Transitional, and Modern architectural styles.
- B. Preserve the historical character of the Plaza and the surrounding commercial district.
- C. Encourage owners of eligible buildings, structures, or sites to seek historic landmark status and to invest in restoration efforts using preservation incentives.
- D. Conserve the many examples of early residential building styles found in the city's older neighborhoods, from Bayside to Arcata Heights.
- E. ~~En~~Assure that new construction and changes additions to existing ~~historically-designated~~historically designated buildings maintain the character and livability of the historic neighborhoods.
- F. Promote interest in and appreciation of the value of Arcata's history and its heritage of historic buildings.
- G. Encourage tourism and economic development through historic resource preservation.
- H. Avoid impacts to archeological and tribal cultural resources, including but not limited to landscapes, gathering areas, sacred locations, and archeological sites. Prevent destruction of archaeological and cultural resources and assure that any artifacts receive proper disposition.
- I. Acknowledge the Wiyot Community as the indigenous owners of the lands in which the City of Arcata occupies and make efforts to incorporate the Wiyot Community in future decisions affecting the Arcata area and to dismantle the legacy narratives of settler colonialism. Maintain communication with the Wiyot community about cultural resources within the City and its surrounds as traditional stewards of this land.

5.5 POLICIES

The Historical Preservation Element contains the following policies:

- H-1 Designated Historic Landmarks
- H-2 ~~Noteworthy~~ Potentially Historic Structures
- H-3 Arcata Plaza Area Historic District
- H-4 Neighborhood Conservation Areas
- H-5 Controls on Demolitions of Structures
- H-6 Incentives, Information, and Education ~~Public Participation, Information, and Education Policy~~
- H-7 Archaeological and Cultural Resources

POLICY H-1 DESIGNATED HISTORIC LANDMARKS

Objective. Designate and preserve ~~significant~~ significant buildings, structures, and sites that are representative of the city's social and physical development; that are reminders of past eras, events, and persons important in local, state, or national history; which provide significant examples of architectural styles of the past; or which are unique and irreplaceable assets to the city, and the neighborhood in which the building, structure or site is located. Support property owners' efforts to preserve, restore, and adapt use of significant structures and sites for continuing significance.

H-1a **National Register and State Historic Landmarks designations.** The City encourages owners of eligible buildings, structures or sites to request National Register and State Historical Landmarks designations for their properties. As of ~~2022~~1998, ~~nine~~ three National Register ~~buildings~~sites have been designated and counting: the Arcata Hotel (on the Plaza), the Whaley House (14th and H Streets), the Chapman House (974 10th Street), the Jacoby Building (791 8th Street), the Phillips House (71 E. Seventh Street), Arcata's Pythian Castle (1100 H Street), the Stone House (902 Fourteenth Street), and the Schorlig House (1050 12th Street) and the Keiser House (1022 10th Street). The Jacoby ~~Building~~ Storehouse is listed as a California Historical Landmark as are the historical sites of Camp Curtis and the Arcata Mad River Railroad Company. ~~among the State Registered Historical Landmarks.~~

H-1b **Local Historic Landmarks designations.** Buildings, ~~S~~structures, or sites having special character ~~-or special~~ historic, architectural, or aesthetic interest or value ~~may~~shall be designated as local Historic Landmarks. Such buildings, structures or sites shall be protected from demolition and inappropriate alterations. Locally designated Historic Landmarks are shown in Figure HP-a and are listed in Table HP-1, at the end of the Element. An updated inventory of buildings, structures and sites that are designated or potentially historic ~~eligible for designation as a Local Historic Landmark~~ shall be set by the City Council and maintained by the City. In addition to overall high integrity and exhibiting exceptional preservation, ~~o~~One or more of the following criteria shall be required for a structure or site to be eligible for listing:

1. The building, structure, or site is particularly representative of a distinct architectural period, type, style, or way of life.
2. The building or structure is of a type or style which was once common but is now rare.
3. The building, structure, or site is at least 50 years old.
4. The building, ~~or structure or~~ site is associated ~~connected~~ with a person or event important to local history.
5. The architect or builder is famous or well-recognized.
6. The building's style, construction method, or materials are unusual or significant.
7. The overall effect of the design or building details are beautiful or unusual.
8. The building contains original materials or workmanship of high or unusual value.

H-1c Historic Landmarks (HL) combining zone. The City shall formally designate Historic Landmarks with a special combining zone in the Land Use Code. The zone shall serve as a disclosure of the importance of the structure and of the limitations placed on its alteration or demolition. The request for designation may be initiated by the owner, City Council, Planning Commission, or the Historical Landmarks Commission. If initiated by the City, the owner shall be notified and be able to contest the process.

~~H-1d Discretionary review and approval requirements for demolition. [See policy H-5].~~

H-1de Design review approval for alterations and additions. The following types of changes to a structure designated by the HL combining zone shall not be permitted without first obtaining approval of the ~~Historic Landmarks Commission~~ appropriate Review Authority:

1. ~~Any eAny~~ exterior modifications or alterations, including changes in materials.
2. Interior alterations that would affect the exterior appearance.
3. Any addition to the designated structure.
4. Construction of a new building on a parcel with a designated Historic Landmark.

H-1ef Design criteria for alterations of and additions to local Historic Landmarks. At the discretion of ~~the the appropriate Review Authority~~ Community Development Director and/or Historical Landmarks Commission, an owner proposing any construction or alteration that may affect the historical character of the structure may be required to obtain an analysis of the proposed changes by a cultural resources consultant or other knowledgeable professional to determine the impact on the building's historical features.



In modifying historic structures, the distinguishing original qualities or character of the building as described in the parcel's HL ordinance shall not be destroyed. The removal or alteration of any historic material or distinctive architectural feature shall be avoided whenever possible. Whenever practicable deteriorated architectural features shall be repaired or restored rather than replaced. Contemporary designs for additions are not prohibited when such additions are compatible with and do not destroy the historical or architectural character of the property.

~~H-1g Preservation Incentives. To encourage property owners to obtain local Historic Landmark designation, the City will provide the following incentives or assistance:~~

- ~~1. Exemption from the requirements to provide any additional off-street parking, except for additions 200 or more square feet in size.~~
- ~~2. Exemption, for nonconforming uses, from Land Use Code limitations pertaining to non-conforming structures and site conditions.~~
- ~~3. Compliance with the State Historic Building Code and portions of the Uniform Code for Building Conservation, rather than the current edition of the Uniform Building Code.~~
- ~~3. Conservation easements for facades that may provide tax advantages to the donor.~~
- ~~3. Establishment of a Mills Act program, by the City, which would provide property tax reductions for historic properties subject to program criteria.~~
- ~~3. Facade rehabilitation grants or loans, through the Community Development Agency, for designated historic commercial structures.~~

H-1f ~~Historical Landmarks on the Humboldt State University~~ **Historical Landmarks on the Humboldt State University Cal Poly Humboldt campus.**

The City encourages HSU to initiate procedures for the recognition and protection, in compliance with Public Resources Code Section 5024, of historic structures and other historic resources on the campus. The following structures, located on the HSU campus, are hereby identified as local Historic Landmarks.

- | | |
|-------------------|------------------|
| 1. Founders Hall. | 3. Nelson Hall. |
| 2. Gist Hall. | 4. Jenkins Hall. |

~~H-1i~~ **Historical Landmarks Commission (HLC).** ~~The City Council shall create a Historical Landmarks Commission consisting of persons having knowledge, by virtue of training or experience, of Arcata's social and building history or of design and building. The Commission shall promote public involvement and education in preserving Arcata's heritage of older buildings.~~

POLICY H-2 POTENTIALLY HISTORIC NOTEWORTHY BUILDINGS/STRUCTURES

Objective. Identify and encourage retention of buildings, structures or sites that which could qualify as historical ~~landmarks, but~~ landmarks but are not currently designated. Apply similar review procedures and rehabilitation incentives to potentially historic structures as are applied to :HL properties. ~~Although some of these structures may lack the level of significance attached to designated landmarks, they have an architectural or design character which represents particular building styles or eras in the overall character and historical texture of-~~

H-2a Noteworthy-Potentially historic struct
~~the Historical Landmarks Commission t~~
Historic Noteworthy Buildings/Structure



structures. ~~Noteworthy~~ Potentially historic structures are those which may not have complete documentation as to their historical or architectural merit but ~~which have notable~~ have been identified as having notable characteristics, with an emphasis on aesthetic characteristics. ~~In addition to overall high integrity, the building, order to be eligible for listing,~~ a structure or site should have one of the following attributes in order to be eligible for listing:

1. Representative of a particular architectural style identified herein (Settlement, Victorian, Transitional, Modern).
 2. Representative of a period in the city's historical development as identified in the City's Context Statement.
 3. Associated with social history of the city as identified in the City's Context Statement.
 4. Of unusual or special design character.
- 4.

~~H-2b Incentives, list preparation, notification, and future landmark designation. Those properties listed as noteworthy structures shall be eligible for the following incentives and assistance:~~

1. ~~Exemption from the requirements to provide any additional off street parking, except for additions 200 or more square feet in size.~~
2. ~~Exemption, for nonconforming uses, from Land Use Code limitations pertaining to non-conforming structures and site conditions.~~
3. ~~Compliance with the State Historic Building Code and portions of the Uniform Code for Building Conservation, rather than the current edition of the Uniform Building Code.~~
4. ~~Conservation easements for facades that may provide tax advantages to the donor.~~
5. ~~Facade rehabilitation grants or loans, through the Community Development Agency, for designated historic commercial structures.~~

The City Historical Landmarks Commission shall notify the owners of property being considered for placement on the list. Those owners shall be given the opportunity to contest and appeal the listing. There shall also be a procedure established for properties to be removed from the list. The owners of properties listed as having noteworthy potentially historic structures are encouraged to apply for Historic Landmark designation.

H-2be Potentially Historic ~~Noteworthy buildings structures on the Cal Poly Humboldt Humboldt State University campus. The Wagner House located on campus is hereby identified as a noteworthy structure.~~ The City requests that all structures of historic value be preserved

Historical Preservation Element



and protected from demolition and from alterations or additions that are incompatible with their historical character. ~~Noteworthy-Potentially historic~~ structures on the HSU campus will be added to the ~~noteworthy-potentially historic~~ structures list developed by the City pursuant to H-2a.

H-2d **Design review approval.** Design Review ~~Commission review~~ and approval shall be required from the appropriate review authority, ~~with input from the Historic Landmarks Commission, shall be required~~ for all exterior alterations to ~~noteworthy-potentially historic~~ potentially historic structures, when or if alterations require a building permit, including changes in types of materials and additions. ~~The review authority may request a recommendation from the Historic Landmarks Committee prior to its decision on the project.~~

H-2ce **Design criteria for alterations and additions.** Prior to approval of any exterior change, the ~~Design Review Commission~~ Review Authority shall make findings of fact that the alteration or addition maintains the historic integrity of the resource, and is compatible with and does not destroy the historical or architectural character of the property and the immediate neighborhood.

~~H-2f Demolition Controls. [See policy H-5].~~

POLICY H-3 ARCATA PLAZA AREA HISTORIC DISTRICT

Objective. Protect and preserve the Arcata Plaza and the older structures that border the adjacent streets and help define the Plaza's character, for the unique historical, architectural, aesthetic, and economic values that it represents to the city.

H-3a **Arcata Plaza Historic District.** The Plaza Area has a special character and unique historical, aesthetic and cultural interest and significance to the residents and businesses of Arcata. Reflecting its central place in Arcata's heritage and identity, the Plaza Area, as shown on Figure HP-a, is hereby designated as a local historic district.



~~Figure HP-a Arcata Plaza Area Historic District~~

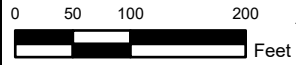


Attachment: G. Ch 5 Design Historic (4214 : GP #XVII FBC & GP wrap-up)

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1 inch = 167 feet 1:2,000



Figure HP-a
Arcata General Plan: 2045

ARCATA PLAZA HISTORIC DISTRICT

H-3b **Landmark Historic District combining zone.** The Plaza Area Historic District shall, by ordinance pursuant to the Land Use Code, be designated within the Landmark Historic District _____ Combining Zone. Structures within the Historic District shall receive the same _____ protections as are provided to individually-designated Historic Landmarks.

~~H-3c **Controls on demolition.** [Policy H-5 applies.]~~

H-3cd **Design review approval.** Review and approval by the Design Review Authority ~~Commission~~ shall be required for all exterior alterations of and additions to structures located within the Plaza Area Historic District.

~~H-3e **Design criteria for alterations and additions.** [policies D-2e and D-2g shall apply]~~

~~H-3f **Regulatory and other incentives for preservation.** In addition to the regulatory and other incentives in H-1g, the following shall also apply:~~

- ~~1. The City Council, with the help of the Historic Landmarks Commission and the Historical Sites Society of Arcata, shall develop a commemorative plaque program to provide special identification of historical structures.~~
- ~~2. Non-conforming uses that have historical value will be allowed to continue.~~

H-3dg **Arcata Plaza as a historic site.** The Arcata Plaza Historic District includes at its center the city park known as the Arcata Plaza. The following principal features of the Plaza which define its historical character shall be preserved:

- ~~1. The McKinley Statue at the center of the Plaza.~~
- ~~1.2. The generally symmetrical pattern of walkways.~~
- ~~2.3. The open nature of the Plaza and the absence of buildings within it.~~
- ~~3.4. The Women's Christian Temperance Union drinking fountain on "H" Street.~~
- ~~4.5. The existing Plaza palm trees.~~

POLICY H-4 NEIGHBORHOOD CONSERVATION AREAS (NCAs) ~~AND SPECIFIC PLANS~~

Objective. ~~I Designate the Central Arcata, Arcata Heights, and Bayview neighborhoods, are designated and Bayside areas as~~ Neighborhood Conservation Areas. ~~E and ensure~~ that new construction, modifications or alterations of ~~noteworthy historic~~ structures, and significant changes to other structures are harmonious with established and planned neighborhood elements within ~~the existing character of the Central Arcata, Arcata Heights, and Bayview se~~ neighborhoods.

H-4a **Neighborhood Conservation Areas.** ~~Arcata's~~ The following NCAs, with the boundaries shown in Figure HP-b, are ~~hereby established~~:

1. Bayview Conservation Area.
2. Arcata Heights Conservation Area.
3. ~~“ “Central” Conservation Area.~~

~~H 4b Demolition controls. [See policy H 5.]~~

~~H-4be Design review. All structures located within an NCA, including single family houses, historic structures identified in specific plans, and existing structures with exterior alterations or renovation of more than 25 percent of the floor and/or exterior wall area, shall be subject to design review and approval. Prior to approval of any exterior change requiring a building permit, the Design Review Authority shall make a finding must be made that the alteration or addition is compatible with established and planned neighborhood elements and does not destroy the historical or architectural elements of the property. , design will be compatible with the existing character of the NCA.~~

~~H-4d Rehabilitation assistance programs. Any City sponsored or assisted rehabilitation programs shall give priority to qualifying structures within the boundaries of NCAs. Such rehabilitation shall be consistent with the architectural and aesthetic character of the area and the individual structure.~~

~~H 4e Design criteria for alterations and additions. Prior to approval of any exterior change requiring a building permit, the Design Review Commission shall make a finding that the alteration or addition is compatible with and does not destroy the historical or architectural character of the property and the surrounding neighborhood conservation area.~~

~~H-4f Specific Plan Districts. The following Specific Plan Districts, with boundaries shown in Figures HP b and HP c, are hereby established:~~

- ~~1. Bayside Specific Plan District.~~
- ~~2. “South of Samoa” (SOS) Specific Plan District.~~

~~Specific Plans prepared for these Districts will include preservation measures for historic and noteworthy structures.~~

POLICY H-5 CONTROLS ON DEMOLITIONS AND RELOCATIONS OF HISTORIC STRUCTURES

Objective. To prevent the premature demolition of existing buildings without first evaluating whether they are ~~contributory to the historical or architectural character of the City or neighborhood~~ historically significant, and to consider the potential for preservation of those found to contribute to such character.

H-5a **Discretionary demolition review required.** No building within the City shall be demolished, in whole or in part, without review and approval by the Review Authority ~~Historical Landmarks Commission~~ prior to issuance of a Demolition Permit. The Review

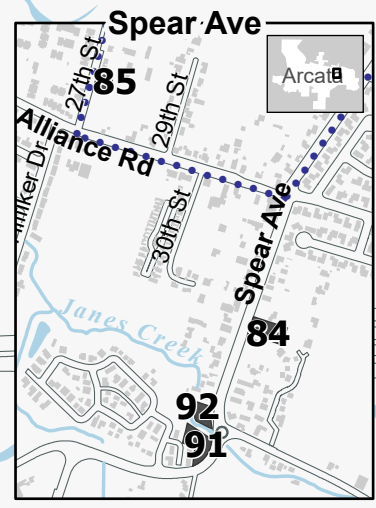
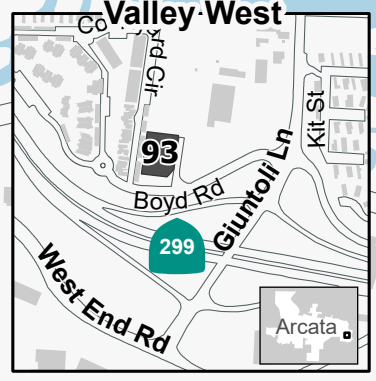
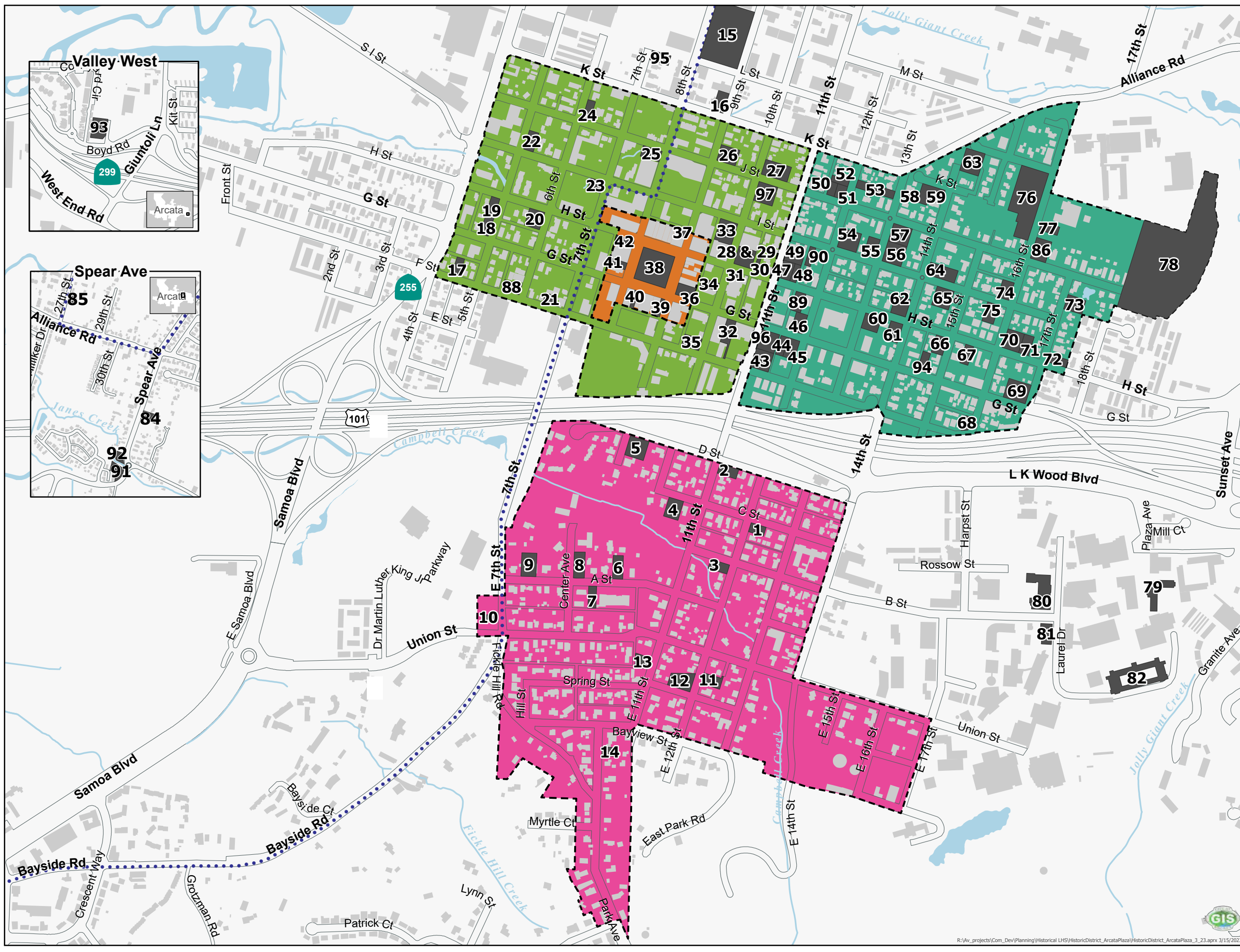
Authority shall consider the recommendations of the Historic Landmarks Committee unless a project is determined to not be a potentially historic resource. A Notice of Proposed Demolition shall be provided to all property owners within a 300-foot radius and to the Historical Sites Society of Arcata. Demolition or relocation projects within an :HL combining zone, or for potentially historic resources eligible for listing shall be reviewed and noticed in accordance with City Design Review and Public Hearing procedure.

~~Figure HP-b Historic Landmarks and Neighborhood Conservation Areas (11 x 17, front side)~~

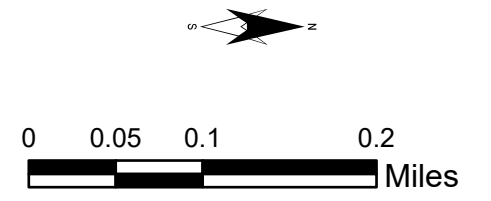
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Figure HP-b
Arcata General Plan: 2045

LOCAL HISTORIC LANDMARKS AND NEIGHBORHOOD CONSERVATION AREAS



- Coastal Zone Boundary
- Landmark Historic Parcel Combining Zone
- Neighborhood Conservation Districts**
- Arcata Heights Conservation Area
- Arcata Plaza Historic District
- Bayview Conservation Area
- Central Conservation Area



Adopted XXXXXXX, XXX

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Prior to its decision, the [Review Authority](#) shall consider the recommendations of the Historical Sites Society of Arcata or its designated representative. For partial demolitions, the applicant shall be required to submit a demolition plan showing those portions to be demolished and those to be retained. The following findings shall be required to approve demolition permit:

1. The building ~~does not contribute to the historical or aesthetic character of the neighborhood or the city~~ is not historically significant, based on criteria outlined for [potentially historic structures](#).
2. Although the building does have historical or aesthetic merit, it:
 - a. has sustained substantial damage to key structural components, and
 - b. there are no feasible alternatives to demolition of the building.

H-5b ~~Waiting period~~ [Required delay of Action](#) for demolition of designated landmarks. Any approved demolition ~~or permit~~ [relocation permit](#) for designated historical landmarks shall be automatically subject to a delay of 180 days before the building permit for demolition may be issued by the ~~City~~ [City, unless the a waiver to the delay granted by the Review Authority.](#)

H-5c **Deconstruction of older buildings.** In those instances where demolition is authorized, it is encouraged that the buildings be deconstructed and that building components, fixtures, and materials be salvaged for future re-use.

POLICY H-6 [PUBLIC PARTICIPATION](#), [INCENTIVES](#), INFORMATION, AND EDUCATION [POLICY](#)

Objective. Promote public awareness of the City's historical heritage and resources, provide information and education about the methods and techniques to protect and enhance the quality of these resources, and encourage public participation in preserving Arcata's historical heritage.

H-6a [General Incentives for Historic Structures and Sites.](#) Those properties listed as Historic Landmark or as Potentially Historic structures or sites shall be eligible for the following incentives and assistance:

1. [Exemption from the requirements to provide any additional off-street parking.](#)
2. [Exemption, for nonconforming uses, from Land Use Code limitations pertaining to non-conforming structures and site conditions.](#)
3. [Compliance with the State Historic Building Code and portions of the Uniform Code for Building Conservation, rather than the current edition of the Uniform Building Code.](#)
4. [Conservation easements for facades that may provide tax advantages to the donor.](#)

5. At the option of the City, façade rehabilitation grants or loans for designated historic commercial structures, to the extent available and as approved by the City.

H-6b Specific Incentives for Structures and Sites within the :HL District. The City Council may grant any or all of the following rehabilitation incentives to a site or structure that is designated within the :HL combining zone, in addition to the general incentives listed above.

1. Adaptive Reuse in Residential Zones. In order to encourage the economic viability and preservation of Historic Landmark Structures in the residential zoning districts, Historic Landmark Structures within the :HL combining zone may pursue occupancy by land uses that are not otherwise allowed within the primary residential zoning district.

2. Fee Waivers. Permit fee waivers or reductions as granted by the City Council.

3. Mills Act Property Tax Abatement Program. Acceptance of a Mills Act agreement between the City and property owner, which provides property tax reductions for historic properties subject to program criteria.

4. State and Federal Incentives. Other incentives may include Federal Rehabilitation Tax Credits, and the California Heritage Fund Grant Program.

5. Other Incentives. Other incentives deemed necessary to encourage preservation, as approved by the City Council.

H-6c Public Information and Education. The City shall work in partnership with the Historic Landmarks Committee, Planning Commission, and Historic Sites Society of Arcata as appropriate to promote education and appreciation of the community's heritage sites, and contribute to Arcata's tourism attractions. City staff shall continue to update informational materials describing the advantages of having structures or sites designated as local Historic Landmarks or as potentially historic structures.

H-6d Role of Historical Sites Society of Arcata (HSSA). The City shall send notice to the HSSA on all matters pertaining to the preservation of historical resources. These include, but are not limited to, historical educational opportunities for the community, requests for comments on proposed Historic Landmarks, additions to the list of noteworthy potentially historic structures, and proposed alteration or demolition of such structures.

~~The HSSA may submit its comments to the Review Authority. Historical Landmarks Commission. In consultation with the HSSA and the HLC, the Community Development Department shall prepare a brochure which provides guidelines and federal standards for restoration, alteration, and additions to historical landmark and noteworthy structures. The HSSA is also designated as the organization to oversee the operation of Phillips House Museum of the City of Arcata.~~

~~H-6b **Commemorative** In order to identification of structures and to commemorative developed. The lead organization~~



~~plaques for historical structures, provide greater recognition and designated historical landmark comply with federal standards, a plaque program should be HSSA is encouraged to be the for this effort.~~

~~H-6c **Informational guides and walking tours.** The City encourages the HSSA and Arcata Chamber of Commerce to prepare informational guides or walking tour guides for Arcata's most significant historical resources. The purposes of the tour guide(s) are to promote appreciation of the community's heritage sites and contribute to Arcata's tourism attractions.~~

~~H-6d **Brochure on benefits and advantages for designating historical landmarks.** The Arcata Community Development Department shall prepare an informational brochure which describes the benefits and advantages of having structures or sites designated as local Historic Landmarks.~~

POLICY H-7 ARCHEOLOGICAL AND CULTURAL RESOURCES AND WIYOT TRIBAL HISTORY

Objective. ~~Protect, t and preserve~~ and enrich Native American and Euro-American archeological sites and cultural resources within the City of Arcata. Preserve and enrich Native American and Euro-American archeological sites and cultural resources within the City of Arcata, particularly resources related to Wiyot Tribal history. Adopt a series of policies that ensure pre-project consultation and review as well as collaborative government to government relations with the Wiyot Tribe, Bear River Band, and Blue Lake Rancheria.

H-7a **Cultural Resources Project Review.** As part of the environmental and project review process, ~~the City of Arcata shall enter into a Memorandum of Agreement (MOA) with the Northwest Information Center of the Historical Resources Information System of the State of California. Under the MOA, all proposed discretionary projects under the California Environmental Quality Act shall be subject to cultural resources sensitivity review by the local area Tribal Historical Preservation Officers (THPOs) or their designated representatives, and the Northwest Information Center. In order to provide a context for city projects, for the evaluation of cultural significance and for the interpretation of the results of cultural resources project reviews, the City of Arcata shall contract for a general prehistoric, ethnographic, and historic overview of the city and its environs.~~

~~H-7b **Archaeological Surface Reconnaissance.** If the cultural resources project review determines that the project is located in an area with a high probability of archaeological resources, an archaeological survey by a professional archaeologist or~~

other qualified expert shall be performed to the satisfaction of the THPOs or their designated representatives.

If the results of reconnaissance show that the project area contains a resource of cultural significance, and if it is demonstrated that a project will cause damage to such a resource, the City shall confer with appropriate agency and governmental partners. In the case of sites potentially containing resources of tribal cultural significance, all local Tribal Historic Preservation Officers or their designated representatives shall be notified. This consultation may require reasonable efforts to be made to permit any or all of these resources to be preserved in place or left in an undisturbed state. Examples of other treatment include, but are not limited to, the following:

1. Formally evaluating the site/find to determine its eligibility for the California Register of Historical Resources.
2. Modifying the project to avoid portions of the site with archaeological resources.
3. Providing or conveying easements or other deed restrictions.
4. Capping or covering archaeological resources with a soil layer before construction.
5. Planning open space to incorporate archaeological sites.

H-7be Inadvertent Archeological Discoveries and associated mitigations.

Standard Operating Procedures for handling “post-review” of inadvertent archeological discoveries and/or human remains shall be adopted for all phases and aspects of work carried out by or for the City of Arcata and will be housed in the City’s Zoning Ordinance. This protocol shall be developed in partnership with, and approved by, local Tribal Historic Preservation Officers and/or their respective Tribal Councils. The intent of these procedures will generally be to stop work upon discovery, evaluate the discovery with support from qualified archeologists and THPOs or their designees, consult with the Tribes and other interested parties on the inadvertent discovery, determine significance, and if resources are determined significant per the California Register of Historical Places, avoid or minimize direct or indirect impacts to significant archaeological or Native American discoveries.

~~**Mitigation of potential impacts on archeological resources.** If the results of the surface reconnaissance show that the project area contains a resource of cultural significance, and if it is demonstrated that a project will cause damage to such a resource, the City may require reasonable efforts to be made to permit any or all of these resources to be preserved in place or left in an undisturbed state. Examples of other treatment include, but are not limited to, the following:~~

- ~~1. Modifying the project to avoid portions of the site with archaeological resources.~~
- ~~2. Providing or conveying easements or other deed restrictions.~~
- ~~3. Capping or covering archaeological resources with a soil layer before construction.~~

4. ~~Planning open space to incorporate archaeological sites.~~

5.6 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURE	RESPONSIBLE PARTY	TIME FRAME
faça deH -1	Historic and Potentially Historic resources inventory Update historical resources inventory to include both designated Landmark Sites and Undesignated Historic Sites, in partnership with the Historic Landmarks Committee.	Comm. Devel. Dept./ Historical Landmarks Committee	Year 2-5
H-2	Preservation Incentives and Educational Materials Develop additional regulatory and other incentives for preservation in partnership with the Historic Landmarks Committee and Historical Sites Society of Arcata. Such incentives may include: 1. Development of a commemorative plaque program to provide special identification of historical structures, buildings, or sites. 2. Informational guides and walking tours to promote appreciation of the community’s heritage sites and contribute to Arcata’s tourism attractions. 3. Brochure on benefits and advantages for designating historical landmarks which describes the benefits and advantages of having structures or sites designated as local Historic Landmarks.	Comm. Devel. Dept./ Historical Landmarks Committee/Planning Commission/Historic al Sites Society	Ongoing
H-3	Local Tribal Input to City Historic Context Statement In order to provide a context for city projects, for the evaluation of cultural significance and for the interpretation of the results of cultural resources project reviews, the City of Arcata shall contract for a general prehistoric, ethnographic, and historic overview of the city and its environs.	Comm. Devel. Dept.	Year 1
H-4	Local Tribal Input to Sensitivity Mapping Collaborate with the three area THPOs to undertake periodic review of the “Archeologic Sensitivity Map” to ensure updated information regarding tribally significant resources is shared.	Community Development Dept.	Every 5 years beginning in year 5

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~~H 7d **Monitoring of Construction.** In appropriate circumstances, when archaeological resources are likely to be present at a construction site, monitoring of excavation and other soil disturbing activities by archeological and/or Native American observers shall be required.~~

~~H 7f **Discovery of archeological resources.** Upon discovery of archeological or paleontological materials, all grading or other land disturbing construction activities at the site shall be suspended until the nature of the cultural resources has been ascertained and the appropriate disposition method determined.~~

TABLE HP – 1 DESIGNATED HISTORICAL SITES LIST

#	NAME	#	Street Address	Built	Historic Period
1	Fry House	290	12 th Street	1874	Settlement Period
2	Dillon House	188	11 th Street	1886	Settlement Period
3	Cates House	185	12 th Street	1884	Settlement Period
4	Malvich House	1030	C Street	1914	Craftsman Period
5	Moore House	930	D Street	1903	Transitional Period
6	Shuman House	965	A Street	1906	Transitional Period
7	Aaron Alden House	947	A Street	1908	Transitional Period
8	Putnam-McCready House	913	A Street	1910	Transitional Period
9	Simms-Hunt House	855	A Street	1905	Transitional Period
10	Phillips House	71	7 th Street	1855	Settlement Period
11	Beacom House	68	12 th Street	1889	Settlement Period
12	Maronich House	87	12 th Street	1889	Victorian Period
13	Susan House	67	11 th Street	1874	Settlement Period
14	Leveque House	320	Park Avenue		
15	Ca. Central Creameries Plant	2151	9 th Street	1918	Craftsman Period
16	Thomas Devlin House	885	K Street	1900	Transitional Period
17	Burrows House	453	F Street		
18	Simpson House	493	G Street		
19	Truesdell House	494	H Street	1876	Settlement Period
20	Fleckenstein-Newton House	588	H Street		
21	Monette House	665	F Street	1885	Settlement Period
22	Senevey-Menefee House	513	I Street	1880	Settlement Period
23	Baiocchi House	895	7 th Street	1925	Craftsman Period
24	Selvage House	609	J Street	1874	Settlement Period
25	Eddy-Greene House	987	8 th Street		
26	Vaissade House	927	J Street	1905	Transitional Period
27	Nixon House	1022	10 th Street	1858	Settlement Period
28	Minor Theatre	1013	H Street	1914	Craftsman Period
29	Minor Building	1015	H St.	1914	Craftsman Period
30	Epicurean Restaurant	1057	H Street	1884	Victorian Period
31	First National Bank	1000	H Street	1913	Craftsman Period
32	Arcata Theater	1036	G Street	1937	Modern Period
33	Seely & Titlow Building	970	I Street		
34	Chevret House	739	10 th Street		
35	Murray House	987	F Street	1901	Victorian Period
36	Hotel Arcata	780	9 th Street	1915	Craftsman Period
37	Hunt House	839	9 th Street		
38	Plaza		Block 167	1850	Settlement Period
39	Union Building	898	G Street	1890	Victorian Period
40	KXGO Radio/ Western Auto	826	G Street		
41	Old Post Office Building	735	8 th Street		
42	Jacoby Building	791	8 th Street	1857	Settlement Period
43	Packer's House	630	11 th Street	1870	Settlement Period
44	Sowash House	1160	G Street	1914	Craftsman Period
45	Morgan House	1192	G Street	1901	Victorian Period
46	Miller House	1193	G Street	1900	Transitional Period
47	Pythian Castle	1100	H Street	1884	Victorian Period

#	NAME	#	Street Address	Built	Historic Period
48	Stewart Foster House	1139	H Street		
49	Moulton-Barlow House	860	11 th Street	1907	Transitional Period
50	Wagner Block House	1157	J Street	1920	Craftsman Period
51	Wagner Block House	1187	J Street	1920	Craftsman Period
52	Wagner Block House	1087	12 th Street	1920	Craftsman Period
53	Schorlig House	1050	12 th Street	1885	Victorian Period
54	C.E. Daniels House	918	12 th Street	1885	Victorian Period
55	Beers-Ely House	1285	I Street	1877	Settlement Period
56	Bair-Stokes House	916	13 th Street	1888	Victorian Period
57	Horel House	980	13 th Street	1904	Transitional Period
58	McCormack/ MacMillan House	1056	13 th Street	1903	Transitional Period
59	Gastman-Gaynor Beer House	1362	K Street	1902	Transitional Period
60	David Wood House	1318	H Street	1910	Craftsman Period
61	Ericson House	1376	H Street	1870	Settlement Period
62	Whaley House	1395	H Street	1855	Settlement Period
63	Zehndner-Parton House	1164	14 th Street	1902	Transitional Period
64	Stone House	902	14 th Street	1888	Victorian Period
65	Cullberg House	1452	I Street	1862	Settlement Period
66	A. Gastman House	1492	H Street	1898	Victorian Period
67	William-Smith House	1542	H Street	1888	Victorian Period
68	Jako-Wagner House	1593	F Street		
69	Barter Bungalow/ Court	1645	G Street	1930	Craftsman Period
70	Smith-Mckenzie House	1619	H Street	1877	Settlement Period
71	Scribner House	1661	H Street	1885	Victorian Period
72	Sweet House	1717	H Street	1911	Craftsman Period
73	Godden House	938	17 th Street	1919	Craftsman Period
74	Davidson-Carroll Store/ House	1593	I Street	1914	Craftsman Period
75	Keller House	1566	I Street	1894	Victorian Period
76	Stewart School Building	1125	16 th Street	1925	Craftsman Period
77	St. Mary's Church	1090	16 th Street	1884	Victorian Period
78	Greenwood Cemetery	1757	J Street	1860	Settlement Period
79	Nelson Hall		Humboldt St. Univ.	1940	Craftsman Period
80	Gist Hall		Humboldt St. Univ.	1933	Craftsman Period
81	Jenkins Hall		Humboldt St. Univ.	1950	Craftsman Period
82	Founders Hall		Humboldt St. Univ.	1921	Craftsman Period
83*	Bayside Post Office	1786	Old Arcata Road		
84*	Chaffey House	1220	Spear Avenue	1887	Victorian Period
85*	O'Grady House	1630	27 th Street		
86*	Strobel House	1621	J Street	1874	Settlement Period
87*	Zehndner-Harpst-Austin House	1860	11 th Street	1870	Settlement Period
88*	McMillian House	589	F Street	1877	Settlement Period
89	Poinsett House	3501	Boyd Road	1899	Victorian Period
90	Devlin Cottage	1188	7 th Street	1904	Transitional Period
91	First Presbyterian Church of Arcata	670	11 th Street	1916	Craftsman Period
92	Chapman House	974	10 th Street	1876	Settlement Period

*Numbers do not appear on map.

5.6 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURE	RESPONSIBLE PARTY	TIME FRAME
LU-1	Historic Landmarks, conservation easements (LUC Amendment) A. _____ Revise development standards for designated landmarks (delete additional uses, restrict conversion of res. Structures in res. Zones to non-residential uses, add other incentives). B. _____ Revise Historical Landmarks combining zone re: demolition controls. C. _____ Add authority for conservation (facade) easements. D. _____ Define role of Historical Landmarks Commission. E. _____ Add requirement for demolition permit. F. _____ Create authority for Mills Act contracts.	Community Development Dept./ Planning Commission	Year 1
H-1	Create Historic Landmarks Commission The City shall appoint a Historic Landmarks Commission for the purpose of pursuing Certified Local Government status, developing a Noteworthy Structure List and promoting other historic preservation activities. This includes coordinating with HSU for listing of State owned historic structures.	City Council	Year 1
H-2	Historic resources inventory Update historical resources inventory (add structures throughout the City based on age, and add Bayside area to inventory).	Comm. Devel. Dept./ Historical Landmarks Commission	Year 2
H-3	Certified local government status Apply to the State Office of Historic Preservation for Certified Local Government status.	Historical Landmarks Commission	Year 2
H-4	Training and information program Develop a design brochure(s) and a brochure on benefits of landmark designation.	Historical Landmarks Commission	Year 2
H-5	Noteworthy Structures List The Historical Landmarks Committee shall prepare and maintain a noteworthy structures list.	Historical Landmarks Commission	Year 1

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Chapter 6 – Health and Safety

Public Safety Element

Noise Element

Health Element

PUBLIC SAFETY ELEMENT

Rev. 04.25 updated

6.1 INTRODUCTION

State requirements for Safety Elements has evolved over time from the identification of unreasonable risks, such as flooding and seismic hazards, and policies for the protection of the community from such risks, to requiring that climate change vulnerabilities and adaptations be integrated and aligning the General Plan with the Humboldt County Operational Area Hazard Mitigation Plan. The City of Arcata Public Safety Element addresses the protection of life and property from natural hazards, including seismic hazards, such as earthquake and tsunami; geologic hazards including landslides; wildfire; severe weather and flooding; and human-caused hazards including those related to hazardous materials use, storage, transport, and disposal, as well as aviation hazards. This Element also addresses hazards that may be intensified by climate change, including more severe storms that worsen landslides and flooding and droughts and heat that may increase wildfire frequency or severity. The overview of each hazard in this Element includes a summary of the climate change impacts on the hazard based on the climate change vulnerability assessment prepared for the Hazard Mitigation Plan.

The goal of this Element is intended to facilitate safety from hazards, achieve racial and social equity, and enhance community preparedness and resilience. Risks can be reduced by considering hazards, and the potential influence of future climate change on those hazards, in land use and development decisions, and by implementing policies and programs to reduce losses to life and property. Effective land use planning, site design, and construction decisions can significantly reduce potential hazard levels and can also facilitate recovery after a disaster.

Overview of Emergency Preparedness and Police and Fire Services. Preparation for, and timely and adequate response to emergencies are essential community services that require planning and community-wide coordination. Emergency preparedness is the combined responsibility of the City, emergency response organizations, and community residents. The City adopted the Hazard Mitigation Plan on March 18, 2020, to guide pre- and post-disaster mitigation of the hazards identified. The City of Arcata Emergency Operations Plan also informs the City's response to and recovery from emergencies/disasters and outlines coordination among departments and regional partners. The 2021 update to the Emergency Operations Plan integrated the Hazard Mitigation Plan's evaluation of hazards affecting the City and the hazard mitigation action plan.

The Arcata's Volunteer Fire Department-Fire Protection District and City Police Department provide critical emergency response services and leadership, and leadership and serve as the community's primary response agencies in emergency situations. These agencies are most efficient when they can rely on emergency response organizations, such as the State of

[California's Governor's Office of Emergency Services and the County Office of Emergency Services](#), community groups, and individuals to provide organized support when needed. The [Neighborhood Arcata Emergency Services Response Teams \(NESTCERT\)](#) consists of residents who have been trained in areas such as fire prevention and suppression, light search and rescue, and disaster medical operations in order to be able to assist their community in an emergency that exist in several of Arcata's neighborhoods are good examples of local organizations with the resources and training to respond to emergencies. [The Emergency Operations Plan](#) integrates the HPM hazard assessment and identifies the

Overview of Arcata's Potential Seismic and Geologic Hazards. The northern coast is one of California's most seismically active regions, and special precautions must be taken to protect residents from the effects of seismic events and hazards. Faults in and around the community, and the offshore Cascadia subduction zone (a 750-mile long major thrust fault) are considered active. Recent geotechnical studies identify fault zones, as well as areas susceptible to ground shaking, liquefaction, and tsunami run-up that represent safety hazards to the community. The potential for a major seismic event [that may generate a tsunami has prompted emergency response organizations, such as the Humboldt Earthquake Education Center at Humboldt State University, to provide information promoting earthquake and tsunami awareness and emergency preparedness](#) the formation of the Redwood Coast Tsunami Work Group, an organization comprised of local, state, Tribal, federal agencies and non-governmental organizations to define the needs of local jurisdictions to mitigate the North Coast earthquake and tsunami hazard and to promote a coordinated, consistent mitigation program for all coastal areas. [The California Geological Survey Seismic Hazards Program has completed probabilistic tsunami hazards modelling and public tsunami hazard area maps for Humboldt County in 2021, which show tsunami inundation potentially impacting the South G Street, South of Samoa, and portions of the Windsong neighborhood. The California Geological Survey is now in the process of developing land use guidance pursuant to the Seismic Hazards Mapping Act for areas subject to potential tsunami inundation that is expected to be available to guide local land use planning by the end of 2022.](#) Geologic safety hazards also include potential [landslides and](#) slope instability on Arcata's hillsides and soil erosion. Mapped hazard areas are shown on Figure PS-a, located in a map pocket at the end of this Element.

The Cascadia Subduction Zone

North of the Mendocino Triple Junction, the Gorda Plate and its northern extension, the Juan de Fuca Plate, move eastward on a collision course with the North American Plate. The Gorda Plate slowly descends beneath the North American Plate along the Cascadia Subduction Zone. Most scientists believe that the upper 50 miles or so of the contact between the Gorda Plate and the North American Plate is locked. This boundary is called the megathrust.
Source: [Living on Shaky Ground](#).
Humboldt State University

Potential Flooding Hazards Overview. Arcata is bordered by the Mad River to the ~~north, and north and~~ bisected by several streams. These watercourses and their associated floodplains have been altered over time and are subject to future change. They have the potential, during peak flows, to overtop their banks with sufficient volume and velocity to damage adjacent structures, facilities, and natural areas. [The California Division of Safety of Dams maintains dam breach inundation mapping for Matthews Dam on the Mad River in Trinity County. A dam breach could inundate portions of Boyd Road area and the Mad River Parkway area.](#) A number of Arcata's creeks have been modified with culverts, channels, and coverings. These structures inhibit natural infiltration and flood protection, resulting in increased flood volumes and velocities downstream. There is strong community interest in returning local creeks to more natural conditions, and incorporating measures, such as detention basins and natural infiltration, that will reduce flood velocities and volumes.



ARCATA'S DRAINAGE MASTER PLAN GOALS INCLUDE:

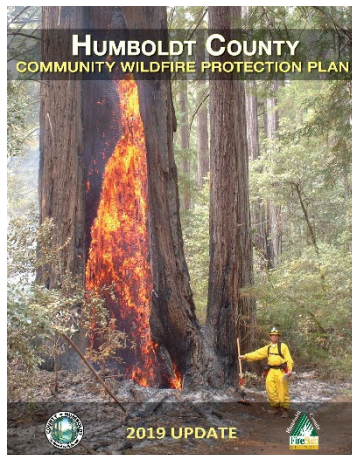
- Minimize increases in the volume and the flow of stormwater runoff associated with new development, so as to minimize increases in the hazards and the costs associated with flooding.
- Minimize the erosion potential from a development or construction site so as to prevent deposition of sediment into streams and other receiving water bodies.
- Maintain the integrity of stream hydrology by preventing stream channel erosion so as to sustain the hydrologic functions of streams.
- Reduce the pollutant load in stormwater runoff from developing and urbanizing areas so as to preserve the natural biological functions of streams and other receiving water bodies (and flood management and stream habitat quality).
- To the extent practical, acquire easements and properties necessary for effective drainage management.

~~**Hazardous Materials Overview.** Human exposure to hazardous materials and conditions must be minimized to ensure the well being of the community. Reducing this exposure must include cleaning up contamination from the past and avoiding use of hazardous materials in the future. Hazardous materials used primarily in wood products processing and auto-related operations have resulted in site and groundwater contamination that remain today. Minimizing future health hazards can be accomplished by:~~

- ~~1. Elimination of many toxic substances.~~
- ~~2.1. Substitution of environmentally safe alternatives.~~
- ~~3.1. Safer handling and disposal procedures of those materials still available.~~

~~There is community interest in replacing harmful substances and materials with biodegradable and less damaging substitutes, and cleaning up existing contaminated sites.~~

Fire Hazards Overview. Despite the coastal influences and moist climate, the Office of the State Fire Marshal has classified the- Planning Area surrounding the City as “Moderate” Fire Hazard Severity Zone with small pockets of “High” fire severity, indicating the relative level of wildfire hazard.- Areas throughout the State are aggregated into Moderate, High, or Very High zones, based on fuel loading, slope, fire weather, and other relevant factors including areas where winds may be a major cause of wildfire spread. In addition to state Fire Hazard Severity Zone mapping, the U.S. Forest service has identified approximately 70 percent of the City as a wildland urban interface, or “WUI”, which is the area where structures and other human development meet or intermingle with undeveloped wildland or vegetative fuels.



The portions of the Planning Area outside City limits are within State Responsibility Area, which means that CAL FIRE is financially responsible for preventing and suppressing wildfires, as defined in Section 4102 of the Public Resources Code. All areas in State Responsibility Areas are zoned with a wildfire hazard rating, including non-wildland areas and water bodies. Areas within City boundaries are Local Responsibility Area. The Arcata Fire Protection District (Fire District) boundary includes the City and sphere of influence, and most of Planning Area. The upper Jacoby Creek area and a portion of the Planning Area on the western side of Fickle Hill Road contain rural residential development and are outside the boundaries any fire related district.

The Fire District maintains three fire stations, two located within the City (the Arcata Station at 631 9th Street and the Mad River Station at 3235 Janes Road) to provide rapid response to all incidents within its service area. The Fire District maintains effective mutual aid agreements with Humboldt Bay Fire, Samoa Peninsula Fire Department, Blue Lake Fire Protection District, Fieldbrook Fire Department, KFPD, Westhaven Volunteer Fire Department, and CAL FIRE.

Hazardous Materials Overview. Human exposure to hazardous materials and conditions must be minimized to ensure the well-beingwellbeing of the community. Reducing this exposure must include cleaning up contamination from the past and avoiding use of hazardous materials in the future. Hazardous materials used primarily in wood products processing and auto-related operations have resulted in site and groundwater contamination that remain today. Minimizing future health hazards can be accomplished by:

1. Elimination of many toxic substances.
2. Substitution of environmentally safe alternatives.
3. Safer handling and disposal procedures of those materials still available.

There is community interest in replacing harmful substances and materials with biodegradable and less damaging substitutes, and cleaning up existing contaminated sites.

~~**Human Health Hazards Overview.** Sound physical and mental health are important factors when considering the well being of the community. Access to health care providers, social services, counseling, and facilities such as hospitals, clinics, temporary shelters and food distribution sites, should be taken into consideration when planning the community's future. These services and facilities are typically provided by agencies and organizations other than the City; however, the City can play an important role in minimizing health hazards by identifying locations where treatment and assistance are available, and by promoting accessibility to these services.~~

Airspace Protection Overview

~~The Humboldt County Airport Land Use Compatibility Plan (ALUCP) Airspace Compatibility Policy Map for California Redwood Coast-Humboldt Airport (Figure 4-3), and as also shown on Public Safety Element Figure XX, shows the aircraft precision instrument approach path from the southwest for California Redwood Coast-Humboldt Airport overlaying portions of the City limits and Planning Area. The Federal Aviation Administration (FAA) establishes standards to protect airspace by defining imaginary surfaces that establish limit maximum building heights above ground level in areas around airports. The California Redwood Coast-Humboldt Airport approach surfaces are hundreds of feet above ground level within the City and new development will likely not pose new hazards, or an become an obstruction, to navigation. Nonetheless, new development in areas underlying airspace protection areas, as identified in Humboldt County ALUCP Airspace Compatibility Policy Map for California Redwood Coast-Humboldt Airport (Figure 4-3), should be evaluated to ensure that such development does not result in hazards to air navigation by penetrating approach surfaces or creating other hazards to navigation such as hazardous wildlife attractants, sources of glare, light displays which could disorient pilots, and sources of electromagnetic interference.~~

Climate Change Adaptation and Resiliency Overview

~~The City of Arcata recognizes that the current hazard risk landscape is changing, and will continue to change, as the impacts of climate change affect ocean levels, weather and climatic patterns, extreme heat, soil and forest humidity and fire risk, among other environmental factors. The City is responding to these significant challenges by planning adaptation pathways and by building stronger, more resilient social networks, governmental systems, and partnerships to address the added stressors we anticipate climate change will bring.~~

~~Some of this work has been initiated, but much is left to be done. Consistent with state guidance, the City has conducted vulnerability assessments for sea level rise, has drafted policy to guide our response, and is developing adaptation strategies that are flexible to meet the needs of the community. This body of work is included in the Local Coastal Element, the Coastal Zoning Ordinance, and the adaptation planning associated with these regulatory documents.~~

~~The City has also adopted the Countywide Hazard Mitigation Plan. These documents address the near-term correlated impacts of climate change and significant hazard events. But the mid-term effects of increased fire hazard, and other hazards that will change in the next 50 years, should be assessed and addressed over the General Plan planning period.~~

This Element addresses policy that supports regional planning and action collaborations, land-use that has a positive climate impact, and strategies to build resilient networks and systems to help our community adapt, accommodate, and respond to current and future impacts of climate change. The adaptation policies generally seek to reduce greenhouse gas emissions. The resilience policies seek to build community connections.

Guiding Principles and Goals.

- A. Establish and maintain emergency response capabilities and services at both the City and neighborhood levels.
- B. Promote seismic safety by restricting development in high risk areas, and strengthening buildings and infrastructure to withstand seismic events.
- C. Recognize that certain slopes and soils lack stability and are best left undisturbed.
- D. Establish and maintain sufficiently sized flood areas along Arcata's watercourses, to accommodate flood flows without damaging property or injuring residents.
- E. Reduce fire hazards through prevention and suppression.
- F. Protect residents and the natural environment from exposure to hazardous materials and substances.
- G. Promote a sense of security and safety by maintaining and supporting police services.
- H. Promote social services and programs that improve the physical and mental ~~well~~ being of community residents.
- I. Continue participating in the Humboldt-Del Norte Hazardous Materials Response Joint Powers Authority, and promote hazardous materials reductions and safe handling.
- J. ~~Promote climate adaptation and resiliency measures that protect~~ address climate change impacts on the natural and built environment through protection, adaptation, accommodation, and resiliency from being compromised by climate change impacts.

6.2 POLICIES

The Public Safety Element contains the following policies:

- PS-1 Emergency Preparedness
- PS-2 Seismic Hazards
- PS-3 Other Geologic Hazards [and Airspace Protection](#)
- PS-4 Flood Hazards
- PS-5 Fire Hazards
- PS-6 Hazardous Materials
- PS-7 Crime Prevention and Response
- ~~PS-8 Human Health Hazards~~
- ~~PS-8 Climate Adaptation and Resiliency~~

POLICY PS-1 EMERGENCY PREPAREDNESS

Objective: Ensure that the City, its residents, businesses, agencies, and organizations are prepared for emergencies or disasters and have effective response and recovery plans in place.

PS-1a **City Emergency Response Plan.** The City shall maintain a comprehensive response plan for emergencies, including seismic events, tsunamis, slope failures, floods, storms, fires, and materials spills or contamination. The plan will provide for timely and coordinated response to emergencies that threaten community residents, property, and vital services. The plan will identify City and other emergency response agencies that should be contacted, and also identify neighborhood resources available for more localized assistance and relief.

PS-1b **Evacuation routes/transportation facilities.** Emergencies such as floods, storms, fires, ~~distantly generated~~ tsunamis, and hazardous materials spills may necessitate immediate evacuation of affected areas. A map of evacuation routes shall be included in City and neighborhood emergency response plans. These plans will ~~also~~ include evacuation ~~methods~~ methods using a variety of available modes for transportation for residents who are without, or unable to operate, vehicles. An emergency access plan shall be developed for access to the east side of town (east of State Route 101) after a major seismic event.

While a majority of the neighborhoods in the City have at least two potential evacuation routes in the event of an emergency, there are some areas with only one evacuation route . Residential areas with only one evacuation route include upper Diamond Drive, California Avenue, Fickle Hill Road, Beverly Way, a portion of Beverly Drive, and others

in the Grotzman Creek drainage. The City's emergency operations plan shall identify and include evacuation options for these areas and shall include public education and outreach.

PS-1c **Disaster preparedness coordination using the Standardized Emergency Management System.** City staff responsible for emergency response shall be trained in Standardized Emergency Management System (SEMS) implementation, which is necessary to receive reimbursement from the State of California for disaster response related costs. This training includes instruction about the Incident Command System (ICS) which is used to manage emergency incidents or non-emergency events.

APPLICATIONS FOR THE INCIDENT COMMAND SYSTEM:

- Fires, hazardous materials (HAZMAT), and multi-casualty incidents.
- Multi-jurisdiction and multi-agency disasters.
- Wide area search and rescue missions.
- Pest eradication programs.
- Oil spill response and recovery incidents.
- Single and multi-agency law enforcement actions.
- Air, rail, water, and ground transportation accidents.
- Planned events (celebrations, parades, concerts).
- Private sector emergency management programs.
- State or local major natural hazards management.

PS-1d **Siting and design of critical facilities.** Adequate shelter and continued operation of essential services, including communications, medical treatment, water delivery, fire and police services, and key transportation facilities are vital for responding to emergencies. These facilities and services shall be located and designed to withstand disaster impacts and have backup systems, such as emergency generators and water storage (including private and open water sources), that allow for their continuous operation during emergencies. New critical facilities shall not be located in areas with high physical hazards, including high liquefaction potential, flood zones, and tsunami hazards. Critical facilities shall be designed to be functional at peak capacity, following a magnitude 7.7 earthquake in the Mad River fault zone.

PS-1e **Development & design standards for emergency response.** New and renovated structures, as well as streets, driveways, and alleyways, shall be designed to provide adequate entry and exit by emergency vehicles and personnel. This includes visible street numbering, emergency vehicle turn-arounds, accessible building entry points and stairways, lighting, and interior evacuation routes.

Attachment: H. Ch 6 Health Safety (4214 : GP #XVII FBC & GP wrap-up)

PS-1f **Citizen training/**~~Neighborhood and Business~~**Community Emergency Services Response**



Teams. The City of Arcata Police Department, [in cooperation with the Humboldt CERT Coalition and the Humboldt County Office of Emergency Service,](#) coordinates the organization and training of [the Arcata Community Emergency Services Response Teams Neighborhood Emergency Services Teams and Business Emergency Services Teams \(NEST and BESTCERT\).](#) The [CERT](#) program is essential for mobilizing neighborhood response to emergencies. The [CERT](#) program shall be

expanded to all neighborhoods.

PS-1g [Humboldt County Operational Area Hazard Mitigation Plan \(Hazard Plan\).](#) The 2019 Humboldt County Operational Area Hazard Mitigation Plan and City of Arcata Annex is adopted by reference into the General Plan. General Plan policies and actions shall support its goals and objectives and incorporate policies and programs to address vulnerabilities identified in the City of Arcata Annex.

PS-1h [Severe Weather Hazards.](#) Ensure that utilities, buildings, and infrastructure in the City can meet vital community needs during and after severe weather.

POLICY PS-2 SEISMIC HAZARDS

Objective: Protect existing and new structures from seismic hazards. Identify and map seismic hazards and assure that any development within such hazard areas does not proceed until geologic and soils conditions are adequately investigated and appropriate mitigation measures, if any, are incorporated into development plans.

PS-2a **Development within fault zone/surface rupture areas.** The City shall maintain current seismic information that identifies fault zones and probable surface rupture areas. Development in these areas shall be avoided, unless it can be demonstrated that structures and facilities can be designed to withstand effects of faulting and surface rupture. Building setbacks from faults, surface ruptures, and other seismic hazards, as specified in the most current Uniform Building Code, shall be maintained.



PS-2b **Mitigation of ground-shaking hazards.** Arcata will experience ground-shaking during an earthquake. The City maintains seismic data files that identify areas where ground-shaking will most likely damage buildings and infrastructure. New construction and

renovation shall incorporate the most current and effective seismic engineering measures to strengthen building foundations and infrastructure in these areas.

PS-2c **Mitigation of surface rupture and groundshaking hazards.** The City's Alquist-Priolo Special Studies Zone map (Figure PS-a) and Geologic Hazard Land Use Matrix (Table PS-1) identify areas highly susceptible to surface rupture and groundshaking. Construction in these areas shall be restricted, unless it can be demonstrated, in geotechnical reports prepared by qualified personnel, that structures and facilities can be designed to withstand liquefaction hazards induced by seismic events.

PS-2d **Requirement for and review of "Geotechnical Reports."** New building and infrastructure construction, and substantial renovations in areas with seismic hazards, shall incorporate geotechnical report specified measures into project design. Geotechnical reports shall be required for structures or infrastructure in seismic hazard areas. Required reports, prepared by a registered geologist, certified engineering geologist, or registered engineer with expertise in seismic engineering, shall recommend mitigation for seismic impacts and identify alternative solutions. The City may require independent review of the geotechnical reports.

GEOLOGIC HAZARD LAND USE MATRIX – KEY TERMS AND ABBREVIATIONS

Term **Explanation**

P Development prohibited

R1 Engineering geologic report and soils engineering report required. Engineering geologic report must be prepared by a Certified Engineering Geologist®. Soils engineering report may be prepared by a Registered Civil Engineer® with appropriate geotechnical knowledge and experience or by a Certified Engineering Geologist® with appropriate geotechnical knowledge and experience.

R2 Engineering geologic report required. Engineering geologic report may be prepared by a Registered Geologist® with appropriate geotechnical knowledge and experience.

® All registrations and certifications must be licensed by the State of California.

D Report requirement is left to the discretion of the City Building Inspector.

* Fault Rupture Hazard.
“SSZ” refers to Alquist-Priolo Special Studies Zone.
“PAF” refers to Potentially Active Fault.
See Hazards Map (Figure PS-a) and Seismic Safety Policies

** See Slope Stability Hazard Map (Figure PS-b)

*** High Liquefaction indicates soil saturation and instability in moderate seismic events. Medium Liquefaction indicates soil saturation and instability in major seismic events. Low Liquefaction indicates soil saturation and instability in major seismic events.
See Hazards Map (Figure PS-a)

Attachment: H. Ch 6 Health Safety (4214 : GP #XVII FBC & GP wrap-up)

TABLE PS - 1 GEOLOGIC HAZARD LAND USE MATRIX

BUILDING TYPE/ LAND USE	EARTH-QUAKE SHAKING HAZARD	FAULT RUPTURE HAZARD*		SLOPE STABILITY HAZARD**				LIQUEFACTION POTENTIAL***				
		SSZ	PAF	V	IV	III	II&I	LOW	→	HIGH		
Hazardous Hazardous substance storage, reservoirs, natural gas storage tanks Essential Hospitals, fire and police stations, emergency control centers, power plants, power and communications substations, schools, theaters Private Auditoriums, hotels, large motels, major office buildings, high density residential	R1											
			R2	D	D	R1	R1	R1	R1	R1	P	
Moderate Risk Residential structures on existing lots with footing loads greater than typical 2 story wood frame dwellings or residential structures with 3 stories or more Major Subdivisions Heavy Industrial	D	R2	D	D	D	R2	R1	D	D	R1	R1	
		R2	R2	D	D	R2	R1	D	D	R1	R1	
		R2	D	D	D	R2	R1	D	D	R1	R1	
Low Risk Multi family structures greater than 4-plexes Minor subdivisions Light industrial, warehousing, commercial Residential wood frame structures 2 stories or less on existing lots	D	R2	D	D	D	R2	R1	D	D	R1	R1	

Attachment: H. Ch 6 Health Safety (4214 : GP #XVII FBC & GP wrap-up)

GEOLOGIC HAZARD LAND USE MATRIX — KEY TERMS AND ABBREVIATIONS

<u>Term</u>	<u>Explanation</u>
P	Development prohibited
R1	Engineering geologic report and soils engineering report required. Engineering geologic report must be prepared by a Certified Engineering Geologist®. Soils engineering report may be prepared by a Registered Civil Engineer® with appropriate geotechnical knowledge and experience or by a Certified Engineering Geologist® with appropriate geotechnical knowledge and experience.
R2	Engineering geologic report required. Engineering geologic report may be prepared by a Registered Geologist® with appropriate geotechnical knowledge and experience.
®	All registrations and certifications must be licensed by the State of California.
D	Report requirement is left to the discretion of the City Building Inspector.
*	Fault Rupture Hazard. "SSZ" refers to Alquist Priolo Special Studies Zone. "PAF" refers to Potentially Active Fault. See Hazards Map (Figure PS-a) and Seismic Safety Policies
**	See Slope Stability Hazard Map (Figure PS-b)
***	High Liquefaction indicates soil saturation and instability in moderate seismic events. Medium Liquefaction indicates soil saturation and instability in major seismic events. Low Liquefaction indicates soil saturation and instability in major seismic events. See Hazards Map (Figure PS-a)

~~PS-2d Requirement for and review of "Geotechnical Reports." New building and infrastructure construction, and substantial renovations in areas with seismic hazards, shall incorporate geotechnical report specified measures into project design. Geotechnical reports shall be required for structures or infrastructure in seismic hazard areas. Required reports, prepared by a registered geologist, certified engineering geologist, or registered engineer with expertise in seismic engineering, shall recommend mitigation for seismic impacts and identify alternative solutions. The City may require independent review of the geotechnical reports.~~

PS-2e **Shoreline hazards (tsunami inundation, tidal flooding).** ~~A—State of California Geological Survey Seismic Hazards Program probabilistic tsunami hazards modelling and public tsunami hazard area maps for Humboldt County study (Planning Scenario in Humboldt and Del Norte Counties, California, for a Great Earthquake on the Cascadia Subduction Zone, Special Publication 115, California~~



Attachment: H. Ch 6 Health Safety (4214 : GP #XVII FBC & GP wrap-up)

~~Department of Conservation, Division of Mines & Geology, 1995~~ indicates that the Arcata Bay shoreline and adjacent areas, between McDaniel Slough and Mad River Slough including the South G Street, South of Samoa, and portions of the Windsong neighborhood, could be inundated by tsunami run-up. The City shall prohibit the location of critical facilities in the tsunami run-up area, use California Geologic Survey Guidelines for Evaluating and Mitigating Tsunami Hazards when evaluating land use proposals within the tsunami hazard zone, and use available emergency broadcasting systems to communicate tsunami warnings. Should coastal access within the tsunami run-up zone be provided in the future, appropriate evacuation route signage shall be posted.

PS-2f **Failure of Matthews Dam.** All new buildings designed for human occupancy or use that are located in the area of potential inundation resulting from a catastrophic failure of Matthews Dam shall have an early-warning system and evacuation plan in place for those persons living and working there.

PS-2g **Earthquake-resistant building and infrastructure standards.** The current Uniform Building Code standards for strengthening buildings and infrastructure to withstand earthquakes shall be enforced. The competency of existing road and utility networks shall be evaluated and, where necessary, upgraded to withstand the most current ground acceleration standards.

PS-2h **Public information and disclosure.** Redwood Coast Tsunami Work Group (RCTWG)~~The Humboldt Earthquake Education Information Center, at Humboldt State University,~~ and the City of Arcata have earthquake/tsunami web-based mapping and awareness information that is available to the public. The RCTWG Center ~~and the City will continue to make the most current information available to help the community prepare for and respond to seismic events.~~

THE HUMBOLDT EARTHQUAKE EDUCATION INFORMATION REDWOOD COAST TSUNAMI WORK GROUP-CENTER:

- Prepares and distributes earthquake education materials.
- Maintains an informative and comprehensive website with interactive tsunami zone maps
- Provides information when significant earthquakes occur.
- Conducts workshops for emergency planners, teachers and others.
- Has speakers for community group conferences and meetings.
- Operates the Humboldt earthquake hotline.
- ~~Maintains a website.~~
- ~~Conducts intensity studies of regional earthquakes.~~

Attachment: H. Ch 6 Health Safety (4214 : GP #XVII FBC & GP wrap-up)

POLICY PS-3 OTHER GEOLOGIC HAZARDS AND AIRSPACE PROTECTION

Objective. Protect existing and new structures from non-seismic geologic hazards such as unstable slopes and soils. Require that all non-seismic geologic hazards be adequately addressed and mitigated. Ensure new development does not result in hazards to air navigation.

- PS-3a **Slope stability hazards.** Slope areas greater than 15%, shown on Figure PS-a, and certain less steep slopes with erosive soils may become unstable if disturbed. The City shall restrict grading, vegetation removal, and new construction in areas with unstable soils unless it can be demonstrated that these activities can occur without impacts. All grading of slope areas shall follow natural contours to maximize stability.
- PS-3b **Grading standards for erosion and sedimentation control.** The design, extent, and location of grading shall minimize disturbance of the natural terrain and land features and shall not impact offsite areas. Trees and native vegetation shall be retained around graded areas to stabilize hillsides; retain moisture; reduce erosion, siltation and nutrient runoff; and retain the natural beauty of the area. Cleared areas susceptible to erosion shall be stabilized so that no materials are transported offsite.
- PS-3c **Hillside development standards.** The land areas subject to hillside development standards are shown in Figure PS-a. Development on lands within these areas shall be subject to the following standards:
1. Each new lot created in areas subject to hillside development standards shall be required to contain a contiguous buildable area of at least 4,000 square feet. All parts of the building area shall have a natural slope of less than 15%. No more than 4,000 square feet of any existing lot which was legally created prior to adoption of this plan, and does not meet the preceding 15% standard, may be disturbed for development.
 2. At least 50% of the area of any lot undeveloped as of the date of this plan shall be included in a "natural area" which shall remain in a natural, ungraded, undeveloped state. All slopes in excess of 25% shall be included in the natural area.
 3. Vegetation removal in the natural area of each lot shall be subject to review and approval by the City.
 4. All access roads and driveways shall be kept to the minimum feasible width and shall be designed to minimize grading and disruption of vegetation.
 5. Access roads shall not exceed 15% slope for any distance. Individual driveways shall not exceed 17% slope.
 6. Access roads or driveways shall be designed to avoid steep slopes and shall not have cuts exceeding fifty feet (50') nor fills exceeding twenty-five feet (25') in height.
 7. The design, scope and location of grading for development should cause the minimum disturbance of the terrain and natural features of the land. Unavoidable grading should complement natural land forms. Mass grading of large pads and terraces shall not be permitted.

8. All manufactured slopes shall be planted or otherwise protected from the effects of storm run-off and erosion.
9. Time limits to avoid extensive grading during the wet season shall be imposed on new developments.
10. Any necessary conditions to control erosion and assure site restoration shall be required by the City. Measures shall include short-term controls to minimize erosion at construction sites and long-term controls for minimizing sedimentation and maintaining water quality.
11. Development in areas subject to development standards should demonstrate a concern for the view of the hills as well as the view from the hill.

PS-3d **Slope-density land use restrictions.** Slopes greater than 25% are to be designated as natural areas and shall not be included in density calculations. Minimum parcel sizes on slopes shall be contingent on the site's ability to accommodate a building site with adequate access and utilities.

PS-3e **Geotechnical reports.** Geotechnical reports shall be prepared for development of areas with unstable slopes and/or erosive soils. These reports will be prepared by qualified professionals, consistent with Policy PS-2d. Measures to ensure slope and soil stability shall be incorporated into the project design.

PS-3f **Geologic hazard information.** Provide available information, including potential for site slippage, effects of groundwater on slopes and soils, erosion potential, and other hazards, to homebuilders prior to the start of construction.

PS-3g **Airspace protection.** Inform land use action proponents located below the airspace protection surfaces of California Redwood Coast-Humboldt County Airport, as shown in the Humboldt County ALUCP, of the FAA notification requirements defined in Title 14 of the Code of Federal Regulations, Part 77. Evaluate such land uses to ensure that new development will not result in an obstruction or a potential hazard to aircraft in flight such as uses creating electrical interference, confusing lights, glare, or other disturbances are also restricted from areas beneath the airspace protection surfaces.

POLICY PS-4 FLOOD HAZARDS

Objective. Protect current and future populations and property from flood hazards. Assure that new development within floodplains does not proceed until appropriate mitigation measures are incorporated into development plans.

PS-4a **Floodplain Mapping.** The City shall continue participating in the National Flood Insurance Program and maintain the most current Flood Insurance Rate Maps (FIRM) on file. The City shall also continue requesting that the Federal Emergency Management Agency provide hydrographic modeling (using the 1964 flood levels as a benchmark) and an update of flood mapping for the Mad River using the best available information.

The Federal Emergency Management Agency (FEMA) manages the National Flood Insurance Program providing insurance to the public in participating communities. FEMA is the main federal government agency contact during natural disasters and publishes Flood Insurance Rate Maps, which identify flood potential based on a 100-year flood.

PS-4b **Limitations to development adjacent to Arcata Bay and along Bay shoreline (tidal flooding, tsunami, failure of dikes or tidegates).** The Arcata Bottom and other low lying/low-lying areas adjacent to Arcata Bay, McDaniel, Butcher's and Gannon Sloughs, and Arcata Marsh are susceptible to flooding from extreme Bay tidal action, tsunami run-up, seiche, dike or tidegate failure, heavy rainfall that can't drain, ~~and~~ Mad River flood events. Where not otherwise restricted, habitable structures and related improvements shall be appropriately elevated above flood levels, designed so as not to restrict flood flows, and shall comply with applicable provisions of Resource Conservation and Management Policy RC-2: Streams Conservation and Management, pertaining to Streamside Protection Areas (RC-2b and RC-2c). Land alterations for recreation and natural resource uses shall also not restrict or increase or channelize flood flows in a way that could cause inundation to adjacent areas.

PS-4c **Limitations on development within Flood Zone.** The mapped Flood Zone A as determined by FEMA should be kept free of structures and other obstructions that would restrict flood flows. New construction in Flood Zone A shall be elevated, flood-proofed, designed to not constrict flood flows or drainage, and/or include other features, such as access for evacuation and emergency response, to protect human safety and minimize property damage. Landform alterations shall not impede flood flows in adjacent upstream or downstream areas. Any development in the floodplain must be consistent with City floodplain zoning regulations.

PS-4d **Limitations to development within flood hazard zones.** Arcata's creeks and sloughs have the potential to cause localized flooding and shall be maintained to allow the flow of floodwaters. Structures and other land form/landform alterations in areas susceptible to localized flooding, outside areas defined in policies RC-2b and RC-3c, should be setback from the watercourse. Elevated and flood-proofed structures, and/or floodwater detention basins shall be provided to minimize flood damage and prevent any net increase in flood flow upstream or downstream. The City shall promote flood management practices for entire



watercourses, to minimize the need for sandbagging and other temporary flood control measures that can have detrimental impacts to adjacent areas.

- PS-4e **Flood Insurance.** Flood insurance is available from private insurers to compensate home and business owners for flood-related losses. Flood insurance rates are set by a site's proximity to mapped flood-prone areas. The City shall make information about flood-prone areas available to the public, to aid community residents and business owners in determining whether flood insurance should be purchased.
- PS-4f **Development standards in floodplains (surface drainageways and detention areas).** All plans for new construction that could potentially encroach into a floodplain must incorporate measures for flood protection and show that there will be no adverse impact to the carrying capacity of the floodway. Setbacks, easements covering Flood "Zone A", and minimal use of impervious surfaces are measures strongly encouraged. Elevation of structures, anchoring, flood-proofing, and construction of detention basins are considered secondary and less desirable measures. The City's floodplain administrator shall verify this information and require appropriate certification before any development permits are granted.
- PS-4g **Preventive maintenance of streams and drainageways.** Local streams carry the majority of Arcata's floodwaters and shall be maintained for flood protection as well as natural biological functions. All improvements and maintenance shall be done in accordance with the City's Drainage Master Plan.
- PS-4h **Development review: drainage standards and drainage fees.** All new development shall meet current City drainage standards and pay all applicable drainage fees. The City shall provide incentives to reduce the amount of impervious surface associated with new and renovated uses by reducing drainage fees.

POLICY PS-5 FIRE HAZARDS

Objective. Minimize risk of personal injury and property damage resulting from structural (urban) and wildland fires.

- PS-5a **Management of urban fire hazards (development/design standards).** Structural fires demand immediate response from a combination of onsite and Fire Department resources in order to minimize injury and damage. Fire suppression devices such as extinguishers and sprinklers are important for initial response, reduce fire insurance premiums, and satisfy operations requirements for certain types of businesses. These devices are encouraged in new and renovated non-residential buildings and required in all residential structures with more than four units, even when not required by fire and building code.

All buildings should have adequate lighting, street numbering, and access to ensure

rapid response by fire-fighting vehicles. To ensure urban fire safety, the City shall enforce the Uniform-current California Building and Uniform-current Fire Codes (UBC & UFC) currently in effect, and the Universal Building Code when it is adopted.

PS-5b **Review of development for fire safety.** The Arcata Volunteer Fire Department Arcata Fire Protection District (District) (AFPD) takes an active role in reviewing new development for compliance with fire safety standards. The City shall continue to incorporate review by the AFPD District Fire Department review to ensure that buildings and sites adhere to relevant fire and building codes, including sprinklers, driveways, turns arounds, and other access ways have sufficient width, vertical clearance, and turn-around space for fire fighting vehicles. Roadways shall have an all-weather surface and grades shall not exceed the Fire Department's maximum slope standards for emergency access.



PS-5c **Water supply (fire flow).** The City's fire hydrant system provides the primary source of water for fighting urban fires. The City shall maintain fire hydrant spacing so that no residential structure is more than 500 feet from a hydrant and no commercial or industrial structure is more than 300 feet from a hydrant. Each hydrant shall have 2 1/2 inch and/or 4 inch male national hose adequate appropriate fittings and be capable of providing adequate water flows to meet the California Fire Code requirements Fire Department standards.

The City shall maintain adequate fire flows in its water system. Open-water sources such as ponds, swimming pools, private storage tanks, and reservoirs may be used as a secondary water source by fire-fighting apparatus. These sources shall be equipped with appropriate filtering devices or strainers to prevent clogging of water pumps.

PS-5d **Management of wildland fire hazards.** Fires in the wildland urban interface (WUI) Wildland fires in forested areas of the City can cause property damage and threaten nearby structures. Buildings in WUI forested areas shall use materials such as non-flammable perimeter vegetation and roofing material to prevent exposure to wildland fires. The City shall support the ongoing implementation of the Humboldt County Community Wildfire Protection Plan (CWPP) and encourage residents in WUI areas to follow its recommendations. The City shall encouragesupports the Arcata Fire Department Fire District AFPD to in maintaining its mutual aid agreement with the California Department of Forestry and Fire Prevention (CDFCAL FIRE) to insure rapid response to wildland fires.

PS-5e Wildland Fire Risk Reduction and Defensible Space Around Homes. The City will draw on available local and regional guidance and support efforts to reduce wildland fire risk. To do so, the City encourages creating defensible space around homes and other structures in wildlands and the wildland urban interface. The City shall support

programs and plans that reduce wildland fire risk.

~~PS 5e **Fire suppression services.** The Arcata Volunteer Fire Department's (AVFD) jurisdiction district boundary (shown as District #1 on the AVFD service area map) includes the City's incorporated, sphere of influence, and planning areas, except for an outlying area along Jacoby Creek Road. The AVFD AVFD maintains two three fire stations to provide rapid response to all fire calls within its service area. The AVFD also maintains a robust automatic and mutual aid agreements with Humboldt Bay Fire, Samoa Peninsula Fire Department, Blue Lake Fire Protection District, Fieldbrook Volunteer Fire Department, Kneeland Fire Protection District, Westhaven Volunteer Fire Department and the CDF and the Eureka Fire Department for rapid response to fires within the City and in outlying areas.~~

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POLICY PS-6 HAZARDOUS MATERIALS

Objective. Minimize the personal injury, property damage, and public health risks associated with the production, use, storage, disposal, and transporting of toxic substances or hazardous materials.

- PS-6a **Reduction of hazardous waste (source reduction).** There are increasing numbers of environmentally safe materials and substances available that offer alternatives to hazardous materials. Improved water-based paints that replace oil and lead based paints, cellulose insulation materials that replace asbestos, and biodegradable antifreezes that replace glycol based coolants, are all examples of safer materials and substances currently in use. The City shall request information from County, State, and Federal agencies, as well as manufacturers and suppliers, regarding environmentally safe products and shall have a list of those products available to the public.
- PS-6b **Contaminated sites.** There are sites in and around the City where wood product milling and production, vehicle and equipment storage and repair, agricultural production, and other uses may have resulted in site contamination. Materials such as lead based paints may also contribute to contamination. Many of these sites are inactive and may be appropriate for alternative uses. Environmental Site Assessments shall be required prior to development review and approval of potentially contaminated sites, and cleanup is required prior to reuse. The City shall record and map sites with known contamination.
- PS-6c **Use of potentially harmful materials on public lands and rights-of-way.** The City of Arcata does not use toxic sprays or substances on vegetation in public lands or rights-of-way, and has been persuasive in stopping State agencies, such as Cal-Trans, from using toxic sprays along State rights-of-way within the City limits. The City shall continue this practice and prohibit other public agencies from using toxic sprays or substances within the City limits (see Resource Conservation and Management Element Policy RC-1i).
- PS-6d **Siting of facilities handling hazardous waste.** Businesses and agencies that use, store, or produce hazardous materials shall train employees and other users in safe handling and storage procedures, and shall post current Occupational Safety and Health Act (OSHA) and Humboldt County hazardous materials requirements. Businesses shall also comply with Federal “community right-to-know” regulations. The City shall consider proximity to sensitive receptors, such as schools, hospitals and other health care facilities, day care centers, and other immobile populations, when reviewing new facilities and businesses involved in these activities.
- PS-6e **Household and other small-quantity generators.** Households and small businesses accumulate solvents and cleaners, petroleum products, pesticides, and other toxic substances that are potentially hazardous if spilled, released into the atmosphere, or ingested. The City shall maintain a list of toxic substances which should be avoided and

publicize collection dates and locations where these substances can be disposed of properly. The City shall coordinate with the County Hazardous Materials Department (Humboldt County) and the State Department of Toxic Substances Control (California Environmental Protection Agency) to ensure that collection sites are accessible to community residents.

The City shall also encourage property owners and real estate agents to remove hazardous materials, or disclose their presence, to purchasers as part of property transfers. Property purchasers should have the option of accepting certain materials, such as paint, or requesting removal and cleanup prior to the property transfer.

PS-6f **Hazardous waste management (recycling, treatment, disposal).** All commercial and industrial businesses and other operations that use, store, or produce hazardous materials, shall contract with a licensed hauler for pickup and disposal of waste materials, except for individual disposal complying with County, State and Federal requirements. All hazardous materials shall be stored in safe containers and locations, and use of these materials shall be in compliance with County, State and Federal standards.

PS-6g **Hazardous materials education program.** The City shall work with the Humboldt County Health Department and the California Department of Toxic Substances to develop and promote educational materials explaining hazardous materials' impact on people, plants, and animals, and provide information on alternatives to hazardous materials. ~~This information~~ The City shall also keep a compendium of Material Safety Data Sheets (MSDSs) for all potentially hazardous materials that are used by all departments in the City. This information—including the MSDSs— shall be made available to the public.

POLICY PS-7 CRIME PREVENTION AND RESPONSE

Objective. Promote law enforcement agency and community group efforts to reduce crime and assure timely and effective responses to calls for service.

PS-7a **Development/building and site design standards for crime prevention.** The City shall integrate CPTED (Crime Prevention Through Environmental Design) and related strategies into building and site design to ~~Some criminal acts in and around buildings can be prevented~~ or minimizeed criminal acts, reduce victimization, and build a sense of community safety. by incorporating safety and security precautions into building and site design.—These include a combination of onsite features such as alarm systems, secured entryways, lighting, and visible access. Crime prevention measures, such as providing alarm systems, security lighting, street numbers, and visibility for police surveillance, should be incorporated into, and around, new and renovated buildings. Gated communities hinder police and fire suppression access and shall be prohibited,

consistent with Community Design Element policy.

PS-7b **Community-based policing.** Community-based policing has proven effective in reducing crime by involving citizens in crime prevention. The City Police Department provides training to business and neighborhood groups in how to discourage crime and best respond when crime occurs. The City shall continue to provide citizen training, maintain relationships with community groups, and encourage Business-Community Emergency Services-Response Teams (BESTsCERTs) to reduce crime and augment the essential services of the Police Department. Foot and bike patrols shall be promoted to maximize interaction between citizens and police, to foster friendship, understanding, and mutual help.

PS-7c **Cooperative Law Enforcement.** There are three law enforcement agencies based in Arcata: The Arcata Police Department, the HSU Cal Poly Humboldt Police Department, and the California Highway Patrol. These agencies work together, responding to incidents throughout the City. The City shall continue to cooperate with other law enforcement agencies to maximize public safety within its boundaries. The City Police Department shall be the primary response agency within City limits since it is the only agency directly accountable to the government and citizens of Arcata.



PS-7d **Independent review.** The Arcata Police Department has an established review procedure for investigating complaints against law enforcement personnel. Complaints may also be referred to the Humboldt County Grand Jury.

[Revised by Ordinance No. 1377, September 2008]

PS-7e **Education and crime prevention funding.** The City recognizes that funds spent on education and crime prevention are more effective in reducing crime than funds spent on apprehending, prosecuting, and incarcerating criminals. The City shall pursue a long-term strategy of funding education and crime prevention programs.

PS-7f **Principled Policing.** The Arcata Police Department shall conform to State and Federal law, California Peace Officer Standards and Training (POST) standards, Department policies, and 21st Century Policing best practices to combat racial profiling and bias in policing and to promote de-escalation and principled policing.

POLICY PS-8 CLIMATE ADAPTATION AND RESILIENCY

Objective. The City will continue to support and monitor research about the nature of all hazards in Arcata, including research on prediction and warning systems, community vulnerability and consequences assessments, and improvements to building performance and

resilience. The City will continue to pursue efforts that minimize Arcata's contribution to climate change by reducing greenhouse gas emissions and protecting people, the built environment, and the natural environment from the impacts of climate change.

PS-8a Land Use and Urban Development. Compact urban development has co-beneficial climatic effects, including reducing greenhouse gases, preserving habitats, producing healthy vibrant communities, and facilitating opportunities for environmental justice. The City shall support land use activities that promote compact, climate-friendly design, such as infill development, densification, and regenerative building design. City projects and private developments shall be designed to consider multi-benefit solutions that mitigate hazard risk and support the City's climate goals.

Urban design and development will be informed by the latest climate science to determine how to best protect and modify existing public and private infrastructure and assets and how to design new infrastructure and assets. Urban design and development should also take environmental justice considerations into account, working to eliminate disparities and burdens related to all hazards and the climate crisis for all people in Arcata.

PS-8b Greenhouse Gas Reduction. City actions shall reduce local contributions towards climate change by mitigating greenhouse gases and increasing carbon sequestration. The City shall update relevant plans and policies periodically to meet State and local climate goals.

1. Encourage reduction in greenhouse gas emissions, by providing alternatives to use of gas-powered vehicles. Such alternatives include public transit, alternatively fueled vehicles, bicycle and pedestrian routes, and bicycle- and pedestrian-friendly development design.
2. Encourage and facilitate investments in renewable energy and green technology.
3. Prioritize nature-based solutions that restore ecosystem function and maximize ecological benefits to plants, animals, and people.

PS-8c Regional Collaboration. The City shall continue to participate in regional multi-agency efforts to coordinate climate mitigation and adaptation and ensure Arcata becomes more resilient to the threats of climate change.

PS-8d Sea Level Rise. Incorporate consideration of sea level rise and other associated climate impacts into public and private infrastructure and asset design and land use planning. The City shall continue to work with local, regional, state, and federal partners and utilize best available science to maximize the safety, environmental performance and climate adaptability of all new development.

PS-8e Food security and resilience. The North Coast has abundant resource lands, agricultural lands, and fishing grounds. The City shall develop partnerships, networks,

and systems to promote resilience and redundancy in our local food cultivation, production, distribution, and storage sectors.

PS-8f Wildfire Response and Preparedness. Climate change has the potential to affect multiple elements of the wildfire system: fire behavior, ignitions, fire management, and vegetation fuels. California’s Fourth Climate Change Assessment - North Coast Regional Report states that “(f)uture wildfire projections suggest a longer fire season, an increase in wildfire frequency, and an expansion of the area susceptible to fire.” The City shall continue to work with local, regional and state partners to adapt and improve the City’s fire response and preparedness protocols to respond to areas that will become more susceptible to fire over time within the City limits and Planning Area.

PS-8g Traditional ecological knowledge. The City of Arcata acknowledges the value of Indigenous sciences and knowledge and the need for Indigenous perspectives in responding to the climate change crisis. The City shall work to support Indigenous-led climate adaptation approaches and shall work collaboratively with tribes and tribal governments for mitigation, adaptation, and resilience to climate change. This policy applies to all previous policies in this section.

POLICY PS 8 — HUMAN HEALTH HAZARDS

~~Objective. Promote access to services and programs (prevention, treatment, and long term care) that will improve human health conditions for current and future populations. The City recognizes that its resources are limited but will work with other government agencies, non profit organizations, social service and health care providers to help all residents meet and maintain basic physical and mental health. The City will serve as a model for other governmental entities that are addressing local health needs.~~

~~PS 8a Health care programs and facilities. The City of Arcata does not directly provide health care programs or facilities; however, these facilities are operated in the City by a variety of health care providers and professionals, as well as non profit and other organizations. The City shall allow health care facilities (such as clinics, counseling centers, and doctors offices) to be located in appropriate areas of the City, and encourage programs that serve all segments of the population.~~

~~PS 8b Social services programs. The City administers certain social services, such as temporary and permanent housing programs. Other public agencies, including the State of California, and Humboldt County, offer social services, such as public assistance, and food subsidy programs. There are also non profit, religious, social, and other organizations, as well as businesses that offer social services such as counseling, educational, family assistance, child care, health education, and food subsidy programs. Social service needs shall be monitored, through population trend analysis and other indicators, and information disseminated to other social service providers.~~

~~Larger employers shall be encouraged to provide childcare services. The City shall coordinate with other public agencies and service providers to avoid duplication of services and shall assist in coordination, planning, and evaluating social services delivery. Additional services, such as Travelers Aid, shall also be encouraged, either through City sponsorship or identification of appropriate service providers. Social service facilities shall be located in areas that are accessible to users.~~

~~The General Plan Housing Element goal is to provide housing opportunities for people of all income levels, through the development of a wide range of housing types. The Element also contains affordability and fair housing policies as well as design standards to serve the needs of all population segments. The Housing Element shall be implemented to promote affordable and accessible housing for segments of the population with limited financial resources and limited mobility.~~

~~PS 8c Needs of cultural groups and special populations. Cultural groups, such as Native Americans and other local ethnic populations, and special populations, such as those with physical and mental disabilities, may require more specialized services than those provided in community wide programs. The City shall allow, where appropriate, use of public spaces for cultural group activities, and shall consider their needs of special~~

populations in City programs, activities, and land use planning.

~~PS 8d **Needs of special populations.** Special populations, such as those with physical and mental disabilities, may require more specialized services than those provided in community wide programs. The City shall consider the needs of special populations in City programs, activities, and land use planning.~~

~~PS 8d8e **Improving community health.** Health care costs and other factors, such as lack of knowledge, limited mobility, and cultural beliefs, prevent certain segments of the community from seeking both preventative care and treatment for illness. This can lead to increased rates of infection and the spread of disease, which impact community health. Non-communicable diseases, such as alcoholism and substance addiction, worsen without treatment and can also impact community health. The City shall encourage low cost health providers to offer preventative, urgent, and continuing health care services, including alcoholism and substance abuse programs, that are accessible to all segments of the community.~~

~~Community health may be jeopardized by lack of free access to clean water and sanitation facilities. The City shall work with other government agencies, non-profit organizations, and social service providers to plan, develop, and maintain such facilities.~~

~~The City shall prohibit camping/living in areas such as the Arcata Community Forest, where clean water and sanitation facilities are not available. If feasible, the City may provide or permit a short term camping area for unsheltered persons.~~

6.3 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURE	RESPONSIBLE PARTY	TIME FRAME
LU-1	Update seismic setbacks/structural requirements and hillside development standards (LUC Amendment) Update City land use code for setbacks from known faults and other hazards, geotechnical review and structural requirements, and hillside development standards.	Community Development Dept.	Year 1
PS-1	Neighborhood and Business Community Emergency Services Response Teams (NEST and BESTCERT) Promote the formation of NESTs in all neighborhoods and the formation of BESTs in business communities continuation of the Arcata CERT. The City Police Department shall have a designated NEST/BESTCERT coordinator available to assist neighborhoods with NEST-CERT organization and training, and to supply training materials (such as the <u>Captain's Handbook Disaster Planning Guide</u>).	Police Department	Ongoing
PS-2	Emergency preparedness and response programs Maintain and distribute current information about seismic risks and emergency preparedness (such as <u>Living on Shaky Ground – How to Survive Earthquakes & Tsunamis on the North Coast</u> distributed by the Humboldt Earthquake Education Center, <u>HSU Cal Poly Humboldt</u>). Maintain Standardized Emergency Management System training for senior City staff, including Incident Command System, multi/inter-agency coordination, mutual aid, and operational area concepts.	City Manager's Office/Arcata Fire & Police Departments	Ongoing
PS-3	Evacuation Planning Implement the evacuation planning actions identified in the <u>Emergency Operations Plan and Hazard Mitigation Plan Action ARC19</u> . Evaluate potential alternative routes for residential areas with limited evacuation options. Where there are no feasible alternative routes, identify and implement realistic actions that can provide protection to evacuees from likely hazards.	City Manager's Office/Environmental Services/Police Department & Arcata Fire	Year 2
PS-4	Floodplain mapping and management program Update floodplain mapping throughout the planning area, including continued participation with National Flood Insurance Program.	Federal Emergency Management Agency	Year 5 Ongoing
PS-5	Evaluate renewing a cooperative agreement with CALFIRE Review the City's financial responsibility and wildland fire resources to respond to incidents within the Arcata Community Forest and the Jacoby Creek Forest. Evaluate if the City should consider renewing a cooperative agreement with CAL FIRE to provide direct wildland fire protection services to these areas.	City Manager's Office, Arcata Fire Protection District	Year 1

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#	IMPLEMENTATION MEASURE	RESPONSIBLE PARTY	TIME FRAME
PS- 85	Safer materials and substances list Maintain a list of safer materials and environmentally friendly substances to replace toxic substances and hazardous materials.	Environmental Services Dept.	Year 2 Ongoing
PS- 96	Education program for reduction of hazardous waste (resource reduction) The City shall compile information from County, State and Federal agencies, as well as from manufacturers and suppliers, regarding environmentally safe products and shall make that information available to the public. <u>The City will maintain, make accessible, and promote the availability of all Material Safety Data Sheets for hazardous material utilized by the City.</u>	Environmental Services Dept.	Ongoing
PS- 107	Brownfield sites Record and map sites having known contaminants, contingent upon funds availability.	Community Development Dept.	Year 2
PS- 118	Hazard information and protection Complete the <u>Continue to maintain and</u> updates of the City's GIS base maps for hazards, and link a computer at the Permit Application Counter in Community Development to the GIS <u>make available</u> for public and staff access.	Environmental Services Dept./ Community Development	Year 1 Ongoing
PS- 129	Humboldt County Operational Area Hazard Mitigation Plan <u>Implement the City of Arcata Annex Mitigation Action Plan in the Hazard Mitigation Plan. Continue to support and participate in the Humboldt County Operational Area Hazard Mitigation Plan partnership and periodic Plan updates and periodically update the Hazard Mitigation Plan to reflect changing climate vulnerabilities, new data, available resources, partnership opportunities, and state and federal requirements and amend the Public Health and Safety Element to integrate the updated Hazard Mitigation Plan.</u>	<u>City Manager's Office/Arcata Fire & Police Departments</u> Environmental Services Dept.	Ongoing Ongoing
PS-13	<u>Climate Vulnerability Analysis. The City will continue to utilize best available science to develop guidance for City departments and private developers on how to understand and incorporate the potential range of climate impacts that could occur from various hazards within the project's planning horizon.</u>	<u>Environmental Services Dept./ Community Development</u>	Ongoing

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NOISE ELEMENT

6.4 INTRODUCTION

The Noise Element is one of the seven required General Plan Elements that must be prepared by California cities and counties (Government Code Section 65302). The California General Plan



Guidelines state that the Noise Element of the General Plan provides a basis for comprehensive local programs to control and abate environmental noise and to protect citizens from excessive exposure. The Noise Element is required to identify and appraise noise in the community and follow the guidelines adopted by the Office of Noise Control in the State Department of Health Services. Local governments must analyze and quantify noise levels, and the extent of noise exposure, through actual measurements or the use of noise modeling.

The air into which noise is emitted, and on which it travels, is a common resource of the community. It is a public good and as such its use, as well as the responsibility of maintaining it, belongs to everyone.

THE STATE OF CALIFORNIA GENERAL PLAN GUIDELINES* FOR THE FUNDAMENTAL GOALS OF THE NOISE ELEMENT ARE:

- To provide sufficient information concerning the community noise environment so that noise may be effectively considered in the land use planning process. In so doing, the necessary groundwork will have been developed so that a community noise ordinance may be utilized to resolve noise complaints.
- To develop strategies for abating excessive noise exposure through cost-effective mitigating measures in combination with zoning, as appropriate, to avoid incompatible land uses.
- To protect those existing regions of the planning area whose noise environments are deemed acceptable and also those locations throughout the community deemed "noise sensitive."
- To utilize the definition of the community noise environment, in the form of CNEL or Ldn noise contours as provided in the Noise Element for local compliance with the State Noise Insulation Standards. These standards require specified levels of outdoor to indoor noise reduction for new multi-family residential constructions in areas where the outdoor noise exposure exceeds CNEL (or Ldn) 60 dBA.

(*Appendix A of the Guidelines)

Loud noise is a health issue. The human ear is not designed to accommodate loud noise for long durations. Exposure to sounds louder than 90 decibels (e.g., a power lawnmower) for more than one or two hours begins to damage the inner ear. Therefore, all people, businesses, industry, and organizations have an obligation to respect the health and comfort of others, and to acknowledge that the effects of broadcasted noise are not limited to their own private property. In sharing the community's common resources, everyone has an obligation to

respect the health of others in ways that are compatible with, and do not detract from, other uses. Noise levels from sources commonly found in the community are shown in Figure N-a.

Guiding Principles and Goals.

- A. Reduce noise at the source.
- B. Protect City residents from the harmful and disturbing effects of noise through controls on noise-producing activities.
- C. Promote noise mitigation and attenuation techniques in the design of both noise receptors and noise generators.
- D. Encourage state-of-the-art land use planning methodologies and acoustic control techniques (~~refer to City Noise Control Manual~~) to reduce existing and potential noise conflicts.
- E. Encourage cooperative and voluntary action by businesses and industry, to reduce noise emissions from annoying noise sources.
- F. Promote mediation as a means of reducing noise complaints which result from existing noise sources.
- G. Educate community residents about good neighbor policies, the benefits of reduced noise levels, and living in a community with a high-quality soundscape.
- H. Bring all noise sources into compliance with noise guidelines by strongly encouraging voluntary compliance, pursuing funding for noise attenuation measures to mitigate any financial hardships, and through enforcement of a Noise Ordinance.

Definitions of noise, potential effects on people, and existing ordinances.

Noise, commonly defined as unwanted sound, is an environmental phenomenon to which people are exposed throughout life. Noise is among the most pervasive pollutants, a waste product generated primarily by human activities.

As urbanization and development have intensified globally, regionally, and locally in Arcata, noise levels have also increased. Studies have shown that increased noise levels have a direct adverse impact on the quality of home, work, and recreational environments.

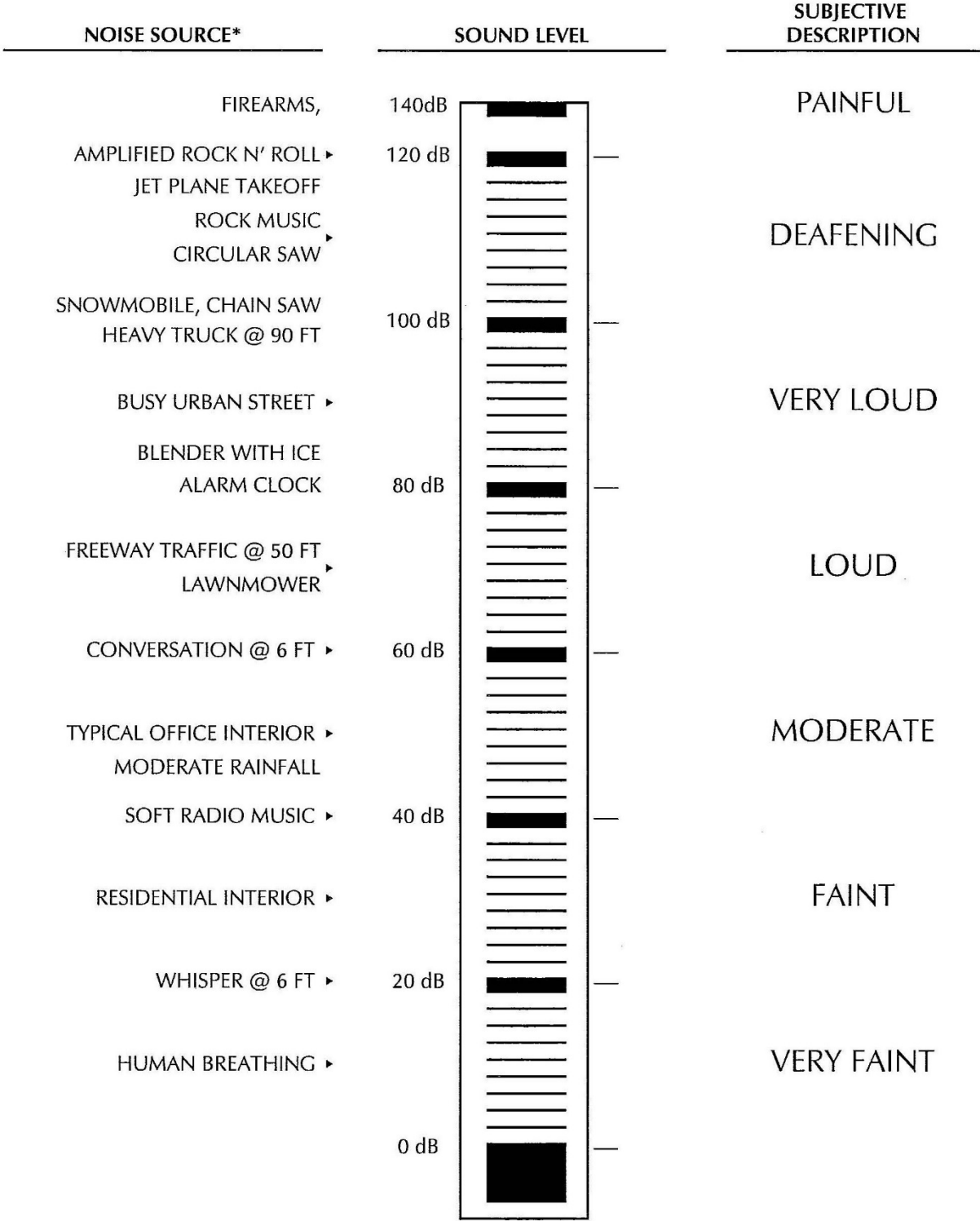
In recognition of these adverse impacts, the Arcata City Council passed a Plaza Area Noise Ordinance (City Ordinance #1249) prohibiting unnecessary, excessive, and annoying noise from all sources. The Noise Ordinance states that excessive noise levels, offensive noise, and unreasonably disturbing noise are detrimental to the public health, welfare and safety and are contrary to the public interest. Detrimental noises interfere with: sleep, communication, relaxation, and the full use of private property; use and enjoyment of the Arcata Plaza by the general public; noise-sensitive land uses, particularly residential apartments and lodging; and productive commerce. Detrimental noises contribute to hearing impairment and a range of adverse physiological stress conditions and adversely affect the value of real property.



It is the intent of the Noise Ordinance to protect the public health, comfort, convenience, safety, welfare, prosperity, peace, and quiet by reducing excessive noise levels and by prohibiting the generation of offensive, or unreasonably disturbing, noise.

Noise environment overview. Arcata has experienced change from its small, rural town beginnings to a growing, urban community with more intensive and diverse uses. These uses are anticipated to continue to intensify as the community grows— Preservation of surrounding open spaces, and General Plan policies for more efficient land uses, have promoted in-fill urban development which intersperses residential uses (noise receptors) with commercial and industrial land uses (noise generators). The proximity of noise generators to noise receptors such as higher volume vehicle travel on local roadways has resulted in increased noise levels at receptor sites. Future development of commercial and industrial uses will increase the potential for future noise conflicts, which is considered an acceptable tradeoff when prioritizing compact infill development over greenfield expansion.

FIGURE N-a A WORLD OF NOISE



* Unless otherwise noted, noise levels are measured at the source

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A September 1997 community noise survey conducted as part of General Plan 2020 background studies showed that typical noise levels in noise-sensitive areas of the City range from 46 dBA to 63 dBA L_{dn}. Traffic, local roadways, major roadways (including State Routes 101, 299 and 255), railroad operations, industrial activities, and neighborhood activities are the primary sources of background noise levels. Some residences have outdoor activity areas directly exposed to major noise sources (such as the railroad tracks, major roadways, and industrial areas) that exceed generally acceptable noise exposure criteria.

Noise complaints received by the City also measure noise levels. The Arcata Police Department has received increasing numbers of noise complaints over the last several years. Police activity reports show ~~866 noise complaints in 1995; 951 in 1996; and 1,043 noise complaints in 1997.~~ In 2022, there were 2,588 complaints, which is a twenty percent increase. Some of the most frequent noise complaints within the City of Arcata are associated with barking dogs, loud parties, boom boxes, car stereos, existing industrial activities, and existing on-site commercial noise sources such as outdoor speaker systems, street sweeping, and early morning garbage pickup. The 2022 complaints were reported for yelling/arguments (393), music (274), parties (93), dogs (59), and 1,361 uncategorized noise complaints. Some of these noises are considered infractions, and are covered in the penal code. Others are referred to other agencies, or determined to be unenforceable, and are not included in the complaint records.

Future noise environment overview. Community noise levels can increase with development. This is especially true for noise along major roadways, such as State Routes 101 and 299. The projected future noise controls are shown in Figure N-b. As development continues, there is a growing potential for noise increases due to commercial or industrial operations. It is now recognized that noise pollution can often be reduced through attenuation measures such as barriers, mufflers, and insulation, with little or no penalty to progress.



Figure N-b Projected Future Noise Contours (11 x 17)

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Figure N-b Projected Future Noise Contours part 2

Attachment: H. Ch 6 Health Safety (4214 : GP #XVII FBC & GP wrap-up)

Responsibilities of a Noise Element. The Noise Element advances the ethic that a low-noise-level environment is a common resource that can be enjoyed by all, and that noise generated by some has the potential to negatively affect others. The Noise Element provides a mechanism for evaluating and mitigating the potential effects of noise on the community. It identifies potential noise sources that exceed acceptable standards and noise sources that may be considered annoying. It also provides criteria for determining acceptable noise exposure. The California General Plan Guidelines state that the Noise Element will be as detailed as necessary to describe the local situation and mitigate local noise problems. This means that the City is not limited to applying noise controls and noise reduction techniques to projects and other activities requiring City review and ~~permits, but~~ permits but can address other noise generating activities in the community.

The Noise Element sets policy direction for evaluating the potential effects of noise due to new industrial processes or commercial noise sources. (Projected future noise contours are shown on Figure N-b.) Many times, however, noise sources are overlooked during the review process, or are added on-site at a later date. Once the project is approved, the administrative actions for responding to complaints of these noise sources generally fall outside of the City's review process. In these cases a noise ordinance is required in order to determine compliance with local standards or the potential for annoyance.

In the event of an individual being annoyed due to a neighbor's barking dog or an existing commercial activity, a noise ordinance would provide a complaint and enforcement mechanism. These types of noises, however, are most effectively addressed through educating community members about "good neighbor policies," since dog ownership, or existing commercial activities, are not subject to City review, and enforcement is usually considered as a last resort. Complaint and enforcement mechanisms include:

- Ordinance 1249, for the Plaza Area only.
- Penal code 415 – Disturbing the Peace.
- Land Use ~~and Development Guide~~ Code regulations.

City responsibilities. The City of Arcata can set an example for business and industry by considering the acoustic concerns and future noise impacts of City operations and business. The City can lead the way by demonstrating that ongoing operations and new noise-producing uses and products can meet specified noise criteria by making effective noise attenuation information available, and by providing a procedure for mediating noise disputes. The City can accomplish this by preferential purchasing of quieter equipment; reducing use of noisy equipment (as demonstrated by City Council action to eliminate the use of leaf blowers in certain public areas); limiting hours of operation for public works projects (except for emergencies) in residential areas; and incorporating acoustic materials and barriers in all new City facilities.

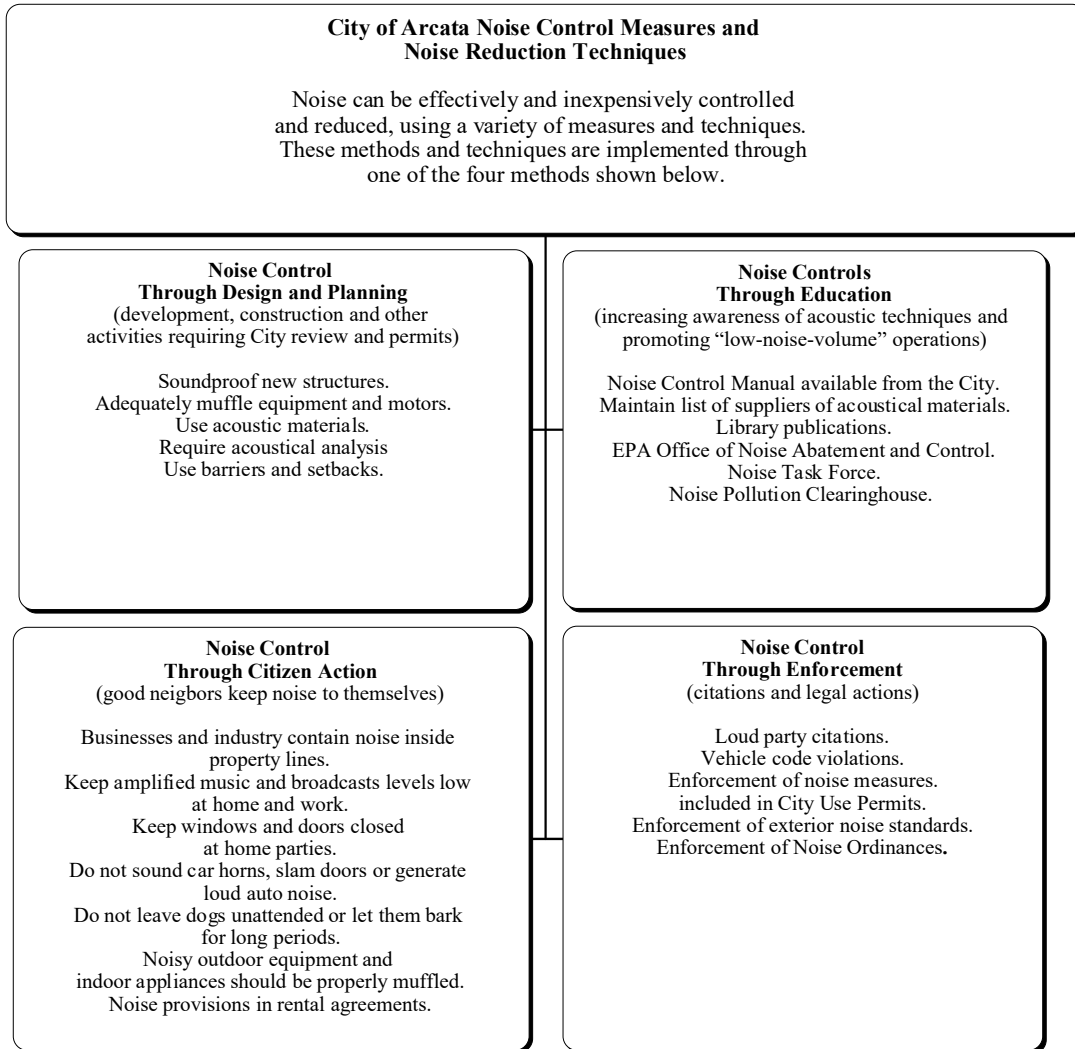


Public surveys on noise show that many people do not know how to effectively address noise issues. Community members in general appear to lack knowledge of how to reduce noise conflicts. There also seems to be a pattern of governmental agencies responding to noise complaints with little or no enforcement or action against noise violations. This pattern may be as typical of Arcata as other urban settings. To address this issue, procedures and educational materials are being developed, including a noise control manual.

Community responsibilities. The quality of the soundscape is a growing environmental concern and is an important aspect of the City's livability. To increase public awareness of achieving and maintaining a high quality soundscape, the community can draw on noise attenuation techniques, such as those in the Noise Control Manual.

One of the most effective ways to contribute to the solution is to be a good neighbor. Most importantly, this means taking responsibility for reducing noise at the source. Businesses and industry can contain noise generating uses and activities within buildings, or construct barriers so that noise does not reach adjacent areas. Residents should also consider how the noise they generate could affect others and reduce the level accordingly. Motorists and other vehicle operators should be conscientious of the potential for vehicle noise to disturb others. Figure N-c shows noise control measures for all neighbors and neighborhoods.

FIGURE N-c NOISE CONTROL MEASURES



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6.5 NOISE POLICIES

The Noise Element contains the following policies:

- N-1 Noise Attenuation
- N-2 Stationary Noise Sources and Levels
- N-3 Transportation Noise Sources and Levels
- N-4 Requirements for Acoustical Analysis
- N-5 Intrusive and Intermittent Noise Sources

POLICY N-1 NOISE ATTENUATION

Objective. Reduce, or eliminate, noise impacts at their source by providing enclosures, barriers, and other on-site noise attenuation measures for noise generating activities. Monitor noise levels to ensure that acceptable noise levels are maintained on adjacent sites.

N-1a **Noise attenuation measures.** Noise attenuation measures, and stationary noise source controls shall include the use of barriers, setbacks, site design, baffles, enclosures, silencers, and improved facade construction techniques.

~~N-1b **Noise attenuation guidelines.** Noise attenuation measures and stationary noise source controls shall follow the guidelines provided in the technical document entitled: Noise Control Manual (which is considered an implementation measure).~~

N-1bc **Noise ~~mitigation~~attenuation.** Where noise ~~mitigation-attenuation~~ measures are required, the emphasis of such measures shall be placed upon site planning and project design. The use of noise barriers shall be considered a means of achieving the noise standards only after all other practical design-related noise ~~mitigation-attenuation~~ measures have been integrated into the project.

N-1c **Noise level reduction.** To reduce existing, objectionable, industrial, and stationary noise levels at the source, the City shall work with local industries to incorporate noise control technology through building and site design and engineering solutions. The City shall make available current acoustical attenuation techniques (Noise Control Manual) for new and retrofitted industrial development.

N-1d **Noise standards enforcement.** Enforcement of noise standards shall be accomplished through development and implementation of a noise control ordinance, post-project compliance testing, and through the use of mediation.

N-1e **Mediation of noise conflicts.** Where noise conflicts may occur, all affected parties shall strive to use innovative and positive solutions to solve those disputes, including the use of trained mediators. The City recognizes that many noise issues can be resolved before they get to the complaint stage if there is a procedure for airing the issue with an

impartial third party.

N-1f **Resolution of noise disputes.** The City shall establish positive ways to discuss and resolve noise issues and disputes, including the use of trained mediators.

POLICY N-2 STATIONARY NOISE SOURCES AND LEVELS

Objective. Establish acceptable noise levels for land uses and activities that will protect community residents from the harmful effects of excessive noise exposure from stationary noise generators. Maintain interior and exterior noise standards that will achieve land use compatibility with respect to community noise.

N-2a **Noise contour maps.** The noise contour map (Figure N-b) and other estimates of noise source levels shall be used in conjunction with the noise source criteria to evaluate the ~~feasibility impacts to and from of~~ a proposed project, as well as the attenuation measures that may be necessary.

N-2b **New development of noise-sensitive uses.** New noise receptors shall not be allowed where the noise level from non-transportation noise generators will exceed noise level standards (Table N-1), unless effective noise ~~mitigation~~ attenuation measures that meet City standards are incorporated.

N-2c **Noise created by new or proposed stationary noise sources.** Noise created by new or proposed stationary noise sources, or the expansion or alteration of an existing use, shall be ~~mitigated~~ attenuated so as not to exceed noise level standards (Table N-1) at noise-sensitive land uses. All noise generators not in compliance with these standards will be encouraged to ~~mitigate~~ attenuate impacts.

N-2d **Acceptable noise levels.** New construction and retrofits at existing buildings shall include appropriate insulation, glazing, and other sound attenuation measures so that they comply with standards contained in Table N-1. These standards are intended to set levels for external noise sources that could potentially impact a new dwelling or other noise-sensitive use.

TABLE N-1 – NOISE STANDARDS FOR NEW STATIONARY SOURCE PROJECTS AND RETROFIT/REDEVELOPMENT BY RECEIVING LAND USE

LAND USE	EXTERIOR			INTERIOR		
	7am-7pm	7-10 pm	10pm-7am	7am-7pm	7pm-10pm	10 pm-7am
Residences, Transient Lodging, Hospitals, Nursing Homes						
Hourly L _{eq}	55 dB _A	50 dB _A	45 dB _A	45 dB _A	40 dB _A	35 dB _A
Maximum	75 dB _A	75 dB _A	70 dB _A	65 dB _A	65 dB _A	60 Db _A

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Auditoriums, Theaters, Libraries, Schools, Churches						
Hourly L_{eq}	55 dBA	55 dBA	n/a	40 dBA	40 dBA	n/a
Maximum	75 dBA	75 dBA	n/a	60 dBA	60 dBA	n/a

1. The City can impose noise level standards which are up to 5 dBA less than those specified above based upon determination of existing low ambient noise levels in the vicinity of the project site.
2. These noise level standards do not apply to residential units established in conjunction with industrial or commercial uses (e.g., caretaker dwellings).
3. The standards will be applied at the outdoor activity areas of the receiving land use, and at the building facade for upper floor receivers which do not have an outdoor activity area facing the noise source. Where no outdoor activity area is identified, the City has the option to apply only the interior noise level performance standards.

POLICY N-3 TRANSPORTATION NOISE SOURCES AND LEVELS

Objective. Establish acceptable noise levels, for land uses and activities, that will protect community residents from the harmful effects of excessive noise exposure due to transportation noise sources. Maintain interior and exterior noise standards that will achieve land use compatibility with respect to community noise.

N-3a New development of noise-sensitive land uses. New development of noise receptors will not be permitted in areas exposed to existing or projected levels of transportation noise exceeding levels specified in Table N-2, unless exterior noise or noise levels in interior spaces can be reduced to meet City Standards (Table N-2).

~~**N-3b Transportation noise.** Transportation noise sources shall be periodically measured, and significant increases mitigated, so as not to exceed the levels specified in Table N-2 for outdoor activity areas or interior spaces of existing receptors.~~

TABLE N-2 - MAXIMUM ALLOWABLE TRANSPORTATION NOISE SOURCES EXPOSURE

LAND USE	OUTDOOR ACTIVITY AREAS ¹ $L_{dn}/CNEL, dBA$	INTERIOR SPACES	
		$L_{dn}/CNEL, dBA$	L_{eq}, dBA^2
Residential	60 ³	45	--
Transient Lodging	60 ⁴	45	--
Hospitals, Nursing Homes	60 ³	45	--
Theaters, Auditoriums, Music Halls	--	--	35
Churches, Meeting Halls	60 ³	--	40
Office Buildings	--	--	45
Schools, Libraries, Museums	--	--	45
Playgrounds, Neighborhood Parks	70	--	--

1. Where the location of outdoor activity areas is unknown, the exterior noise level standard shall be applied to the property line of the receiving land use.
2. As determined for a typical worst-case hour during periods of use.
3. Where it is not possible to reduce noise in outdoor activity areas to 60 dBA L_{dn}/CNEL or less using a practical application of the best-available noise reduction measures, an exterior noise level of up to 65 dBA L_{dn}/CNEL may be allowed provided that available exterior noise level reduction measures have been implemented and interior noise levels are in compliance with this table.
4. In the case of hotel/motel facilities or other transient lodging, outdoor activity areas such as pool areas may not be included in the project design. In these cases, only the interior noise level criterion will apply.

N-3be Roadway projects. To minimize noise impacts, the following criteria may be used as a test of significance for roadway projects:

1. Where existing traffic noise levels are less than 60 dBA L_{dn} at the outdoor activity areas of noise receptors, a +5 dBA L_{dn} increase in noise levels due to a roadway improvement project will be considered significant.
2. Where existing traffic noise levels range between 60 and 65 dBA L_{dn} at the outdoor activity areas of noise receptors, a +3 dBA L_{dn} increase in noise levels due to a roadway improvement project will be considered significant.
3. Where existing traffic noise levels are greater than 65 dBA L_{dn} at the outdoor activity areas of noise receptors, a + 1.5 dBA L_{dn} increase in noise levels due to a roadway improvement project will be considered significant.

POLICY N-4 ACOUSTICAL ANALYSIS REQUIREMENTS

Objective. Establish a consistent procedure and framework for conducting and reviewing acoustical analyses.

N-4a Noise-sensitive land uses. Where receptor land uses are potentially exposed to existing or projected exterior noise levels exceeding the levels specified in Table N-2 or the performance standards of Table N-1, an acoustical analysis shall be required as part of the environmental review process, so that noise mitigation may be included in the project design. An acoustical analysis prepared pursuant to the Noise Element shall:

1. Be the financial responsibility of the applicant.
2. Be prepared by a qualified person experienced in the fields of environmental noise assessment and architectural acoustics.
3. Include representative noise level measurements, with sufficient sampling periods and locations, to adequately describe local conditions and the predominant noise sources.
- ~~4. Estimate existing and projected cumulative (twenty years) noise levels in terms of L_{dn} or CNEL and/or the standards of Table N1, and compare those levels to the adopted policies of the Noise Element.~~

4. Recommend appropriate mitigation to achieve compliance with the adopted policies and standards of the Noise Element, giving preference to proper site planning and design over mitigation measures which require the construction of noise barriers or structural modifications to buildings which contain noise-sensitive land uses.
5. Estimate noise exposure after prescribed mitigation measures are implemented.
- ~~7. Describe a post project assessment program which could be used to evaluate the effectiveness of the proposed mitigation measures.~~

POLICY N-5 INTRUSIVE AND INTERMITTENT NOISE SOURCES

Objective. Protect community residents from the effects of excessive, intrusive, and intermittent noise. Set standards for intrusive and intermittent noise sources for both daytime and nighttime periods. Intrusive noise sources have a qualitative aspect that can be annoying. These sources may contain a tonal component which is absent from the existing general background noise. They may also be rhythmic, reoccurring or impulsive in nature, or comprised mainly of music or speech. Intrusive noise can result in annoyance or interference with sleep. These types of noise sources can include, but are not limited to, industrial processes, warning horns, backup alarms, and pressure release devices.

- N-5a **Intrusive noise.** When intrusive noise sources have been identified, the detrimental effects (sleep interference or the potential for annoyance) shall be disclosed to neighboring receptor properties.
- N-5b **Noise levels due to non-transportation sources.** Noise levels due to non-transportation sources which may be intermittent or recurring, impulsive noises, pure tones, or noises consisting primarily of speech or music, shall be subject to the criteria contained within Table N-1, with a -5 dBA penalty applied to the criteria.
- N-5c **Rhythmic, reoccurring, or impulsive noise sources.** When noise sources have been identified to be rhythmic, reoccurring, or impulsive in nature or comprised mainly of music or speech, they may comply with applicable noise level criteria and still be annoying to individuals. When these types of noise sources have been identified, they may be subject to additional ~~mitigation~~ attenuation or mediation.
- N-5d **Construction site tool or equipment noise.** Construction The following shall apply to construction noise from tools and equipment:
1. The operation of tools or equipment used in construction, drilling, repair, alteration or demolition shall be limited to between the hours of 8 a.m. and 7 p.m. Monday through Friday, and between 9 a.m. and 7 p.m. on Saturdays and Sundays.
 2. ~~No heavy equipment related Heavy equipment shall not construction activities shall be allowed operate~~ on Sundays ~~or~~ and City recognized holidays.

This shall apply to construction noise from tools and equipment ~~which~~ that are subject

to the review of the City, and ~~which that~~ may affect receptor uses. This policy shall not apply to emergency work of public service utilities, City sponsored and/or grant funded projects, or by variance under a noise ordinance or authorization by the appropriate City review authority.

N-5e **Stationary and construction equipment noise.** All stationary and construction equipment shall be maintained in good working order, and fitted with factory approved muffler systems.

N-5f **Noise Ordinance.** The City of Arcata shall ~~develop and adopt a~~enforce its City-wide noise ordinance. ~~The ordinance shall contain noise level criteria consistent with the criteria contained within the noise element.~~The City shall periodically review and update its noise ordinance.

6.6 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURE	RESPONSIBLE PARTY	TIME FRAME
N-1	Citywide and downtown noise ordinances Adopt a citywide noise ordinance to protect public health, safety, welfare, and quiet by reducing existing noise levels and prohibiting the generation of loud noise from new sources. Amend the existing downtown noise ordinance to be consistent with Noise Element standards.	Police Department	Year 1
N-2	Noise Control Manual Maintain and apply a Noise Control Manual that contains techniques for soundproofing new structures and muffling equipment and motors; promotes use of acoustic materials, barriers and setbacks; and requires acoustic analysis to determine potential noise impacts.	Community Development Department/ Planning Commission	Year 1
N - 3	Education programs The City shall maintain current information about noise monitoring and attenuation techniques effective in identifying and reducing noise.	Community Development Dept.	Ongoing
N - 4	Record of Noise complaints and enforcement The Arcata Police Department shall maintain a record of noise complaints for which they receive and to which they respond. The Community Development Department and Code Enforcement Division shall cooperate to address noise complaints related to land use matters as determined necessary by the Community Development Director and Chief of Police, using progressive enforcement starting with mediation.	Arcata Police Dept., Community Development Department, Engineering Department.	Ongoing
N-5	Noise reduction inquiry procedure The City recognizes that many noise issues can be resolved before they get to the complaint stage, if there is a procedure for airing the issue with an impartial third party.	Community Development Dept.	Year 1

Attachment: H. Ch 6 Health Safety (4214 : GP #XVII FBC & GP wrap-up)

HEALTH ELEMENT

2.10 INTRODUCTION

Health Element Organization. The City’s role in supporting health in Arcata is aiding in the creation of physical and social environments that afford its residents the greatest possible opportunities for wellbeing. This charge includes the provision of safe and affordable housing and transportation, as well as opportunities for economic prosperity, physical activity, recreation, education, and community connection.

While this Health Element was developed for the General Plan 2045 the City of Arcata’s recognition of its responsibility to positively affect health and wellbeing in its jurisdiction is not new. Decades of City policy codify the City’s continuous commitment to acting in service of community health. For example, the 1976 *Physical Design Recommendations for Achieving Community Goals Social Goals* report establishes objectives such as: provide for a healthful environment, provide for personal safety, provide for a socially diverse community, promote a sense of community membership and social worth, and more—goals that are reflected and updated in the policy priorities of this Element.

Many of the City’s responsibilities that support health are also detailed in other General Plan Elements (including Housing, Infrastructure and Public Facilities, Transportation Open Space, and Parks and Recreation), and language related to health is integrated throughout them. However, to prioritize Arcatans’ wellbeing, this Element is dedicated to health-specific goals and policies.

This Element also focuses on health equity. Disparities in health and wellbeing outcomes correlate with lower socioeconomic status, race and skin color, and disability status. Some residents face disproportionate barriers to health and wellbeing due to these factors, which should not be determinants of health. This Element includes policies to reduce inequitable health outcomes.

Overview of health and wellbeing in Arcata. Health is defined by the World Health Organization as a state of complete physical, mental and social wellbeing rather than merely the absence of disease or infirmity. Health is shaped by the cultural, social, political, economic, ecological, and physical settings in which we live, work, and play. These “social determinants of health” dictate daily experiences, physical and emotional wellbeing, how long we live, and our ability to change the quality and course of our lives.

“Health equity” refers to efforts to ensure that all people have full and equal access to opportunities that enable them to lead healthy lives while respecting differences in identity. These differences include, but are not limited to, culture, language, race, gender, sexuality, economic status, citizenship, ability, age, and religion.¹

Arcata is home to Mad River Hospital, one of ~~four~~ ~~the two~~ hospitals in Humboldt County. The community is also served by Open Door Clinic, United Indian Health Services (UIHS),

¹ <https://www.ci.richmond.ca.us/ArchiveCenter/ViewFile/Item/6999>

approximately 8 dental providers,² and mental health care providers offering a range of therapy and counseling services. Furthermore, there are many businesses and organizations in Arcata that support physical health and community gathering—including gyms, yoga studios, bike shops, and running clubs.

Some neighborhoods in Arcata have a disproportionate concentration of barriers to healthy living. The Valley West neighborhood specifically is disconnected from many of Arcata’s assets in terms of healthy living. Compared to those nearer to Arcata’s downtown, Valley West residents have decreased access to safe open spaces and healthy food options.

The City of Arcata envisions a community that is a safe and healthy place to live, work, play, and learn. This Health Element contains policies intended to ensure that Arcata’s economy, education, health services, built environment, and social context all support the physical, mental, and emotional wellbeing of all its residents.

Barriers to health and wellbeing in Arcata. As in many rural areas across the country, Humboldt County residents face barriers to accessing affordable medical, dental, and mental health care services. One of the primary obstacles to increasing the availability of these services is Arcata’s lack of affordable housing, which impacts the security and mental health of Arcata residents as well as the community’s ability to retain health professionals. The city’s housing shortage also affects the mental and physical health of its residents; when a household spends a disproportionate amount of its monthly income on housing costs, it impacts their ability to purchase medications, healthy foods, and other items important to health and wellbeing.

Among other consequences, Arcata’s lack of accessible care providers prevents many residents from accessing necessary medical and dental services (much less seeking preventative care, such as cancer screenings). Similarly, the lack of mental health care resources means that some Arcatans with diagnosed mental health disorders are unable to get necessary medication due to the inaccessibility of local psychiatrists able to write prescriptions.³

As there is specific difficulty in finding a primary care provider, dentist, or mental healthcare professional that has availability for new patients, access to care is an issue that disproportionately affects students and other Arcata residents recent to the area.

In addition to the challenges faced in seeking healthcare, many Arcata residents struggle to afford it. Census data shows that the number of Arcatans living below the poverty line is far higher than the state average. Beyond the difficulty poverty presents in accessing health services, economic insecurity inherently results in mental and physical stress. Poverty and low-income status are associated with various adverse health outcomes, including shorter life expectancy and higher death rates.⁴

² <https://findadentist.ada.org/search-results?address=Arcata,California&distance=5>

³ Racial Equity Working Group (2023)

⁴ <https://www.aafp.org/about/policies/all/poverty-health.html>

The limited transportation options between Arcata, Eureka, and McKinleyville also make it difficult for Arcata residents who do not own reliable personal vehicles to get to appointments in neighboring communities or outside the area.⁵ As in many rural areas nationwide, Humboldt County residents frequently have to travel hundreds of miles for medical or dental appointments. This contributes travel expenses to the cost burden faced by those seeking care.

Another barrier to health in Arcata faced by those with lower incomes and limited transportation options is access to healthy foods. Healthy foods are usually more expensive, and it is difficult for residents that do not live near downtown to access places that sell them.

Low-income status also makes it difficult to access stable and safe housing, especially in Arcata's high-cost, high-demand rental market. Living unsheltered poses difficulties to maintaining a healthy diet, securing/showing up for medical appointments, and properly storing medication. For those with conditions that require daily management and attention—such as diabetes—this can make living unhoused immediately life-threatening.⁶

Racism and discrimination present another barrier to health within the Arcata community. The negative health impacts of racism present on individual levels—such as stress and housing discrimination—as well as structural levels, meaning differential access to goods, services, and opportunities.⁷

Assets to health and wellbeing in Arcata. Health and wellbeing in Arcata are benefitted by the city's unique natural resources and the community's commitment to protecting them. Open space lands support public health by providing accessible and low-cost public recreation opportunities to all, regardless of income. Furthermore, they allow for community gardens which can increase access to fresh local produce.

Arcata's small-town nature and the time and energy that its residents put into building community also support health. Loneliness and social isolation have been linked to increased risks of premature death, dementia, heart disease, and stroke, as well as higher rates of depression, anxiety, and suicide.⁸ The wide variety of community classes, recreation activities, events, and other programming offered by the City and other local organizations expands and deepens community connections in Arcata by bringing together people of shared interests. These connections benefit residents of all ages, but especially Arcata's seniors; the CDC has found that many adults aged 50 and older are socially isolated or lonely in ways that put their health at risk.⁹

Furthermore, Arcatans' health and wellbeing is supported by Cal Poly Humboldt and the educational, social, and cultural benefits that the university brings to the community. Lifelong learning has been linked to cognition and memory, social connection, and self-

⁵ Racial Equity Working Group (2023)

⁶ Open Door Community Health Centers

⁷ [What is Health Equity? | Health Equity | CDC](#)

⁸ <https://www.cdc.gov/aging/publications/features/lonely-older-adults.html>

⁹ <https://www.cdc.gov/aging/publications/features/lonely-older-adults.html>

esteem.¹⁰ The opportunities for learning that the university gives to the community—through events, community classes, and the perspectives of its students, faculty, and staff—set Arcata apart from other towns of its size.

Finally, the nonprofit organizations and community groups working to make Arcata a more equitable place to reside—decreasing violence and discrimination related to race, class, gender identity/expression, and sexual orientation and increasing support and opportunities for all Arcatans—are a benefit to the health of the community.

Guiding Principles and Goals.

- A. Ensure all residents can enjoy physical, social, and mental health and wellbeing, regardless of their race, age, gender, sexual orientation, social position, economic position, culture, background, or any other factor of identity.
- B. Understand that a community’s socio-economic, built, and natural environments influence every resident’s health. Recognize both the integrated nature of these systems¹¹ and the City’s responsibility in ensuring that its actions promote physical, social, and mental health and wellbeing.
- C. Partner with and promote local agencies, nonprofits, and community-based organizations that are already working to support the health and wellbeing of Arcata residents.
- D. Eliminate the disproportionate and unjust barriers to health that some Arcata residents face based on neighborhood and/or factors of identity.
- E. Support healthy eating and active living programs and facilities to enable them.
- F. Increase opportunities for community engagement.

2.2 POLICIES

The Health Element contains the following policies areas.

- H-1 Community Health
- H-2 Healthy Behaviors
- H-3 Community Culture
- H-4 Healthy Government

POLICY H-1 COMMUNITY HEALTH

Objective. Increase accessibility of medical and mental healthcare for all Arcatans. Prioritize groups experiencing differential health outcomes.

- H-1a **Determinants of health.** Work with the community and stakeholders in medical, mental, and dental healthcare fields to identify determinants in the Arcata community that impact health and wellbeing, paying specific attention to factors

¹⁰ <https://extension.usu.edu/mentalhealth/articles/the-benefits-of-being-a-lifelong-learner>

¹¹ Racial Equity Working Group (2023)

- affecting specific groups or demographics disproportionately. Facilitate access to financial and professional resources to document and utilize necessary epidemiological and health care data.
- H-1b **Access to services.** Work with stakeholders in medical, mental, and dental healthcare fields to identify barriers to healthcare access and affordability. Explore ways in which the City can aid in reducing those barriers. Work with community partners to aid individuals and families in accessing health resources.
- H-1c **Health equity.** Strive for health equity by identifying and reducing barriers to health faced disproportionately by vulnerable groups within Arcata, including those with lower socioeconomic status as well as children, the elderly, persons with disabilities, and Limited English Proficiency (LEP) individuals.
- H-1d **Support unhoused community members.** Increase City coordination with/support for organizations that provide resources and health services those living unsheltered in Arcata. Work with partners to aid unhoused individuals and families in accessing health-supporting resources such as insulin and medication.
- H-1e **Retain existing healthcare institutions and resources located in Arcata.** City policies should strongly support retention of existing healthcare institutions, facilities, and services located in Arcata, including Mad River Hospital, Open Door Clinic, United Indian Healthcare, and other more specialized medical practices.
- H-1f **Land use regulation to support health care.** Amend land use regulations to support provision and accessibility of healthcare services, including locations of healthcare providers in multiple-use and commercial land use classifications.

POLICY H-2 HEALTHY BEHAVIORS

Healthy Behaviors

Objective. Develop and support systems conducive to physical, mental, and social wellbeing. Design an environment in which the healthiest choice is the easiest choice.

- H-2a **Daily physical activity.** Ensure that residents of all neighborhoods, ages, incomes, and abilities have access to opportunities for daily physical activity. Develop convenient access to safe and attractive outdoor spaces and places throughout the city. Make access to spaces, events, and programs that support physical activity affordable and convenient.
- H-2b **Opportunities for healthy eating.** Ensure-Support City and community programs that improve opportunities for nutritional health, food security, and healthy eating among residents of all races, neighborhoods, ages, genders, incomes, and abilities

- ~~have opportunities for healthy eating.~~
- H-2c **Opportunities for community connection.** Create opportunities to strengthen the sense of community in Arcata through connection, relationships, and trust. This includes expanding offerings of community classes and events, as well as increasing their accessibility and affordability. Engage Arcata residents to identify actions the City can take to help define, share, and support community expressions of Arcata culture.
- H-2d **Commercial tobacco.** Reduce the use of commercial tobacco. Commercial tobacco use is linked to asthma, cancer, COPD, diabetes, tooth loss, heart disease, stroke, and birth outcomes. The City shall prioritize the need of non-smokers to breath smoke-free air in public places.
- H-2e **Alcohol misuse.** Reduce the misuse of alcohol and the prevalence of alcohol-related accidents and injuries, paying specific attention to groups that may be at higher risk for alcohol misuse. Alcohol misuse is linked to injuries, violence, unsafe sexual behavior, adverse birth outcomes, blood pressure, heart disease, stroke, liver disease, cancer, and decreased emotional wellbeing. The City shall consider the impact of land use [and transportation](#) policies, [including public transit service access, availability, and frequency near alcohol serving establishments](#), and programs on alcohol misuse.
- H-2f **Substance use disorders.** Humboldt County's rate of drug overdose deaths is higher than the State and National rates. The City shall work with partners to reduce the prevalence of substance use and overdose deaths in Arcata, increase support for those struggling with substance use, and decrease stigmas. Feeling stigmatized can reduce the willingness of individuals struggling with substance use disorders to seek treatment and increase the social isolation of individuals using drugs.¹²

POLICY H-3 COMMUNITY CULTURE

Objective. Encourage a healthy, cohesive community culture. The strength of relationships and the sense of shared identity between members of a community are linked to emotional wellbeing, safety, and higher levels of investment in health-promoting community resources. This section outlines desired community cohesion outcomes and addresses how the City can support and contribute to a strong, diverse, and collective Arcata identity.

- H-3a **Healthy and equitable schools.** Work with schools to improve and or sustain programs that support mental, physical, emotional, and social wellbeing. Childcare and school environments influence the foods children eat and drink outside the

¹² <https://nida.nih.gov/nidamed-medical-health-professionals/health-professions-education/words-matter-terms-to-use-avoid-when-talking-about-addiction>

home, rates of physical activity, and social and emotional development. Educational attainment is also linked with lifespan. Increase the ways in which Arcata's schools contribute to students' health and wellbeing through:

1. Integrating physical activity into students' daily educational experience through both programming and the design of school sites and structures, and linkages with City facilities.
2. Integrating the arts into students' daily educational experience.
3. Supporting walking and biking to school.
4. Ensuring students have access to healthy foods on school sites.
5. Leveraging schools to remove mental health and safety barriers to physical activity among youth.
6. Encouraging schools to ensure that classrooms and curriculums integrate and dialogue with historically excluded knowledge sources, empower and support students of all races, ethnicities, and cultures, and represent/appreciate all cultures respectfully.

H-3b **Healthy workplaces.** The City shall seek to support resources to increase opportunities for healthy behaviors on workplace sites. Support walking and biking to work. Increase opportunities for racial equity training in workplaces. Increase opportunities for cross-agency/industry professional development training, which provide Arcata's professionals continuing educational opportunities in leadership and facilitate community connections within/between workplaces.

H-3c **Support Arcata arts & artists.** Support artists, arts organizations, and arts endeavors as a means for building and healing community in Arcata. Arcata's artists lead the way in welcoming and celebrating diversity, supporting local businesses, caring for community health & wellness, bringing residents into community with one another, fostering pride of place, igniting community conversations, and much more.

H-3d **Community consciousness of racial equity.** Encourage community dialogue around historic and current systems of advantage and exclusion. Cultivate a collective consciousness of equity and inclusion. Work to make Arcata a Welcoming, safe, and racially equitable community by working to erase racial disparities for all people of color in all aspects of community life including housing, health, education.

POLICY H-4 HEALTHY GOVERNMENT PRACTICES & PROTOCOLS

Objective. Engage and collaborate across City departments, partner agencies, local nonprofits, and the community to develop and achieve a collective vision for a healthy, equitable community. Elected officials and municipal staff play central roles in creating and implementing the policies, programs, and services in a long-range plan. This section addresses practices and protocols that the City of Arcata will implement to ensure the promotion of health and equity throughout its regular activities.

H-4a **Healthy and Equitable City operations.** Ensure City-wide knowledge of, and commitment to, core principles of diversity, equity, and inclusion, and continue to support the equity arcata network as a key partner in achieving the goals of this policy. Integrate trauma-informed practices into City operations and services. Utilizing trauma-informed practices involves considering the following six principles when interacting to provide programs, services, and activities:

1. Safety
2. Trustworthiness and transparency
3. Peer support
4. Collaboration and mutuality
5. Empowerment, voice, and choice
6. Factors such as race, culture, and gender identity

H-4b **Transparent and responsive government.** Increase government transparency and build trust among Arcatans of all ages, races, identities, and income levels. Demonstrate the City's credibility, reliability, openness, and community orientation through establishing a coordinated set of proactive channels for community-wide engagement between City departments, stakeholders, and the public. Respond promptly, reliably, and openly to criticism of City operation and administration from members of the public. Continue this engagement on an ongoing basis regardless of specific plans or projects underway. Evaluate implementation of plans, projects, and programs to ensure that established policies are being carried out. Monitor how implementation is serving different groups within the community ~~in comparison to each other,~~ and respond to community critiques.

H-4c **Health in all policies.** Identify and address existent inequities in City policy that are resulting in disparities, increase equity at the systems and structural levels. Build relationships with community partners to identify ways in which policies can take health outcomes into consideration. Implement procedures that allow the City to prioritize community health in all programs and practices. Educate the community and other stakeholders about Arcata's overall health goals and actions.

H-4d **Collaborative community planning.** Involve and collaborate with residents and community organizations in the development of plans, projects, and programs that affect their health. Establish a coordinated set of proactive, ongoing, and inclusive ways that City departments communicate with the public to discuss health-related issues and values. Make specific efforts to engage Arcata's young people in long-range community planning.

H-4e **Health data.** Establish metrics and develop/adopt tools for measuring community health. Partner with a network of organizations and agencies, including the County

Departments of Public Health and Health & Human Services, to begin collecting community health data on an ongoing basis.

H-4f **Funding for health.** Identify, pursue, and dedicate funding streams that support actions to improve community health outcomes. Incorporate health considerations into City budget and project funding decisions.

H-4g **Youth engagement.** Build capacity among Arcata’s youth to advocate for healthy changes in their community.

H-4h **Senior Engagement.** Support capacity and initiatives by Arcata’s senior citizens and residents to advocate for changes in Arcata’s infrastructure, programs, and City operations to help seniors remain healthy and active.

Attachment: H. Ch 6 Health Safety (4214 : GP #XVII FBC & GP wrap-up)

H-4i **Engage with Arcatans of multiple abilities.** Support capacity among Arcatans of differing abilities and disabilities to represent their interests and concerns to the City with regard to City infrastructure, programs, and operations that can improve possibilities for people of different abilities to pursue healthy, active, mobile lives in Arcata.

2.12 IMPLEMENTATION MEASURES

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
H-1	<p>Access to services</p> <ol style="list-style-type: none"> 1. Identify opportunities to cross-promote local health resources on the City website and in City facilities. 2. Identify opportunities for Arcata City Council and staff participation in coalitions and stakeholder groups addressing health within Arcata and the North Coast Region. <u>3.</u> Pursue partnerships to create centers equitably distributed throughout the City that provide resources to the community and makes residents feel connected, welcome, supported, and cared for. <u>3-4.</u> <u>The City shall work with existing healthcare institutions and medical practices in Arcata to retain services and improve access to them.</u> 	City Manager’s Office and Community Development Department	Ongoing

Attachment: H. Ch 6 Health Safety (4214 : GP #XVII FBC & GP wrap-up)

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
H-1	<p>Health equity</p> <ol style="list-style-type: none"> 1. Support the development of a Health Equity working group within Equity Arcata to coordinate with medical facilities and healthcare organizations. Encourage facilities to provide equity, diversity, inclusion, bias, and/or cultural competency training to all doctors, nurses, and support staff on an annual basis. 2. Encourage community partners that work frequently with Limited English Proficiency populations to collaborate with medical facilities and healthcare organizations to provide translated resources on healthcare, insurance, and mental health/dental/medical care. 2-3. <u>Routinely engage people knowledgeable about needs of people of varied abilities to ensure that City facilities, programs, and practices serve people of varied abilities and disabilities. Strive for Universal Design in all City facilities.</u> 	City Manager’s Office, Equity Arcata	Ongoing
H-2	<p>Opportunities for healthy eating</p> <ol style="list-style-type: none"> 1. Collaborate with food system partners, such as the Humboldt Food Policy Council, the North Coast Growers’ Association, Cal Poly Humboldt, the California Center for Rural Policy (CCRP), Arcata House Partnership, and the Humboldt County Department of Health & Human Services to strategize approaches the City can consider to help grow a healthy food system in Arcata. 2. Support local agricultural projects such as community gardens and farmers’ markets in areas of Arcata beyond downtown. Ensure zoning allows a range of urban agriculture activities in all districts with relevant operating standards. 3. Building off past efforts by the Food Policy Council and California Center for Rural Policy, collaborate with local partners to develop a community food assessment to analyze challenges and opportunities related to Arcata’s local food system. The assessment should pay particular attention to which neighborhoods and/or groups within Arcata may be lacking access to healthy food at higher rates. 3-4. <u>Work with existing commercial businesses to improve access to healthy food.</u> 	Community Development & Environmental Services	Ongoing

Attachment: H. Ch 6 Health Safety (4214 : GP #XVII FBC & GP wrap-up)

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
H-4	<p>Support Arcata arts & artists</p> <ol style="list-style-type: none"> 1. Implement the Arcata Strategic Arts Plan (ASAP) with intermittent re-evaluations of priority projects. 2. Identify and pursue sources of funding that could be used to support arts organizations and endeavors in Arcata. 3. Explore funding sources and possible locations for the development of artist housing/live-work spaces in Arcata. 4. Prioritize support of intersectional arts and equity projects/collaborations in Arcata area schools. 	City Manager’s Office and Community Development	Ongoing
H-5	<p>Equitable City operations</p> <ol style="list-style-type: none"> 1. Provide equity, diversity, inclusion, bias, and/or cultural competency training to City staff and elected/appointed officials on a regular basis. 2. Continue supporting the Equity Arcata network. Alter job descriptions to include involvement with Equity Arcata in key City staff positions that integrate well with the collective’s working groups. 3. Scope and initiate an inclusive, community-driven process to update the Draft Solution Set addressing current racial inequities in Arcata, and assessing progress made since its initial drafting. 	City Manager’s Office - Personnel	Ongoing

Attachment: H. Ch 6 Health Safety (4214 : GP #XVII FBC & GP wrap-up)

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
H-6	<p>Community Engagement Protocol</p> <p>Building on the work that the Community Development Department undertook with the Institute for Local Government through the BOOST Program over the course of 2020, establish a Community Engagement Plan that outlines how the City engages community-based organizations, other partners and stakeholders, and the public on proposed plans, projects, and programs. Update the Plan and “Community Landscape” list periodically to ensure its continued relevancy.</p> <p>Ensure the following concepts are integrated into the Plan:</p> <ol style="list-style-type: none"> 1. Utilize the “Community Landscape” list in outreaching to identified stakeholders and community groups as appropriate. 2. Prioritize outreach to neighborhoods and populations facing disproportionate barriers to healthy living. Prioritize outreach to groups historically excluded from City and community planning processes, such as Black, Indigenous, Hispanic, Asian, LEP, and disabled residents. 3. Continue to pursue opportunities for partnership with Arcata schools (specifically high schools) to present on long-range planning projects and solicit student feedback. 3.4. <u>Respond promptly and openly to critiques of City operations and decisions, including those made through administrative channels, and especially to those engaging with City committees, the Planning Commission, and City Council.</u> 	Community Development Department	Every 10 years starting in year 2
H-7	<p>Health data</p> <p>As part of the City’s Annual General Plan Progress Report submitted to the State Office of Planning and Research, include applicable information from departments, residents, and other stakeholders about how improvements to support health, <u>or changes in programs, policies and services that threaten it</u>, have affected residents. Give specific attention to discrepancies faced by specific neighborhoods and groups.</p>	Community Development Department	Ongoing
H-8	<p>Health in all policies</p> <p>Develop criteria for health and equity project review. Integrate health and equity review into decision-making processes for policy updates and capital projects. Update these review criteria periodically to ensure continued relevancy.</p>	Community Development Department, City Manager’s Office	Every 10 years starting in year 2

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
H-9	<p>Discrimination-free, accessible housing</p> <p>Collaborate with agency partners to establish financial and other incentives for landlords and property managers that go through City-endorsed anti-discrimination trainings such as the Humboldt Tenant Landlord Coalition “Living in Community” Training.¹³</p>	Community Development Department	Ongoing

Key to following Implementation Measures

Measure will be integrated into:

Public Safety
Parks and Recreation
Circulation

PS-X	<p>Exposure to community violence and discrimination</p> <ol style="list-style-type: none"> Increase awareness around the Arcata Police Department’s Juvenile Diversion Program, the primary objectives of which are to reduce the number of youths in the Criminal Justice System and reduce dropout/expulsion rates in Northern Humboldt area school districts. Make implicit bias and cultural humility training available to Public Safety Committee members. Work with the County and other local social service agencies to reduce stress and provide services and support to groups found to be at higher risk of being victims or perpetrators of violence. Explore partnerships with programs, social service agencies, and community health advocates that work to deter, intervene in, and mediate violence in the Humboldt County community. Familiarize City staff that frequently interface with the public with these resources. 	Arcata Police Department	Ongoing
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¹³ Racial Equity Working Group (2023)

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
PS-X	<p>Community policing</p> <ol style="list-style-type: none"> 1. Continually publicize the training that APD officers complete on an ongoing basis related to racial equity, implicit bias, cultural humility, and interacting with individuals who have mental illnesses, intellectual disabilities, and substance use disorders. 2. Increase support for programs such as the Mobile Intervention and Services Teams (MIST), which allows Humboldt County Department of Health & Human Services (DHHS) Behavioral Health Branch staff to work with the APD in responding to incidents that involve people with mental/behavioral health issues and/or substance use disorders. 3. Increase support for programs such as the City of Arcata's Community Ambassador Program, which has a stated mission of welcoming all community members to Arcata businesses and inviting and guiding them to retail, restaurant, entertainment, recreational, and supportive options and services all while enhancing the beautification and safety of Arcata. 4. Raise awareness around the APD's internship program, available to Cal Poly Humboldt and College of the Redwoods students, which allows participants to collaborate on developing best practices in policing and to share information on student expectations of law enforcement in Arcata. 5. Raise awareness around the APD's ride-a-long program, available to any member of the community, which allows residents to see how APD delivers public safety services, meet APD staff, and ask questions. 6. Incentivize City staff and APD officers to engage with local cultural learning opportunities and equity trainings outside of those mandated in their line of duty. 	Arcata Police Department	Ongoing

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
C-X	<p>Safe, convenient, connected, and multi-modal transportation</p> <p>Ensure current versions of the Regional Transportation Plan, Humboldt Regional Bicycle Plan, ADA Transition Plan and Local Road Safety Plan:</p> <ol style="list-style-type: none"> 1. Focus pedestrian and bicycle transportation infrastructure improvements near primary areas of activity, including schools, town centers, commercial clusters, and workplace districts. 2. Ensure public health and equity considerations are included as part of transportation network improvement decisions, <u>including accessibility and mobility considerations for people of diverse abilities and disabilities.</u> 3. Plan capital improvements that will ensure sidewalks and other pedestrian routes are continuous, creating a cohesive network. Continue to assess existing sidewalks and bike lanes (or existent lacks thereof) throughout the City and identify those most in need of construction or improvement as part of yearly CIP planning. 4. Identify opportunities for the creation of a non-vehicular network of paved and/or unpaved trails, paths, or other travel-ways through blocks or open spaces to reduce travel distances and create safer routes for pedestrians and bicyclists. 	Engineering Department, Transportation Safety Committee	Ongoing
C-X	<p>Increase transportation accessibility</p> <ol style="list-style-type: none"> 1. Plan capital improvements that will remove physical barriers to and/or build facilities for walking and biking along transportation routes—especially those with higher volumes of pedestrians and bicyclists, routes known to have concentrations of people without vehicles, and along routes that provide access to major destinations including local schools. 2. Require new projects to build transportation network improvements for pedestrians, bicyclists, and wheelchairs on sites and along adjacent streets as part of all new developments/improvements (and especially those in areas where there are clusters of activity and/or existing transit stops). 	Engineering Department, Transportation Safety Committee	Ongoing

#	IMPLEMENTATION MEASURE DESCRIPTION	RESPONSIBLE PARTY	TIME FRAME
C-X	<p>Targeted improvements to existing transportation network</p> <ol style="list-style-type: none"> 1. Increase street lighting around Arcata to increase safety of walking/biking during early mornings and evenings, especially those with higher volumes of pedestrians and bicyclists, routes known to have concentrations of people without vehicles, school zones, and along routes that provide access to major destinations¹⁴ 2. When designing new or improving existing streets, implement complete streets policy to incorporate pedestrian and bicycle safety improvements. <u>Ensure that facilities and design promote and support mobility by people whose mobility is impaired, who move slowly, or who need mobility assistance.</u> Use cost-effective street improvements such as striping, cones, and temporary features to test improvements throughout the City in a cost-effective manner and address needs temporarily when funding does not permit a more permanent solution. 3. When designing new or improving existing streets, use traffic-calming techniques (such as narrowing traffic lanes or adding flashing beacons/radar signs) to improve street safety and access (especially in residential neighborhoods, school zones and areas with high pedestrian traffic). 3.4. <u>Consider developing City-operated traffic lights with appropriate signals for people with impaired vision, hearing, or mobility.</u> 	Engineering Department, Transportation Safety Committee	Ongoing
PR-X	<p>Increase use of public spaces</p> <p>In collaboration with community organizations, establish a calendar of City-sponsored programs and events taking place in public spaces, including offerings that encourage physical activity, educational opportunities, arts programming, and community outreach.</p>	Recreation Department Community Development Department	Ongoing
PR-X	<p>Natural features and built facilities</p> <p>Prioritize improvements to areas with comparatively less open space/public facilities access as part of the ongoing established public space maintenance schedule</p>	Parks and Recreation Department	Ongoing

Attachment: H. Ch 6 Health Safety (4214 : GP #XVII FBC & GP wrap-up)

¹⁴ Racial Equity Working Group (2023)

Health language proposed for other Elements:

Land Use

Land use and zoning that promote community health and wellbeing. Patterns of land use and development can either promote or deter physical, mental, and social wellbeing in different ways. Support patterns of land use and development that encourage physical, mental, and social wellbeing, as well as shape activity centers, districts, corridors, and neighborhoods to support walking, biking, and transit.

Transportation

Transportation network that promotes wellbeing. Create a transportation network that promotes physical, social, and mental wellbeing. Transportation networks consist of the routes that connect places and shape how people move around their communities. Prioritize the design of transportation infrastructure projects to support convenient and safe active transportation and affordable public transit. Recognize [the systemic barriers that many Arcata residents, specifically persons with disabilities, families with children, and lower-income Arcatans, face accessing that both active transportation and public transit is inaccessible for many Arcata residents, specifically persons with disabilities, families with children, and lower-income Arcatans.](#)

Increase transportation accessibility. Plan a transportation network that is accessible to persons with disabilities and Arcatans of all ages and income levels. Prioritize public transit in planning for Arcata's transportation network.

Parks & Recreation

Create a public space network that promotes physical, mental, and social wellbeing. Design an accessible public space network that promotes physical and mental health, contributes to a sense of community, improves air quality, and supports storm water management (see Open Space and Parks and Recreation Elements for more information).

Public spaces promote community connection, engagement with Arcata's natural environment, and physical activity. Research has also found that residents who live adjacent to green space have lower levels of illness and disease than people of similar income levels who do not, and that that social cohesion is partially dependent on residents' ability to connect through public spaces and facilities within them. Rates of physical activity, in turn, are linked to a range of health outcomes including emotional wellbeing, risk of mental disorders, body weight, blood pressure, heart disease, respiratory illness, and social interaction.

Ensure a diversity of public spaces. Plan for a variety of types of open spaces—including plazas,

parks, playgrounds, and amphitheaters/stages—that accommodate a variety of types of activities (formal, informal, structured, and unstructured) and promote physical activity and social interaction. Consider public arts, both visual (murals, sculptures, etc.) and opportunities for public performing arts (theater, music, etc.), in planning for new public spaces and in activating existing public spaces. Prioritize areas lacking in existing parks, facilities, and/or recreational opportunities in the development of new public spaces.

Resource Conservation & Management

“Greening” efforts. Prioritize “greening” efforts in Arcata’s urban spaces to help dampen ambient noise, cool pavement surfaces, and maintain high air quality. These may include the addition of street trees, community gardens, and green spaces.

Public Safety

Exposure to community violence and discrimination. Continue to ensure that the organizations and individuals aiding those who have experienced violence in the community are well-versed in how best to support the victims of hate crimes, whether they be racially motivated, or crimes committed with bias against an individual’s gender identity/expression or sexual orientation. All City departments share responsibility in providing consideration to groups facing exposure to violence at higher rates in Arcata as well as discrepancies in crime by neighborhood.

Community safety. Improve residents’ safety and perceptions of community safety through reducing crime and violence. Integrate Crime Prevention Through Environmental Design (CPTED) elements in street, open space, and development projects. Prioritize the needs of neighborhoods with higher crime rates.

Equitable policing. Continue to ensure that community members feel safe, protected, represented, and respected when interacting with law enforcement—regardless of race, gender identity/expression, or sexual orientation, and continue to invest in creating opportunities for mental and behavioral health experts to collaborate with the Department in responding to incidents involving mental illness/behavioral health and/or substance addiction. Increase awareness of the Arcata Police Department as a resource for residents who have experienced instances of hate or discrimination in the community.

General Plan Topic	Recommendation	Source	Policy Implications/Staff recommendation	PC Recommendation
Gateway Vision				
Opportunity Sites	Opportunity Site "A" is too large and should be broken into smaller sections to be more realistic	Public Member	Opportunity sites are intended to identify areas of anticipated redevelopment but are not intended to be specific to individual developments. Leave as is.	
Gateway Chapter 1: Land Use				
GA-1b	Require all high density residential buildings, particularly those with very small units, to have storage space available to occupants within the building particularly if mini-storage is going to be eliminated from Gateway Area, as well as bike storage	Public Member	EDC recommended to not include. Staff suggests that such standards be considered in the Form-Based Code.	
GA-1f	"Strengthen GA-1h" ("Relocate Existing Uses Incompatible with Plan Vision")	WCC	The policy should be a goal but not a requirement since it is dependent on future funding not currently available. Staff recommends the policy remain as written to provide future City Councils the flexibility to implement the policy as the need and funding arises.	
GA-1h	Make explicit those existing uses that are deemed incompatible, and where they can possibly be relocated so as to not eliminate existing businesses from Arcata	Public Member	Form Based Code will specify	
Non-conforming uses	Make anything auto oriented(gas stations, car washes) non-conforming	CRTP	The plan includes several balanced transportation policies and seeks to reduce the necessity for reliance on autos. Staff recommends against prohibiting future auto related industry in the district writ large. No changes required.	
Single family homes	Amend plan to include section defining what owners of single family homes on typical lots can do under the plan	Public Member	Form-based code will address in detail. The plan allows all existing uses, including single-family homes, to develop or redevelop. These projects just do not fit in the streamlined process that requires community benefits. No changes required.]	
Land Use Mix	Set percentage of buildings over 2 stories that are required to have commercial space on first floor/be mixed use	Public Member	This policy change would be counterproductive to the purpose of the plan which allows flexibility in uses. The City has a long history of projects not being built because of requirements for ground floor commercial. This policy work is responsive to that history. Staff recommends no change.	
Notification	Make it part of the plan document that notice is required to be given to developers and future occupants about what may happen in their neighborhood	Public Member	Notice of changes to land use designation required by state law and will occur with rest of land use element review; determined to not be necessary or appropriate as a plan policy. The purpose of this work is to engage the community in the design work now to understand how the community may look in the future. Notice in the future does not provide constructive input on projects subject to a Form-Based Code.	

General Plan Topic	Recommendation	Source	Policy Implications/Staff recommendation	PC Recommendation
Zone boundaries	Create new zone type surrounding Creamery District	Public Member	Staff recommends the zoning around the Creamery Building be respectful and considerate of the unique site. Staff recommends these requirements be conveyed in the Form-based Code instead of developing a new district.	
Gateway Chapter 2: Community Benefits and Development Standards				
	Limit to 3-stories	Public Member	Lowering building height will affect housing production, amenities feasibility, lower population, lot coverage, diversity of design. Limiting development will likely not meet the City's housing objectives. Recommend no change.	Concur with staff
Building Height	Limit to 4-stories	Public Member	Lowering building height will affect housing production, amenities feasibility, lower population, lot coverage, diversity of design. Limiting development will likely not meet the City's housing objectives. Recommend no change.	Concur with staff
Building Height	Limit to 6-stories	Architects Stakeholder Group	Lowering building height will affect housing production, amenities feasibility, lower population, lot coverage, diversity of design. Limiting development will likely not meet the City's housing objectives. Recommend no change.	Concur with staff
Building Height	Focus 8-story buildings in Gateway Hub	CRTP, NEC	There are three properties with a small number of parcels large enough to support 8-stories within the Gateway Hub that this policy change would effectively eliminate 8-story buildings from consideration. The highest density opportunities are in the Barrel District. Staff recommends no change.	Concur with staff
Building Height	Limit to 45 feet	RGA	Lowering building height will affect housing production, amenities feasibility, lower population, lot coverage, diversity of design. Limiting development will likely not meet the City's housing objectives. Recommend no change.	Concur with staff
Density	Make Gateway Hub the densest district, minimum size 3 stories	CRTP	The Gateway Hub has only three properties comprising a handful of parcels in Primary Opportunity Sites that could support large stature buildings. Staff recommends retaining the Barrel District, which has the highest potential for redevelopment as the densest district.	Concur with staff
GA-2b	Consider whether all developments over 3 stories should require Planning Commission review to determine whether they meet Arcata's objective standards vs. by-right approval or Zoning Administrator approval.	Public Member	EDC recommended to exclude this recommendation. Staff recommends the process be designed to balance the length of time required to process permits with the sense of immediacy of need for housing and economic development opportunities.	Concur with staff

General Plan Topic	Recommendation	Source	Policy Implications/Staff recommendation	PC Recommendation
GA-2d, IMP-GA-2.2	Determine priorities among and relative value of community benefits and determine if some of amenities should be development requirements.	Public Member	The amenities should satisfy the community need scaled commensurate with the size and impact of the projects. Staff recommends this be considered during development of the Form-based Code.	Concur with staff
Gateway Chapter 3: Housing				
GA-3i Mixed Tenure	Set specific targets for the mix of owner-occupied vs. rental housing for all residential buildings	Public Member	EDC recommended excluding. There are soft targets included in broad categories in Table 5. Anything more granular than this table would be speculation.	
GA-3j	Define "simplified development procedures" and at what point it kicks in	Public Member	EDC recommended excluding. Will be addressed through FBC.	
GA-3k	The implication that increased density results in increased affordability is questionable. What are the minimum densities and how are they determined?	Public Member	This is addressed in the Market Study. This comment is not a specific change to the Gateway Plan, so was excluded on that basis.	
Maintenance of Multifamily structures	Formation of a community maintenance district supported by landowner fees should be considered	Public Member	Assessment districts can be a source of revenue generation to support maintenance of public or common space. However, maintaining the structures themselves are the responsibility of the landowners.	
Rental cost estimates	Include estimates of pricing of new housing	Public Member	It is speculative to estimate costs over the planning horizon of the document. However, the affordability ranges provided in the Housing Element are available for consideration.	
Housing Affordability	Develop an affordability Plan for the range of income levels anticipated for the future of Arcata	RGA	Affordability is outlined as part of state Regional Housing Needs Allocation (RHNA) process. The Plan does call for implementing Inclusionary Zoning at feasible densities.	
Owner Occupancy	Add a 10% minimum requirement of owner occupied units	Humboldt Association of Realtors	Staff recommends either no change - the current policy supports home ownership, or make soft targets to gauge progress achieving goals. Staff does not recommend making this regulatory.	
Gateway Chapter 4: Employment				
Business Development	Allow new businesses in Gateway to expand without adding residential units	Public Member	Current policy proposal allows businesses to expand without adding residential units. No change required.	Concur with staff
Business Development	Require developers to notify prospective tenants of existing business use to avoid noise/traffic complaints or legal action	Public Member	EDC recommended excluding this. Staff does not have a recommendation, but requiring every project to notify prospective tenants/owners that they may experience noise and traffic seems excessive and unnecessary.	
Office/business development	Existing businesses should be able to expand without attaching housing units to their properties and be able to go through normal review process	Public Member	Current policy proposal allows businesses to expand without adding residential units. No change required.	Concur with staff
Office/business development	Purpose-built office/business buildings should not require a UP	Public Member	Housing has been identified as a critical component to this plan for a variety of reasons. Allowing large scale office buildings without housing is at cross purposes with the objectives of the plan. Staff recommends no change.	

General Plan Topic	Recommendation	Source	Policy Implications/Staff recommendation	PC Recommendation
Gateway Chapter 6: Open Space and Conservation				
POPs	Require developers to convey POPs to City ownership but agree to perform ongoing maintenance	CRTP	There is no legal mechanism to require dedication with the kinds of approvals being contemplated. In addition, the location, relationship to other development, and other factors could make such dedications problematic and/or undesirable. Policy has already been updated to reflect obtaining either public easements for recreation space or fee title.	
Play areas for children	Create strict language for play areas for children	Public Member	PRC recommended this not be included. Staff supports the PRC recommendation.	
Parks map	Update existing parks and recreational facilities map to make walk times more realistic, separate out wildlife/riparian habitat from open space	Public Member	PRC recommended this not be included. Staff supports the PRC recommendation.	
GA-6e	Delete this policy and let free market decide what is redeveloped and when	Public Member	PRC recommended this not be included. Staff supports the PRC recommendation.	
Gateway Chapter 7: Mobility				
GA-7a.	Create/ or require potential for parking revenue, and/or ticketing revenue to be used to help support transit, pedestrian, and biking infrastructure [in Barrell District/ or in full Gateway Area.	EDC	Parking ticket revenue is general fund revenue and cannot be earmarked for a specific purpose or area. There are other more effective ways to generate revenue proposed in the plan, including the buss pass program and existing grant programs.	
GA-7a.	Invest proceeds of metered parking in Gateway Area as feasible.	TSC	Parking ticket revenue is general fund revenue and cannot be earmarked for a specific purpose or area. There are other more effective ways to generate revenue proposed in the plan, including the buss pass program and existing grant programs.	
L Street	Maintain current configuration; remove concept of L as an arterial couplet with K from figures	TSC	Direct conflict with plan as drafted. On PC list of concerns and alternatives will be presented with opportunity to discuss.	Concur with staff
L Street	Maintain current configuration of L Street, retain/expand as a linear park	Public Member	Direct conflict with plan as drafted. On PC list of concerns and alternatives will be presented with opportunity to discuss.	Concur with staff
L Street	Retain L Street, make K Street one way and I Street one way with stop lights at Samoa and I	Public Member	Direct conflict with plan as drafted. On PC list of concerns and alternatives will be presented with opportunity to discuss.	Concur with staff
Parking	Ensure adequate parking for local businesses and employees. Consider paid parking and parking structures.	EDC	Plan prioritizes multi-modal transportation and walkability over parking. PC will set specific ratio in FBC.	
Parking	Require at least one parking space per unit and adequate parking for business access	Public Member	Plan prioritizes multi-modal transportation and walkability over parking. PC will set specific ratio in FBC.	
Parking	Require sufficient parking spaces to accommodate at least 50% of units proposed as well as parking for new and existing businesses. Provide regulations to protect nearby neighborhoods from parking overreach.	Public Member	Plan prioritizes multi-modal transportation and walkability over parking. PC will set specific ratio in FBC.	

General Plan Topic	Recommendation	Source	Policy Implications/Staff recommendation	PC Recommendation
Parking	Concerns about the lack of on-street parking resulting from high-density development, parking ratios less than 1 space per unit	Public Member	Plan prioritizes multi-modal transportation and walkability over parking. PC will set specific ratio in FBC.	
Bike Lanes	Make 11th Street a Class 4, not Class 3, boulevard. Make K, L, 8th, 9th Class 4 from the outset	CRTP	Bicycle network improvements are being planned in the General Plan Circulation Element and include recommended bikeway improvements for 11th Street. 8th and 9th Streets are currently the focus of a grant-funded effort to enhance Complete Streets and bicycle connectivity.	
Multi Use Trail conversion	Remove option to convert trail to road in Policy GA-7b(i)	CRTP	Q Street may be necessary for service traffic. If converted, the policy should add "Class I or II trails shall be incorporated into the road design to minimize impacts on the proposed trail system."	
Lane width	Reduce lane widths in cross-sections from 12 feet to 10 feet to slow traffic and reduce ped crossing distance	CRTP	Road widths are proposed at the minimum necessary to safely function at projected volumes.	
Lane width	Limit width of car lanes to 9-11 feet	Public Member	Road widths are proposed at the minimum necessary to safely function at projected volumes.	
Circulation	Decrease driving speed on K and 11th	HCAOG	Speed on K and 11th is currently 25. Staff recommends no change.	
Chapter 11: Infrastructure and Services				
GA-11k	Require energy efficiency and electricification as objective standards	Public Member	EDC recommended excluding this. Energy efficiency and electrification follows building code. The Community Benefits program of the Form-Based Code should include options to exceed the requirements.	

Attachment: I. Other Considerations Gateway and GP Draft 6-22-23 (4214 : GP #XVII FBC & GP wrap-up)